



BUFFALO DIVISION
PLANS FOR
1R MILL AND INLAY, SAFETY IMPROVEMENTS
AND MISCELLANEOUS WORK ON I-90
FROM
MILEPOST 416.90 TO 419.40
IN
ERIE COUNTY

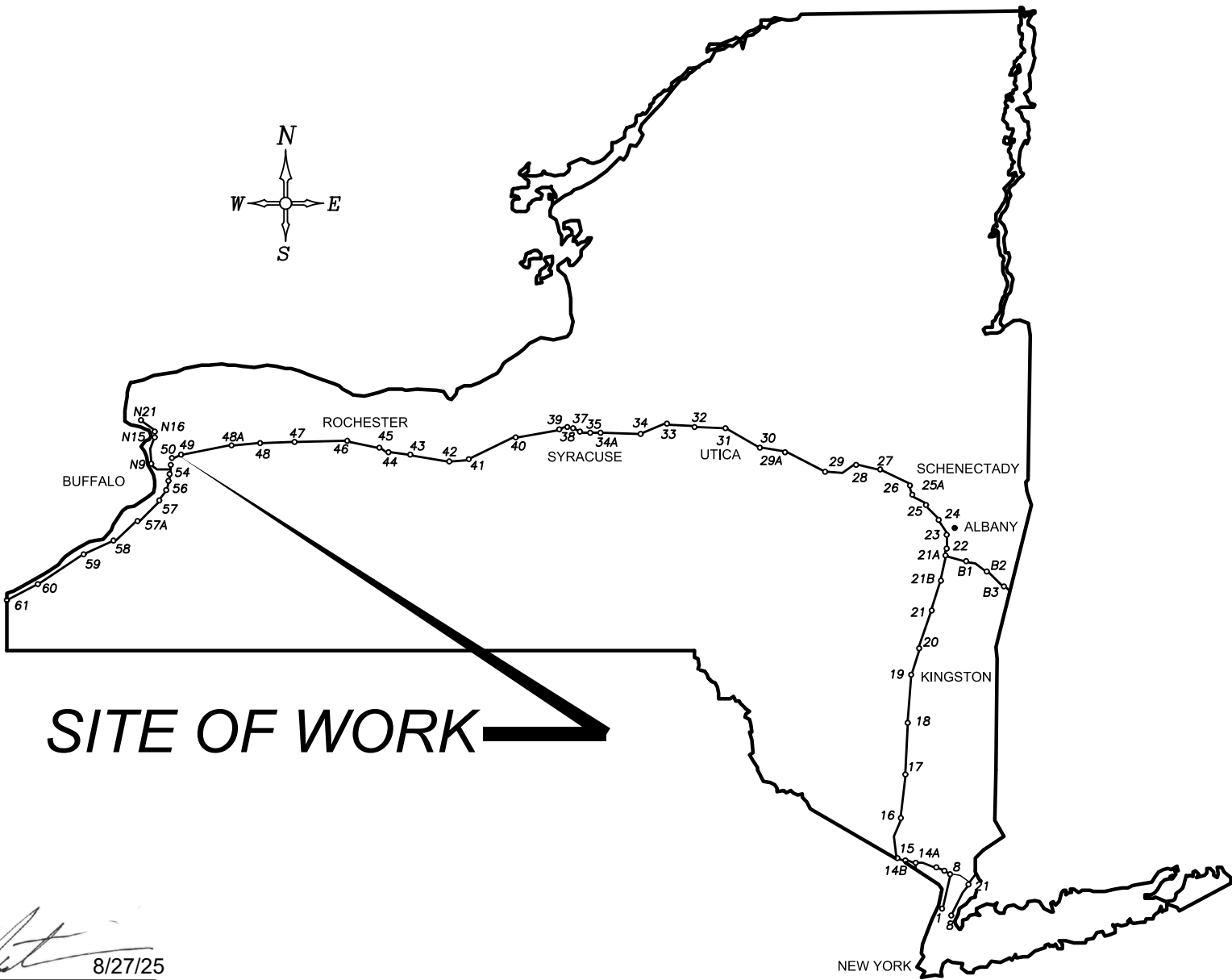
TAB 25-31 D214980 58 SHEETS
BIN's 5030279 5511919 5511929 5512160
CIN 417.6301XX

TYPE OF CONSTRUCTION:
1R PAVEMENT RESURFACING, SAFETY IMPROVEMENTS, AND MISC. WORK

STANDARD SHEETS:
THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY NYSDOT, WHICH ARE CURRENT AS OF THE STANDARD SPECIFICATIONS ADOPTION DATE SHOWN ON THE PROPOSAL COVER SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

THE LATEST REVISIONS OF THE NYSTA STANDARD SHEETS MAINTAINED BY THE AUTHORITY, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) LISTED ON DWG. SS-1 UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

NOTES:
ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (US CUSTOMARY) REFERENCED IN THE CONTRACT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR CHANGES SET FORTH IN THE CONTRACT "PROPOSAL".



UDIG NEW YORK
UNDERGROUND FACILITIES PROTECTION ORGANIZATION
CALL 811

CONTRACTOR'S NAME:	
AWARD DATE:	
COMPLETION DATE:	
FINAL ACCEPTANCE DATE:	
INSPECTION FIRM'S NAME:	
RESIDENT ENG./EIC:	
FINAL COST TOTAL:	
FISCAL SHARE	COST(S)
INSPECTION FIRM CONSULTANT STAMP:	

RECOMMENDED BY: 8/27/25
DIRECTOR, OFFICE OF DESIGN DATE

RECOMMENDED BY: 8/27/25
DIVISION DIRECTOR DATE


RECOMMENDED BY: 08/28/25
TRAFFIC ENGINEER DATE

RECOMMENDED BY: 08/28/25
DIRECTOR, OFFICE OF CONSTRUCTION MANAGEMENT DATE

RECOMMENDED BY: 9/4/2025
DIRECTOR OF MAINTENANCE AND OPERATIONS DATE

APPROVED BY: 09/05/2025
CHIEF ENGINEER DATE

STANDARD SYMBOL (PLANS)	ITEM PAYMENT UNIT: ESTIMATE OF QUANTITIES SHEET	EQUIVALENT NOMENCLATURE: (SPECS/PROPOSAL)
"	-	INCHES
'	LF	LINEAR FEET
mi	MI	MILES
ft ²	SF	SQUARE FEET
YD ²	SY	SQUARE YARD
AC	AC	ACRES
YD ³	CY	CUBIC YARD
GAL	GAL	GALLON
lb	LB	POUND
TON	TON	TON

	TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
	LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/25
	TITLE OF DRAWING INDEX AND ABBREVIATION	DRAWING NUMBER: INDEX

ALIGNMENT			LANDSCAPE			ROADWAY			TRAFFIC WORK ZONE				
STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION		TWZBT_P	BARRIER, TEMPORARY		
	AC	CONTROL (CENTERLINE)		LABL	AREA, BRUSH LINE		RCZ_P	CLEAR ZONE		TWZBTWL_P	BARRIER, TEMPORARY, W/ WARNING LIGHTS		
	AD_P	DETOUR		LAHR	AREA, HEDGE ROW		RG	GUIDE RAIL, MISCELLANEOUS		TWZCD_P	CHANNELIZING DEVICE		
	AT_P	TRANSITION CONTROL		LAPB	AREA, PLANTING BED		RGB	GUIDE RAIL, BOX BEAM		TWZPMRC_P	PAVEMENT MARKING REMOVAL OR COVERING		
BRIDGE				LAWA	AREA, WOODED AREA OUTLINE		RGBM	GUIDE RAIL, BOX BEAM, MEDIAN	UTILITIES				
	BR	RAIL		LAWL	AREA, WATERS EDGE		RGC	GUIDE RAIL, CABLE	STYLE	NAME	DESCRIPTION		
	BSHT	SHEET PILING		LCUT_P	CUT LIMIT		RGCB	GUIDE RAIL, CONCRETE BARRIER		UC	CONDUIT, UNDERGROUND		
CONTROL				LFILL_P	FILL LIMIT		RGP_P	GUIDE POST		UCH	CONDUIT, HANGING		
	CB	BASELINE		LFNC	FENCE		RGW	GUIDE RAIL, W BEAM		UCO	CONDUIT, OVERHEAD		
	CBPR	BASELINE, PROJECTION		LTRC	TREE ROW, CONIFEROUS		RGWM	GUIDE RAIL, W BEAM, MEDIAN		UE	ELECTRIC LINE, UNDERGROUND		
DRAINAGE				LTRD	TREE ROW, DECIDUOUS		RPB	PARKING BUMPER		UEH	ELECTRIC LINE, HANGING		
	DCP	CULVERT PIPE		LWH	WALL, H PILE		RRC	RAIL ROAD, CATENARY		UEO	ELECTRIC LINE, OVERHEAD		
	DCP_P	CULVERT PIPE (DIR)		LWR	WALL, RETAINING		RRER	RAIL ROAD, 3RD RAIL		UETO	ELECTRIC TRANSMISSION, OVERHEAD		
	DDG_P	DITCH, GRASS LINED		LWS	WALL, STONE		RRPLS_P	RAIL, PHOTO, LARGE SCALE		UESS	ELECTRIC, SUBSTATIONS		
	DDP_P	DITCH, PAVED INVERT	ROW MAPPING				RRPS	RAIL, PHOTO, SMALL SCALE		UFO	FIBER OPTIC, UNDERGROUND		
	DDS_P	DITCH, STONE LINED		MDL	DEED LINE		RRS	RUMBLE STRIP		UFOH	FIBER OPTIC, HANGING		
	DFL_P	FLOW LINE		MEE	EASEMENT, EXISTING		RRSLP_P	RAIL, SURVEY, LARGE SCALE		UFOO	FIBER OPTIC, OVERHEAD		
	DSSD	SLOTTED DRAIN		MEP_P	EASEMENT, PERMANENT		RRSS	RAIL, SURVEY, SMALL SCALE		UG	GAS, UNDERGROUND		
	DUD_P	UNDERDRAIN		MEPA_P	EASEMENT, PERMANENT, APPROX.		SIGNS				UGH	GAS, HANGING	
ENVIRONMENTAL				MET_P	EASEMENT, TEMPORARY		STRIPING				UGO	GAS, OVERHEAD	
	EBLHS	BALE, STRAW		META_P	EASEMENT, TEMPORARY, APPROX.			SBLB	BILLBOARDS		UIC	INFORM CABLE, UNDERGROUND	
	ECT	CURTAIN, TURBIDITY		MF_P	FEE ACQUISITION, W/ ACCESS			SM	MULTIPLE POST		UICH	INFORM CABLE, HANGING	
	EDMC	DAM, COFFER		MFA_P	FEE ACQUISITION, APPROXIMATE			SSO	STRUCTURE, OVERHEAD		UO	OIL LINE, UNDERGROUND	
	EDMEC_P	DAM, EARTHEN CHECK		MFS_P	FEE ACQUISITION, SHAPE			SSOC	STRUCTURE, OVHD. CANTILEVER		UOH	OIL LINE, HANGING	
	EDMGSC_P	DAM, GRAVEL BAG/SAND BAG CHECK		MFWOA_P	FEE ACQUISITION, W/O ACCESS		STRIPING				UPBP	POLE, BRACE, PUSH BRACE	
	EDMPC_P	DAM, PREFABRICATED CHECK		MHA	HISTORICAL, ACQUISITION			STB•	BROKEN LINE		UPGW	POLE, GUY WIRE	
	EDMSC_P	DAM, STONE CHECK		MHB	HIGHWAY BOUNDARY			STDB•	DOUBLE BROKEN LINE		USA	SANITARY SEWER, UNDERGROUND	
	EFNS	FENCE, SILT		MHBA	HIGHWAY BOUNDARY, APPROX.			STDL•	DOTTED LINE LONG		USAH	SANITARY SEWER, HANGING	
	EFNSV	FENCE, SILT & VEGETATION		MHBW	HWY BOUNDARY, FACE OF WALL			STDS•	DOTTED LINE SHORT		USAF	SANITARY SEWER, FORCE MAIN, UGND	
	EFNV	FENCE, VEGETATION		MHBWOA	HIGHWAY BOUNDARY, W/O ACCESS			STFB•	FULL BARRIER LINE		USAFH	SANITARY SEWER, FORCE MAIN, HANG	
	EWAA_P	WETLAND, ADJACENT AREA		MJC	JURISDICTION, CITY			STH•	HATCH LINE		UT	TELEPHONE, UNDERGROUND	
	EWL	WETLAND, FEDERAL		MJCY	JURISDICTION, COUNTY			STPB•	PARTIAL BARRIER LINE		UTH	TELEPHONE, HANGING	
	EWFS	WETLAND, FEDERAL AND STATE		MJHD	JURISDICTION, HISTORIC DISTRICT			STRCT	ROUNDABOUT, CAT TRACKS		UTO	TELEPHONE, OVERHEAD	
	EWM	WETLAND, MITIGATION AREA		MJLL	JURIS., (GREAT, MILITARY) LOT LINE			STRYL	ROUNDABOUT, YIELD LINE		UTV	CABLE TV, UNDERGROUND	
	EWS	WETLAND, STATE		MJN	JURISDICTION, NATION			STSB	STOP BAR		UTVH	CABLE TV, HANGING	
				MJPB	JURISDICTION, PUBLIC LANDS			STSE•	SOLID, EDGE		UTVO	CABLE TV, OVERHEAD	
				MJS	JURISDICTION, STATE			STXL	X WALK, LADDER LINE		UUU	UNKNOWN, UNDERGROUND	
				MJT	JURISDICTION, TOWN			STXLB	X WALK, LADDER BAR LINE		UUH	UNKNOWN, HANGING	
				MJV	JURISDICTION, VILLAGE			• = W (WHITE) OR Y (YELLOW)				UUO	UNKNOWN, OVERHEAD
				MPL	PROPERTY LOT LINE			TRAFFIC CONTROL				UW	WATER LINE, UNDERGROUND
				MPLA	PROPERTY LOT LINE, APPROXIMATE				TCSW	SIGNAL, SPAN WIRE		UWH	WATER LINE, HANGING
				MSL	SUB LOT LINE							UWO	WATER LINE, OVERHEAD
1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).													

1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).
2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.).
3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES.
4. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 in ON B SIZE DRAWINGS).
5. MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS.
6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK

LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
LINE SYMBOLOGY

CONTRACT NUMBER:
TAB 25-31

DATE:
08/20/25

DRAWING NUMBER:
LEG-1

ALIGNMENT			DRAINAGE			ITS			ROW MAPPING			SIGNS			UTILITIES																				
CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION																		
	ACC	CENTER OF CURVATURE		DINV	INVERT		IANT_P	ANTENNAS		MDL1P	DEED LINE, TYPE 1		S	SINGLE POST		UEB	ELECTRIC, BOX																		
	ACOGO	COGO		DS	STRUCTURE, RECTANGULAR		IASCTS	ACCOU. SPEED/COUNT SNSR.S		MDL2P	DEED LINE, TYPE 2		S_P	SINGLE POST, PROPOSED		UEM	ELECTRIC, METER																		
	ACS	CURVE TO SPIRAL		DSI	STRUCTURE, INVERT		ICABPAD	CABINET & PAD		MDL3P	DEED LINE, TYPE 3		SB_P	BACK TO BACK, PROPOSED		UEMH	ELECTRIC, MANHOLE																		
	ADPL_P	DETOUR, POINT OF INTERSECT.		DSM	STRUCTURE, MANHOLE		ICCTV	CCTV SITE		MDL4P	DEED LINE, TYPE 4		SDEL	DELINEATORS		UEPT	ELECTRIC, POLE, TRANS.																		
	ADPL_P	DETOUR, POINT ON LINE		DSMTXX_P	STRUCTURE, MANHOLE, TYPE "XX" "XX" = 48, 60, 72, 96		ICDPD	CDPD TRANSCEIVER		MDL5P	DEED LINE, TYPE 5		SPM	PARKING METER		UGM	GAS, METER																		
	AEQN	EQUATION		DSR	STRUCTURE, ROUND		ICELLT	CELL PHONE TOWER		MEEP	EASEMENT, EXISTING		SRM	REFERENCE MARKERS		UGMH	GAS, MANHOLE																		
	AEQNAHD	EQUATION AHEAD		DSR	STRUCTURE, ROUND		ICJB	CONDUIT JACK OR BORING		MEPAP_P	EASEMENT, PERM., APPROX.		SRSC3	SHLD, CTY, 123 DIG.		UGLM	GAS, LINE MARKER																		
	AEQNBK	EQUATION BACK		DST"X"CB_P	STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R		ICNTLCAB	CONTROLLER CABINET		MEPP_P	EASEMENT, PERM., BACK LINE		SRSC4	SHLD, CTY, 4 DIG.		UGP	GAS/FUEL PUMP																		
	AEVT	EVENT STATION		DST"X"CB_P	STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R		ICPB	COMMUNICATION PULL BOX		MEPSP_P	EASEMENT, PERM., SHAPE		SRSCT2	SHLD, CTY TOUR, 1-2 DIG.		UGV	GAS, VALVE																		
	APC	POINT OF CURVATURE		DST"X"CB_P	STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R		ICTD	CONDUIT TURNING DOWN		MFAP_P	FEE ACQUISITION, APPROX.		SRSCT4	SHLD, CTY TOUR, 3-4 DIG.		UGVT	GAS, VENT																		
	APCC	POINT OF COMPOUND CURVATURE		DST"X"CB_P	STRUCTURE, RECT., TYPE "X" "X" = I, K, L, M, O, P, U		ICTU	CONDUIT TURNING UP		MFP_P	FEE ACQUISITION, BACK LINE		SRSI	SHLD, INTERSTATE		ULP	LIGHTING, POLE																		
	API	POINT OF INTERSECTION	ENVIRONMENTAL				ICVTRT	COMM. VEH. ROAD TRANSCEIVER		MFSP_P	FEE ACQUISITION, SHAPE		SRSN2	SHLD, NATIONAL, 2 DIG.		ULPM	LIGHTING, POLE, MEDIAN																		
	APOB	POINT OF BEGINNING					IDEFAULT	DEFAULT		MHBAP	HIGHWAY BNDRY., APPROX.		SRSN3	SHLD, NATIONAL, 3 DIG.		ULPP	LIGHTING, POLE, PED.																		
	APOC	POINT OF CURVATURE		EIOP_P	STR., INLET, OUTLET PROT.		IEZR	E-ZPASS READER		MHBCP	HISTORICAL, BLDG. CORNERS		SRSS2	SHLD, STATE, 2 DIG.		UMFC	MISC. FILLER CAP																		
	APOE	POINT OF END		EIPGB_P	STR., INLET PROT., GRAVEL BAG		IEZTR	TRANSMITTAL READER		MHBP	HIGHWAY BNDRY, PT.		SRSS3	SHLD, STATE, 3 DIG.		UOLM	OIL, LINE MARKER																		
	APOL	POINT ON LINE		EIPHS_P	STR., INLET PROT., HAY/STRAW		IFXCAB	FIBER OPTIC X-CONNECT CABINET		MJCP	PT., JURIS. CITY		SRSS4	SHLD, STATE, 4 DIG.		UP	POLE, WITH UTILITY																		
	APOS	POINT ON SPIRAL		EIPHS_P	STR., INLET PROT., HAY/STRAW		IFUSSPL	FUSION SPLICE		MPBC	PT., BUILDING CORNER	TRAFFIC CONTROL				UPD	POLE, DEAD (NO UTILITY)																		
	APOT	POINT ON TANGENT		EIPP_P	STR., INLET PROT., PREFAB.		IHARADV	HAR ADVISORY SIGN		MPCC	PT., CROSS CUT					UPL	POLE, WITH LIGHT																		
	APOVC	POINT ON VERTICAL CURVE		EIPSF_P	STR., INLET PROT., SILT FENCE		IHARST	HAR SITE		MPDH	PT., DRILL HOLE		TCBJ	BOX, JUNCTION		USMH	SANITARY SEWER MANHOLE																		
	APOVT	POINT ON VERTICAL TANGENT		EIPSF_P	STR., INLET PROT., SILT FENCE		ILC	LOAD CENTER		MPF	PT., FENCE LOCATION		TCBP	BOX, PULL BOX		UTB	TELEPHONE, BOOTH																		
	APORC	POINT ON REVERSE CURVE		ETRS_P	TRAP, SEDIMENT		IMECSPL	MECHANICAL SPLICE		MPIP	PT., IRON PIPE		TCBS	BOX, SPLICE		UTLM	TELEPHONE, LINE MARKER																		
	APT	POINT OF TANGENCY		ETRS_P	TRAP, SEDIMENT		IMSCS	PORT. SPEED & COUNT SENSOR		MPIR	PT., IRON ROD		TCMC	MICROCOMPUTER CABINET		UTMH	TELEPHONE, MANHOLE																		
	APVC	POINT OF VERTICAL CURVATURE		ETRS_P	TRAP, SEDIMENT		IMSCTS	MICRO SPEED & COUNT SENSOR		MPM	PT., MONUMENT		TCPP	PED POLE		UTVLM	CABLE TV, LINE MARKER																		
	APVCC	POINT OF VERT. CMPND CURVE		ETRS_P	TRAP, SEDIMENT		IMT	MICROWAVE TRANSCEIVER		MPMM	PT., MONUMENT, MISC.		TCSH	SIGNAL HEADS		UTVPB	CABLE TV, PULL BOX																		
	APVI	POINT OF VERT. INTERSECTION		ETRS_P	TRAP, SEDIMENT		IOVHMS	PERM. OVERHEAD VMS		MPN	PT., NAIL		TCSP	SIGNAL POLE		UUB	UNKNOWN, BOX																		
	APVRC	POINT OF VERT. REVERSE CURVE		ETRS_P	TRAP, SEDIMENT		IPASCS	PORT. ACCOU. SPD & CNT. SENSOR		MPRS	PT., RAILROAD SPIKE	TRAFFIC WORK ZONE				UUB	UNKNOWN, BOX																		
	APVT	POINT OF VERTICAL TANGENCY		ETRS_P	TRAP, SEDIMENT		IPEDS	PEDESTRIAN SIGNAL HEAD		MPST	PT., SPIKE					UUB	UNKNOWN, BOX																		
	ASC	SPIRAL TO CURVE		ETRS_P	TRAP, SEDIMENT		IPSS	PAVEMENT SURFACE SENSOR		MPTW	PT., TREE W/ WIRE		TWZAP_P	ARROW PANEL		UUB	UNKNOWN, BOX																		
	ASPI	SPIRAL POINT OF INTERSECTION		ETRS_P	TRAP, SEDIMENT		IPVMS	PERM. VMS		MPWL	PT., WALL LOCATION		TWZAPC_P	ARROW PANEL, CAUTION MODE		UUB	UNKNOWN, BOX																		
	ASTS	SPIRAL TO SPIRAL		ETRS_P	TRAP, SEDIMENT		IRM	RAMP METER	ROW ACQUISITION				TWZAPT_P	ARROW PANEL, TRAILER OR SUPPORT		UUB	UNKNOWN, BOX																		
	AST	SPIRAL TO TANGENT		ETRS_P	TRAP, SEDIMENT		IRWIS	RDWY WEATHER INFO. SENSOR					TWZBCD_P	BARRICADE (TYPE III)		UUB	UNKNOWN, BOX																		
	ATS	TANGENT TO SPIRAL		ETRS_P	TRAP, SEDIMENT		ISP	SOLAR PANEL		MFS_P.T	FEE ACQUISITION		TWZCMS_P	CHANGEABLE MESSAGE SIGN (PVMS)		UUB	UNKNOWN, BOX																		
	AVEVT	VERTICAL EVENT POINT		ETRS_P	TRAP, SEDIMENT		ISST	SPREAD SPECT. TRANSCEIVER		MEPS_P.T	EASEMENT, PERMANENT		TWZFLG_P	FLAGGER		UUB	UNKNOWN, BOX																		
	AVHIGH	VERTICAL HIGH POINT		ETRS_P	TRAP, SEDIMENT		ITDB	TELEPHONE DEMARCATION BLK		METS_P.T	EASEMENT, TEMPORARY		TWZFT_P	FLAG TREE		UUB	UNKNOWN, BOX																		
	AVLOW	VERTICAL LOW POINT		ETRS_P	TRAP, SEDIMENT		ITP	SUBSURFACE TEMP. PROBE		METS_P.T	OCCUPANCY, TEMPORARY		TWZIA_P	IMPACT ATTENUATOR / CRASH CUSHION (TEMPORARY)		UUB	UNKNOWN, BOX																		
BRIDGE				ETRS_P	TRAP, SEDIMENT		IVTRT	VEHICLE TO RDWY TRANSCEIVER		METS_P.T	OCCUPANCY, TEMPORARY		TWZLUM_P	LUMINAIRE (TEMPORARY)		UUB	UNKNOWN, BOX																		
				ETRS_P	TRAP, SEDIMENT		IWIMD	WEIGHT IN MOTION DETECTOR		METS_P.T	OCCUPANCY, TEMPORARY		TWZSDT_P	SYMBOL, DIRECTION OF TRAFFIC		UUB	UNKNOWN, BOX																		
	BSC	BRIDGE, SCUPPER		ETRS_P	TRAP, SEDIMENT		IWVRC	WIRELESS VIDEO REPEATER		METS_P.T	OCCUPANCY, TEMPORARY		TWZSDTD_P	SYMBOL, DIRECTION OF TEMPORARY TRAFFIC DETOUR		UUB	UNKNOWN, BOX																		
CONTROL				ETRS_P	TRAP, SEDIMENT		IWVR	WIRELESS VIDEO REPEATER		MFS_P.T	FEE ACQUISITION W/O ACCESS		TWZSGN_P	SIGN (TEMPORARY)		UUB	UNKNOWN, BOX																		
				ETRS_P	TRAP, SEDIMENT		IWVRC	WIRELESS VIDEO RECEIVER		MFS_P.T	FEE ACQUISITION W/O ACCESS		TWZSIG_P	SIGNAL, TRAFFIC OR PEDESTRIAN (TEMPORARY)		UUB	UNKNOWN, BOX																		
	CBP	BASELINE, POINT		ETRS_P	TRAP, SEDIMENT		IWVTT	WIRELESS VIDEO TRANSMITTER	ROADWAY				TWZWL_P	WARNING LIGHT		UUB	UNKNOWN, BOX																		
	CBPOL	BASELINE, POINT ON LINE		ETRS_P	TRAP, SEDIMENT		RES_P	ELEVATION, SPOT					TWZWV_P	WORK VEHICLE		UUB	UNKNOWN, BOX																		
	CBSP	BASELINE, SPUR POINT		ETRS_P	TRAP, SEDIMENT		RGA	GUIDE RAIL, ANCHOR		TWZWVA_P	WORK VEHICLE WITH TRUCK MOUNTED ATTENUATOR		UUB	UNKNOWN, BOX																					
	CBTP	BASELINE, TIE POINT	1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).				RGP	GUIDE POST, SINGLE	3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES.										UUB	UNKNOWN, BOX															
	CPBM	BENCHMARK					UUB	UNKNOWN, BOX																											
	CPH	POINT, HORIZ. PHOTOGRAMMETRY	2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.).			6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.													UUB	UNKNOWN, BOX															
	CPSM	POINT, SURVEY MARKER, PERM.																	UUB	UNKNOWN, BOX															
	CPSV	POINT, VERT., PHOTOGRAMMETRY	5. MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS.															6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.										UUB	UNKNOWN, BOX						
																												UUB	UNKNOWN, BOX						

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK
LOCATION OF PROJECT
MP 418.90 TO MP 419.40

CONTRACT
TAB
DATE:

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK

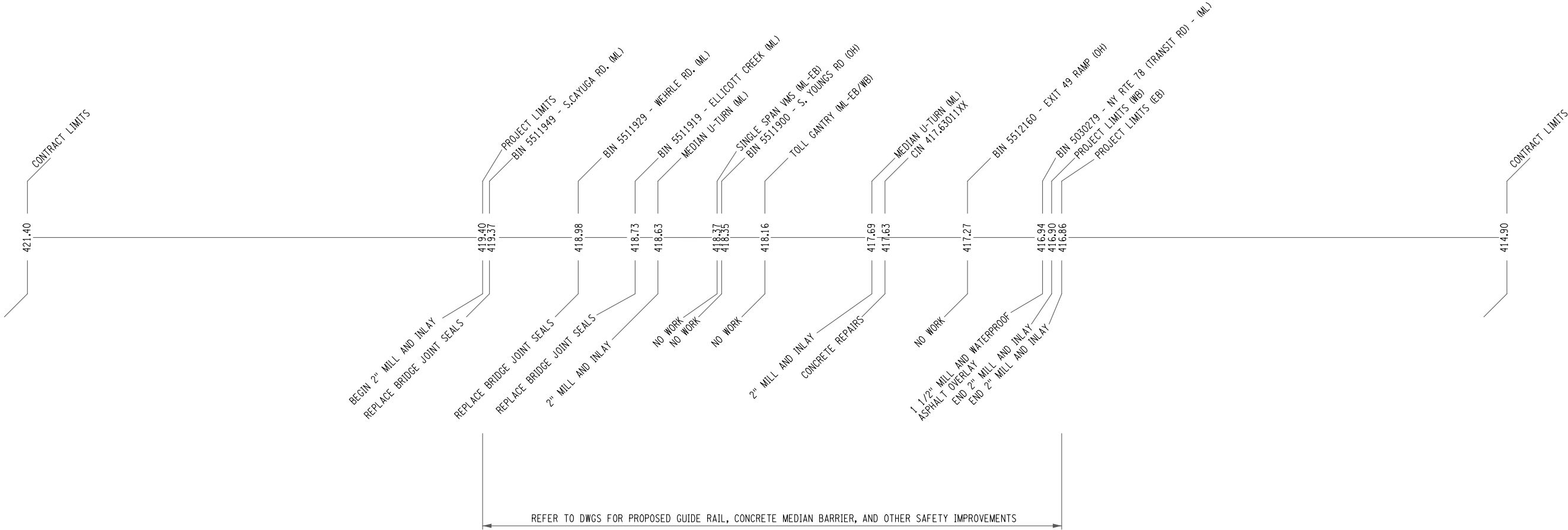
LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
POINT SYMBOLOGY

CONTRACT NUMBER:
TAB 25-31

DATE:
08/20/25

DRAWING NUMBER:
LEG-2

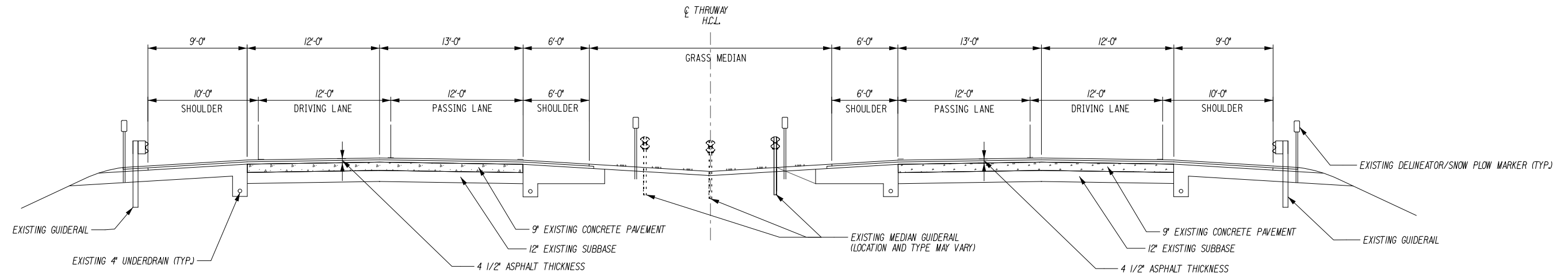
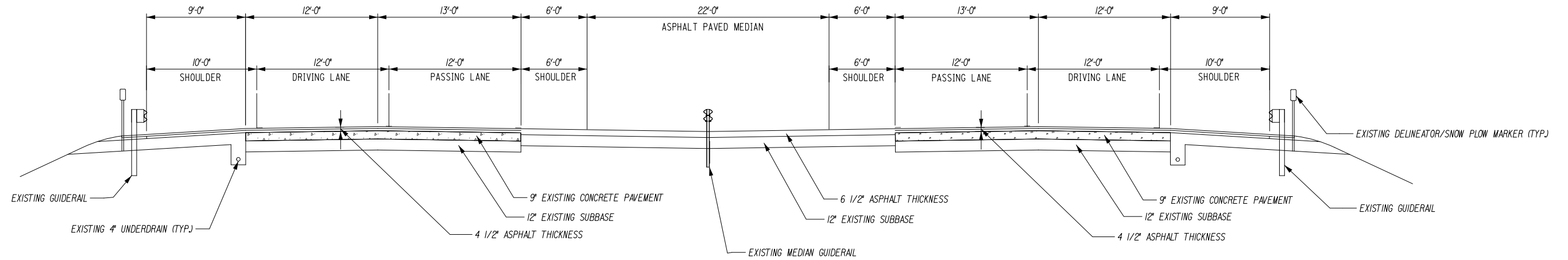



ALTERED ON:	AFFIXED ON: 08/21/2025
SIGNATURE: STAMP:	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
	LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/25
	TITLE OF DRAWING PROJECT MILEPOST DIAGRAM	DRAWING NUMBER: PMD-1



ALTERED ON:	AFFIXED ON: 08/21/2025
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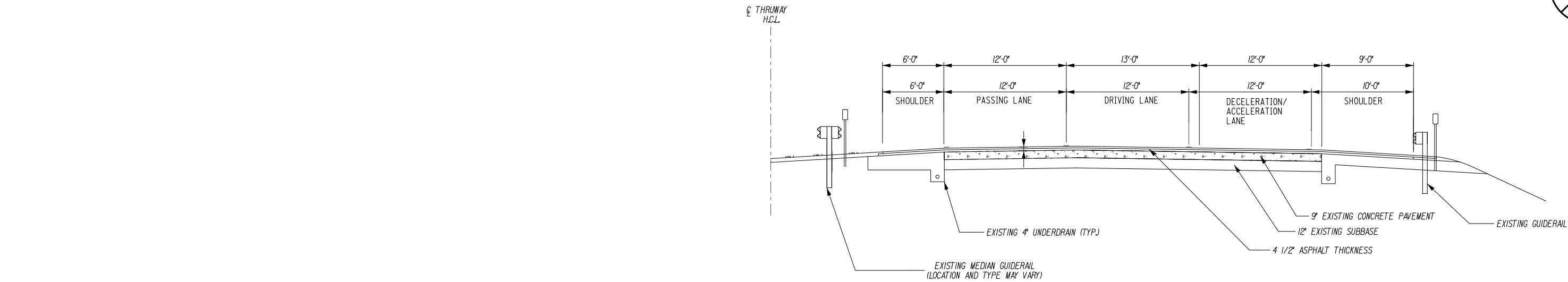
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

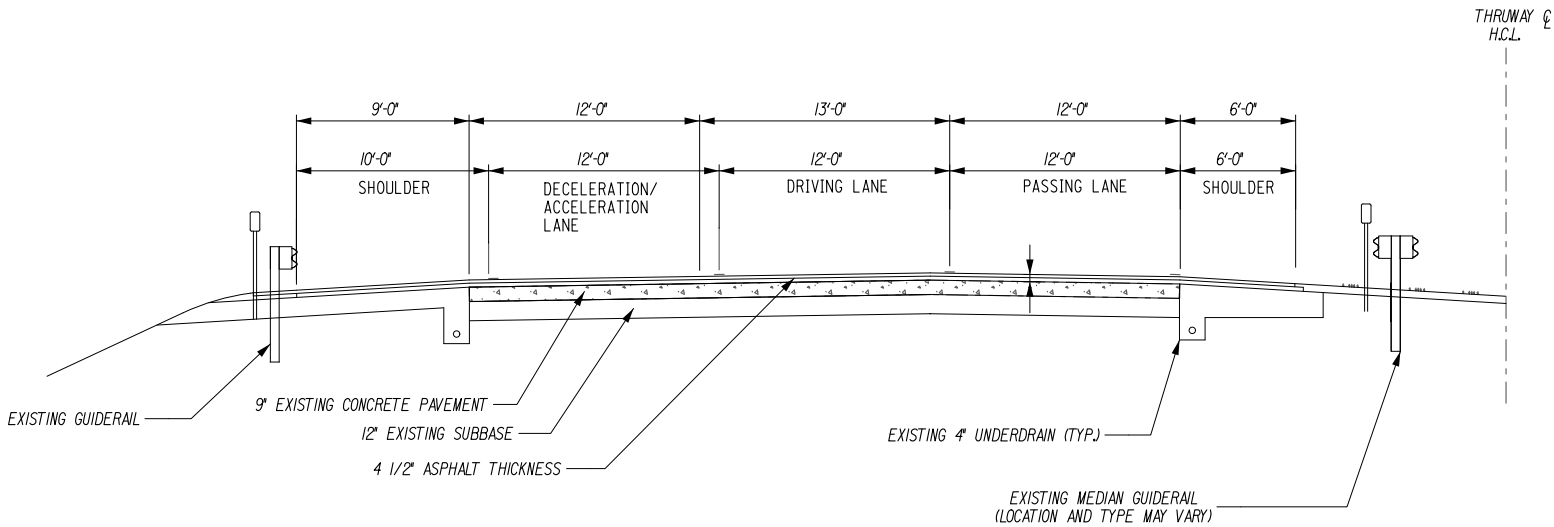


**Thruway
Authority**


TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/25
TITLE OF DRAWING EXISTING TYPICAL SECTIONS	DRAWING NUMBER: TYP-1



EXISTING I-90 TYPICAL SECTION (E3)
M.P. 416.90 TO M.P. 419.40
N.T.S.




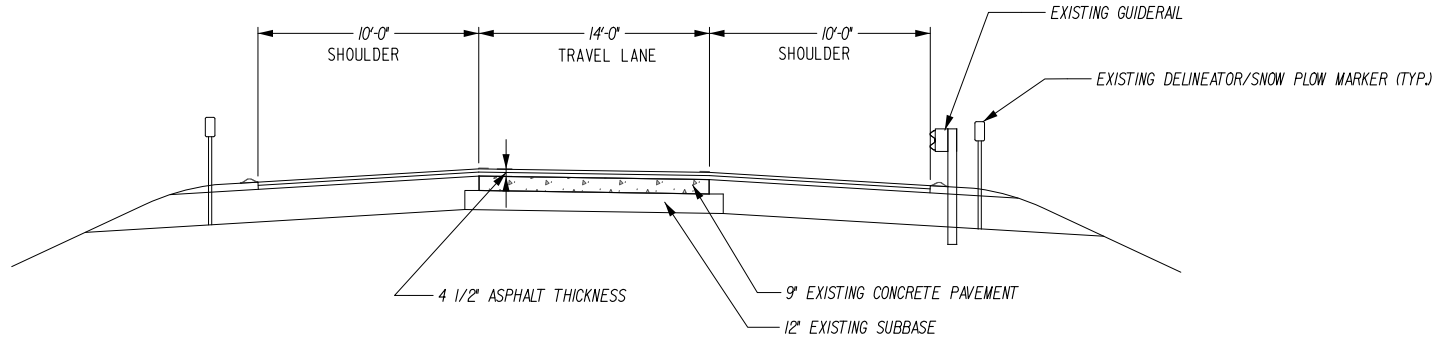
EXISTING I-90 TYPICAL SECTION (E4)
M.P. 416.90 TO M.P. 419.40
N.T.S.

ALTERED ON:	AFFIXED ON: 08/21/2025
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

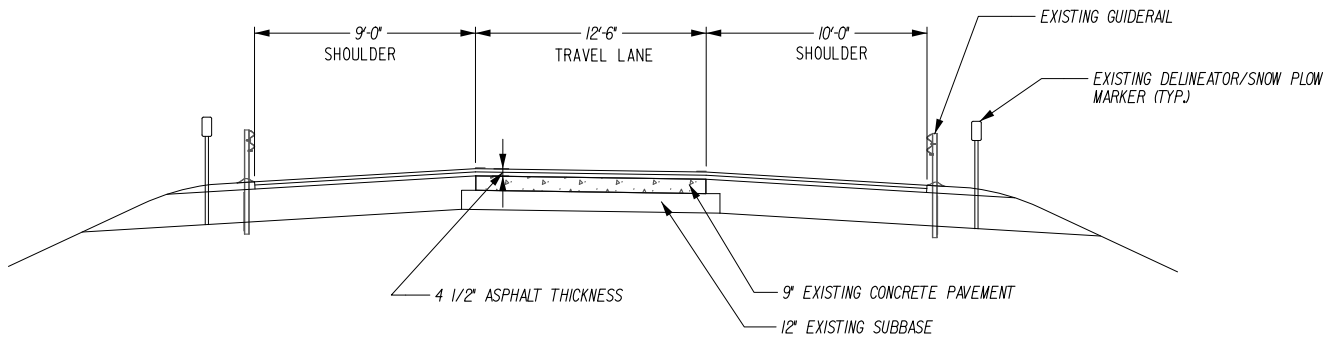
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

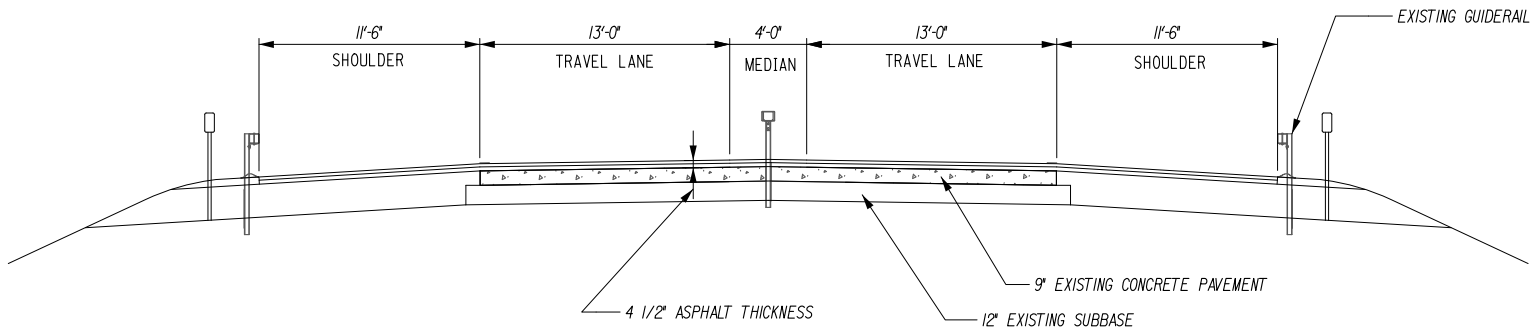
 Thruway Authority	TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
	LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/25
	TITLE OF DRAWING EXISTING TYPICAL SECTIONS	DRAWING NUMBER: TYP-2



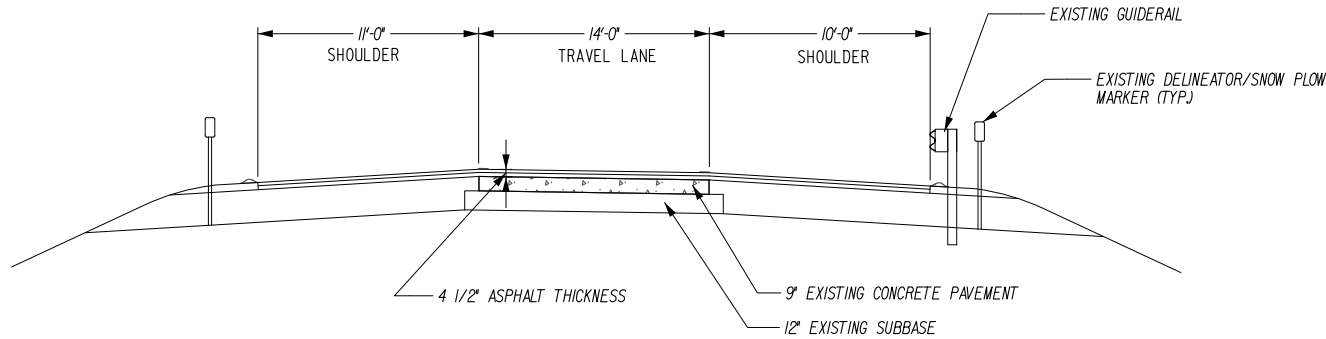
EXISTING I-90 TYPICAL SECTION
EXIT 49 WB OFF RAMP
N.T.S.




EXISTING I-90 TYPICAL SECTION
EXIT 49 EB ON RAMP
N.T.S.



EXISTING I-90 TYPICAL SECTION
EXIT 49 WB ON AND OFF RAMP
N.T.S.



EXISTING I-90 TYPICAL SECTION
EXIT 49 EB OFF RAMP
N.T.S.

ALTERED ON:	AFFIXED ON: 08/21/2025
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



Thruway
Authority

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK

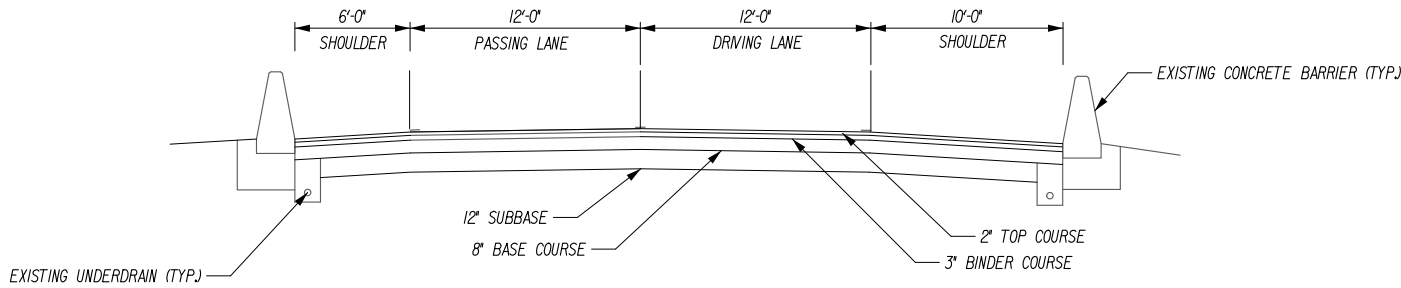
LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
EXISTING TYPICAL SECTIONS

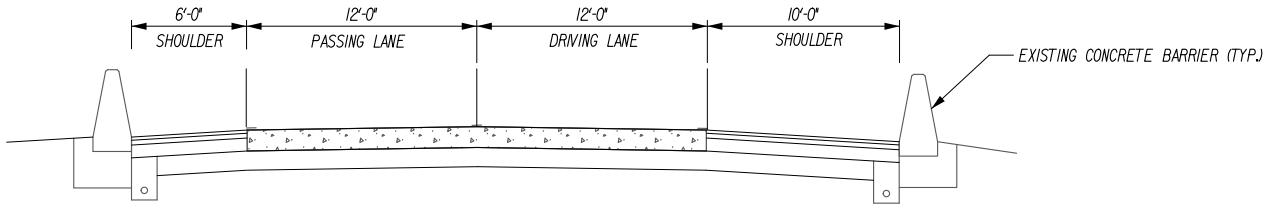
CONTRACT NUMBER:
TAB 25-31

DATE:
08/20/25

DRAWING NUMBER:
TYP-3




EXISTING I-90 TYPICAL SECTION (E1)
TOLL GANTRY
M.P. 416.90 TO M.P. 419.40
N.T.S.




EXISTING I-90 TYPICAL SECTION (E1)
TOLL GANTRY
M.P. 416.90 TO M.P. 419.40
N.T.S.

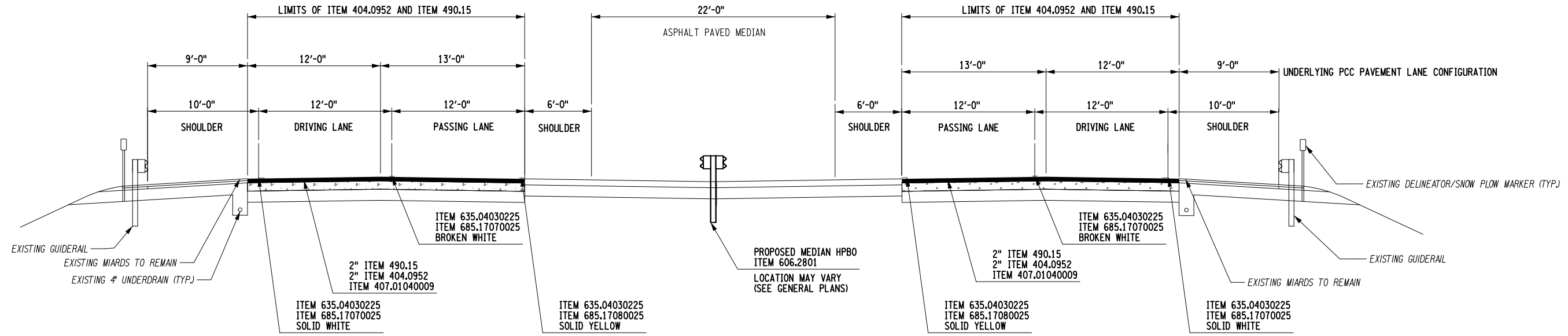
NOTES:
1. NO WORK TO BE DONE TO GANTRY CONCRETE PAVEMENT SEE GENERAL PLANS FOR DETAILS.
REFER TO SPECIAL NOTES IN THE PROPOSAL FOR WORKING NEAR AND UNDER TOLL GANTRIES.

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
	LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/25
TITLE OF DRAWING EXISTING TYPICAL SECTIONS		DRAWING NUMBER: TYP-4



PROPOSED I-90 MAINLINE
TYPICAL SECTION (P1)
ONE COURSE MILL AND INLAY NO SHOULDERS
M.P. 418.50 TO M.P. 419.40 (WESTBOUND)
M.P. 418.50 TO M.P. 419.40 (EASTBOUND)
N.T.S.

■ - ITEM 404.0952, ITEM 490.15, ITEM 407.01040009

ITEM	DESCRIPTION	UNIT
203.02	UNCLASSIFIED EXCAVATION	CY
203.24000025	SHOULDER BACKUP MATERIAL	TON
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF
490.15	PRODUCTION COLD MILL SURFACE PLANING OF BITUMINOUS CONCRETE	SY
606.2801	HPBO (MOD.) CORRUGATED MEDIAN BARRIER	LF
619.0901	TEMPORARY PAVEMENT MARKING STRIPES (TRAFFIC PAINT)	LF
627.50140008	SAWCUTTING PAVEMENT	LF
635.0403225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF
649.01	MILLED IN AUDIBLE ROADWAY DELINEATION (MIARDS)	LF
685.11060025	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES 6' x 20 MILS	LF
685.12060025	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES 6' x 20 MILS	LF
685.17070025	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6" x 20 MILS	LF
685.17080025	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6" x 20 MILS	LF

- NOTES:
- CROSS SLOPES MAY VARY NORMAL CROWN TO FULL SUPERELEVATION (BOTH BANKED LEFT AND RIGHT), THE PROPOSED TRAVEL LANE AND SHOULDER CROSS SLOPE SHALL MATCH THE EXISTING CROSS SLOPE IN ALL CASES.
 - TEMPORARY PAVEMENT MARKING STRIPES (TRAFFIC PAINT) ARE REQUIRED WHEN PERMANENT STRIPING LANES ARE REMOVED AND WILL BE INSTALLED PRIOR TO OPENING TRAVEL LANES TO TRAFFIC. PAYMENT UNDER 619.0901. REFER TO STANDARD SHEET TA 685-04.
 - ALL DEBRIS ON MILLED SURFACE SHALL BE REMOVED IN ACCORDANCE WITH SECTION 490-COLD MILLING OF THE NYSDOT STANDARD SPECIFICATIONS.
 - THE CONTRACTOR SHALL ALIGN THE PROPOSED LONGITUDINAL JOINTS WITH EXISTING LONGITUDINAL JOINTS. SEE DTL-9 FOR DETAIL ON CONSTRUCTION OF LONGITUDINAL JOINTS.
 - PLACE TACK COAT ON ALL EXPOSED SURFACES WHICH WILL BE IN CONTACT WITH NEW ASPHALT, COST TO BE PAID FOR UNDER ITEM 407.01040009, FOR APPLICATION RATES, SEE TABLE 407-1 OF NYSDOT SPECIAL SPECIFICATION.
 - ITEM 418.7603, ASPHALT JOINT ADHESIVE, SHALL BE PLACED ON CENTERLINE JOINT AND INTERFACE BETWEEN DRIVING AND RAMP LANES.
 - PERMANENT PAVEMENT STRIPES ARE TO BE REPLACED IN KIND AFTER ALL MILL AND INLAY OPERATIONS ARE COMPLETED, COST TO BE PAID FOR UNDER ITEMS 685.17070025 AND 685.17080025. RAMP STRIPES SHALL BE PAID UNDER 685.11060025 AND 685.12060025. ALL PAVEMENT MARKINGS SHALL BE INSTALLED PER NYSTA STANDARD SHEET TA 685-01.

ALTERED ON:

AFFIXED ON:
08/21/2025

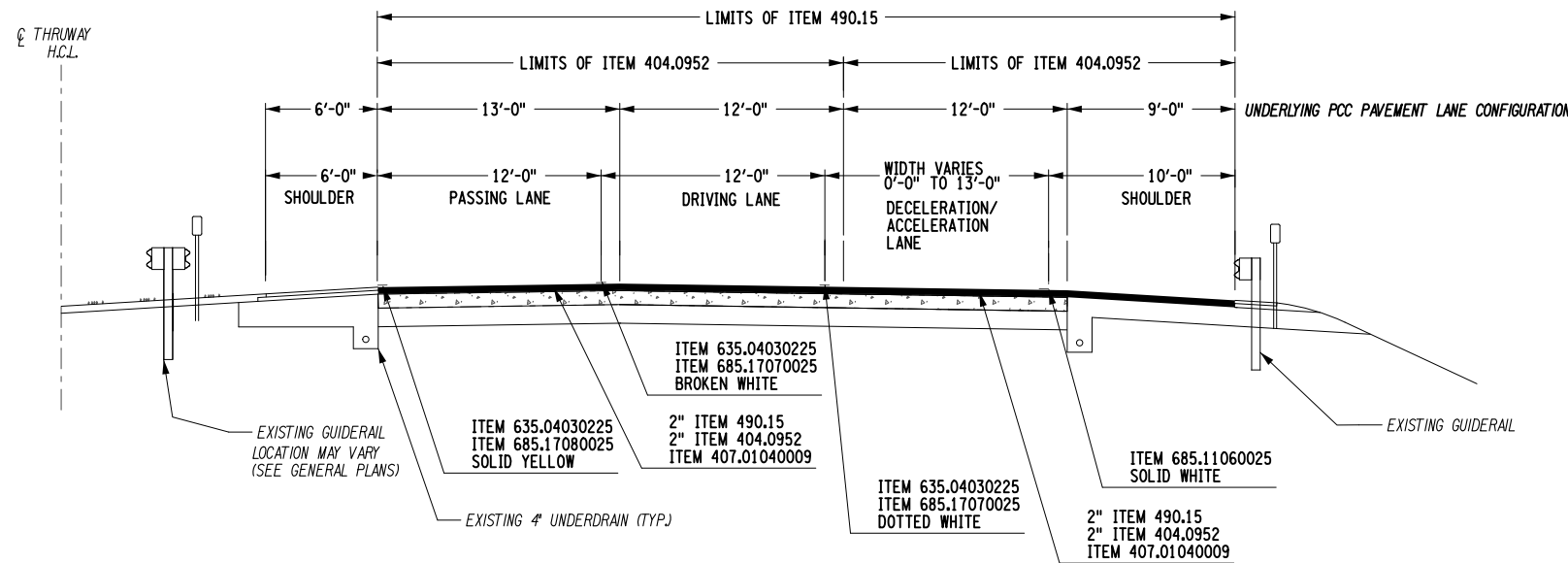
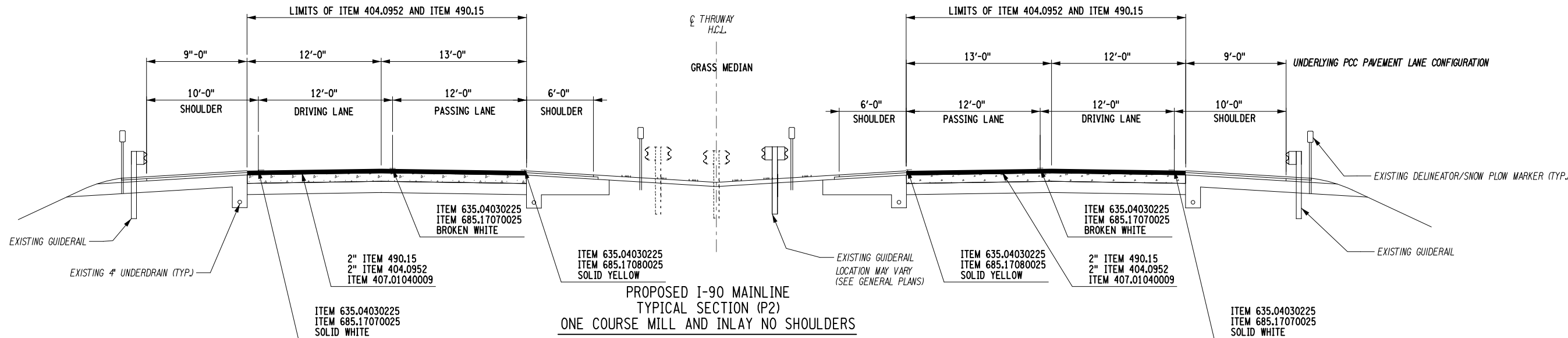
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK
LOCATION OF PROJECT
MP 416.90 TO MP 419.40
TITLE OF DRAWING
PROPOSED TYPICAL SECTIONS

CONTRACT NUMBER:
TAB 25-31
DATE:
08/20/25
DRAWING NUMBER:
TYP-5



PROPOSED I-90 MAINLINE
TYPICAL SECTION (P3)
ONE COURSE MILL AND INLAY WITH RAMP LANE AND SHOULDER

M.P. 417.40 TO M.P. 417.60 (EASTBOUND)
M.P. 417.12 TO M.P. 417.30 (WESTBOUND)
M.P. 417.56 TO M.P. 417.74 (WESTBOUND)

N.T.S.
SHOWN DIRECTION OF TRAVEL

ITEM	DESCRIPTION	UNIT
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF
490.15	PRODUCTION COLD MILL SURFACE PLANING OF BITUMINOUS CONCRETE	SY
606.2801	HPBO (MOD.) CORRUGATED MEDIAN BARRIER	LF
619.0901	TEMPORARY PAVEMENT MARKING STRIPES (TRAFFIC PAINT)	LF
635.0403225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF
649.01	MILLED IN AUDIBLE ROADWAY DELINEATION (MIARDS)	LF
685.11060025	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES 6' x 20 MILS	LF
685.12060025	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES 6' x 20 MILS	LF
685.17070025	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6" x 20 MILS	LF
685.17080025	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6" x 20 MILS	LF

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



Thruway
Authority

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK

LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
PROPOSED TYPICAL SECTIONS

CONTRACT NUMBER:

TAB 25-31

DATE:

08/20/25

DRAWING NUMBER:

TYP-6

NOTES:

1. CROSS SLOPES MAY VARY NORMAL CROWN TO FULL SUPERELEVATION (BOTH BANKED LEFT AND RIGHT), THE PROPOSED TRAVEL LANE AND SHOULDER CROSS SLOPE SHALL MATCH THE EXISTING CROSS SLOPE IN ALL CASES.

2. TEMPORARY PAVEMENT MARKING STRIPES (TRAFFIC PAINT) ARE REQUIRED WHEN PERMANENT STRIPING LANES ARE REMOVED AND WILL BE INSTALLED PRIOR TO OPENING TRAVEL LANES TO TRAFFIC. PAYMENT UNDER 619.0901. REFER TO STANDARD SHEET TA 685-04.

3. ALL DEBRIS ON MILLED SURFACE SHALL BE REMOVED IN ACCORDANCE WITH SECTION 490-COLD MILLING OF THE NYSDOT STANDARD SPECIFICATIONS.

4. THE CONTRACTOR SHALL ALIGN THE PROPOSED LONGITUDINAL JOINTS WITH EXISTING LONGITUDINAL JOINTS. SEE DTL-9 FOR DETAIL ON CONSTRUCTION OF LONGITUDINAL JOINTS.

5. PLACE TACK COAT ON ALL EXPOSED SURFACES WHICH WILL BE IN CONTACT WITH NEW ASPHALT, COST TO BE PAID FOR UNDER ITEM 407.01040009, FOR APPLICATION RATES, SEE TABLE 407-1 OF NYSDOT SPECIAL SPECIFICATION.

6. ITEM 418.7603, ASPHALT JOINT ADHESIVE, SHALL BE PLACED ON CENTERLINE JOINT AND INTERFACE BETWEEN DRIVING AND RAMP LANES.

7. PERMANENT PAVEMENT STRIPES ARE TO BE REPLACED IN KIND AFTER ALL MILL AND INLAY OPERATIONS ARE COMPLETED, COST TO BE PAID FOR UNDER ITEMS 685.17070025 AND 685.17080025. RAMP STRIPES SHALL BE PAID UNDER 685.11060025 AND 685.12060025. ALL PAVEMENT MARKINGS SHALL BE INSTALLED PER NYSTA STANDARD SHEET TA 685-01.

- ITEM 404.0952, ITEM 490.15, ITEM 407.01040009

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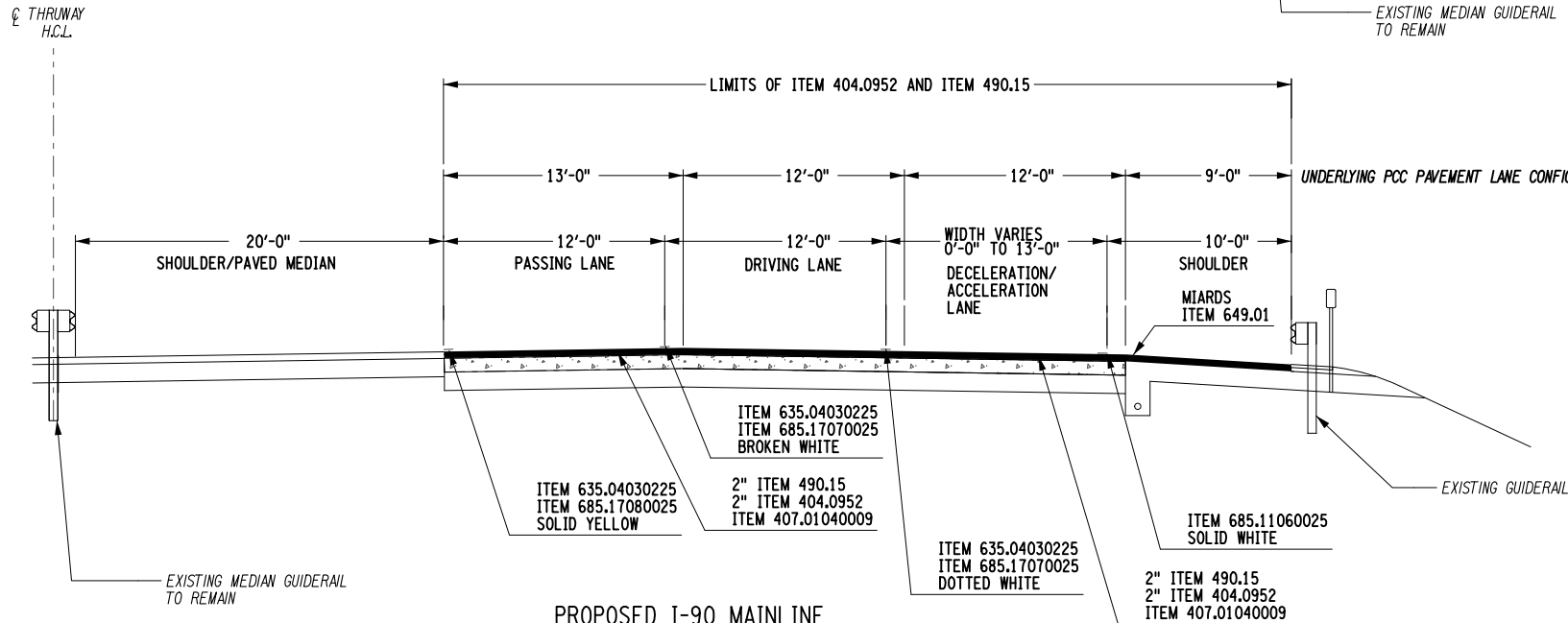
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PROPOSED I-90 MAINLINE
TYPICAL SECTION (P1)
ONE COURSE MILL AND INLAY WITH PAVED RAMP LANE
M.P. 416.94 TO M.P. 417.04 (EASTBOUND)
LOOKING DIRECTION OF TRAVEL
N.T.S.


PROPOSED I-90 MAINLINE
TYPICAL SECTION (P1)
ONE COURSE MILL AND INLAY
M.P. 416.90 TO M.P. 417.04 (WESTBOUND)
LOOKING DIRECTION OF TRAVEL
N.T.S.

- ITEM 404.0952, ITEM 490.15, ITEM 407.01040009

ITEM	DESCRIPTION	UNIT
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF
490.15	PRODUCTION COLD MILL SURFACE PLANING OF BITUMINOUS CONCRETE	SY
619.0901	TEMPORARY PAVEMENT MARKING STRIPES (TRAFFIC PAINT)	LF
627.50140008	SAWCUTTING PAVEMENT	LF
635.0403225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF
649.01	MILLED IN AUDIBLE ROADWAY DELINEATION (MIARDS)	LF
685.11060025	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES 6' x 20 MILS	LF
685.12060025	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES 6' x 20 MILS	LF
685.17070025	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6" x 20 MILS	LF
685.17080025	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6" x 20 MILS	LF


NOTES:

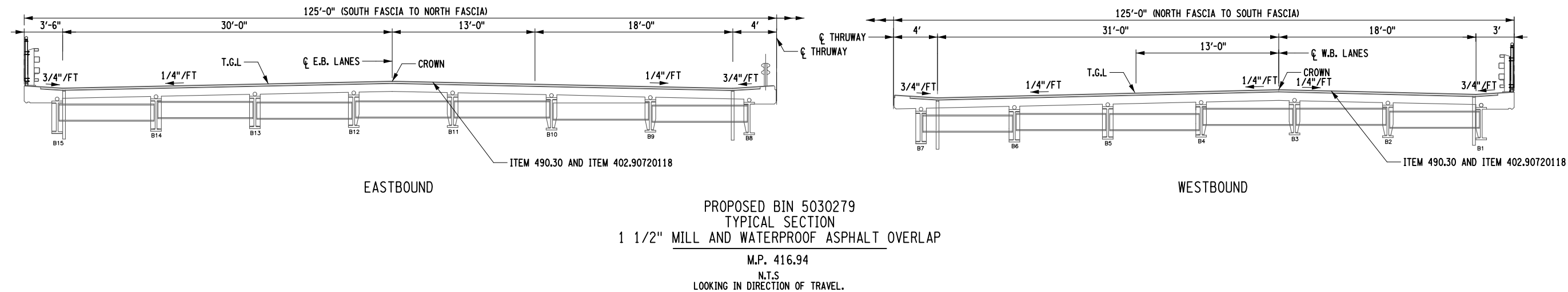
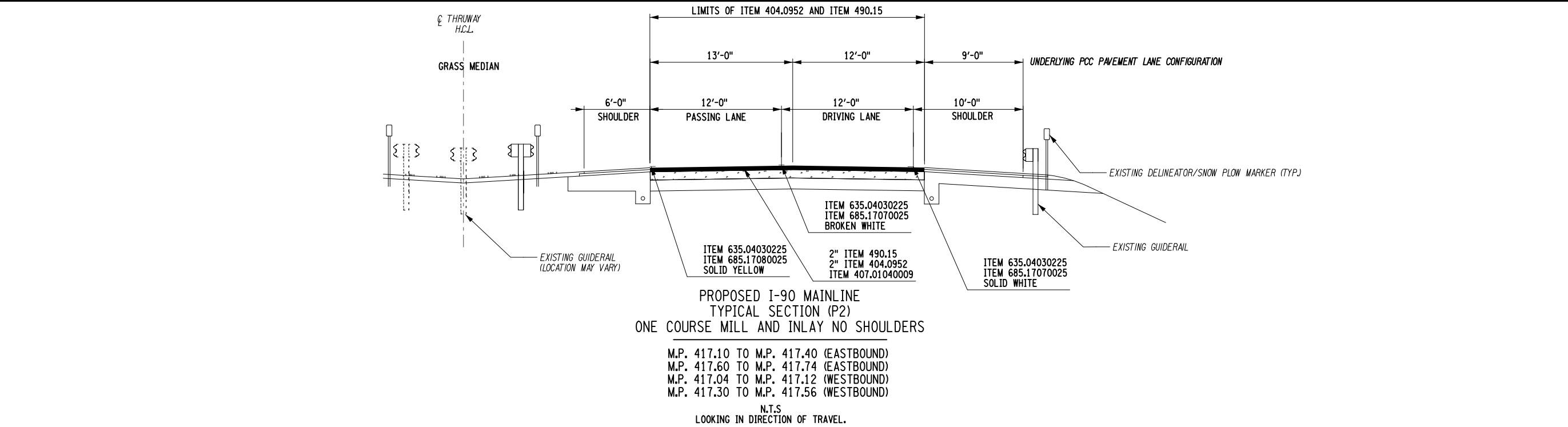
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- TEMPORARY PAVEMENT MARKING STRIPES (TRAFFIC PAINT) ARE REQUIRED WHEN PERMANENT STRIPING LANES ARE REMOVED AND WILL BE INSTALLED PRIOR TO OPENING TRAVEL LANES TO TRAFFIC. PAYMENT UNDER 619.0901. REFER TO STANDARD SHEET TA 685-04.
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- PLACE TACK COAT ON ALL EXPOSED SURFACES WHICH WILL BE IN CONTACT WITH NEW ASPHALT, COST TO BE PAID FOR UNDER ITEM 407.01040009, FOR APPLICATION RATES, SEE TABLE 407-1 OF NYSDOT SPECIAL SPECIFICATION.
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
	LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/25
TITLE OF DRAWING PROPOSED TYPICAL SECTIONS		DRAWING NUMBER: TYP-7



- NOTES:
- CROSS SLOPES MAY VARY NORMAL CROWN TO FULL SUPERELEVATION (BOTH BANKED LEFT AND RIGHT), THE PROPOSED TRAVEL LANE AND SHOULDER CROSS SLOPE SHALL MATCH THE EXISTING CROSS SLOPE IN ALL CASES.
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 - ITEM 418.7603, ASPHALT JOINT ADHESIVE, SHALL BE PLACED ON CENTERLINE JOINT AND INTERFACE BETWEEN DRIVING AND RAMP LANES.
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- ITEM 404.0952, ITEM 490.15, ITEM 407.01040009

ALTERED ON:

AFFIXED ON:
09/11/2025

SIGNATURE:
STAMP:

ITEM	DESCRIPTION	UNIT
402.90720118	WATERPROOFING BRIDGE DECK OVERLAY F2, 70 SERIES COMPACTION	TON
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF
490.15	PRODUCTION COLD MILL SURFACE PLANING OF BITUMINOUS CONCRETE	SY
490.30	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	SY
619.0901	TEMPORARY PAVEMENT MARKING STRIPES (TRAFFIC PAINT)	LF
635.0403225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF
685.11060025	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES 6' x 20 MILS	LF
685.12060025	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES 6' x 20 MILS	LF
685.17070025	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6" x 20 MILS	LF
685.17080025	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6" x 20 MILS	LF

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK
LOCATION OF PROJECT
MP 416.90 TO MP 419.40
TITLE OF DRAWING
PROPOSED TYPICAL SECTIONS
BIN 5030279

CONTRACT NUMBER:
TAB 25-31
DATE:
08/20/25
DRAWING NUMBER:
TYP-8

New York State Thruway Authority Standard Sheets

The following NYS Thruway Authority standard sheets, marked with an "X" in first column, apply to this project.

X	SHEET NO.	SUBJECT
X	TA 201-01	Clearing and Grubbing (Dwg. CG)
X	TA 203-01	Shoulder Backup 1R Projects (Dwg. SB)
	TA 203-02	Slope Flattening Details
X	TA 404-01	Highway Pavement Repair Details (Dwg. PRD)
X	TA 404-02	Bridge Deck Wearing Course Resurfacing (Dwg. BDR)
	TA 404-03	Overhead Bridge Underclearance Improvement (Dwg. BU)
	TA 603-01	Culvert Extension Details
	TA 605-01	Underdrain Details
	TA 606-01	Modified Thrie Beam (Mod.) Guiderail (Dwg. GR-1)
	TA 606-02	Vacant
	TA 606-03	Corrugated Median Barrier to Corrugated Beam Guide Railing Transition Detail D (Dwg. GR-4)
	TA 606-04	Box Beam to 42" Single Slope Half Section Concrete Barrier Pier Protection (Dwg. GR-5)
	TA 606-05	HPBO (Mod.) Corrugated Beam to 42" Single Slope Half Section Concrete Barrier Pier Protection (Dwg. GR-6)
X	TA 606-06	Typical U-Turn Median Rail Layout and Roadway Transverse Section
	TA 606-07	Modified Thrie Beam Guiderail with Rock Rail
	TA 606-08	Transition HPBO Corrugated Beam Median Guide Railing to HPBO Corrugated Beam Guide Railing
	TA 611-01	Living Snow Fences
	TA 614-01	Tree Removal
X	TA 619-01	Work Zone Traffic Control Tables & Legend
X	TA 619-02	General Work Zone Traffic Control Notes & Channelizing Devices
X	TA 619-03	Shoulder Closure Short-Term or Intermediate-Term Stationary
	TA 619-04	Shoulder Closure Short-Duration Stationary and Mobile
X	TA 619-05	Signing & Delineation for Shoulder Work Spaces with Temporary Concrete Barrier
	TA 619-06	Work Beyond Shoulder
X	TA 619-07	Be Prepared to Stop and Uneven Lanes Signing
X	TA 619-08	Single Lane Closure Short- or Intermediate-Term Stationary: 65 MPH Zone
	TA 619-09	Double Lane Closure Short- or Intermediate-Term Stationary: 65 MPH Zone
	TA 619-10	Center Lane Closure Short- or Intermediate-Term Stationary: 65 MPH Zone
	TA 619-11	Lane Shift: 65 MPH Zone
	TA 619-12	Single Lane Closure Short- or Intermediate-Term Stationary: 55 MPH Zone
	TA 619-13	Double Lane Closure Short- or Intermediate-Term Stationary: 55 MPH Zone
	TA 619-14	Center Lane Closure Short- or Intermediate-Term Stationary: 55 MPH Zone
	TA 619-15	Lane Shift: 55 MPH Zone
X	TA 619-16	Work Zone Traffic Control at Interchanges, Service Areas and Parking Areas
X	TA 619-17	Work Zone Traffic Control for Miscellaneous Operations
X	TA 619-18	Mobile Lane Closure
	TA 619-19	Mobile Lane Closure: Narrow Shoulder Area
X	TA 619-20	Short-Duration Lane Closure
	TA 619-21	Short-Duration Double Lane Closure
X	TA 619-22	Work Zone Traffic Control Guide for Pavement Striping Operations
X	TA 619-23	Mobile Lane Closure for Pavement Striping Operations
	TA 619-24	Mobile Lane Closure for Pavement Striping Operations: Narrow Shoulder Area
X	TA 619-25	Work Zone Traffic Control for Pavement Striping Operations at Interchanges, Service Areas and Parking Areas
	TA 619-26	Temporary Rock Catchment Barrier (Sheets 1-3)
	TA 619-27	Workzone Overhead Gantry Signing
	TA 619-30	New York Division Traffic Management Tables (Sheets 1-28)
	TA 619-31	Albany Division 1,150 Veh/Hr/Lane Traffic Management Tables (Sheets 1-18)
	TA 619-32	Albany Division 1,300 Veh/Hr/Lane Traffic Management Tables (Sheets 1-18)
	TA 619-33	Syracuse Division 1,150 Veh/Hr/Lane Traffic Management Tables (Sheets 1-18)
	TA 619-34	Syracuse Division 1,300 Veh/Hr/Lane Traffic Management Tables (Sheets 1-18)
	TA 619-35	Buffalo Division 1,150 Veh/Hr/Lane Traffic Management Tables (Sheets 1-37)
X	TA 619-36	Buffalo Division 1,300 Veh/Hr/Lane Traffic Management Tables (Sheets 1-37)
	TA 625-01	ROW and Survey Markers
	TA 645-01	Wrong Way Deterrence Sign
	TA 646-01	Reference Marker Details (Sheets 1-2)
	TA 670-01	Fiber Optic & Backbone Handhole Relocation Details
	TA 680-01	Inductance Loop Installation
	TA 680-02	Highway Advisory Radio (Sheets 1-9)
X	TA 685-01	Pavement Marking Details: Asphalt and Concrete Pavement (Sheets 1-2)
	TA 685-02	Pavement Marking Details: Tapered Acceleration and Deceleration Lanes
	TA 685-03	Vacant
X	TA 685-04	Temporary Pavement Marking Details
	TA 690-01	Loop and Treadle Plan (Sheets 1-2)
	TA 690-02	Toll Lane Slab Reinforcement Plan
	TA 690-03	10 ft Treadle Frame (Sheets 1-4)

The officially adopted New York State Thruway Authority Standard Sheets book is available on the Thruway Authority's website at: <http://www.thruway.ny.gov/business/contractors/standard-sheets/index.shtml>

REV. 02/25

Highway Work Type

The marked types & treatments apply to the indicated milepost range(s) below.

MILEPOST FROM:	416.90 WB	419.40 EB					
	TO: 419.40 WB	416.86 EB					
PROJECT TYPE	X	X	X	X	X	X	X
1R Resurfacing	X	X					
2R Resurfacing							
3R Rehabilitation							
Reconstruction							
Safety Improvements	X	X					
Drainage	X	X					
Rock Slope Remediation							
Pavement Striping							
Other:							
PAVEMENT TREATMENT	X	X	X	X	X	X	X
Isolated Pavement Repairs Only							
Thin Overlay without Milling							
Thin Overlay with Milling							
1" Mill & Inlay without Shoulders							
1" Mill & Inlay with Shoulders							
2" Mill & Inlay without Shoulders	X						
2" Mill & Inlay with Shoulders							
Mill to Concrete with 4" Overlay							
Mill to Concrete with 4.5" Overlay							
Mill to Concrete with 5" Overlay							
Crack and Seat with Overlay							
Rubblize with Overlay							
Other:							

Structure Work Type


The marked types apply to the indicated milepost(s) below.

MILEPOST:	416.94	417.27	418.73	418.98			
PROJECT TYPE	X	X	X	X	X	X	X
Bridge Washing							
Scour Protection							
Channel Cleaning							
Railing System							
Protective Screening							
Painting							
Steel Repair							
Wearing Surface Treatment							
Deck Repairs							
Joint Rehabilitation			X	X			
Joint Replacement							
Bearing Rehabilitation							
Bearing Replacement							
Hanger Pin Replacement							
Security							
Seismic Retrofit							
Substructure Rehabilitation							
Electrical							
Cathodic Protection System							
Fendor or Pier Protection System							
Deck Replacement							
Superstructure Replacement							
Bridge Replacement							
Added Bridge (New Location)							
Abandoned Bridge							
Other:	X	X					





New York State Department of Transportation Standard Sheets

The latest revisions of the New York State Department of Transportation Standard Sheets maintained by NYSDOT, which are current as of the Standard Specifications adoption date shown on the Proposal cover, shall be considered to be in effect. All pay items and work contained in the Contract and any additional pay items and work encountered during the course of the Contract shall be subject to the applicable standard sheet(s) unless otherwise specified in the Contract documents.

The officially adopted New York State Department of Transportation Standard Sheets book is available on the NYSDOT website at: <https://www.dot.ny.gov/main/business-center/engineering/specifications/busi-e-standards-usc>

REVISIONS					Thruway Authority	TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
DATE	DESCRIPTION	BY	SYM.			LOCATION OF PROJECT MP 416.90 TO 419.40	DATE: 08/20/25
						TITLE OF DRAWING NYSTA STANDARD SHEETS LISTING AND WORK TYPE TABLES	DRAWING NUMBER: SS-1

17. CLEAN AND RESTORE ALL DISTURBED AREAS.

ALTERED ON: SIGNATURE: STAMP:		AFFIXED ON: 08/21/2025 SIGNATURE: STAMP:																																	
																																			
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EROSION AND SEDIMENT CONTROL NOTES:


1. CONTRACTOR SHALL CLEAN SEDIMENT AND DEBRIS OFF ALL PAVED SURFACES AT THE END OF EACH DAY AS DIRECTED BY THE ENGINEER. COST OF CLEANING SHALL BE INCLUDED IN THE VARIOUS ITEMS OF THE CONTRACT.
2. THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE ENGINEER A WRITTEN PROPOSED MEASURES FOR TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL WORK AND SCHEDULE OF OPERATIONS AS REQUIRED BY SECTION 209 OF THE NYSDOT STANDARD SPECIFICATIONS.
3. INSPECTION, PERIODIC CLEANING AND MAINTENANCE OF TEMPORARY SOIL EROSION AND POLLUTION CONTROL DEVICES SHALL BE PERFORMED ON A SCHEDULE BASIS IN ACCORDANCE WITH SECTION 209 OF THE NYSDOT SPECIFICATION. THE COST OF INSTALLING, CLEANING AND REMOVING TEMPORARY SOIL EROSION AND WATER POLLUTION CONTROL DEVICES SHALL BE PAID FOR UNDER THE ITEMS SHOWN.
4. ALL NECESSARY PRECAUTIONS SHALL BE TAKEN TO PREVENT CONTAMINATION OF ANY STREAM OR WATERWAY BY SILT, SEDIMENT, FUELS, SOLVENTS, LUBRICANTS, EPOXY COATINGS, CONCRETE OR SLURRY LEACHATE OR ANY OTHER POLLUTANT ASSOCIATED WITH CONSTRUCTION AND CONSTRUCTION PROCEDURES.
5. ALL CONTROLS SHALL BE PLACED PRIOR TO STARTING EARTHWORK OPERATIONS AND SHALL REMAIN IN PLACE UNTIL NEW SLOPES ARE STABILIZED WITH SEEDING AND/OR SLOPE PROTECTION.
6. CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ANY ADDITIONAL EROSION CONTROL MEASURES THAT MAY BE REQUIRED OR AS ORDERED BY THE ENGINEER.
7. CONTRACTOR STAGING AREAS SHALL BE EXISTING AREAS OF NON-DISTURBABLE COVER (I.E. ASPHALT MILLING, ASPHALT OR CONCRETE PAVEMENT). IF THE CONTRACTOR CREATES A STAGING AREA ON DISTURBABLE COVER (I.E. GRASS) THEN THE ENTIRE AREA SHALL BE ENCLOSED WITH SILT FENCE.
8. CONTRACTOR SHALL ESTABLISH A STABILIZED CONSTRUCTION ENTRANCE INTO AND OUT OF EACH WORK AREA AND EACH STAGING AREA CONSTRUCTED ON DURABLE COVER.
9. THE ALLOWABLE DISTURBANCE AREA IS 1.0 ACRE; THE CONTRACTOR IS RESPONSIBLE FOR APPLICABLE PERMITS IF ACREAGE OVER 1.0 ACRE IS EXCEEDED, AT NO COST TO THE AUTHORITY.

UTILITY NOTES:

1. LOCATIONS OF UTILITIES, PUBLIC AND/OR PRIVATE, INDICATED AS EXISTING AND/OR TO BE CONSTRUCTED AS SHOWN ON THE PLANS ARE APPROXIMATE ONLY. THEIR EXACT LOCATION SHALL BE DETERMINED IN THE FIELD. ADDITIONAL UTILITY LINES, WHETHER ABANDONED OR IN SERVICE, MAY EXIST AND IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONDUCT HIS OPERATIONS AND TAKE THE NECESSARY PRECAUTIONS TO PREVENT INTERFERENCE WITH OR DAMAGE TO THESE OR OTHER FACILITIES DURING THE COURSE OF CONSTRUCTION.
2. IN THE EVENT THE CONTRACTOR DAMAGES AN EXISTING UTILITY SERVICE CAUSING AN INTERRUPTION IN SAID SERVICE, HE/SHE SHALL IMMEDIATELY STOP WORK AND RESTORE SERVICE AND MAY NOT COMMENCE HIS/HER WORK OPERATION UNTIL SERVICE IS RESTORED.
3. THE METHOD OF REMOVAL OF EXISTING ROADWAY OR SHOULDER PAVEMENT IN THE IMMEDIATE VICINITY OF UNDERGROUND UTILITIES SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.
4. THE AUTHORITY'S FIBER OPTIC INFRASTRUCTURE RUNS ALONG THE RIGHT SIDE OF THE WESTBOUND LANES.
5. THE AUTHORITY'S FIBER OPTIC INFRASTRUCTURE IS LOCATED WITHIN THE ENTIRE WORK LIMITS OF THIS PROJECT, THE CONTRACTOR IS ADVISED TO CONTACT UDIG NY AT 811 OR 1-800-962-7962, PRIOR TO ANY EXCAVATION.
6. WARNING - EXISTING UNDERGROUND UTILITIES MAY BE LOCATED WITHIN THE WORK LIMITS AND MAY BE ENCOUNTERED DURING CONSTRUCTION. EXTREME CARE SHOULD BE EXERCISED TO AVOID DAMAGE TO THESE FACILITIES. ANY DAMAGE SHALL BE IMMEDIATELY REPORTED TO THE ENGINEER AND TO THE OWNER TO THE UTILITY. THE CONTRACTOR IS RESPONSIBLE FOR THE COST OF REPAIRS. IN ACCORDANCE WITH 16NYCRR, PART 753, "PROTECTION OF UNDERGROUND FACILITIES", UDIG NY SHALL BE CONTACTED PRIOR TO ANY EXCAVATION AT 1-800-962-7962 OR 811. THRUWAY AUTHORITY UTILITIES WILL BE LOCATED BY THRUWAY STAFF.


CONCRETE MEDIAN BARRIER NOTES:

1. IF CONTRACTOR CHOOSES TO UTILIZE "CAST IN PLACE" METHOD FOR SINGLE SLOPE CONCRETE MEDIAN BARRIER (ITEM 606.3041), NO HORIZONTAL JOINT WILL BE ALLOWED TO BE LOCATED BETWEEN THE TOP OF THE BARRIER TO THE BOTTOM OF 9" EMBEDMENT. THEREFORE, THE CROSS SECTION FROM TOP OF BARRIER TO BOTTOM OF 9" EMBEDMENT MUST BE CAST IN PLACE AS ONE MONOLITHIC POUR.
2. IF THE CONTRACTOR PROPOSES TO USE A "FOOTER", THE "FOOTER" SHALL BE BELOW THE 9" EMBEDMENT.

ALTERED ON:	AFFIXED ON: 08/21/2025
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 Thruway Authority	TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
	LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/25
	TITLE OF DRAWING GENERAL NOTES	DRAWING NUMBER: GNN-2

ASPHALT FULL DEPTH REPAIRS (416.9 to 419.4)													
Location		MP	Length (LF)	Width (LF)	Area (SF)	Depth (IN)	Sawcut (LF)	Excavation (CY)	Tack (Gal) (.06 App. Rate)	Binder (TON)	Base (TON)	Subbase (CY)	Comment
							627.50140008	203.02	407.01040009	404.1979	404.3779	304.12	
EB	1	419.1 to 419.0	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	2		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	3		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	4	418.9 to 418.8	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	5		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	6		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	7		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	8		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	9		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	10	418.8 to 418.7	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	11	418.7 to 418.6	20	12	240	25.5	64	18.89	1.60	6.66	13.32	8.89	Full Depth Repair
	12	417.6 to 417.5	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	13		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	14		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	15	417.5 to 417.4	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	16		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	17		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	18	417.4 to 417.3	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	19	417.3 to 417.2	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	20		20	12	240	25.5	64	18.89	1.60	6.66	13.32	8.89	Full Depth Repair
	21		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	22	417.2 to 417.1	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	23		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	24		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	25	417.0 to 416.9	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	26		8	13	104	25.5	42	8.19	0.69	2.89	5.77	3.85	Full Depth Repair
	27		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	28		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	29		8	13	104	25.5	42	8.19	0.69	2.89	5.77	3.85	Full Depth Repair
WB	30	417.0 to 417.1	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	31		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	32		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	33	417.1 to 417.2	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	34		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	35		20	12	240	25.5	64	18.89	1.60	6.66	13.32	8.89	Full Depth Repair
	36	417.5 to 417.6	8	13	104	25.5	42	8.19	0.69	2.89	5.77	3.85	Full Depth Repair
	37		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	38		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	39	417.6 to 417.7	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	40		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	41		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	42		20	12	240	25.5	64	18.89	1.60	6.66	13.32	8.89	Full Depth Repair
	43		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	44	417.7 to 417.8	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	45		8	13	104	25.5	42	8.19	0.69	2.89	5.77	3.85	Full Depth Repair
	46		418.3 to 418.4	20	12	240	25.5	64	18.89	1.60	6.66	13.32	8.89
	47	418.8 to 418.9	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	48	419.3 to 419.4	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	49		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
EXIT 49 RAMP	50	EB	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	51		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	52	WB	8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	53		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
	54		8	12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair
55	8		12	96	25.5	40	7.56	0.64	2.66	5.33	3.56	Full Depth Repair	
TOTALS							2,328	475	40	167	335	223	
TOTALS PLUS 15%							2,677	546	46	192	385	257	

ALTERED ON:

AFFIXED ON:

08/22/2025

SIGNATURE:
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REVISIONS

DATE	DESCRIPTION	BY	SYM.



Thruway
Authority

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK

LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
MISCELLANEOUS TABLES

CONTRACT NUMBER:

TAB 25-31

DATE:

08/20/25

DRAWING NUMBER:

MST-1

TABLE OF GUIDE RAIL AND MEDIAN BARRIER


	START			END				APPROX. LENGTH OF PROPOSED WORK																							DESCRIPTION OF WORK
RUN #	GNP	MP	SIDE	GNP	MP	SIDE	DIRECTION		568.70	606.10	606.100002	606.11	606.120102	606.1203	606.2801	606.2802	606.3041	606.7101	606.72	606.73	606.74	606.79200125	606.7946	606.8805	606.8903	606.8905	606.9003	606.9004	606.92010125		
								LF	LF	LF	LF	EA	EA	LF	EA	LF	LF	LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	EA	EA		
1	GP-1	419.342	LT	GP-2	418.989	LT	EB	1863.8						1750				1850							2					REPLACE CORR. MEDIAN BARRIER WITH HPBO FROM BIN 5511949 TO BIN 5511929	
2	GP-2	418.964	LT	GP-3	418.738	LT	EB	1193.3						1013				1113							2					REPLACE CORR. MEDIAN BARRIER WITH HPBO FROM BIN 5511929 TO BIN 5544919	
3	GP-3	418.738	LT	GP-3	418.707	LT	EB	163.7																				160	REPLACE SPLICE BOLTS		
4	GP-4	418.436	RT	GP-4	418.430	RT	EB	31.7					1								1									REPLACE TYPE 3 BOX BEAM END ASSEMBLY	
5	GP-5	418.045	LT	GP-6	417.885	LT	EB	844.8						801	1			801					1							NEW TRANSITION AND REPLACE CORR. MEDIAN BARRIER WITH HPBO	
6	GP-6	417.971	RT	GP-6	417.967	RT	EB	21.1					1								1									REPLACE TYPE 3 BOX BEAM END ASSEMBLY	
7	GP-6	417.910	LT	GP-6	417.710	LT	EB	1056.0						1157	1			1157					1							REPLACE CORR. MEDIAN BARRIER WITH HPBO	
8	GP-8	417.364	RT	GP-8	417.359	RT	EB	26.4					1																	REPLACE TYPE 3 BOX BEAM END ASSEMBLY	
9	GP-8	417.215	LT	GP-9	416.950	LT	EB	1399.2						1288				1338							1					NEW TRANSITION AND REPLACE CORR. MEDIAN BARRIER WITH HPBO	
RAMPS																															
		STA			STA																										
10	GP-8	797.450	LT	GP-10	1955.120	LT					120					990					2424			2						REPLACE BOX BEAM MEDIAN BARRIER WITH CONCRETE SINGLE SLOPE BARRIER AND TRANSITIONS	
11	GP-11	2565.910	LT	GP-8	3365.150	LT					60					753								1						REPLACE BOX BEAM MEDIAN BARRIER WITH CONCRETE SINGLE SLOPE BARRIER AND TRANSITIONS	
12	GP-11	2289.920	RT	GP-11	2449.590	RT										100	148								1		1			INSTALL SINGLE SLOPE BARRIER AT GANTRY	
13	GP-11	2346.320	RT	GP-11	2507.080	RT										100	196								2		1	1		INSTALL SINGLE SLOPE BARRIER AT GANTRY	
14	GP-8	3295.870	RT	GP-8	3484.190	RT			34	953		1										1								EXTEND AND REPLACE BOX BEAM BRIDGE APPROACH RAILING	
15	GP-8	3717.700	RT	GP-10	1074.350	RT			34				1																		REPLACE BOX BEAM BRIDGE APPROACH RAILING
16	GP-10	1479.220	RT	GP-10	3717.660	RT			34			126	1																		EXTEND REPLACE BOX BEAM BRIDGE APPROACH RAILING
17	GP-8	3369.230	RT	GP-8	3484.630	RT			34																						REPLACE BOX BEAM BRIDGE APPROACH RAILING AND TIE INTO EXISTING BOX BEAM
TOTALS									136	953	126	180	3	3	6009	2	1943	344	6259	1021	2424	6	2	3	3	5	2	1	160		

ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES:
568.70	TRANSITION BRICE RAILING	LF	606.74	REMOVING AND DISPOSING BOX BEAM MEDIAN BARRIER	LF	
606.10	BOX BEAM GUIDE RAILING	LF	606.79200125	REMOVE AND DISPOSE END TERMINAL FOR BOX BEAM GUIDE RAIL AN	EA	
606.100002	BOX BEAM GUIDE RAILING (SHOP BENT OR SHOP MITERED)	LF	606.7946	REMOVE AND DISPOSE ANCHORAGE UNIT FOR HPBO (MOD) CORRUGATED	EA	
606.11	BOX BEAM MEDIAN BARRIER	LF	606.8805	TRANSITION BETWEEN BOX BEAM MEDIAN BARRIER AND SINGLE SLOPE	EA	
606.120102	BOX BEAM GUIDE RAILING END ASSEMBLY TYPE 1	EA	606.8903	TRANSITION - HPBO (MOD.) CORR BEAM GUIDERAILING TO SINGLE SL	EA	
606.1203	BOX BEAM END ASSEMBLY, TYPE 3	EA	606.8905	TRANSITION - HPBO (MOD) CORRUGATED BEAM MEDIAN BARRIER TO WE	EA	
606.2801	HPBO (MOD.) CORRUGATED BEAM MEDIAN BARRIER	LF	606.9003	TRANSITION - HALF SECTION AND FULL SECTION SINGLE SLOPE CONC	EA	
606.2802	ANCHORAGE UNIT FOR HPBO (MOD.) CORRUGATED BEAM MEDIAN BARRIER	EA	606.9004	TRANSITION - HALF SECTION AND FULL SECTION SINGLE SLOPE CONC	EA	
606.3041	SINGLE-SLOPE CONCRETE MEDIAN BARRIER (OPTIONAL)	LF	606.92010125	REPLACE GUIDE RAIL SPLICE BOLTS	EA	
606.7101	REMOVE AND DISPOSE HPBO (MOD.) CORRUGATED BEAM GUIDE RAIL	LF				
606.72	REMOVING AND DISPOSING CORRUGATED BEAM MEDIAN BARRIER	LF				
606.73	REMOVING AND DISPOSING BOX BEAM GUIDE RAILING	LF				

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



Thruway
Authority

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK

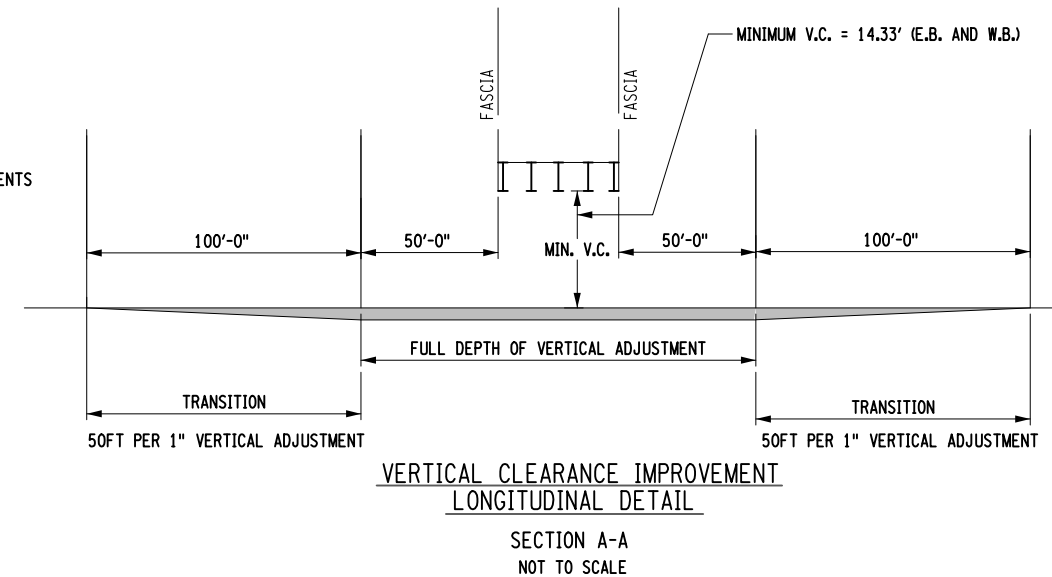
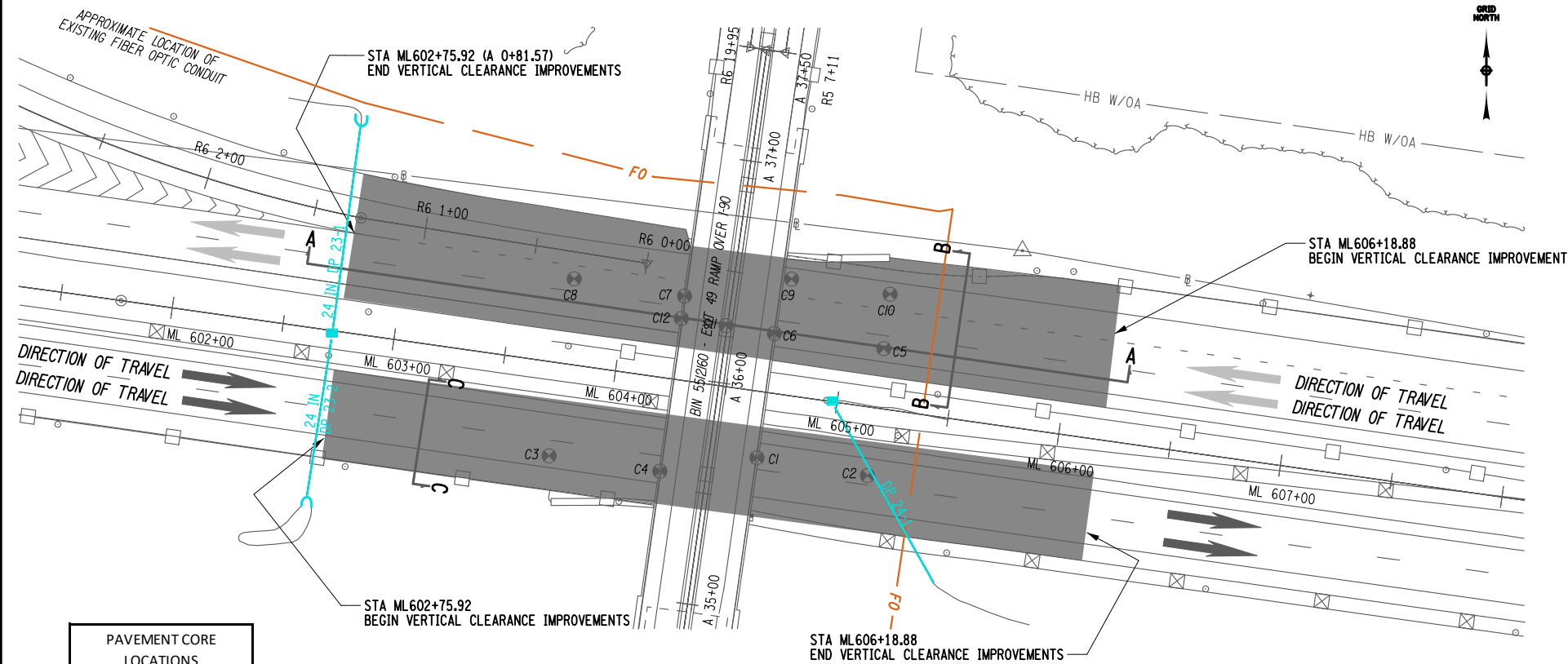
LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
MISCELLANEOUS TABLES

CONTRACT NUMBER:
TAB 25-31

DATE:
08/20/25

DRAWING NUMBER:
MST-2

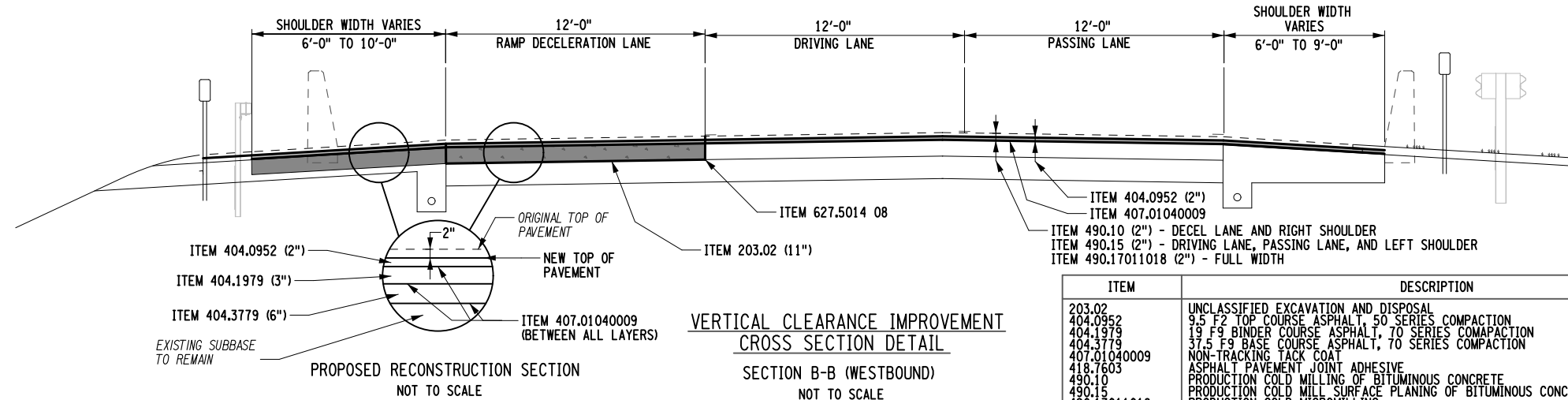
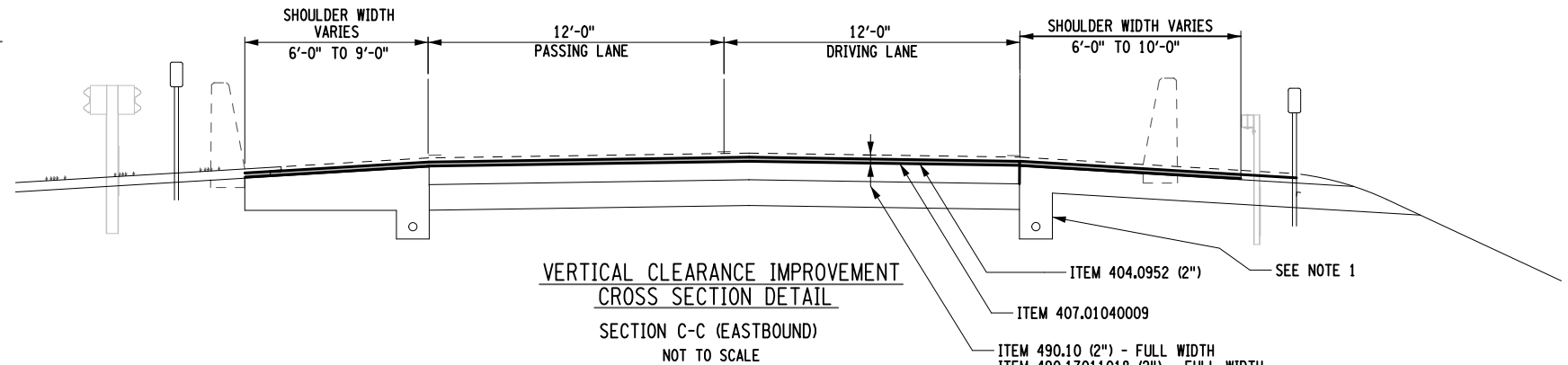


PAVEMENT CORE LOCATIONS	
ID #	RECORDED ASPHALT DEPTH (IN)
C1	13
C2	13.5
C3	13.5
C4	13.5
C5	13.5
C6	5.5
C7	12
C8	13
C9	4
C10	5
C11	12
C12	13

VERTICAL IMPROVEMENT DETAIL
MP 417.27
NOT TO SCALE

BRIDGE CLEARANCE TABLE					
MILEPOST	BIN	DIRECTION OF TRAVEL	EXISTING CLEARANCE (FT)	PROPOSED CLEARANCE (FT)	VERTICAL ADJUSTMENT (IN)
417.27	5512160	WB	14.33	14.5	2
417.27	5512160	EB	14.33	14.5	2

NOTES:
1. REPLACE OR INSTALL UNDERDRAIN AS DIRECTED BY THE EIC. THE FOLLOWING ITEMS SHOULD BE USED: 206.0201, 605.1001, 605.1602, AND 605.21015025.



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AFFIXED ON: 08/21/2025

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SIGNATURE: [Signature]

STATE OF NEW YORK
KEITH R. GAWRONSKI
089722
LICENSED PROFESSIONAL ENGINEER

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DATE	DESCRIPTION	BY	SYM.

Thruway Authority

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK

LOCATION OF PROJECT
MP 416.90 TO MP 419.40

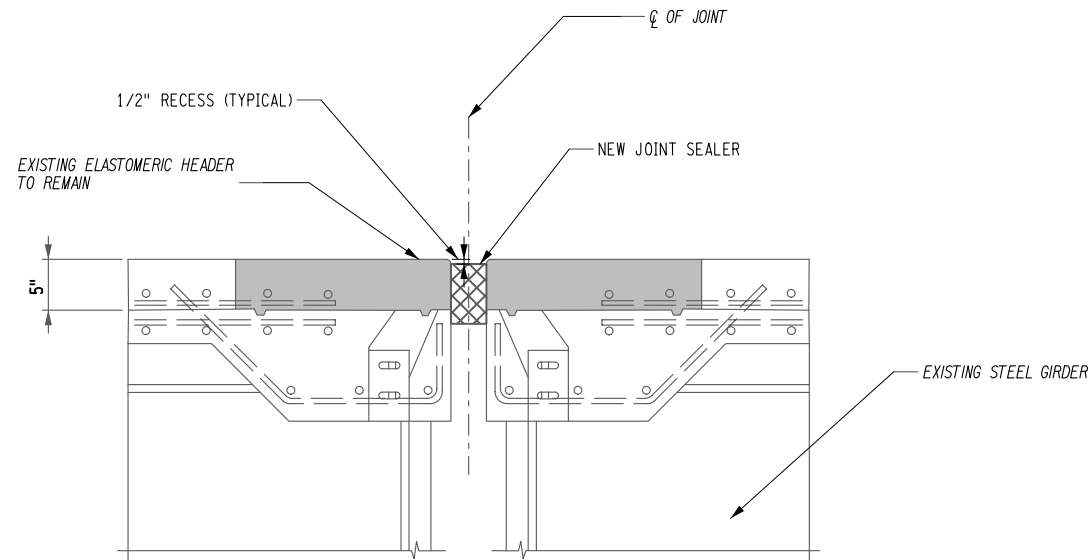
TITLE OF DRAWING
VERTICAL CLEARANCE IMPROVEMENT DETAILS

CONTRACT NUMBER:
TAB 25-31

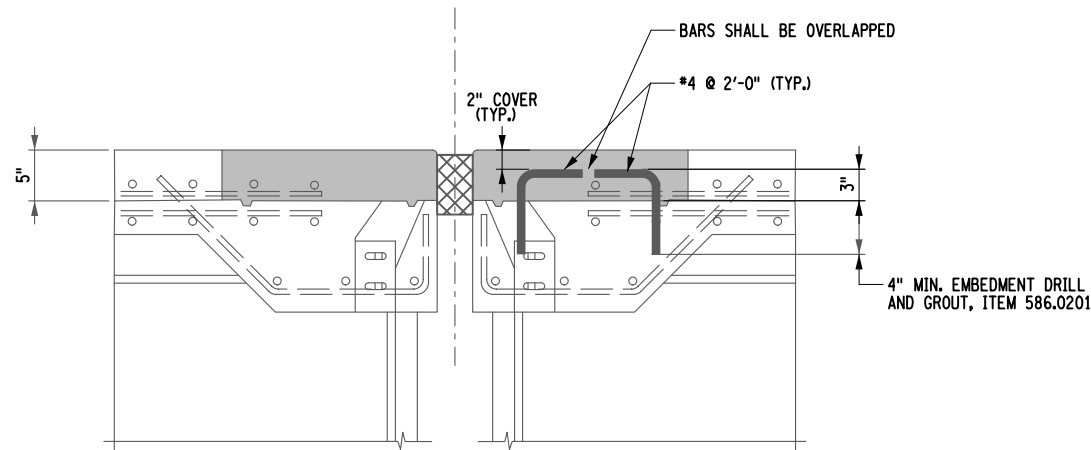
DATE:
08/20/25

DRAWING NUMBER:
DTL-1

ITEM	DESCRIPTION	UNIT
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON
404.1979	19 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION	TON
404.3779	37.5 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF
490.10	PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE	SY
490.15	PRODUCTION COLD MILL SURFACE PLANING OF BITUMINOUS CONCRETE	SY
490.17011018	CUTTING PAVEMENT	LF
627.5014 08		



PROPOSED JOINT SEAL REPLACEMENT
MP 418.73 AND MP 418.98
(NOT TO SCALE)



PROPOSED HEADER REPAIRS
BIN 5511929
(NOT TO SCALE)

NOTES:

THE "JOINT OPENING" SHALL BE DETERMINED AT THE TIME OF PLACEMENT, USING THE TABLE PROVIDED ON THIS DRAWING.

THE CONTRACTOR SHALL SUPPLY A JOINT SEAL WITH AN UNCOMPRESSED WIDTH EQUAL TO OR GREATER THAN THE DIMENSION NOTED AS "MIN. SEAL WIDTH" IN THE TABLE ABOVE. THE SEAL SUPPLIED SHALL NOT EXCEED THE MANUFACTURER'S COMPRESSION LIMITS AT THE "MINIMUM OPENING" NOTED IN THE TABLE.

THE SUPERSTRUCTURE TEMPERATURE USED TO DETERMINE THE JOINT OPENING SHALL BE THE AVERAGE TEMPERATURE OF THE BOTTOM OF THE GIRDER TOP FLANGES, (OR) THE TEMPERATURE OF THE SIDE OF A PRESTRESSED BEAM NOT DIRECTLY IN SUNLIGHT. A MINIMUM OF TWO TEMPERATURE READINGS PER SPAN SHALL BE USED TO CALCULATE THE AVERAGE SUPERSTRUCTURE TEMPERATURE.

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Authority**

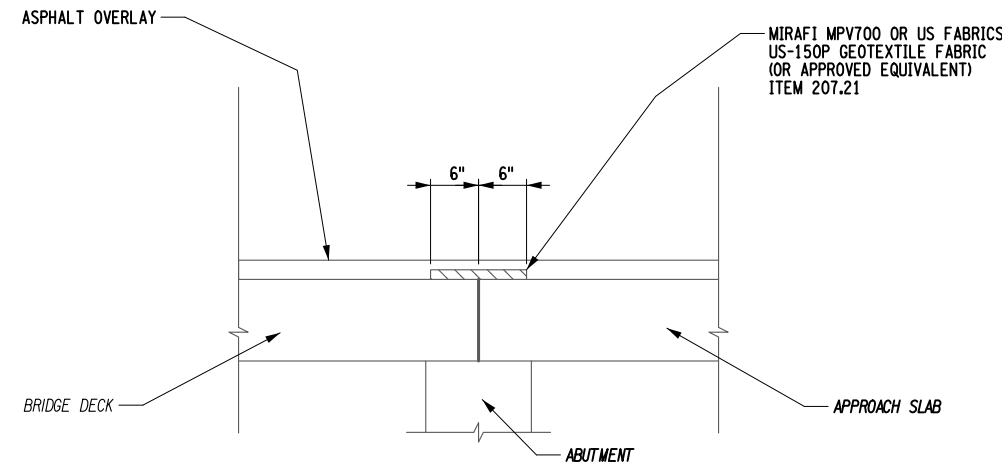
TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/25
TITLE OF DRAWING BRIDGE JOINT DETAILS	DRAWING NUMBER: DTL-2

MP 418.98 - PIER 1 JOINT	
EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 1/4
35	1 1/4
40	1 1/8
45	1 1/8
50	1 1/8
55	1 1/8
60	1 1/8
65	1 1/8
70	1 1/8
75	1 1/8
80	1
85	1
90	1
95	1
100	1
105	1
110	1
NOTES: MIN. OPENING (IN.) = 1 MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.375 TOTAL MOVEMENT (IN.) = 0.375	

MP 418.98 - PIER 2 JOINT	
EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	2
35	2
40	2
45	1 7/8
50	1 7/8
55	1 7/8
60	1 3/4
65	1 3/4
70	1 5/8
75	1 5/8
80	1 5/8
85	1 1/2
90	1 1/2
95	1 1/2
100	1 3/8
105	1 3/8
110	1 3/8
NOTES: MIN. OPENING (IN.) = 1.375 MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 2.5 TOTAL MOVEMENT (IN.) = 1.125	

MP 418.73 - JOINT 1	
EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	2 1/8
35	2
40	2
45	1 7/8
50	1 7/8
55	1 3/4
60	1 3/4
65	1 3/4
70	1 3/4
75	1 3/4
80	1 5/8
85	1 5/8
90	1 1/2
95	1 1/2
100	1 3/8
105	1 3/8
110	1 3/8
NOTES: MIN. OPENING (IN.) = 1.375 MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 2.625 TOTAL MOVEMENT (IN.) = 1.375	

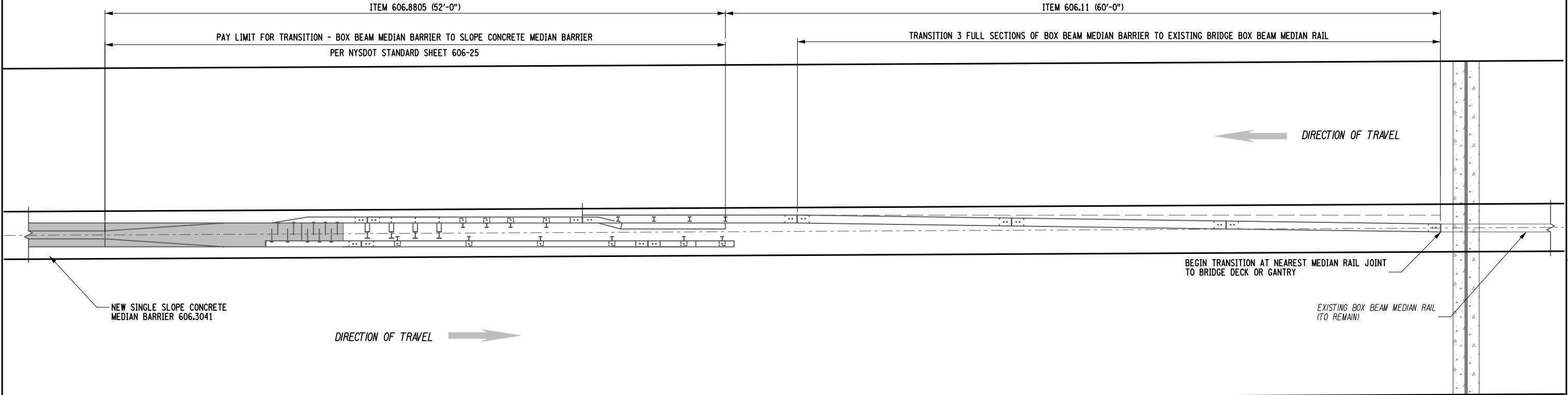
ITEM	DESCRIPTION	UNIT
207.21	GEOTEXTILE SEPARATION	SY
556.0202	EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES	LB
567.62	BRIDGE JOINT HEADER	CF
567.63	BRIDGE JOINT SEAL	LF
580.01	REMOVAL OF STRUCTURAL CONCRETE	CY
586.0201	DRILLING AND GROUTING BOLTS OR REINFORCEMENT BARS	EACH



SAWED JOINT GEOTEXTILE FABRIC DETAIL
(NOT TO SCALE)

NOTES:


AT JOINTS BETWEEN ROLLS, OVERLAP FABRIC A MINIMUM OF 6".



NOTES


PLAN - TRANSITION BETWEEN EXISTING BOX BEAM MEDIAN BARRIER AND SINGLE SLOPE CONCRETE BARRIER

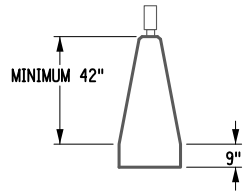
1. THE TRANSITION PORTION FOR ITEM 606.8805 WILL BE ALLOWED TO BE CAST IN PLACE.

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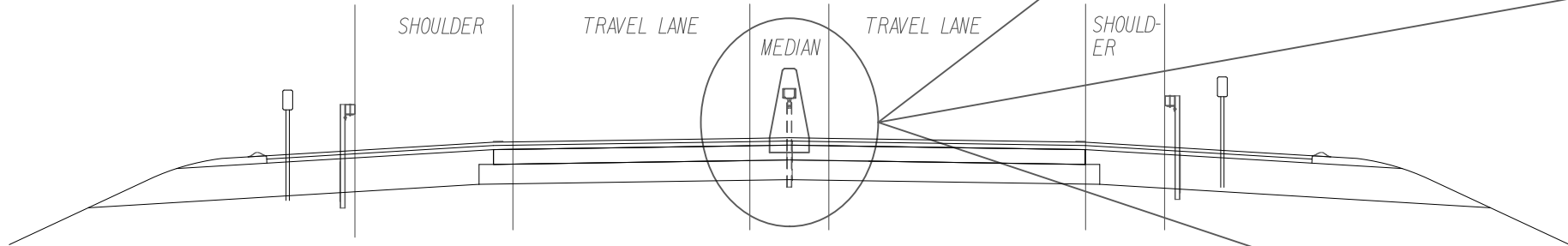
	Thruway Authority	TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
		LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING MEDIAN BARRIER TRANSITION		DRAWING NUMBER: DTL-3	



SECTION OF PRECAST SINGLE-SLOPE
CONCRETE MEDIAN BARRIER

NOTES:

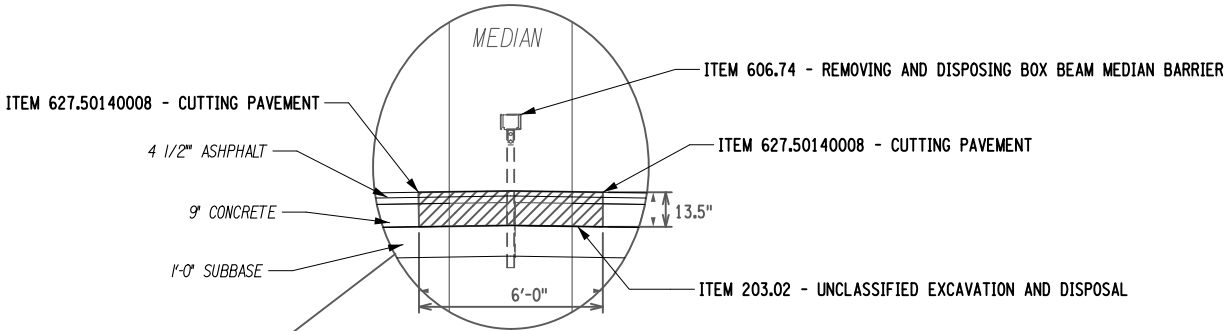
1. CONTRACTOR SHALL VERIFY EXPOSURE HEIGHT OF CONCRETE MEDIAN BARRIER.
2. MINIMUM EXPOSURE HEIGHT ON EITHER SIDE OF THE CONCRETE MEDIAN BARRIER IS 42".



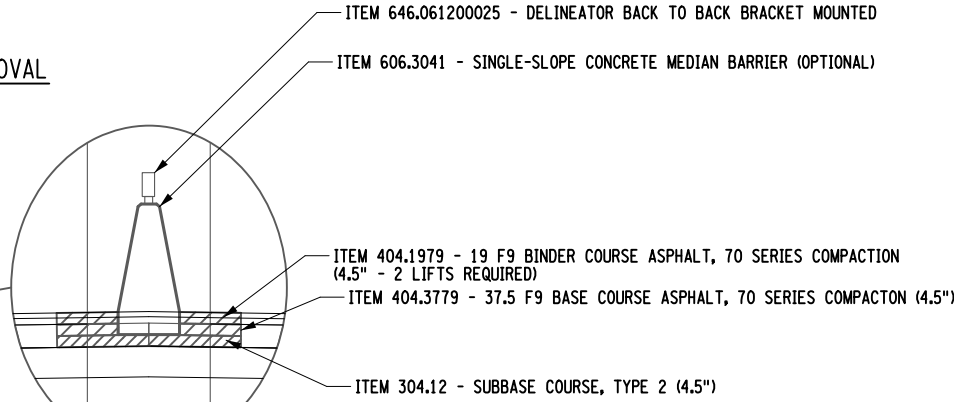
EXISTING RAMP TYPICAL SECTION
RAMP B AND C
N.T.S.

NOTES:

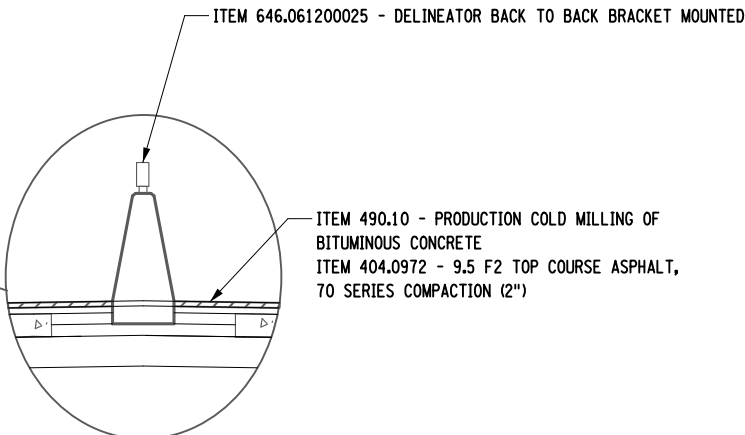
1. IF CONTRACTOR CHOOSES TO UTILIZE "CAST IN PLACE" METHOD FOR SINGLE SLOPE CONCRETE MEDIAN BARRIER (ITEM 606.3041), NO HORIZONTAL JOINT WILL BE ALLOWED TO BE LOCATED BETWEEN THE TOP OF THE BARRIER TO THE BOTTOM OF 9" EMBEDMENT. THEREFORE, THE CROSS SECTION FROM TOP OF BARRIER TO BOTTOM OF 9" EMBEDMENT MUST BE CAST AS ONE MONOLITHIC POUR.
2. IF THE CONTRACTOR PROPOSES TO USE A "FOOTER", THE "FOOTER" SHALL BE BELOW THE 9" EMBEDMENT.



PHASE 1
EXISTING MEDIAN BARRIER REMOVAL



PHASE 2
INTERMEDIATE PROPOSED TYPICAL SECTION
AT SINGLE SLOPE BARRIER



PHASE 3
FINAL PROPOSED TYPICAL SECTION
AT SINGLE SLOPE BARRIER

ITEM	DESCRIPTION	UNIT
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY
304.12	SUBBASE COURSE, TYPE 2	CY
404.0972	9.5 F2 TOP COURSE ASPHALT, 70 SERIES COMPACTION	TON
404.1979	19 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION	TON
404.3779	37.5 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF
606.74	BOX BEAM MEDIAN BARRIER	LF
606.3041	SINGLE-SLOPE CONCRETE MEDIAN BARRIER (OPTIONAL)	LF
606.74	REMOVING AND DISPOSING BOX BEAM MEDIAN BARRIER	LF
606.74	TRANSITION BETWEEN BOX BEAM MEDIAN BARRIER AND SINGLE SLOPE CONCRETE M.	EACH
627.50140008	CUTTING PAVEMENT	LF

ALTERED ON:

AFFIXED ON: 08/22/2025

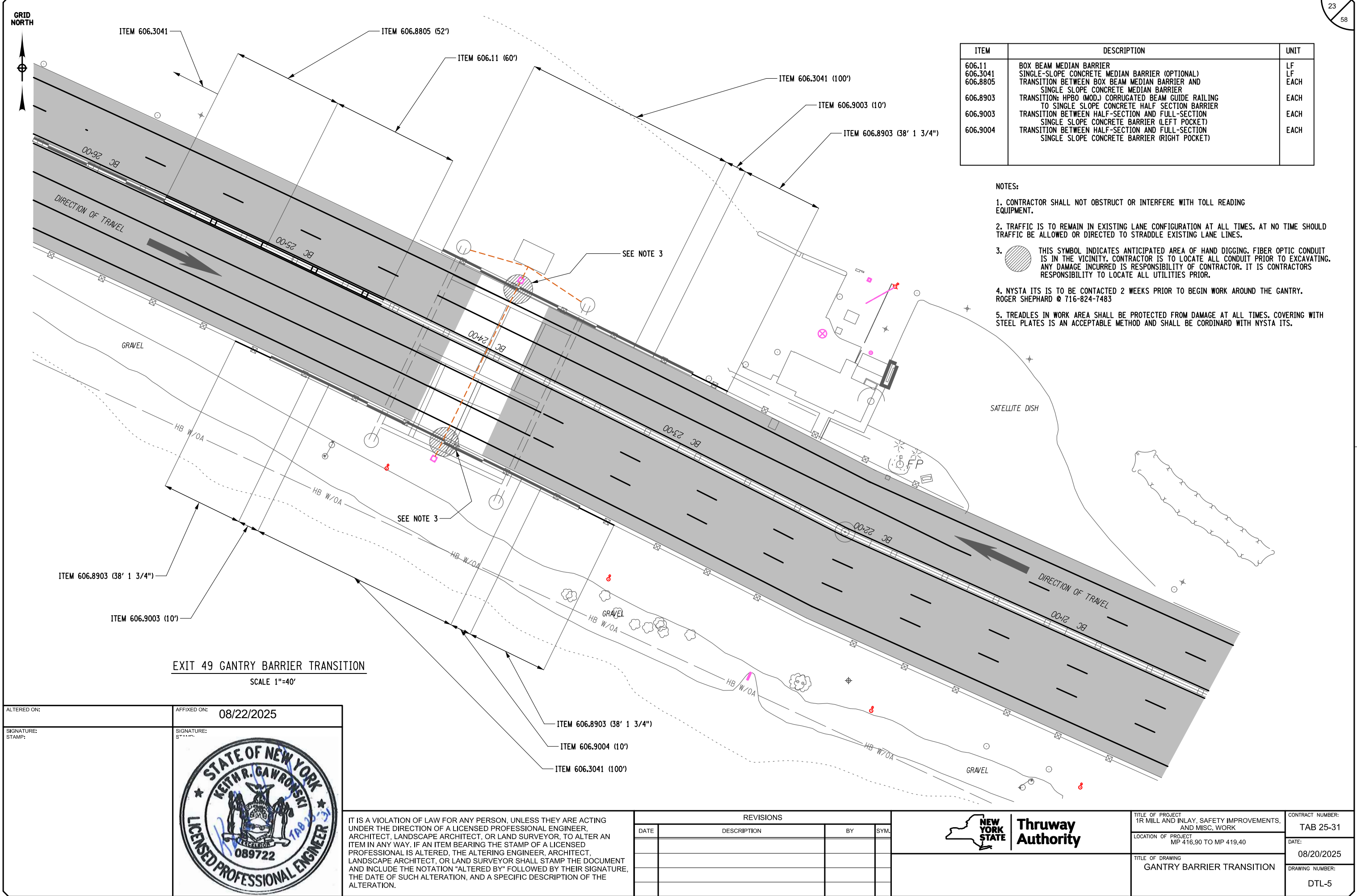
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1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK
LOCATION OF PROJECT
MP 416.90 TO MP 419.40
TITLE OF DRAWING
MEDIAN BARRIER TRANSITION

CONTRACT NUMBER:
TAB 25-31
DATE:
08/20/2025
DRAWING NUMBER:
DTL-4



ITEM	DESCRIPTION	UNIT
606.11	BOX BEAM MEDIAN BARRIER	LF
606.3041	SINGLE-SLOPE CONCRETE MEDIAN BARRIER (OPTIONAL)	LF
606.8805	TRANSITION BETWEEN BOX BEAM MEDIAN BARRIER AND SINGLE SLOPE CONCRETE MEDIAN BARRIER	EACH
606.8903	TRANSITION: HPBO (MOD.) CORRUGATED BEAM GUIDE RAILING TO SINGLE SLOPE CONCRETE HALF SECTION BARRIER	EACH
606.9003	TRANSITION BETWEEN HALF-SECTION AND FULL-SECTION SINGLE SLOPE CONCRETE BARRIER (LEFT POCKET)	EACH
606.9004	TRANSITION BETWEEN HALF-SECTION AND FULL-SECTION SINGLE SLOPE CONCRETE BARRIER (RIGHT POCKET)	EACH


- NOTES:
- CONTRACTOR SHALL NOT OBSTRUCT OR INTERFERE WITH TOLL READING EQUIPMENT.
 - TRAFFIC IS TO REMAIN IN EXISTING LANE CONFIGURATION AT ALL TIMES. AT NO TIME SHOULD TRAFFIC BE ALLOWED OR DIRECTED TO STRADDLE EXISTING LANE LINES.
 -  THIS SYMBOL INDICATES ANTICIPATED AREA OF HAND DIGGING. FIBER OPTIC CONDUIT IS IN THE VICINITY. CONTRACTOR IS TO LOCATE ALL CONDUIT PRIOR TO EXCAVATING. ANY DAMAGE INCURRED IS RESPONSIBILITY OF CONTRACTOR. IT IS CONTRACTORS RESPONSIBILITY TO LOCATE ALL UTILITIES PRIOR.
 - NYSTA ITS IS TO BE CONTACTED 2 WEEKS PRIOR TO BEGIN WORK AROUND THE GANTRY. ROGER SHEPHARD @ 716-824-7483
 - TREADLES IN WORK AREA SHALL BE PROTECTED FROM DAMAGE AT ALL TIMES. COVERING WITH STEEL PLATES IS AN ACCEPTABLE METHOD AND SHALL BE CORDINARD WITH NYSTA ITS.

EXIT 49 GANTRY BARRIER TRANSITION
SCALE 1"=40'

ALTERED ON:


SIGNATURE:
STAMP:

AFFIXED ON: 08/22/2025



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1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK

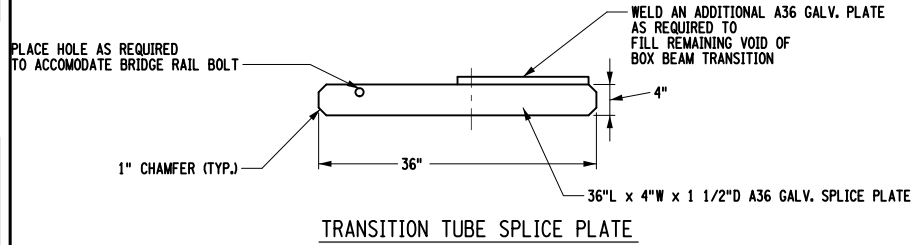
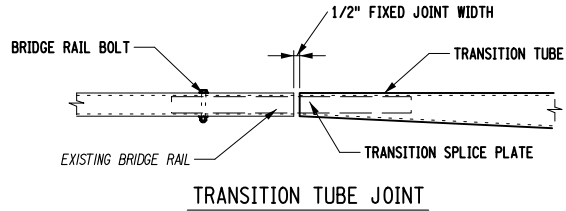
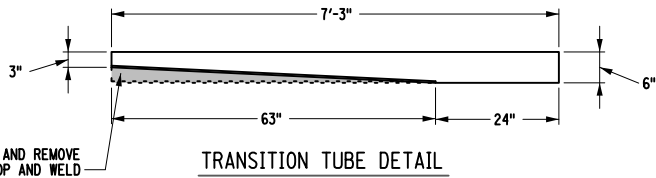
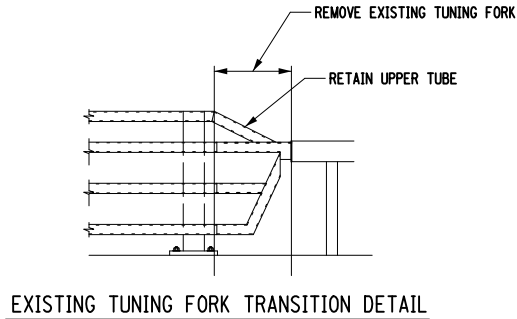
LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
GANTRY BARRIER TRANSITION

CONTRACT NUMBER:
TAB 25-31

DATE:
08/20/2025

DRAWING NUMBER:
DTL-5

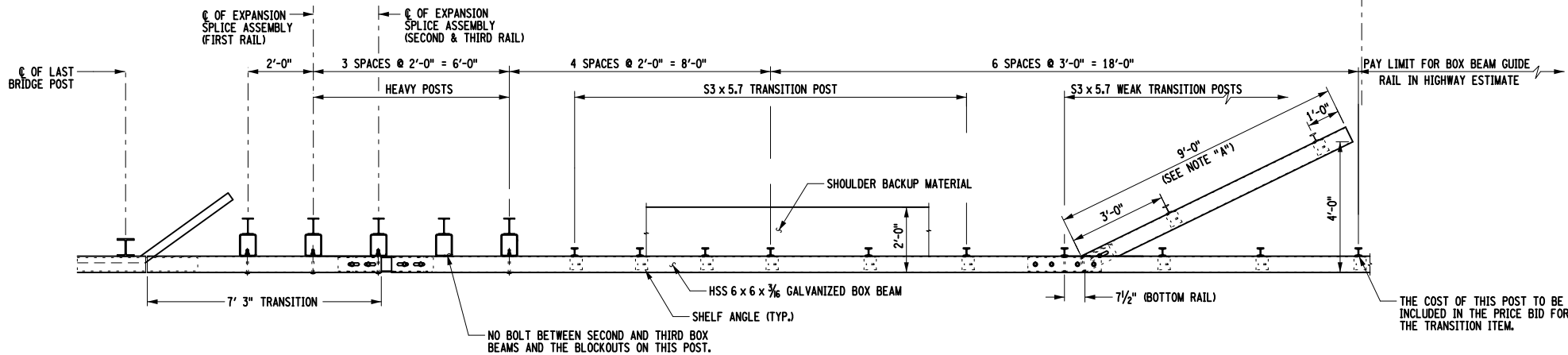
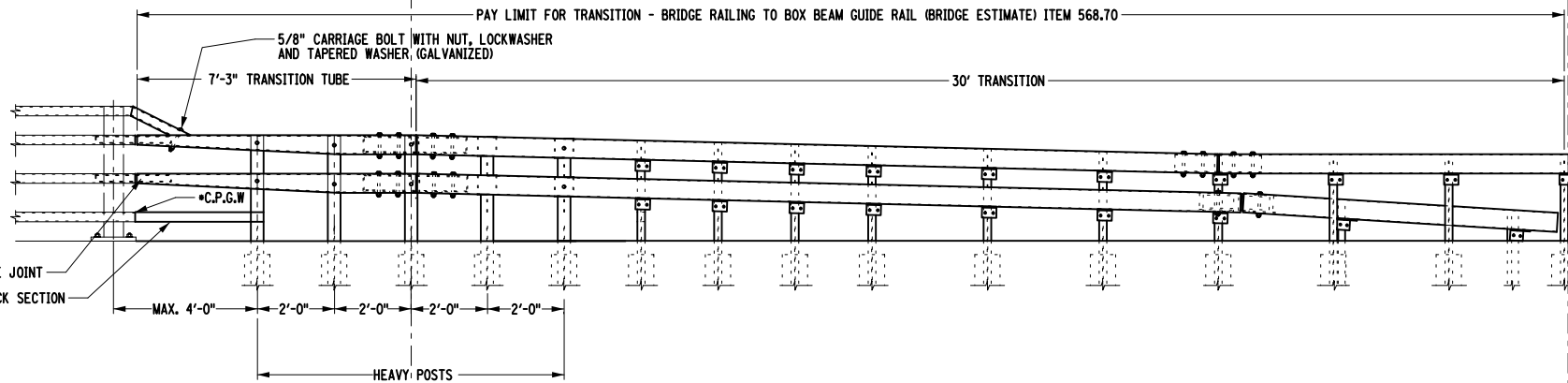
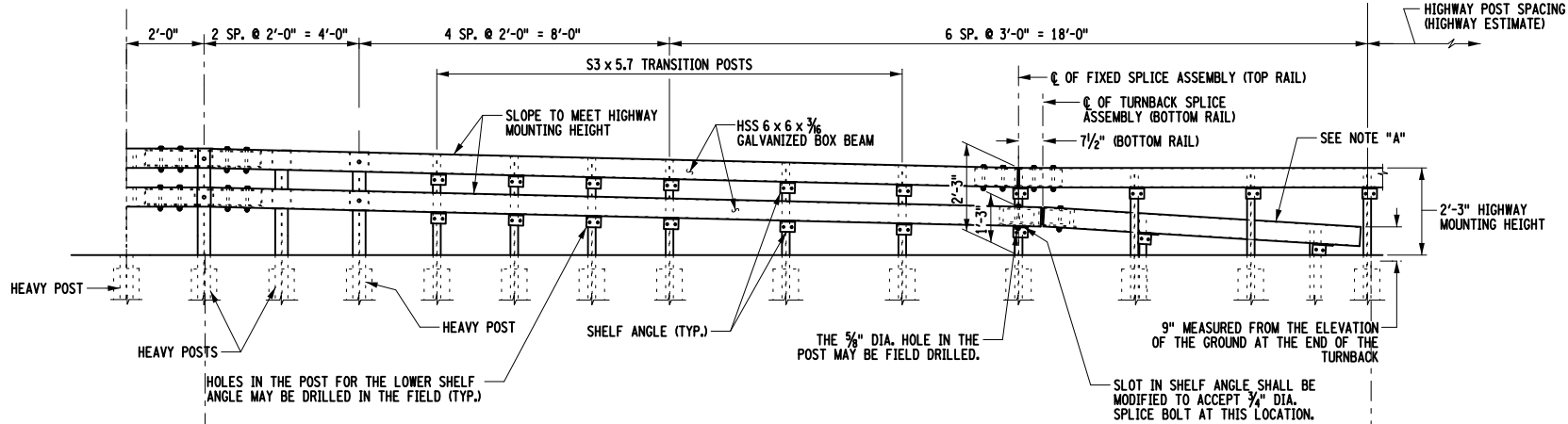


NOTES:

NOTE "A":
THE COST OF THE POSTS, SPLICE TUBE AND RAIL FOR THE TURNBACK IS INCLUDED IN THE PRICE BID FOR THE TRANSITION ITEM.

* C.P.G.W. = COMPLETE PENETRATION GROOVE WELD.

ANY MODIFICATIONS MADE IN THE FIELD THAT DAMAGES GALVANIZATION SHALL BE REPAIRED IN ACCORDANCE WITH SUBSECTION 719-01 OF THE NYS STANDARD SPECIFICATIONS.



ALTERED ON:

AFFIXED ON:

08/22/2025

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REVISIONS

DATE	DESCRIPTION	BY	SYM.



Thruway
Authority

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK

LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
BRIDGE RAIL TRANSITION

CONTRACT NUMBER:

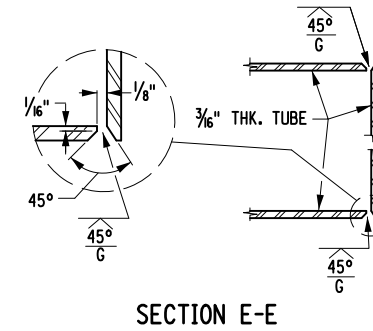
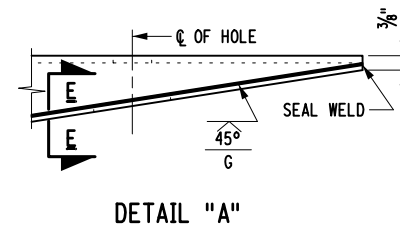
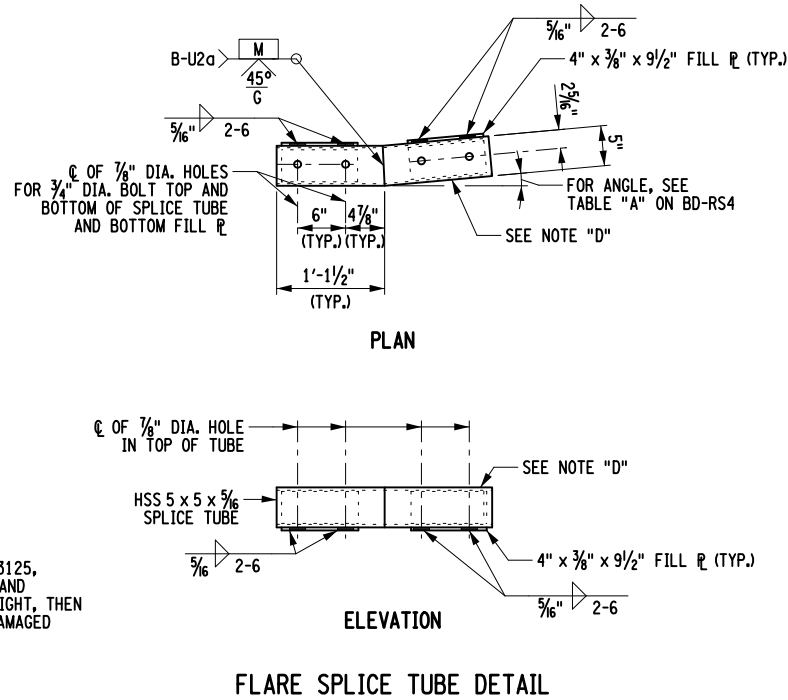
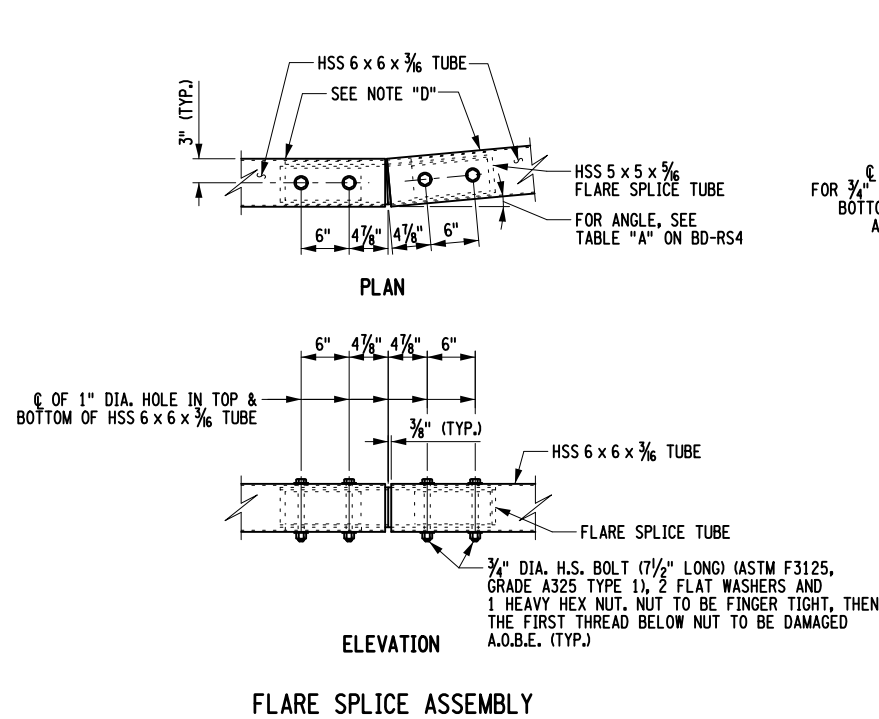
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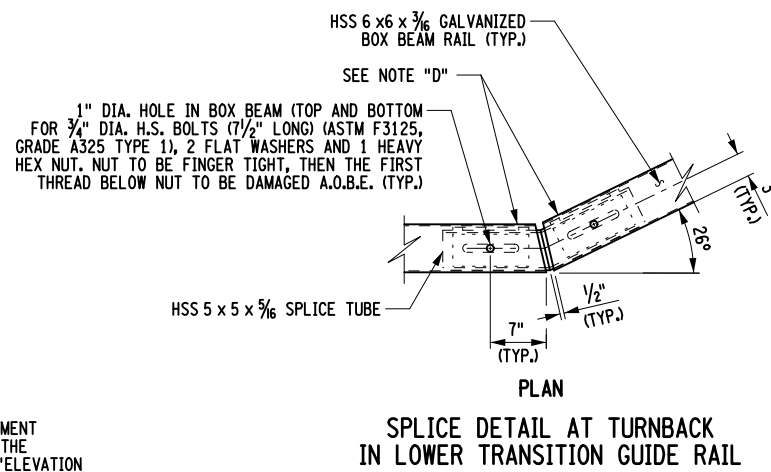
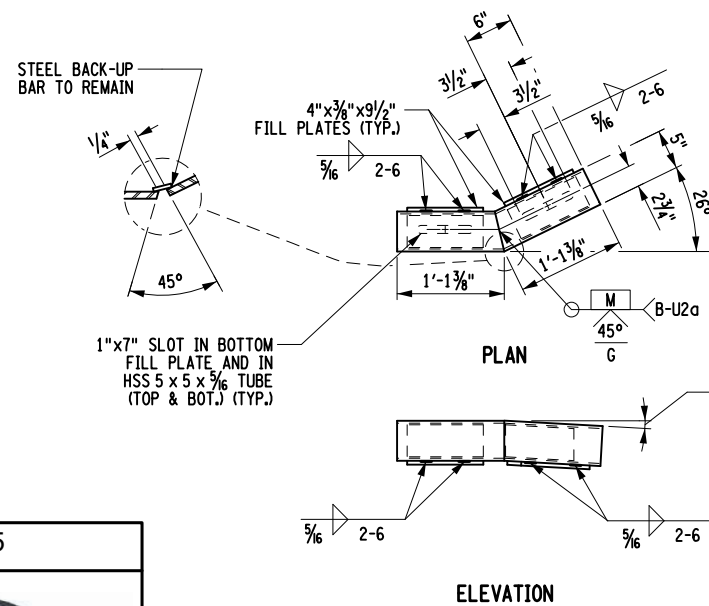
08/20/2025

DRAWING NUMBER:

DTL-6



TRANSITION TUBE WELD DETAILS



NOTES:
1. NOTE "D"
PROTRUSIONS CAUSED BY WELDING OR GALVANIZING ARE NOT PERMITTED
ON THE ADJOINING SURFACES OF THE BOX BEAM RAILS, SPLICE TUBES
AND FILL PLATES.

ALTERED ON:

AFFIXED ON: 08/22/2025

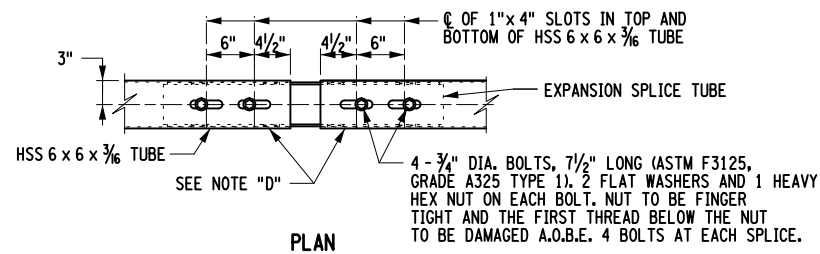
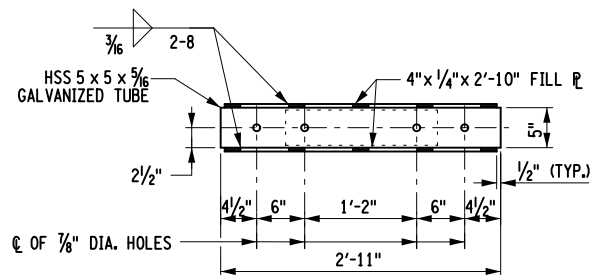
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

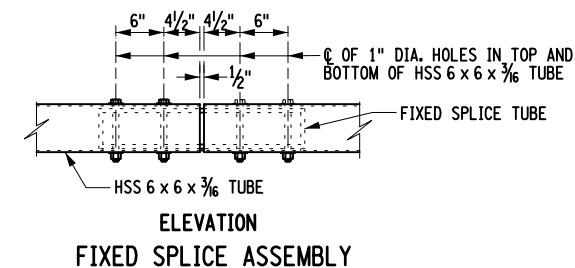
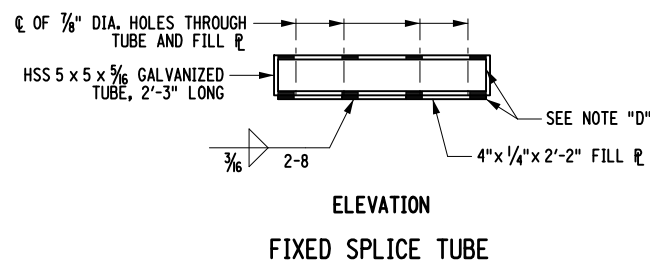
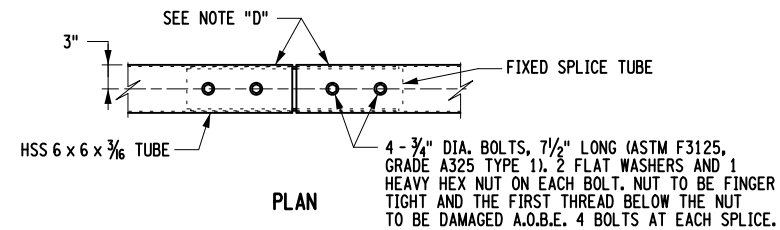
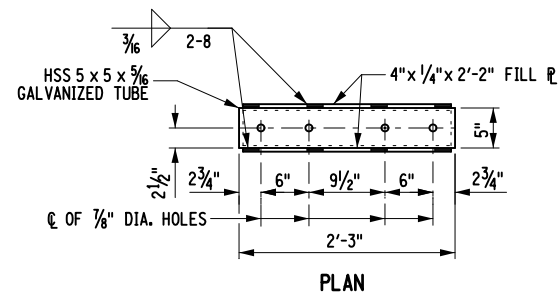
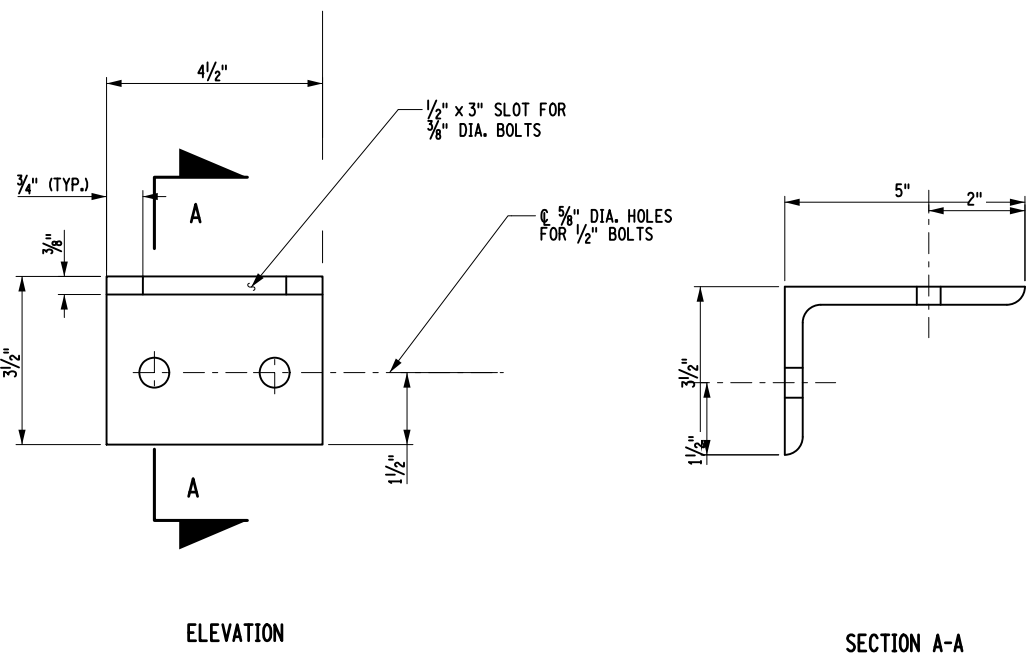
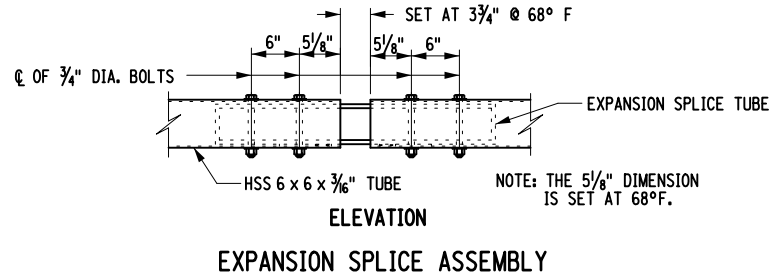
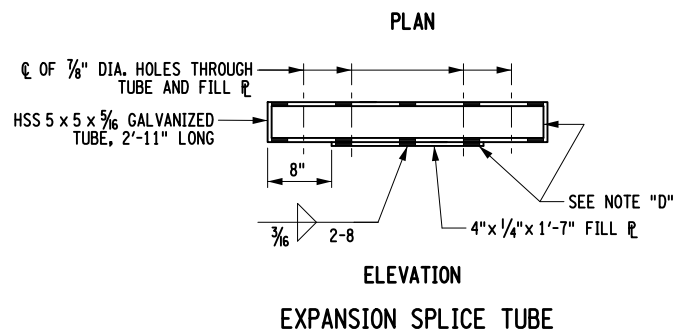
TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK
LOCATION OF PROJECT
MP 416.90 TO MP 419.40
TITLE OF DRAWING
BRIDGE RAIL TRANSITION

CONTRACT NUMBER:
TAB 25-31
DATE:
08/20/2025
DRAWING NUMBER:
DTL-7




NOTES:

1. NOTES "D" PROTRUSIONS CAUSED BY WELDING OR GALVANIZING ARE NOT PERMITTED ON THE ADJOINING SURFACES OF THE BOX BEAM RAILS, SPLICE TUBES AND FILL PLATES.



SHELF ANGLE DETAILS

SECTION A-A

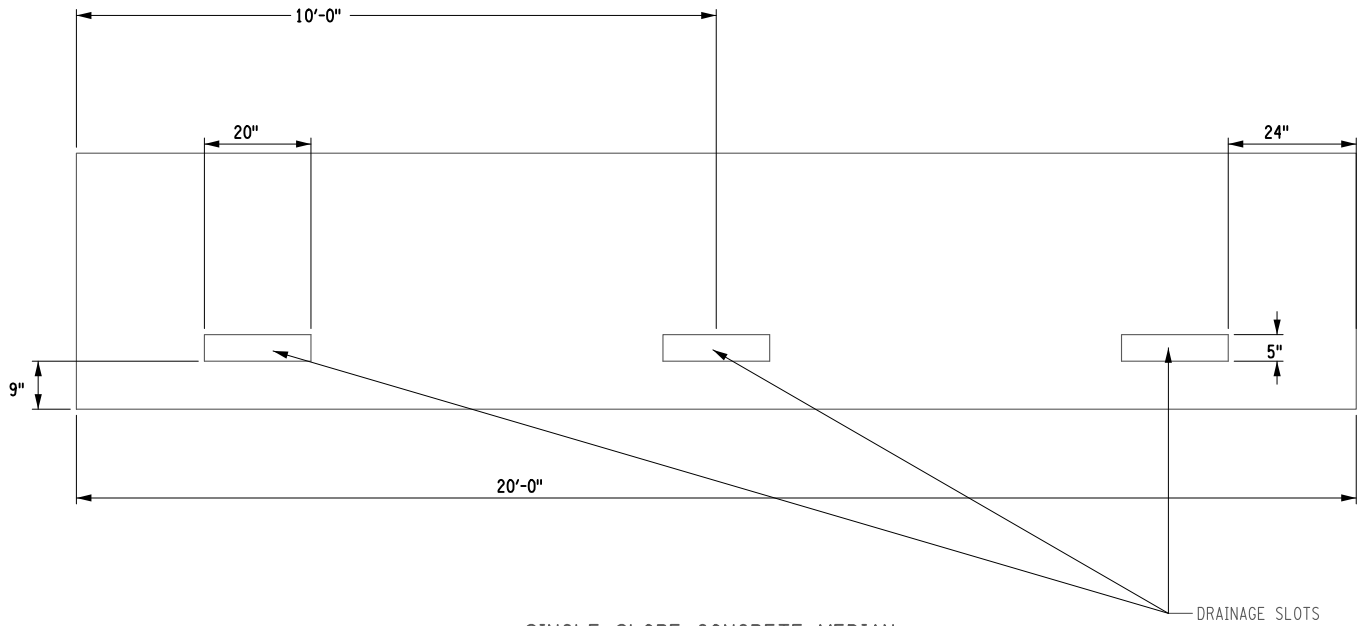
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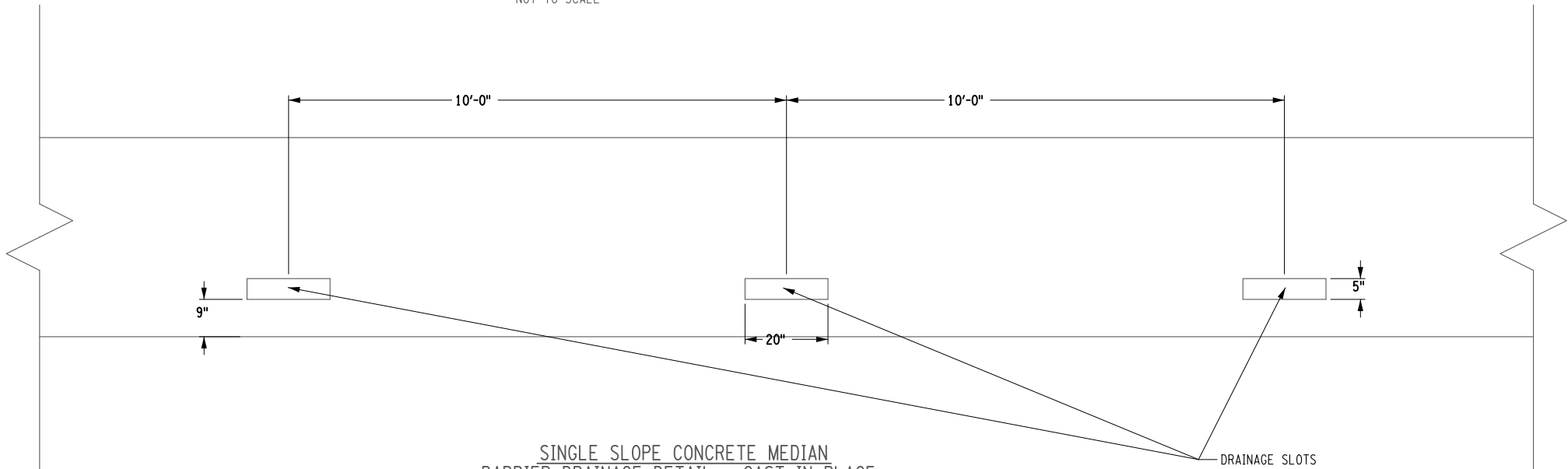
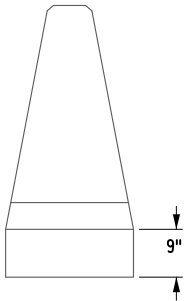
REVISIONS			
DATE	DESCRIPTION	BY	SYM.

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Authority**

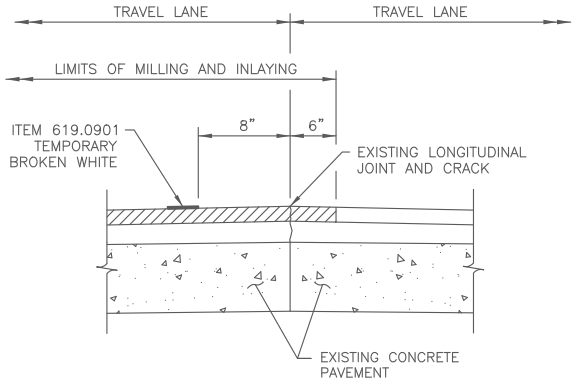
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LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING BRIDGE RAIL TRANSITION	DRAWING NUMBER: DTL-8



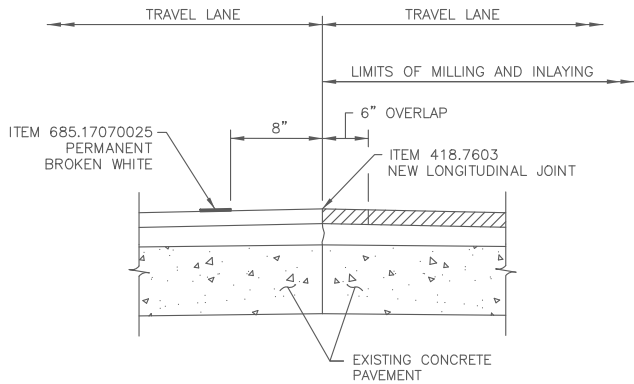
SINGLE SLOPE CONCRETE MEDIAN
BARRIER DRAINAGE DETAIL - PRECAST
NOT TO SCALE



SINGLE SLOPE CONCRETE MEDIAN
BARRIER DRAINAGE DETAIL - CAST IN PLACE
NOT TO SCALE



LONGITUDINAL PAVEMENT JOINT - FIRST PASS
N.T.S.



LONGITUDINAL PAVEMENT JOINT - SECOND PASS
N.T.S.

ALTERED ON:	AFFIXED ON: 09/17/2025
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NOTES:
1. DRAINAGE SLOTS IN NEW SINGLE-SLOPE CONCRETE MEDIAN BARRIER SHALL BE INCLUDED UNDER PAY ITEM 606.3041 - SINGLE SLOPE CONCRETE MEDIAN BARRIER (OPTIONAL).

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



Thruway
Authority

TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING MEDIAN BARRIER DRAINAGE AND LONGITUDINAL JOINTS	DRAWING NUMBER: DTL-9

GENERAL WORK ZONE AND TRAFFIC CONTROL NOTES

1. WORKZONE TRAFFIC CONTROL SHALL BE PROVIDED THROUGHOUT THE LENGTH OF THE CONTRACT IN ACCORDANCE WITH THE PROVISIONS OF SECTION 619 - WORK ZONE TRAFFIC CONTROL - OF THE NYSDOT STANDARD SPECIFICATIONS. THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND ANY PROVISIONS CONTAINED IN THE CONTRACT DOCUMENTS, AND AS ORDERED BY THE ENGINEER (A.O.B.E). THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (NATIONAL MUTCD) SHALL BE DEFINED AS THE NATIONAL MUTCD, JULY 2022 EDITION WITH SUPPLEMENTS.
2. THE CONTRACT DOCUMENTS AND THE TRAFFIC MAINTENANCE SCHEMES SHOWN IN SECTION 6 OF THE NATIONAL MUTCD ARE CONSIDERED TO BE MINIMUM REQUIREMENTS. ADDITIONAL SIGNS, DELINEATION, AND/OR TRAFFIC CONTROL DEVICES MAY BE REQUIRED AS DETERMINED BY THE ENGINEER. THE COST OF ANY ADDITIONAL DEVICES SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - WORK ZONE TRAFFIC CONTROL.
3. THE CONTRACT DOCUMENTS ARE NOT INTENDED TO BE ALL INCLUSIVE, BUT RATHER SERVE AS A GUIDE FOR THE SAFE & EFFICIENT SEQUENCING OF CONSTRUCTION OPERATIONS. THE TRAFFIC CONTROL PLANS PROVIDE FOR THE BASIC REQUIREMENTS & RESTRICTIONS OF THIS CONTRACT. ANY INCIDENTAL ADDITIONS & REVISIONS TO THESE PLANS, AS DIRECTED BY THE ENGINEER, SHALL BE INCLUDED IN THE PRIDE BID FOR ITEM 619.01 - WORK ZONE TRAFFIC CONTROL.
4. PRIOR TO THE START OF WORK, THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, ANY PROPOSED CHANGES TO THE TRAFFIC CONTROL PLANS FOR APPROVAL. ANY CHANGES WHICH ALTER THE BASIC CONCEPT OF THESE PLANS MUST BE APPROVED BY THE ENGINEER.
5. IF THE ENGINEER NOTIFIES THE CONTRACTOR OF ANY HAZARDOUS CONSTRUCTION PRACTICES AND/OR CONDITIONS, ALL OPERATIONS IN THAT AREA SHALL BE DISCONTINUED AND IMMEDIATE REMEDIAL ACTION SHALL BE TAKEN TO CORRECT THE SITUATION, TO THE SATISFACTION OF THE ENGINEER, BEFORE WORK IS RESUMED.
6. ON LONG LANE CLOSURES, ADDITIONAL DEVICES (DRUMS, OVERSIZED VEHICLES PANELS, ETC.) SHALL BE PLACED PERPENDICULAR TO TRAFFIC IN THE CLOSED LANE, EVERY 800 FEET. IN ADDITION, ADDITIONAL DEVICES SHALL BE SIMILARLY PLACED IN ADVANCE OF ANY TRAVERSE DROP-OFF OF 1 1/2 INCHES OR GREATER.
7. THE CONTRACTOR SHALL PLAN AND INCORPORATE ACCESS POINTS INTO THE WORK ZONE SUCH THAT, TO THE EXTENT PRACTICAL, THE CONTRACTOR'S VEHICLES ENTERING AND LEAVING THE WORK ZONE SHALL NOT IMPEDE THE MOVEMENT OF THROUGH TRAFFIC IN THE ADJACENT OPEN LANE(S).
8. CONSTRUCTION EQUIPMENT AND MATERIALS SHALL BE REMOVED FROM THE CLEAR ROADSIDE AREA DURING THE HOURS THAT THE CONTRACTOR IS NOT WORKING. THIS REQUIREMENT SHALL NOT BE LIMITED TO THE CONTRACT LIMITS.
9. ADDITIONAL WORK ZONE TRAFFIC CONTROL NOTES ARE INCLUDED ELSEWHERE IN THE TRAFFIC CONTROL PLANS, OTHER CONTRACT DRAWINGS AND THE NYSTA CURRENT STANDARD SHEETS.
10. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THESE HIGHWAYS WILL BE DESIGNATED AS NON-RESTRICTED HIGHWAYS UNDER THIS CONTRACT.
11. WHEN TWO OR MORE WORK AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL ENSURE THERE IS NO CONFLICT IN SIGNING AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.

DELINEATION AND CHANNELIZATION

1. THE CONTRACTOR SHALL DELINEATE AREAS WHERE THERE IS A DROP-OFF NEAR OF THE EDGE OF THE TRAVELED WAY AND AREAS ON WHICH IT IS UNSAFE TO TRAVEL. THE PROVISIONS FOR DELINEATION SHALL BE IN ACCORDANCE WITH NYSDOT STANDARD SPECIFICATIONS 619-3.02 AND NYSTA STANDARD SHEETS ON SS-1 OF THE PLANS. THESE SECTIONS INDICATE THE MINIMUM DELINEATION REQUIREMENTS. ADDITIONAL DELINEATION MAY BE REQUIRED AS DIRECTED BY THE ENGINEER OR AS SHOWN ON THE PLANS. MUTCD COMPLIANT CHANNELIZATION DEVICES MEETING 619 SPECIFICATIONS SHALL BE USED AT HAZARDOUS LOCATIONS DETERMINED BY THE ENGINEER. DELINEATION DEVICES SHALL REMAIN IN PLACE UNTIL SATISFACTORY PROTECTION IS PROVIDED OR THE HAZARD REMOVED. THEY SHALL BE PLACED AT A DISTANCE NOT TO EXCEED 40 FEET OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR MAY ELECT, AT NO ADDITIONAL COST TO THE AUTHORITY, TO INSTALL TEMPORARY POSITIVE BARRIER TO ELIMINATE DROP-OFFS OR HAZARDS. THE COST OF ALL DELINEATION AND DROP-OFF PROTECTION SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL.
2. PAVEMENT MARKINGS PLACED DURING CONSTRUCTION SHALL BE APPLIED PER SECTION 619 OF THE NYSDOT STANDARD SPECIFICATIONS AND SHALL CONFORM TO THE PROJECT'S PERMANENT MARKINGS WITH RESPECT TO COLOR AND PATTERN. PAYMENT WILL BE MADE UNDER ITEM 619.0901 - TEMPORARY PAVEMENT MARKINGS STRIPES.
3. THE CONTRACTOR IS RESPONSIBLE FOR MONITORING THE WORK ZONE. ALL DISLODGED DELINEATION DEVICES AND/OR OBSCURED PAVEMENT MARKINGS WILL BE RESET AND/OR RE-MARKED AS REQUIRED, OR A.O.B.E.
4. THE CONTRACTOR SHALL REMOVE EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH TEMPORARY PAVEMENT MARKINGS REQUIRED FOR CONSTRUCTION. THE COST TO REMOVE THE EXISTING PAVEMENT MARKINGS, UNLESS REMOVED DURING EXCAVATION, WILL BE INCLUDED IN THE PRICE BID FOR ITEMS 619.080101 AND 619.080102 - REMOVE PAVEMENT MARKING STRIPES (TRAFFIC PAINT) AND REMOVE PAVEMENT MARKING STRIPES (EPOXY PAINT).
5. THE CONTRACTOR SHALL NOT MIX DELINEATION DEVICES IN A LINEAR CLOSURE OR TAPER, (I.E., CONES, VERTICAL PANELS, CANDLES, OR DRUMS SHALL NOT BE USED IN THE SAME TAPER OR CLOSURE). HOWEVER, DIFFERENT WORK AREAS WITHIN THE PROJECT LIMITS MAY BE USE DIFFERENT DELINEATION DEVICES.
6. THE MAXIMUM LENGTH OF ANY CONTINUOUS WORK SPACE SHALL NOT EXCEED 2 MILES EXCEPT UNDER FOUR-LANE, TWO-WAY OPERATION (FLTWO). ALL TRAFFIC SHALL BE RE-ESTABLISHED TO ITS NORMAL LANE CONFIGURATION FOR A MINIMUM 2 MILES PRIOR TO A SUCCESSFUL LANE CLOSURE. (THE SEPARATION BETWEEN SUCCESSIVE LANE CLOSURES IS MEASURED FROM THE LAST SIGN OF THE FIRST LANE CLOSURE TO THE BEGINNING OF THE MERGING TAPER OF THE SECOND LANE CLOSURE).

CONSTRUCTION SIGNS

1. DIAMOND-SHAPED ADVANCE WARNING SIGNS SHALL BE USED FOR ALL ADVANCE WARNING SIGNS SHOWN IN THE NATIONAL MUTCD. COLOR REQUIREMENTS SHALL BE BLACK TEXT ON ORANGE BACKGROUND. COLOR REQUIREMENTS FOR REGULATORY & GUIDE SIGNS USED FOR CONSTRUCTION SIGNING SHALL BE AS SPECIFIED IN THE NATIONAL MUTCD FOR EACH SIGN, OR AS NOTED ON THE PLANS.
2. LONGITUDINAL PLACEMENT OF CONSTRUCTION SIGNS SHALL BE SHOWN ON THE TRAFFIC CONTROL PLANS AND AS DIRECTED BY THE ENGINEER. THE LATERAL CLEARANCE FROM THE EDGE OF THE TRAVEL LANE TO THE EDGE OF THE CONSTRUCTION SIGN SHALL BE A MINIMUM OF 3 FEET, OR AS A.O.B.E.. SIGNS SHALL BE PLACED TO PROVIDE OPTIMUM VISIBILITY.
3. MOUNTING HEIGHT REQUIEMENTS FOR ALL LONG DURATION CONSTRUCTION SIGNS SHALL BE 7 FEET MINIMUM MEASURED FROM THE GROUND SURFACE TO THE BOTTOM OF THE SIGN. SHORT DURATION CONSTRUCTION SIGNS MAY BE MOUNTED AT A MINIMUM HEIGHT OF 5 FEET, OR A.O.B.E.
4. DURING THE PROGRESSION OF CONSTRUCTION, SOME EXISTING SIGNS MAY REQUIRE MINOR RELOCATION DUE TO TEMPORARY PAVEMENT WIDENING, SHIFT OF TRAFFIC LANES, OR CHANGE IN THE FLOW OF TRAFFIC. THE COST OF THESE RELOCATIONS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL.
5. ALL FLAGGING OPERATIONS SHALL USE 24" (MIN.) OCTAGONAL SHAPED STOP/SLOW PADDLES HAVING A 6' STAFF. FLAGGERS SHALL BLOCK ALL ENTRANCE LANES/RAMPS (INCLUDING ENTRANCES FROM SERVICE SREAS, PARKING AREAS, REST AREAS, TANDEM TRUCK LOTS, ETC.) WITHIN THE AREA OF THE TRAFFIC SLOWDOWN/STOP. AN ADVANCE WARNING SIGN, FLAGGER SYMBOL (W20-7) OR ROAD WORK AHEAD (W20-1), FOR EACH RAMP SHALL BE PLACED 300 TO 500 FEET IN ADVANCE OF THE FLAGGING STATION.
6. THE COST OF ALL TEMPORARY SIGNS NECESSARY FOR WORK ZONE TRAFFIC CONTROL AS NOTED IN THE PLANS, STANDARD SPECIFICATIONS, NATIONAL MUTCD, AND A.O.B.E, SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL.
7. THE CONTRACTOR WILL BE REQUIRED TO COVER AND UNCOVER EXISTING AND CONSTRUCTION SIGNS DURING THE PROJECT. THIS PROCESS WILL BE DIRECTED BY THE ENGINEER-IN-CHARGE AND SHALL BE INCLUDED IN ITEM 619.01 AT NO ADDITIONAL COST.
8. THE CONTRACTOR SHALL REMOVE, STORE, AND RE-SET EXISTING SIGNS WHICH CONFLICT WITH CONSTRUCTION SIGNS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE EXISTING SIGNS DUE TO THE REMOVAL, STORAGE, AND RE-SETTING OF THE SIGNS.
9. THE DIMENSIONS OF THE WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCD. ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY THE EIC.

CONSTRUCTION ACTIVITIES

1. UNLESS OTHERWISE SHOWN OR APPROVED BY THE ENGINEER, WORK AREAS ON OPPOSITE SIDES OF THE ROAD SHALL NOT OVERLAP OR CO-EXIST.
2. NO MATERIAL IS TO BE STORED WITHIN THE CLEAR ROADSIDE AREA OTHER THAN THAT MATERIAL WHICH WILL BE PLACED THAT DAY, WITHOUT THE APPROVAL OF THE ENGINEER. THE CLEAR ROADSIDE AREA IS DEFINED AS THE AREA WITHIN 30 FEET OF THE EDGE OF TRAVEL LANE.
3. CONSTRUCTION EQUIPEMENT SHALL BE REMOVED FROM THE CLEAR ROASIDE AREA DURING HOURS THAT THE CONTRACTOR IS NOT WORKING. THIS REQUIREMENT SHALL NOT BE LIMITED TO THE CONTRACT LIMITS.
4. NO PRIVATE VEHICLES (OWNED BY THE CONTRACTOR'S EMPLOYEES) SHALL BE PARKED ON THRUWAY PROPERTY. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL MARSHALL AT AN OFF-SITE YARD AND TRAVEL TO THE WORK SITE IN CONTRACTOR OWNED VEHICLES.
5. ALL OPERATIONS, INCLUDING PAVING, ROLLING, GRADING, SUPPLYING OF MATERIAL, AND THE TRAVEL OF SUPERVISORY PERSONNEL THROUGH THE WORK ZONE, AND THE ENTIRE LENGTH OF THE CONTRACT, WILL ALWAYS BE IN THE DIRECTION OF NORMAL TRAFFIC EXCEPT WHERE SPECIFICALLY ALLOWED OTHERWISE BY THE ENGINEER TO PERFORM A PARTICULAR OPERATION.
6. ALL UNEVEN TRAVERSE PAVEMENT SURFACES (I.E. PAVEMENT REBATES, END OF PAVED SURFACES) SHALL BE POSTED WITH "BUMP" SIGNS (W8-1) AND EITHER A TYPE 1 OBJECT MARKER (OM1-3) OR A CONSTRUCTION DRUM WITH A FLASHING WARNING LIGHT ON BOTH SIDES OF THE ROADWAY, TO BE LOCATED A.O.B.E COST TO BE INCLUDED IN ITEM 619.01.

NIGHTTIME OPERATIONS NOTES

1. WORK ZONE TRAFFIC CONTROL FOR NIGHTTIME CONSTRUCTION SHALL BE CONDUCTED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.
2. TO THE EXTENT PROJECT AND TRAFFIC CONDITIONS PERMIT, THE CONTRACTOR SHALL TAKE ADVANTAGE OF DAYLIGHT HOURS BEFORE AND AFTER THE SHIFT FOR SETUP AND REMOVAL OF TRAFFIC CONTROLS, AS WELL AS OTHER PREPARATIONS AND CLEANUP WORK.
3. ALL ARROW BOARDS USED AT NIGHT SHALL BE PROPERLY DIMMED AS DESCRIBED BY THE MUTCD AND THE STANDARD SPECIFICATIONS TO AVOID GLARE FOR APPROACHING MOTORISTS.
4. NIGHTTIME OPERATIONS, INCLUDING LANE CLOSURES OR OBSTRUCTIONS, WILL BE PERMITTED AS NOTED IN THE "SCHEDULE AND SUSPENSION OF WORK" LOCATED IN THE PROPOSAL.
5. NIGHTTIME LANE CLOSURES SHALL BE LOCATED AWAY FROM OTHER CONFLICT POINTS SUCH AS RAMPS WHENEVER POSSIBLE.
6. THE CONTRACTOR SHALL PROVIDE LIGHTING FOR ALL NIGHTTIME OPERATIONS IN ACCORDANCE WITH ITEM 619.24.
7. THE CONTRACTOR IS ADVISED THAT ILLUMINATIONS IS TO BE PROVIDED THROUGHOUT THE ENTIRE AREA OF CONSTRUCTION OPERATIONS, WHICH INCLUDES ALL WORK AREAS OCCUPIED BY THE CONTRACTOR'S PERSONNEL (TRUCK STAGING AREAS, CLEAN OUT AREAS, LAYOUT AND MEASUREMENT AHEAD OF ACTUAL WORK, ETC.). ALL SUCH WORK AREAS ARE TO BE SHOWN ON THE LIGHTING PLAN SUBMITTED TO THE ENGINEER.

CONES, DRUMS, BARRICADES, AND MARKERS

1. ALL CHANNELIZATION/DELINEATION DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM OF ONE FOOT CLEARANCE TO THE TRAVELED WAY UNLESS OTHERWISE SHOWN ON THE PLANS. THE CONTRACTOR SHALL MAKE CERTAIN THAT PLACEMENT OF CONES, DRUMS AND MARKERS OR BARRICADES SHALL NOT INTERFERE WITH SIGHT DISTANCE.
2. LONGITUDINAL SPACING SHALL BE 40 FT. OTHER SPACING MAY BE REQUIRED AS SHOWN ON THE PLANS OR A.O.B.E.
3. WHERE POSSIBLE A LATERAL BUFFER SPACE OF ONE FOOT SHOULD BE PROVIDED BETWEEN THE WORK ZONE AND THE LINE OF DELINEATION DEVICES.
4. THE COST OF ANY DELINEATION AND GUIDING DEVICES (CONES, DRUMS, VERTICAL PANELS, ETC.) SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL.
5. DRUMS ARE THE PREFERRED CHANNELIZING DEVICES TO BE USED DURING THE HOURS OF DARKNESS. DRUMS MUST BE USED ON TAPERS BOTH DAY AND NIGHT.

LOCAL ROADS - WORK ZONE TRAFFIC CONTROL


1. WHEN THE VISIBILITY OF THE TRAVELING PUBLIC IS RESTRICTED DUE TO WEATHER CONDITIONS, AND/OR THE WORK OPERATIONS COMMENCE PRIOR TO DAWN OR CONTINUE BEYOND DUSK, FLASHING BEACONS (TYPE 'A' LIGHTS) SHALL BE PROVIDED.
2. THE CONTRACTOR SHALL COORDINATE WITH NYSDOT REGION 5, PRIOR TO THE START OF ANY CONSTRUCTION THAT MAY IMPACT TRANSIT ROAD (NY ROUTE 78).


LANE CLOSURES

1. LANE CLOSURES FOR ANY CONTRUCTION OPERATIONS MUST BE CONDUCTED IN ACCORDANCE WITH THE 'LANE CLOSURE CHARTS' ON NYSTA STANDARD SHEET TA 619-36.
2. THE CONTRACTOR SHALL PROVIDE FLAGGERS A.O.B.E. TO CONTROL TRAFFIC MOVEMENT ALONG RAMP WORK OPERATIONS REQUIRING TEMPORARY SHIFTING OF TRAFFIC FLOW ON PORTIONS OF THE TRAVEL WAY, FLAGGING IS NOT PERMITTED ON THRUWAY MAINLINE.
3. NIGHTLY INTERCHANGE RAMP CLOSURES WILL BE ALLOWED ON A LIMITED BASIS FOR CONCRETE BARRIER WORK AND PAVINGOPERATIONS AS APPROVED BY BOTH THE PROJECT ENGINEER AND DIVISION TRAFFIC SUPERVISION. THE CONTRACTOR SHALL SUBMIT TO THE PROJECT ENGINEER FOR APPROVAL THE PROPOSED INTERCHANGE RAMP CLOSURE DETAILS, INCLUDING DETOUR PLAN, 21 DAYS PRIOR TO THE DESIRED CLOSURE. COST ASSOCIATED WITH PROPOSED INTERCHANGE RAMP CLOSURE(S) SHALL BE INCLUDED IN THE UNIT PRICE FOR WORK ZONE TRAFFIC CONTROL ITEMS.

PAVEMENT MARKINGS

1. THE COST OF REMOVAL OF EXISTING PAVEMENT MARKINGS, WHERE REQUIRED, SHALL BE INCLUDED IN THE PRICE BID FOR ITEMS 619.080101 AND 619.080102 - REMOVE PAVEMENT MARKING STRIPES (TRAFFIC PAINT) AND REMOVE PAVEMENT MARKING STRIPES (EPOXY PAINT).
2. THE CONTRACTOR SHALL PROVIDE PAVEMENT MARKINGS AT ALL TIMES ON ALL PAVEMENT, WHETHER EXISTING, TEMPORARY OR NEW, UNTIL PERMANENT MARKINGS ARE RENEWED OR RESTORED. THIS SHALL INCLUDE AT ALL APPROPRIATE LOCATIONS: EDGE LINES, LANE LINES (SOLID OR BROKEN), CHANNELIZING LINES, DOTTED LINES, PLUS ANY MARKINGS ORDERED BY THE ENGINEER. ALL MARKINGS SHALL BE APPLIED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, CONTRACT PLANS AND THE NEW YORK STATE MUTCD SUPPLEMENT AND SHALL INDICATE ACTUAL CONDITIONS AT ALL TIMES. THESE MARKINGS SHALL BE PAID FOR AS SHOWN ON THE CONTRACT PLANS.
3. TEMPORARY WORK ZONE PAVEMENT MARKINGS, SHALL BE USED AS FOLLOWS:
-MAINLINE - USE ITEM 619.0901
-RAMPS - USE ITEM 619.0901

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	DATE	DESCRIPTION	BY	SYM.			LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/25
							TITLE OF DRAWING TRAFFIC CONTROL NOTES	DRAWING NUMBER: TCN-1

PORTABLE VARIABLE MESSAGE DISPLAY UNITS

- THE CONTRACTOR SHALL PROVIDE UP TO 2 PORTABLE VARIABLE MESSAGE SIGNS (PVMS) WITH COMMUNICATION ABILITY (ITEM 619.110512) TO BE USED FOR THE DURATION OF THIS CONTRACT. PVMS ARE INTENDED TO SUPPLEMENT FIXED SIGNS AND ARE NOT INTENDED TO REPLACE THEM.
- TYPICAL APPLICATIONS FOR VARIABLE MESSAGE DISPLAY UNITS INCLUDE LOCATIONS WHERE:
 - TRAFFIC SPEED IS EXPECTED TO DROP SUBSTANTIALY.
 - SIGNIFICANT QUEUING OR DELAYS ARE EXPECTED.
 - CHANGES IN ALIGNMENT OR SURFACE CONDITIONS OCCUR.
 - LANES, RAMPS OR ROADWAYS ARE CLOSED.
 - SIGNIFICANT ACCIDENTS OR INCIDENTS HAVE OCCURED.
 - NIGHTTIME WORK IS PERFORMED. PERIOD OF ADVANCED NOTIFICATION TO BE DETERMINED BY THE ENGINEER.
- LOCATIONS:
 - VARIABLE MESSAGE SIGNS ARE TO BE PLACED BEYOND THE SHOULDER WHEREVER PRACTICAL.
 - VARIABLE MESSAGE SIGNS SHALL BE PLACED WHERE THEY ARE VISIBLE TO TRAFFIC FROM AT LEAST 2800 FEET AWAY BOTH DAY AND NIGHT OR APPROVED BY THE ENGINEER.
- PORTABLE VARIBLE MESSAGE SIGNS SHALL BE LOCATED AS DEEMED NECESSARY BY THE AUTHORITY'S PROJECT ENGINEER AND BY THE NYSTA DIVISION TRAFFIC SUPERVISOR. ALTHOUGH PAYMENT IS PER SIGN, THE CONTRACTOR MAY BE REQUIRED TO MOVE THE SIGNS AS OFTEN AS REQUESTED BY EITHER THE ENGINEER OR THE NYSTA DIVISION TRAFFIC SUPERVISOR. NO ADDITIONAL PAYMENT WILL BE MADE FOR SIGN MOVEMENTS.
- ALL MESSAGES DISPLAYED BY THE VARIABLE PORTABLE MESSAGE SIGNS SHALL BE APPROVED BY THE NYSTA DIVISION TRAFFIC SUPERVISIOR PRIOR TO USE.

EMERGENCY VEHICLE ACCESS

- THE CONTRACTOR MUST PROVIDE SAFE AND CONVENIENT EMERGENCY ACCESS AT ALL TIMES DURING CONSTRUCTION. PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE LOCAL FIRE, POLICE, AND AMBULANCE SERVICES TO COORDINATE THE PROPOSED CONSTRUCTION SCHEDULE, AND TO DEVELOP AN EMERGENCY ACCESS PLAN FOR ALL ROADS AND STREETS WITHIN THE PROJECT LIMITS.
- THE CONTRACTOR SHALL PROVIDE A WRITTEN LIST OF INDIVIDUALS RESPONSIBLE FOR EMERGENCY REPAIRS AND EMERGENCY COORDINATION. THIS "CALL OUT" LIST SHALL BE PROVIDED TO THE LOCAL FIRE, POLICE, AND AMBULANCE SERVICES, THE VILLAGE GOVERNMENT CENTER, AND THE ENGINEER. THIS LIST SHALL INCLUDE THE NAME, ADDRESS, AND TELEPHONE NUMBER(S) OF THE PERSONS AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS DURING NON-WORKING HOURS.
- IF EMERGENCY ACCESS IS REQUIRED THROUGH ANY CLOSED WORK AREA, THE CONTRACTOR SHALL BE RESPONSIBLE TO ALWAYS BE PREPARED TO ALLOW EMERGENCY VEHICLES THROUGH THE CLOSED AREA. THIS MAY REQUIRE TRENCHES TO BE COVERED WITH STEEL PLATES, EXCAVATION DROP OFFS TO BE BACKED-UP AND RAMPED, OR THE ACTUAL PROGRESSION OF WORK TO BE SEQUENCED. THE COST OF ANY ADDITIONAL WORK NECESSARY TO MEET THIS REQUIREMENT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - WORK ZONE TRAFFIC CONTROL.

DELINEATORS

- SEE NYSTA STANDARD SHEET TA 646-01 FOR DELINEATOR AND MILE MARKER DETAILS.
- EXISTING DELINEATORS AND MILE MARKERS DURING CONSTRUCTION:
 - RELOCATION AND BACK-TO-BACK DELINEATOR AND MILE MARKERS COSTS SHALL BE INCLUDED UNDER ITEM 619.01.
 - DELINEATORS AND MILE MARKERS SHALL BE CLEARLY VISIBLE AT ALL TIMES.
- REMOVE AND REPLACE EXISTING DELINEATORS AND MILE MARKERS AT COMPLETION OF PROJECT IN ACCORDANCE WITH NYSTA STANDARD SHEET TA 646-01. PAYMENT SHALL BE MADE UNDER DELINEATOR AND MILE MARKERS REMOVAL AND NEW DELINEATOR PAYMENT ITEMS.

'BE PREPARED TO STOP' SIGNS

- THE CONTRACTOR SHALL HAVE ON HAND "BE PREPARED TO STOP" SIGNS (W3-4) TO BE INSTALLED IN ADVANCE OF LANE CLOSURE SETUPS WHERE THE ENGINEER DETERMINES THEY ARE NECESSARY. THE SIGNS AS WELL AS ADDITIONAL W20-1 "ROAD WORK" SIGNS SHALL BE PLACED AND RELOCATED AS NECESSARY IN ACCORDANCE WITH SECTION 619-3.02H.5 OF THE STANDARD SPECIFICATIONS AND AS MODIFIED BY THE CONTRACT PROPOSAL. THE CONTRACTOR SHALL HAVE THESE SIGNS (48" X 48") TRUCK MOUNTED AND READY FOR AN INCIDENT ON SHORT NOTICE.

CONSTRUCTION INGRESS AND EGRESS


- VEHICLES, MATERIALS, AND/OR EQUIPMENT, INCLUDING OUT OF SERVICE SIGNS, SHALL NOT BE PARKED OR STORED WITHIN 30 FT. OF THE ROADWAY USED BY THE GENERAL PUBLIC OR ANY OTHER AREAS DEEMED HAZARDOUS BY THE EIC.
- VEHICLES PARKED BEHIND GUIDERAIL/BARRIER SHALL BE PARKED BEYOND DEFLECTION DISTANCE AND WITHIN THE POINT OF NEED COVERAGE.

BARRIER/SHADOW VEHICLES

- IN ADDITION TO THE REQUIREMENTS OF SECTION 619-2.02G AND 619-3.02G OF THE NYS STANDARD SPECIFICATIONS, AND UNLESS OTHERWISE APPROVED BY THE ENGINEER, THE CONTRACTOR SHALL PROVIDE BARRIER/SHADOW VEHICLES FOR ALL ACTIVE WORK AREAS OCCUPYING ANY TRAVEL LANE OR SHOULDER CLOSED TO TRAFFIC WITH CONES, DRUMS OR OTHER CHANNELIZING DEVICES. A SEPARATE BARRIER/SHADOW VEHICLE SHALL BE PROVIDED IN EACH CLOSED LANE AND EACH CLOSED SHOULDER 8 FT OR GREATER IN WIDTH.
- EXCEPTIONS TO THE REQUIREMENT MAY BE MADE WHERE, IN THE JUDGEMENT OF THE ENGINEER, THE BARRIER/SHADOW VEHICLE PLACEMENT WOULD BE INEFFECTIVE OR WOULD INTERFERE WITH THE SAFE OPERATION OF TRAFFIC AT OR NEAR RAMPS OR CONNECTORS.
- THE BARRIER/SHADOW VEHICLES SHALL TYPICALLY BE LOCATED AS INDICATED ON THE PLANS, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. IF THE WORK AREAS MOVE WITHIN A STATIONARY LANE OR SHOULDER CLOSURE THE BARRIER/SHADOW VEHICLE SHALL BE RE-POSITIONED ACCORDINGLY.
- PAYMENT FOR TYPICAL BARRIER/SHADOW VEHICLES SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL.
- THE ARROW PANEL SHALL OPERATE IN THE "CAUTION MODE" WHEN IN USE. A "4 CORNER FLASH" SHALL ONLY BE PERMITTED. A FLASHING BAR DISPLAY SHALL NOT BE PERMITTED FOR THE CAUTION MODE. USE "4 LAMPS FLASHING SIMULTANEOUSLY" OR "2 PAIRS OF LAMPS FLASHING IN A PATTERN". A DIRECTION WILL NOT BE INDICATED.
- ANY ROADWAY DEBRIS PICKUP BY CONTRACTOR PERSONNEL SHALL BE PROTECTED BY A SHADOW VEHICLE WITH IMPACT ATTENUATOR.


DRAINAGE

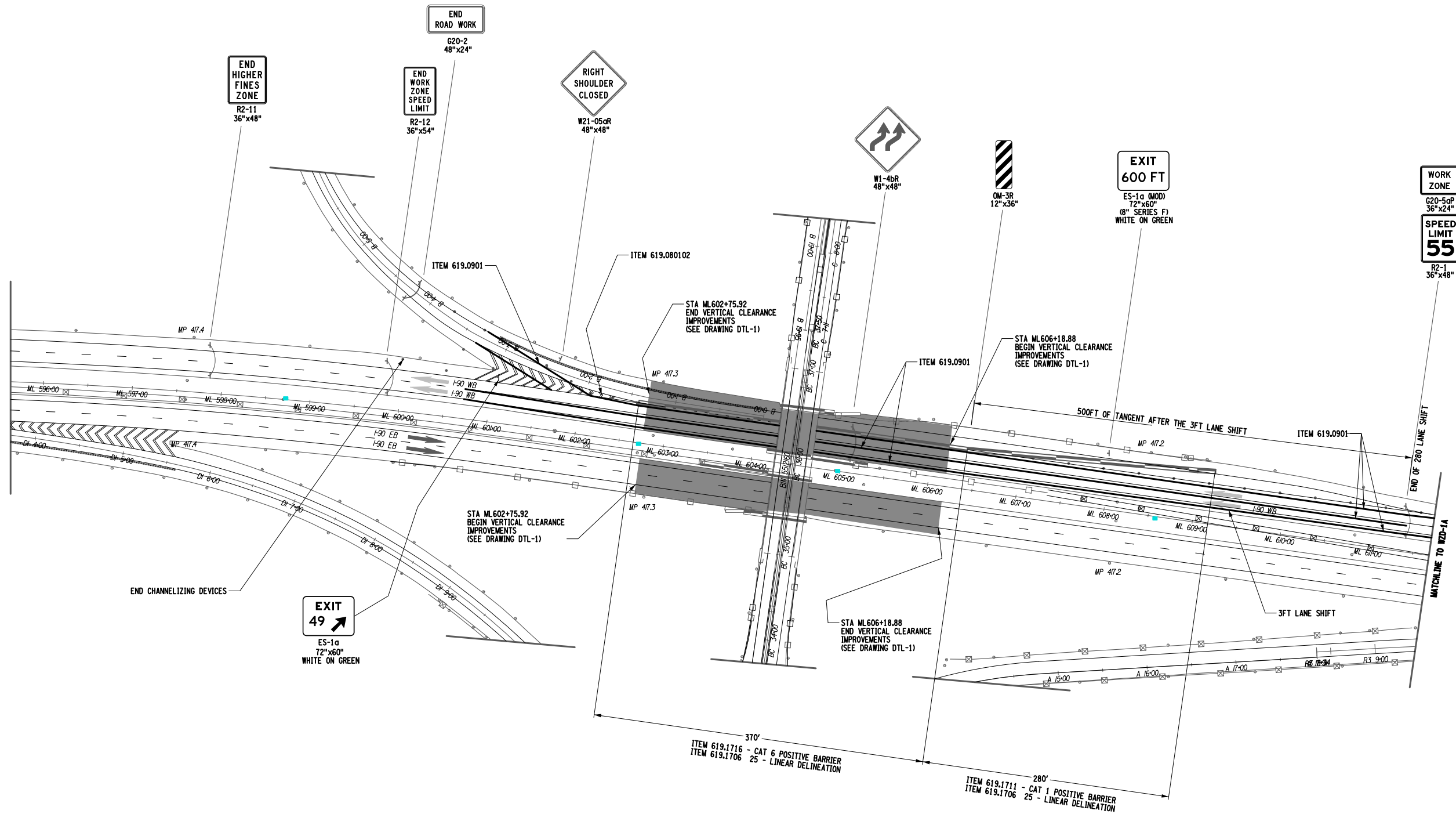
- THE CONTRACTOR MUST MAINTAIN EXISTING DRAINAGE AT ALL TIMES DURING CONSTRUCTION.

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 Thruway Authority	TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
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	TITLE OF DRAWING TRAFFIC CONTROL NOTES	DRAWING NUMBER: TCN-2



WORK ZONE DETAIL FOR VERTICAL CLEARANCE IMPROVEMENTS AT MP 417.27
NOT TO SCALE

ALTERED ON:	AFFIXED ON: 09/16/2025
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 - VERTICAL CLEARANCE IMPROVEMENT AREA

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ITEM	DESCRIPTION	UNIT
490.17011018	PRODUCTION COLD MICROMILLING	SF
619.1706 25	LINEAR DELINEATION SYSTEM	LF
619.080102	REMOVE PAVEMENT MARKING STRIPES - EPOXY PAINT	LF
619.0901	TEMPORARY PAVEMENT MARKINGS, STRIPES - TRAFFIC PAINT	LF
619.1711	TEMPORARY POSITIVE BARRIER - CATEGORY 1 (PINNING PROHIBITED)	LF
619.1714	TEMPORARY POSITIVE BARRIER - CATEGORY 4 (PINNING PERMITTED)	LF

NOTES:

1. FOR WORK ZONE SIGNAGE AND DETAILS REFER TO TA 619-11 AND 619-16.
2. FOR CHANNELING DEVICE SPACING REFER TO STANDARD SPECIFICATION TA 619.
3. FIRST FULL HEIGHT SECTION (IN DIRECTION OF TRAFFIC) OF TEMPORARY POSITIVE BARRIER MUST BE 12' FROM EDGE OF TRAVELED WAY.
4. THERE IS A TRANSMIT SITE ATTACHED TO THE UNDERSIDE OF BIN 5512160. CARE SHOULD BE TAKEN TO AVOID DAMAGE. CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE TO THE EQUIPMENT.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



**Thruway
Authority**

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK

LOCATION OF PROJECT
MP 416.90 TO MP 416.40

TITLE OF DRAWING
WORK ZONE DETAIL

CONTRACT NUMBER:

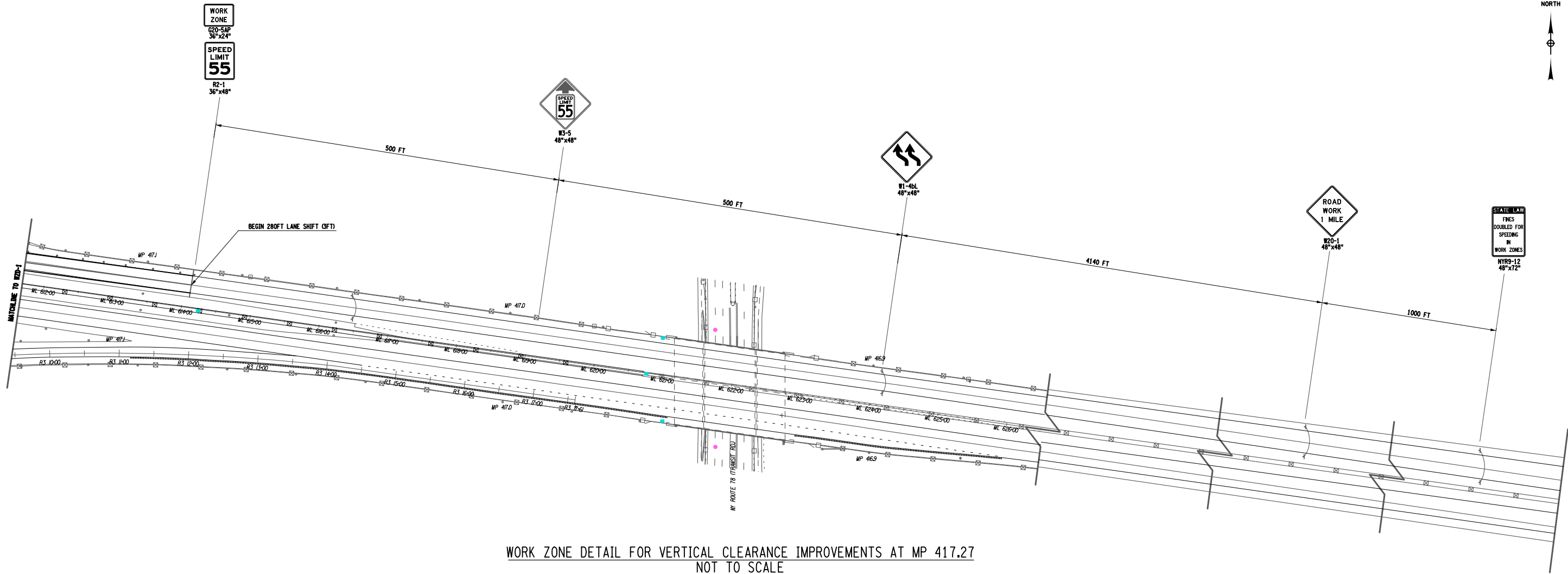
TAB 25-31

DATE:

08/20/2025

DRAWING NUMBER:

WZD-1



WORK ZONE DETAIL FOR VERTICAL CLEARANCE IMPROVEMENTS AT MP 417.27
NOT TO SCALE

ALTERED ON:

SIGNATURE:
STAMP:

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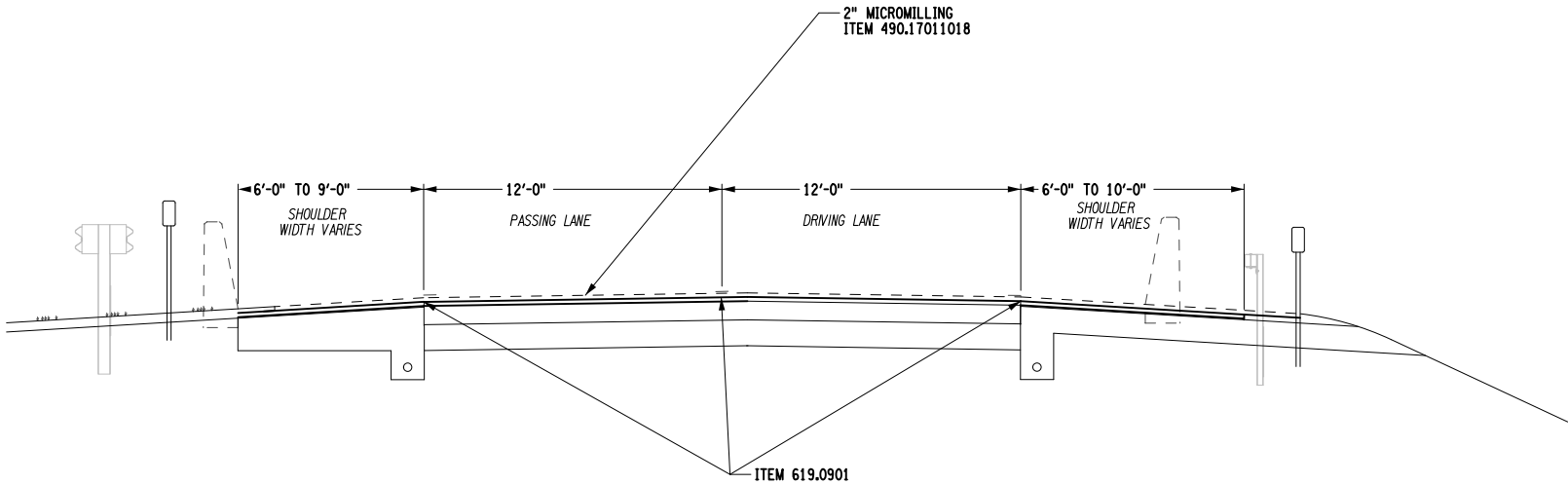
ITEM	DESCRIPTION	UNIT
490.17011018	PRODUCTION COLD MICROMILLING	SF
619.080102	REMOVE PAVEMENT MARKING STRIPES - EPOXY PAINT	LF
619.0901	TEMPORARY PAVEMENT MARKINGS, STRIPES - TRAFFIC PAINT	LF
619.1711	TEMPORARY POSITIVE BARRIER - CATEGORY 1 (PINNING PROHIBITED)	LF
619.1714	TEMPORARY POSITIVE BARRIER - CATEGORY 4 (PINNING PERMITTED)	LF

- NOTES:
1. FOR WORK ZONE SIGNAGE AND DETAILS REFER TO TA 619-11 AND 619-16.
 2. FOR CHANNELING DEVICE SPACING REFER TO STANDARD SPECIFICATION TA 619.
 3. FIRST FULL HEIGHT SECTION (IN DIRECTION OF TRAFFIC) OF TEMPORARY CONCRETE BARRIER MUST BE 12' FROM EDGE OF TRAVELED WAY.

REVISIONS					TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER:
DATE	DESCRIPTION	BY	SYMBOL			TAB 25-31
					LOCATION OF PROJECT MP 416.90 TO MP 416.40	DATE: 08/20/2025
					TITLE OF DRAWING WORK ZONE DETAIL	DRAWING NUMBER: WZD-1A

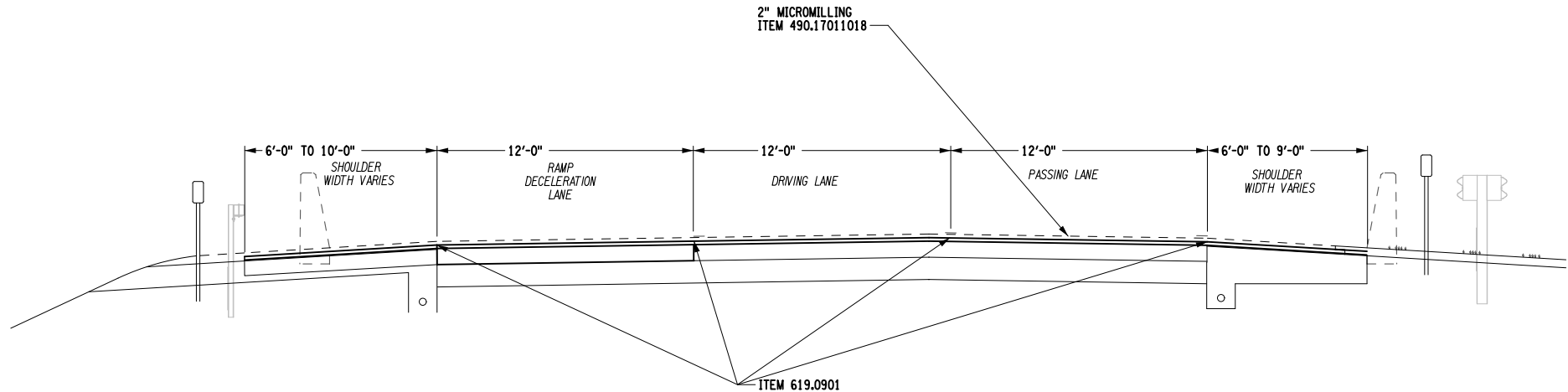
PHASE 1

-2" MICROMILL EXISTING SURFACE
-ADD TEMPORARY PAVEMENT STRIPING



VERTICAL CLEARANCE IMPROVEMENT
CROSS SECTION DETAIL

SECTION C-C (EASTBOUND)
NOT TO SCALE




VERTICAL CLEARANCE IMPROVEMENT
CROSS SECTION WORK ZONE DETAIL

SECTION B-B (WESTBOUND)
NOT TO SCALE

ITEM	DESCRIPTION	UNIT
490.17011018 619.0901	PRODUCTION COLD MICROMILLING TEMPORARY PAVEMENT MARKINGS, STRIPES - TRAFFIC PAINT	SY LF

NOTES:

- SEE DTL-1 FOR VERTICAL CLEARANCE IMPROVEMENT DETAIL AND REFERENCES TO SECTIONS.
- TRAFFIC WILL BE PERMITTED TO DRIVE ON MICROMILLED SURFACE.

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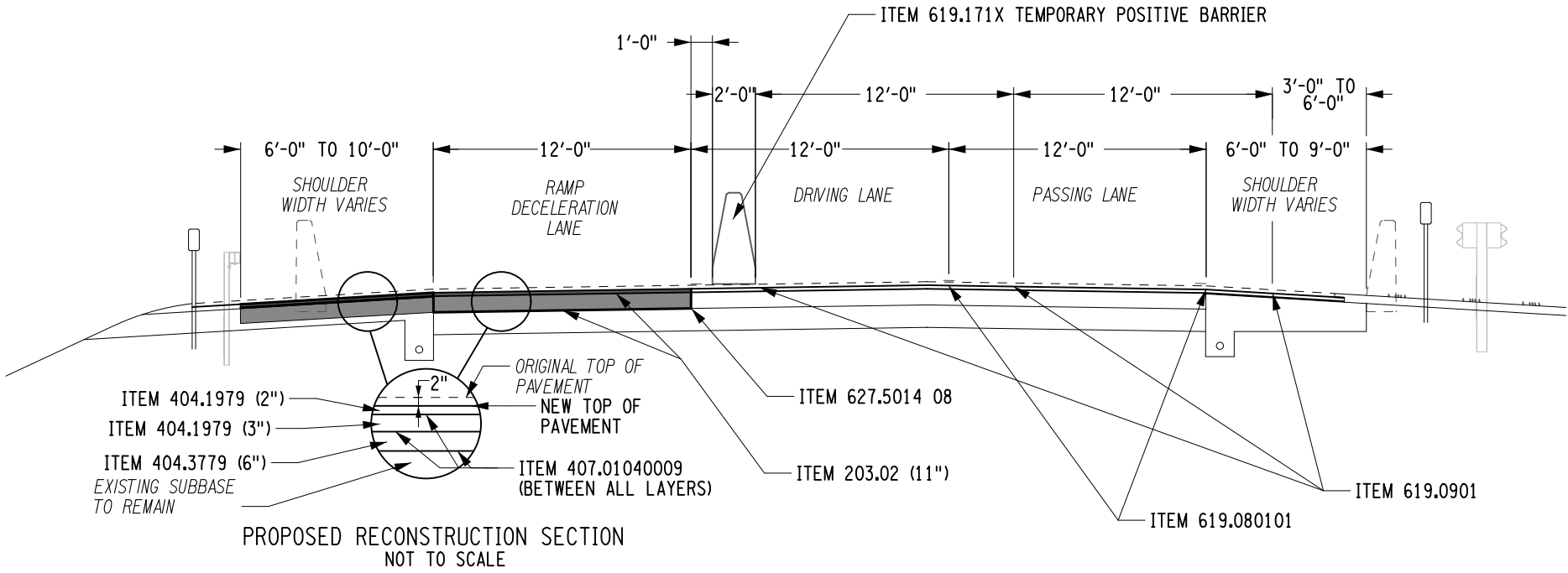
Thruway
Authority

TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK
LOCATION OF PROJECT MP 416.90 TO MP 419.40
TITLE OF DRAWING WORK ZONE DETAIL

CONTRACT NUMBER: TAB 25-31
DATE: 08/20/2025
DRAWING NUMBER: WZD-2

PHASE 2

- RECONSTRUCT DECELERATION LANE UP TO NEW SURFACE
- TEMPORARY POSITIVE BARRIER
- REMOVE TEMPORARY PAVEMENT STRIPING
- ADD NEW TEMPORARY PAVEMENT STRIPING



VERTICAL CLEARANCE IMPROVEMENT
CROSS SECTION WORK ZONE DETAIL
SECTION B-B (WESTBOUND)
NOT TO SCALE

ITEM	DESCRIPTION	UNIT
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY
404.1979	19 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION	TON
404.3779	37.5 F9 BASE COUSE ASPHALT, 70 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
619.080101	REMOVE PAVEMENT MARKING STRIPES - TRAFFIC PAINT	LF
619.0901	TEMPORARY PAVEMENT MARKINGS, STRIPES - TRAFFIC PAINT	LF
619.171X	TEMPORARY POSITIVE BARRIER	LF
627.5014 08	CUTTING PAVEMENT	LF


NOTES:
1. SEE DTL-1 FOR VERTICAL CLEARANCE IMPROVEMENT DETAIL AND REFERENCES TO SECTIONS.


- EXISTING ASPHALT
- EXISITNG CONCRETE

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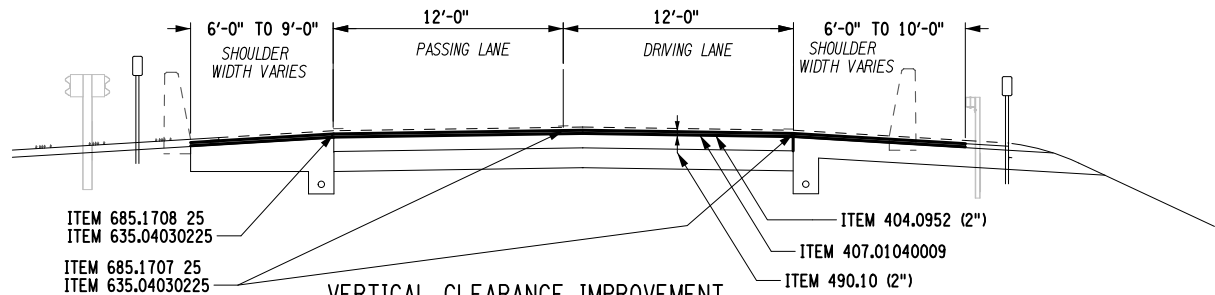
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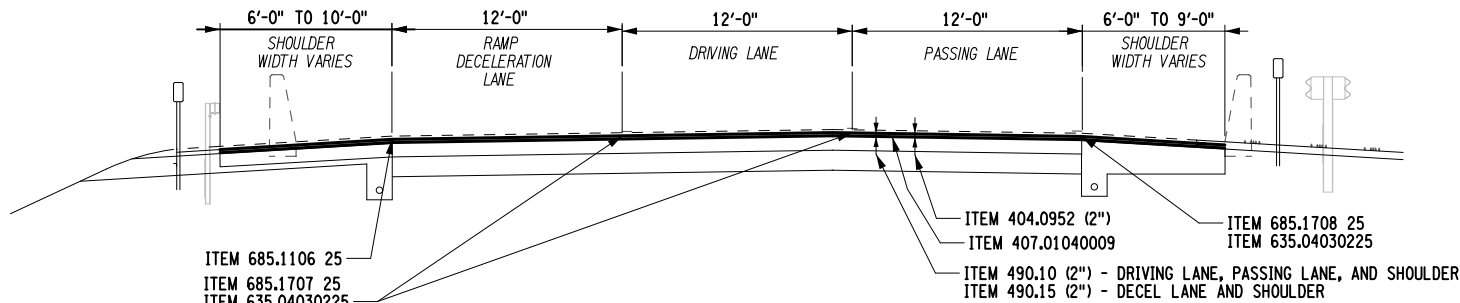
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	DATE	DESCRIPTION	BY	SYM.			LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
						WORK ZONE DETAIL	TITLE OF DRAWING	DRAWING NUMBER: WZD-2A

PHASE 3

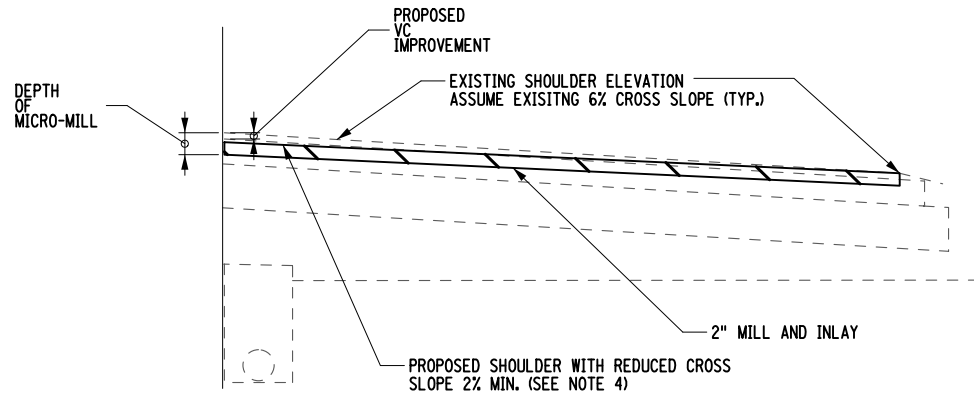
- REMOVE TEMPORARY POSITIVE BARRIER
- 2" MILL AND REPLACE WITH 2" 9.5 TOP COURSE
- ML PAVEMENT STRIPING
- RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS



VERTICAL CLEARANCE IMPROVEMENT
CROSS SECTION DETAIL
SECTION C-C (EASTBOUND)
NOT TO SCALE



VERTICAL CLEARANCE IMPROVEMENT
CROSS SECTION DETAIL
SECTION B-B (WESTBOUND)
NOT TO SCALE




TYPICAL SHOULDER SECTION VC IMPROVEMENT
NOT TO SCALE

NOTES:

1. THE CONTRACTOR IS RESPONSIBLE TO MEASURE THE VERTICAL CLEARANCE(S) AT THE BRIDGE LOCATIONS LISTED HERE-IN PRIOR TO THE START OF MILLING AND AFTER COMPLETION OF THE PAVING OPERATION. THE CONTRACTOR IS RESPONSIBLE TO GAIN VERTICAL CLEARANCE A.O.B.E. THE CONTRACTOR SHALL SUBMIT PRE-PAVING MEASUREMENTS TO THE AUTHORITY FOR REVIEW PRIOR TO PAVING. THE CONTRACTOR SHALL SUBMIT TO THE AUTHORITY POST-PAVING MEASUREMENTS WITHIN ONE WEEK OF PAVING COMPLETION AT THE RESPECTIVE BRIDGE LOCATIONS.
2. CROSS SLOPES OF THE THRUWAY MAY VARY FROM NORMAL CROWN TO FULL SUPER ELEVATION (BOTH BANKED LEFT AND RIGHT). THE PROPOSED TRAVEL LANE CROSS SLOPE SHALL MATCH THE EXISTING CROSS SLOPE IN ALL CASES.
3. PLACE TACK COAT ON ALL EXPOSED SURFACES WHICH WILL BE IN CONTACT WITH NEW ASPHALT. COST TO BE PAID FOR UNDER ITEM 407.01040009.
4. EXISTING EDGE OF SHOULDER ELEVATION SHALL REMAIN THE SAME. THE SHOULDER CROSS SLOPE SHALL BE REDUCED TO GAIN VERTICAL CLEARANCE AND STILL ALLOW DRAINAGE. SHOULDER CROSS SLOPE SHALL NOT BE LESS THAN 2% PER NYSTA STANDARD SHEET TA 402-03.
5. BRIDGE WIDTHS SHALL BE FIELD VERIFIED.
6. SEE DTL-1 FOR VERTICAL CLEARANCE DETAIL AND REFERENCES TO SECTIONS.


ITEM	DESCRIPTION	UNIT
404.0952	9.5 F9 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
490.10	PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE	SY
490.15	PRODUCTION COLD MILL SURFACE PLANING OF BITUMINOUS CONCRETE	SY
635.04030225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF
685.1106 25	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES 6" x 20 MILS	LF
685.1707 25	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF
685.1708 25	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF

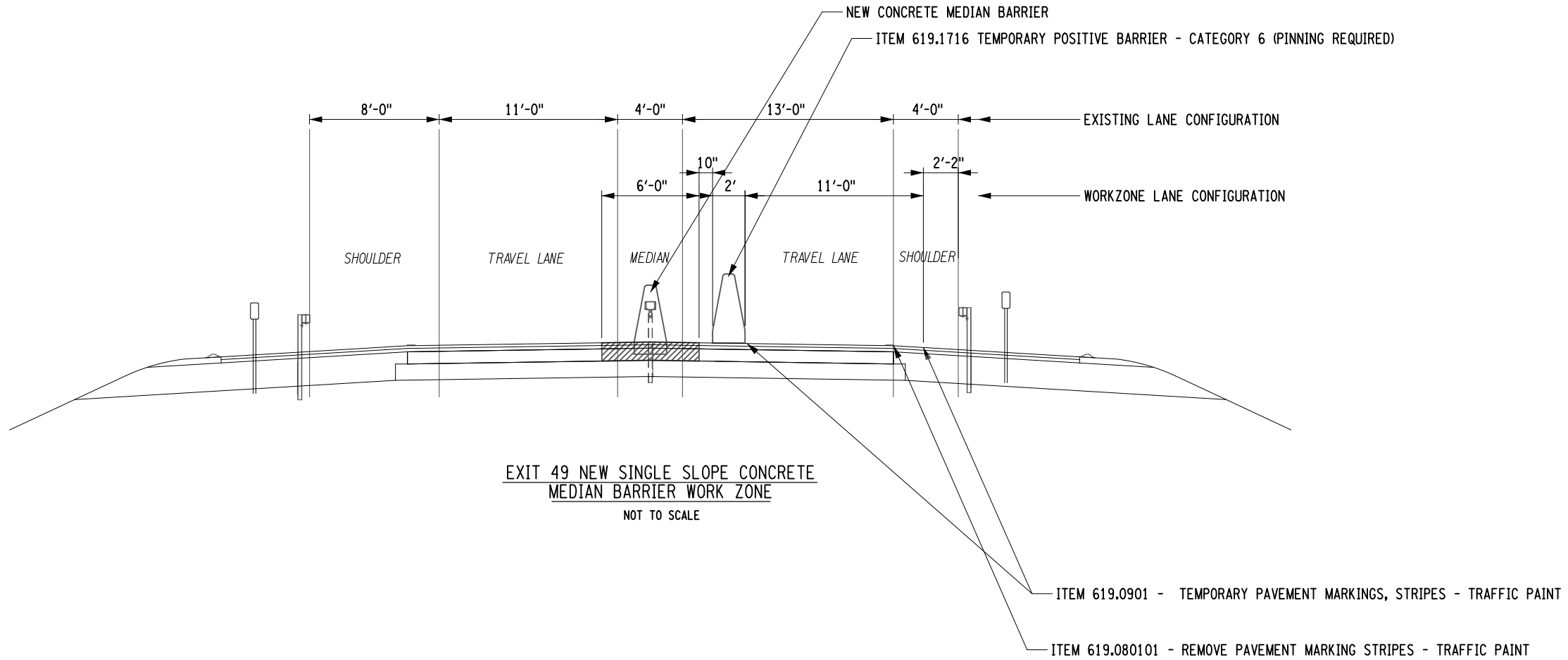
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 - NEW TOP OF PAVEMENT

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	LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING WORK ZONE DETAIL		DRAWING NUMBER: WZD-2B



ITEM	DESCRIPTION	UNIT
619.04	TYPE III CONSTRUCTION BARRICADES	EACH
619.080101	REMOVE PAVEMENT MARKING STRIPES - TRAFFIC PAINT	LF
619.080102	REMOVE PAVEMENT MARKING STRIPES - EPOXY PAINT	LF
619.0901	TEMPORARY PAVEMENT MARKINGS, STRIPES - TRAFFIC PAINT	LF
619.1716	TEMPORARY POSITIVE BARRIER - CATEGORY 6 (PINNING REQUIRED)	LF
619.1803	TEMPORARY IMPACT ATTENUATOR - REDIRECTIVE (TEST LEVEL 3)	EACH

 - WORK AREA

ALTERED ON:	AFFIXED ON: 09/09/2025
SIGNATURE: STAMP:	SIGNATURE: 

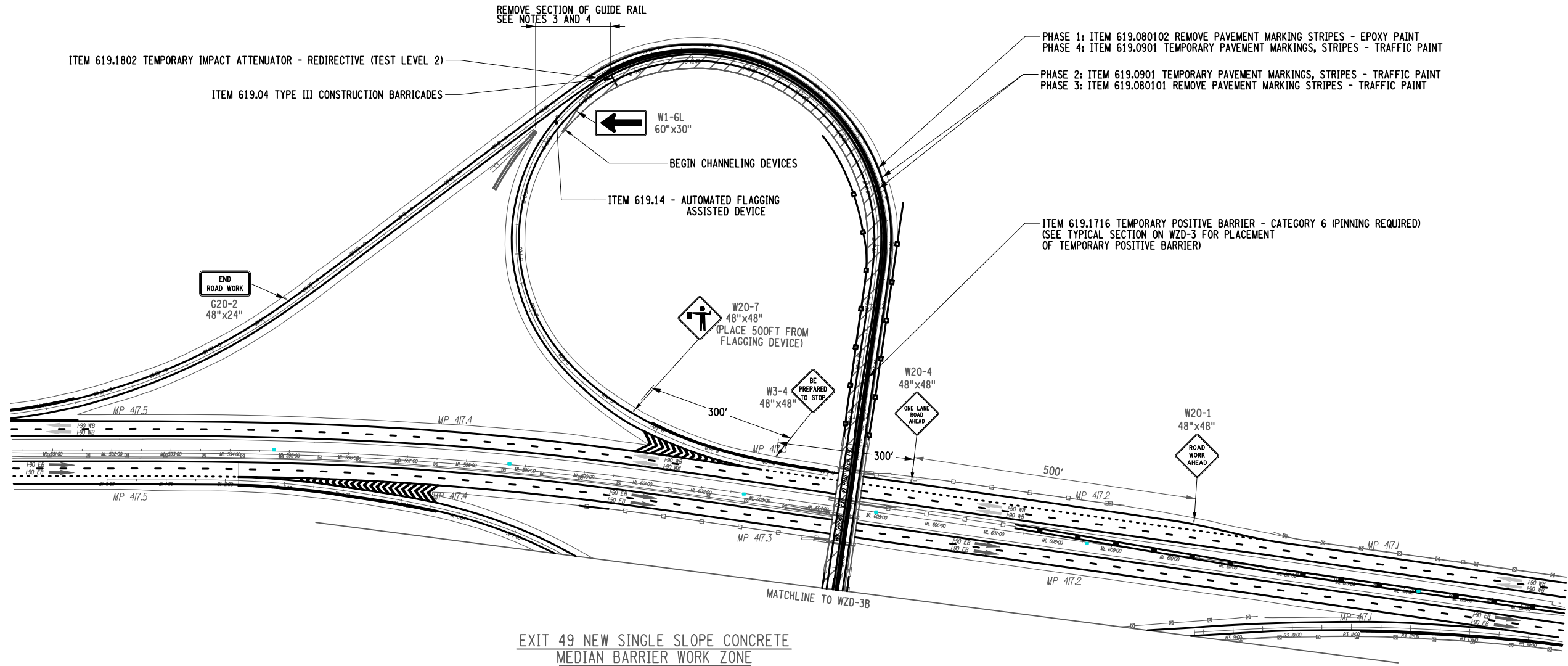
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



**Thruway
Authority**

TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING MEDIAN BARRIER WORK ZONE	DRAWING NUMBER: WZD-3




EXIT 49 NEW SINGLE SLOPE CONCRETE
MEDIAN BARRIER WORK ZONE
NOT TO SCALE

NOTES:

1. FLAGGING OPERATIONS SHALL BE ALLOWED DURING NIGHT TIME OPERATIONS. SEE 619 STANDARD SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS FOR NIGHT TIME FLAGGING.
2. ALL SIGNS MAY BE FOUND IN THE MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES. ALL "W" SIGNS SHALL BE BLACK ON FLUORESENT ORANGE IN COLOR.
3. SEE TRAFFIC CONTROL NOTE PLAN SHEETS FOR ADDITIONAL DETAILS.
4. A SECTION OF BOX BEAM MEDIAN BARRIER TO BE REMOVED A LENGTH EQUAL TO APPROXIMATELY 162 FT OR A.O.B.E. TEMPORARY POSITIVE BARRIER CATEGORY 6 (PINNING REQUIRED) WITH TEMPORARY IMPACT ATTENUATORS ON THE EXPOSED ENDS TO BE PLACED BETWEEN EXISTING BOX BEAM MEDIAN BARRIER AND ONE WAY TRAVEL LANE A.O.B.E. ONCE TEMPORARY BARRIER HAS BEEN PLACED AND APPROVED, THE BOX BEAM MEDIAN BARRIER CAN BE REPLACED WITH CONCRETE MEDIAN BARRIER.
5. WORK IN THIS SECTION MAY REQUIRE ADDITIONAL MEDIAN BARRIER TO BE REMOVED. ADDITIONAL FLAG PERSON/SIGNAGE MAY BE REQUIRED TO PROPERLY CONTROL TRAFFIC IN THIS SECTION.
6. CONTRACTOR TO CONTROL TRAFFIC TO ONE LANE WITH AUTOMATED FLAGGER ASSISTANT ON RAMP DURING NIGHTTIME CONSTRUCTION OPERATIONS.
7. AT THE END OF DAILY CONSTRUCTION, THE AUTOMATED FLAGGER SHALL BE REMOVED AND CLOSELY SPACED DELINEATION DEVICES SHALL BE PLACED IN THE OPENINGS IN THE MEDIAN BARRIER AND ALL FLAGGING SIGNS (OR OTHER NON APPLICABLE SIGNS) SHALL BE COVERED OR REMOVED.
8. NIGHTLY INTERCHANGE RAMP CLOSURES WILL BE ALLOWED ON A LIMITED BASIS FOR CONCRETE BARRIER WORK AND PAVING OPERATIONS AS APPROVED BY BOTH THE PROJECT ENGINEER AND DIVISION TRAFFIC SUPERVISION. THE CONTRACTOR SHALL SUBMIT TO THE PROJECT ENGINEER FOR APPROVAL THE PROPOSED INTERCHANGE RAMP CLOSURE DETAILS INCLUDING DETOUR PLAN, 21 DAYS PRIOR TO THE DESIRED CLOSURE. COSTS ASSOCIATED WITH PROPOSED INTERCHANGE RAMP CLOSURE(S) SHALL BE INCLUDED IN THE UNIT PRICE BID FOR WORK ZONE TRAFFIC CONTROL ITEMS.
9. TRAFFIC QUEUES CREATED FROM THE FLAGGING OPERATION SHALL NOT EXTEND OR DISRUPT MAINLINE I-90 THROUGH TRAFFIC. IF THIS OCCURS, IMMEDIATELY PLACE MOBILE "BE PREPARED TO STOP" SIGNAGE PRIOR TO EXIT 49 UNTIL QUEUE IS CLEARED.


ITEM	DESCRIPTION	UNIT
619.04	TYPE III CONSTRUCTION BARRICADES	EACH
619.080101	REMOVE PAVEMENT MARKING STRIPES - TRAFFIC PAINT	LF
619.080102	REMOVE PAVEMENT MARKING STRIPES - EPOXY PAINT	LF
619.0901	TEMPORARY PAVEMENT MARKINGS, STRIPES - TRAFFIC PAINT	LF
619.14	AUTOMATED FLAGGING ASSISTANT DEVICE	DVDY
619.1716	TEMPORARY POSITIVE BARRIER - CATEGORY 6 (PINNING REQUIRED)	LF
619.1802	TEMPORARY IMPACT ATTENUATOR - REDIRECTIVE (TEST LEVEL 2)	EACH

 - WORK AREA

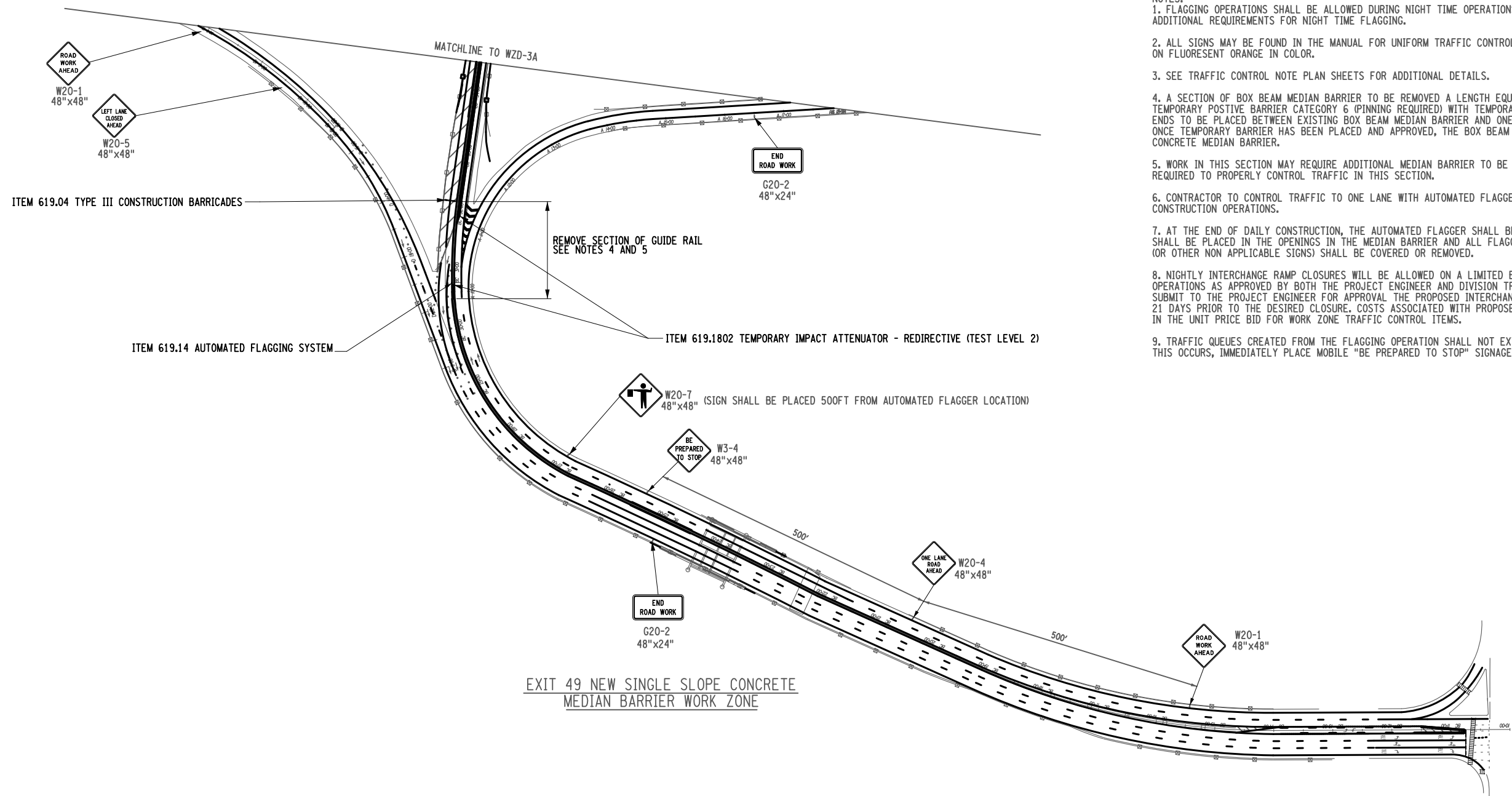
ALTERED ON:	AFFIXED ON: 09/16/2025
SIGNATURE: STAMP:	SIGNATURE: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.


REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
	LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING MEDIAN BARRIER WORK ZONE		DRAWING NUMBER: WZD-3A

DESIGN SUPERVISOR • K. GAWRONSKI



- NOTES:
1. FLAGGING OPERATIONS SHALL BE ALLOWED DURING NIGHT TIME OPERATIONS. SEE 619 STANDARD SPECIFICATIONS FOR ADDITIONAL REQUIREMENTS FOR NIGHT TIME FLAGGING.
 2. ALL SIGNS MAY BE FOUND IN THE MANUAL FOR UNIFORM TRAFFIC CONTROL DEVICES. ALL "W" SIGNS SHALL BE BLACK ON FLUORESCENT ORANGE IN COLOR.
 3. SEE TRAFFIC CONTROL NOTE PLAN SHEETS FOR ADDITIONAL DETAILS.
 4. A SECTION OF BOX BEAM MEDIAN BARRIER TO BE REMOVED A LENGTH EQUAL TO APPROXIMATELY 162 FT OR A.O.B.E. TEMPORARY POSITIVE BARRIER CATEGORY 6 (PINNING REQUIRED) WITH TEMPORARY IMPACT ATTENUATORS ON THE EXPOSED ENDS TO BE PLACED BETWEEN EXISTING BOX BEAM MEDIAN BARRIER AND ONE WAY TRAVEL LANE A.O.B.E. ONCE TEMPORARY BARRIER HAS BEEN PLACED AND APPROVED, THE BOX BEAM MEDIAN BARRIER CAN BE REPLACED WITH CONCRETE MEDIAN BARRIER.
 5. WORK IN THIS SECTION MAY REQUIRE ADDITIONAL MEDIAN BARRIER TO BE REMOVED. ADDITIONAL FLAG PERSON/SIGNAGE MAY BE REQUIRED TO PROPERLY CONTROL TRAFFIC IN THIS SECTION.
 6. CONTRACTOR TO CONTROL TRAFFIC TO ONE LANE WITH AUTOMATED FLAGGER ASSISTANT ON RAMP DURING NIGHTTIME CONSTRUCTION OPERATIONS.
 7. AT THE END OF DAILY CONSTRUCTION, THE AUTOMATED FLAGGER SHALL BE REMOVED AND CLOSELY SPACED DELINEATION DEVICES SHALL BE PLACED IN THE OPENINGS IN THE MEDIAN BARRIER AND ALL FLAGGING SIGNS (OR OTHER NON APPLICABLE SIGNS) SHALL BE COVERED OR REMOVED.
 8. NIGHTLY INTERCHANGE RAMP CLOSURES WILL BE ALLOWED ON A LIMITED BASIS FOR CONCRETE BARRIER WORK AND PAVING OPERATIONS AS APPROVED BY BOTH THE PROJECT ENGINEER AND DIVISION TRAFFIC SUPERVISION. THE CONTRACTOR SHALL SUBMIT TO THE PROJECT ENGINEER FOR APPROVAL THE PROPOSED INTERCHANGE RAMP CLOSURE DETAILS INCLUDING DETOUR PLAN, 21 DAYS PRIOR TO THE DESIRED CLOSURE. COSTS ASSOCIATED WITH PROPOSED INTERCHANGE RAMP CLOSURE(S) SHALL BE INCLUDED IN THE UNIT PRICE BID FOR WORK ZONE TRAFFIC CONTROL ITEMS.
 9. TRAFFIC QUEUES CREATED FROM THE FLAGGING OPERATION SHALL NOT EXTEND OR DISRUPT MAINLINE I-90 THROUGH TRAFFIC. IF THIS OCCURS, IMMEDIATELY PLACE MOBILE "BE PREPARED TO STOP" SIGNAGE PRIOR TO EXIT 49 UNTIL QUEUE IS CLEARED.

ALTERED ON:	AFFIXED ON: 09/16/2025
SIGNATURE: STAMP:	SIGNATURE: 

ITEM	DESCRIPTION	UNIT
619.04	TYPE III CONSTRUCTION BARRICADES	EACH
619.080101	REMOVE PAVEMENT MARKING STRIPES - TRAFFIC PAINT	LF
619.080102	REMOVE PAVEMENT MARKING STRIPES - EPOXY PAINT	LF
619.0901	TEMPORARY PAVEMENT MARKINGS, STRIPES - TRAFFIC PAINT	LF
619.11.0	AUTOMATED FLAGGING ASSISTANT	DVYD
619.116	TEMPORARY POSITIVE BARRIER - CATEGORY 6 (PINNING REQUIRED)	LF
619.1802	TEMPORARY IMPACT ATTENUATOR - REDIRECTIVE (TEST LEVEL 2)	EA

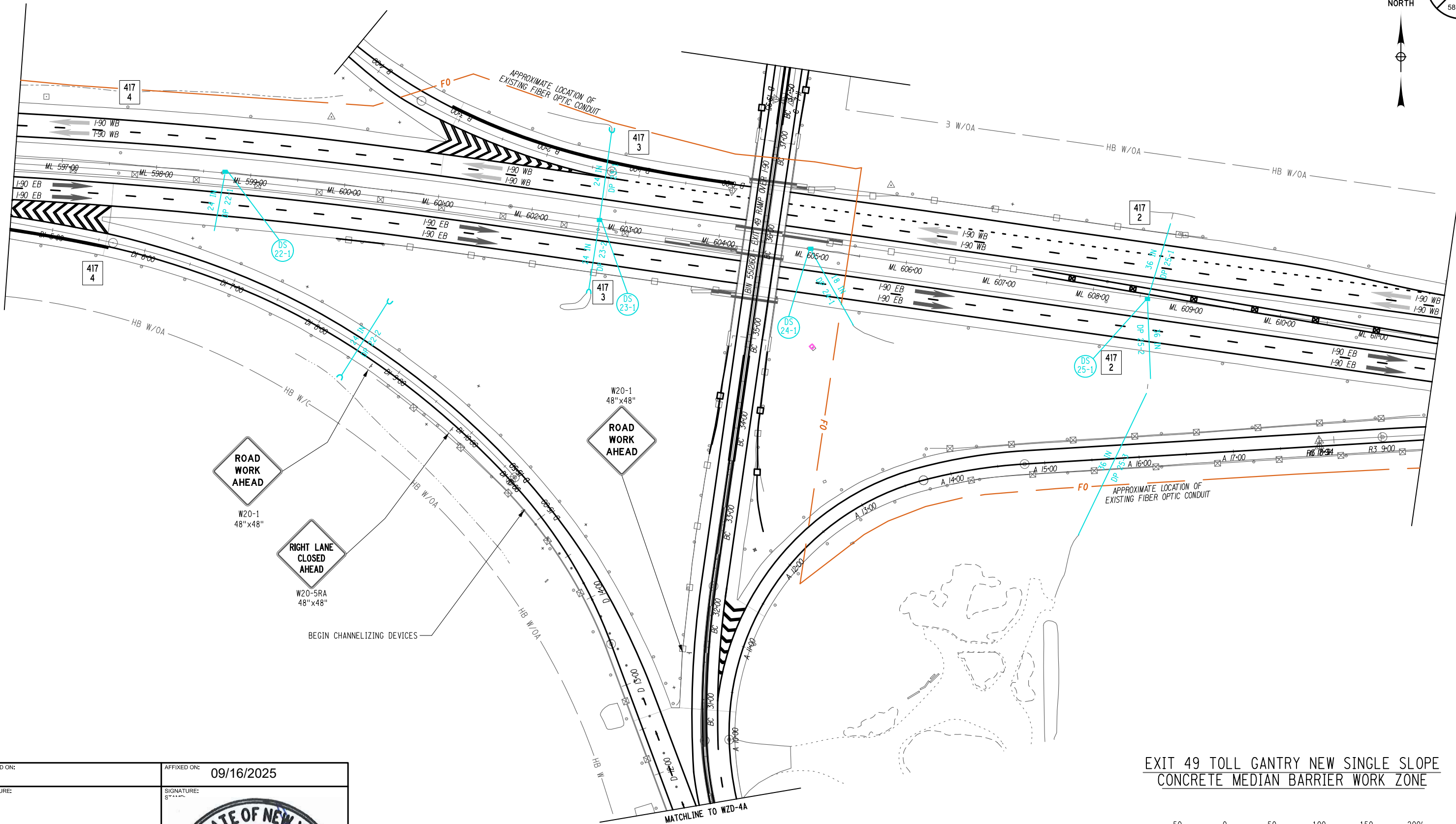
 - WORK AREA

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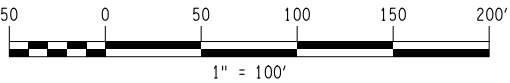
REVISIONS			
DATE	DESCRIPTION	BY	SYM

**Thruway
Authority**

TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK		CONTRACT NUMBER: TAB 25-31
LOCATION OF PROJECT MP 416.90 TO MP 419.40		DATE: 08/20/2025
TITLE OF DRAWING MEDIAN BARRIER WORK ZONE		DRAWING NUMBER: WZD-3B



EXIT 49 TOLL GANTRY NEW SINGLE SLOPE
CONCRETE MEDIAN BARRIER WORK ZONE



ALTERED ON:	AFFIXED ON: 09/16/2025
SIGNATURE: STAMP:	SIGNATURE: STAMP:

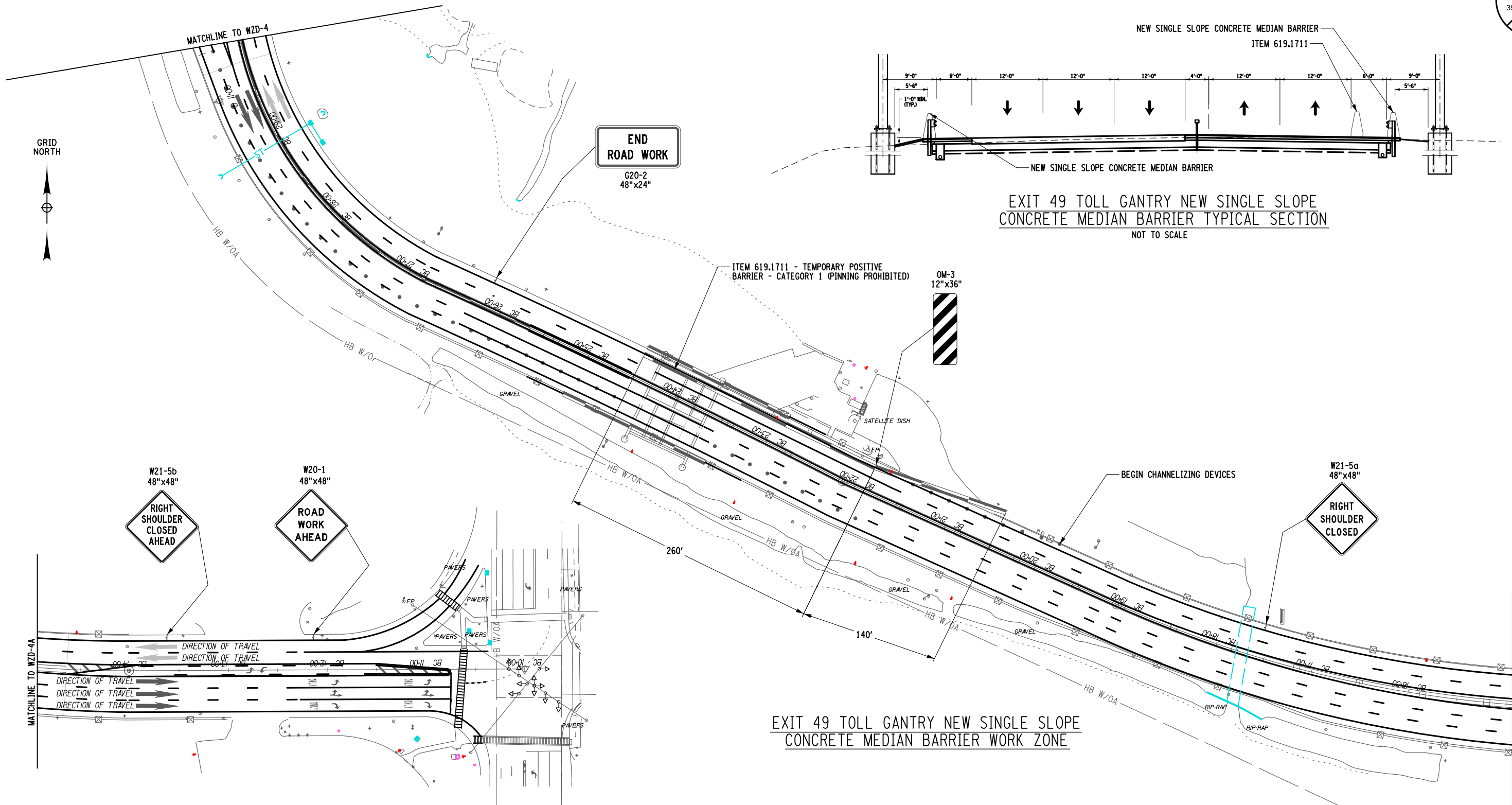
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



Thruway
Authority

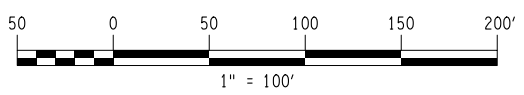
TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING GANTRY BARRIER WORK ZONE	DRAWING NUMBER: WZD-4



ITEM	DESCRIPTION	UNIT
619.1711	TEMPORARY POSITIVE BARRIER - CATEGORY 1 (PINNING PROHIBITED)	LF

NOTES:

1. FIRST FULL HEIGHT SECTION (IN THE DIRECTION OF TRAFFIC) OF TEMPORARY POSITIVE BARRIER MUST BE 12' FROM EDGE OF TRAVELED WAY.



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway
Authority**

TITLE OF PROJECT	1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK
LOCATION OF PROJECT	MP 416.90 TO MP 419.40
TITLE OF DRAWING	GANTRY BARRIER WORK ZONE

CONTRACT NUMBER: TAB 25-31
DATE: 08/20/2025
DRAWING NUMBER: WZD-4A



DRAINAGE TABLE -- SHEET 1 OF 3										
DS #	STATION	DESCRIPTION OF WORK	EXIST T.G. ELEV.	PROP T.G. ELEV.	SIDE / SIZE / INVERT ELEV.	SIDE / SIZE / INVERT ELEV.	SIDE / SIZE / INVERT ELEV.	SIDE / SIZE / INVERT ELEV.	621.04	621.03
									(EA)	(LF)
DS 1-1	ML 492+57.25	ALTER FRAME AND GRATE ELEVATION AND CLEAN STRUCTURE	701.43	701.06	N. / 18" / 697.01	S. / 18" / 697.01			1	
DP 1-1		CLEAN PIPE FROM DS 1-1 TO DS 1-2			18" / 697.91	18" / 697.01				66
DS 1-2	ML 493+00.92	CLEAN STRUCTURE	702.57	702.57	S. / 18" / 697.91				1	
DP 1-2		CLEAN PIPE FROM DS 1-1 TO DS 1-3			18" / 697.91	18" / 697.01				64
DS 1-3	ML 493+05.97	CLEAN STRUCTURE	702.66	702.66	N. / 18" / 697.91				1	
DS 2-1	ML 503+57.49	ALTER FRAME AND GRATE ELEVATION AND CLEAN STRUCTURE	709.83	709.46	W. / 12" / 703.35				1	
DP 2-1		CLEAN PIPE FROM DS 2-1 TO DS 2-2			12" / 703.35	12" / 702.37				272
DS 2-2	ML 500+81.24	ALTER FRAME AND GRATE ELEVATION AND CLEAN STRUCTURE	707.70	707.33	E. / 12" / 702.37	W. / 12" / 702.30			1	
DP 2-2		CLEAN PIPE FROM DS 2-2 TO DS 2-3			12" / 702.30	12" / 700.67				299
DS 2-3	ML 497+78.37	ALTER FRAME AND GRATE ELEVATION AND CLEAN STRUCTURE	705.49	705.12	E. / 12" / 700.67	W. / 12" / 700.63			1	
DP 2-3		CLEAN PIPE FROM DS 2-3 TO DS 2-4			12" / 700.63	12" / 698.21				295
DS 2-4	ML 494+80.04	ALTER FRAME AND GRATE ELEVATION AND CLEAN STRUCTURE	704.03	703.66	E. / 12" / 698.21	N. / 18" / 698.21			1	
DP 2-4		CLEAN PIPE FROM DS 2-4 TO OUTLET			18" / 698.21	18" / 698.00				108
DS 3-1	ML 506+79.45	ALTER FRAME AND GRATE ELEVATION AND CLEAN STRUCTURE	709.83	709.46	E. / 12" / 704.78				1	
DP 3-1		CLEAN PIPE FROM DS 3-1 TO DS 3-2			12" / 704.78	12" / 703.68				299
DS 3-2	ML 509+82.11	ALTER FRAME AND GRATE ELEVATION AND CLEAN STRUCTURE	708.53	708.16	W. / 12" / 703.68	E. / 12" / 703.67			1	
DP 3-2		CLEAN PIPE FROM DS 3-2 TO DS 3-3			12" / 703.67	12" / 699.07				294
DS 3-3	ML 512+80.47	ALTER FRAME AND GRATE ELEVATION AND CLEAN STRUCTURE	704.46	704.09	W. / 12" / 699.07	N. / 18" / 699.06			1	
DP 3-3		CLEAN PIPE FROM DS 3-3 TO OUTLET			18" / 699.06	18" / 698.00				120
DS 4-1	ML 514+91.12	CLEAN STRUCTURE	700.79	700.79	S. / 18" / 700.74				1	
DP 4-1		CLEAN PIPE FROM DS 4-1 TO DS 4-3			18" / 700.74	18" / 695.63				51
DS 4-2	ML 514+78.72	CLEAN STRUCTURE	700.91	700.91	N. / 18" / 700.85				1	
DP 4-2		CLEAN PIPE FROM DS 4-2 TO DS 4-3			18" / 700.85	18" / 695.65				50
DS 4-3	ML 515+03.12	CLEAN STRUCTURE	699.58	699.58	N. / 18" / 695.63	S. / 18" / 695.65	E. / 12" / 695.57		1	
DP 4-3		CLEAN PIPE FROM DS 4-3 TO DS 4-4			12" / 695.57	12" / 689.76				273
DS 4-4	ML 517+80.00	CLEAN STRUCTURE	694.38	694.38	W. / 12" / 689.76	N. / 18" / 682.90			1	
DP 4-4		CLEAN PIPE FROM DS 4-4 TO OUTLET			18" / 682.90	18" / 682.33				78
DS 5-1	ML 521+77.17	CLEAN STRUCTURE	691.43	691.43	N. / 18" / 684.49				1	
DP 5-1		CLEAN PIPE FROM DS 5-1 TO OUTLET			18" / 684.49	18" / 683.92				100
DS 6-1	ML 526+29.76	CLEAN STRUCTURE	690.13	690.13	N. / 18" / 685.32				1	
DP 6-1		CLEAN PIPE FROM DS 6-1 TO OUTLET			18" / 685.32	18" / 679.35				109
SHEET 1 TOTALS									16	2478

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AFFIXED ON: 08/21/2025

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Thruway
Authority

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK

LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
DRAINAGE TABLE

CONTRACT NUMBER:

TAB 25-31

DATE:

08/20/2025

DRAWING NUMBER:


DRT-1

DRAINAGE TABLE -- SHEET 2 OF 3										
DS #	STATION	DESCRIPTION OF WORK	EXIST T.G. ELEV.	PROP T.G. ELEV.	SIDE / SIZE / INVERT ELEV.	SIDE / SIZE / INVERT ELEV.	SIDE / SIZE / INVERT ELEV.	SIDE / SIZE / INVERT ELEV.	621.04	621.03
									(EA)	(LF)
DS 7-1	ML 530+40.49	ALTER FRAME AND GRATE ELEVATION AND CLEAN STRUCTURE	690.39	690.02	E. / 18" / 684.83				1	
DP 7-1		CLEAN PIPE FROM DS 7-1 TO DS 7-2			18" / 684.83	18" / 684.63				128
DS 7-2	ML 531+73.01	CLEAN STRUCTURE	689.17	689.17	W. / 18" / 684.63	S. / 18" / 684.61			1	
DP 7-2		CLEAN PIPE FROM DS 7-2 TO OUTLET			18" / 684.61	18" / 681.79				87
DS 8-1	ML 534+74.40	CLEAN STRUCTURE	689.68	689.68	S. / 36" / 683.29	N. / 36" / 683.64			1	
DP 8-1		CLEAN PIPE FROM DS 8-1 TO OUTLET			36" / 683.64	36" / 683.92				85
DP 8-2		CLEAN PIPE FROM DS 8-1 TO OUTLET			36" / 683.29	36" / 682.69				93
DS 9-1	ML 539+73.49	CLEAN STRUCTURE	691.37	691.37	S. / 18" / 686.62				1	
DP 9-1		CLEAN PIPE FROM DS 9-1 TO OUTLET			18" / 686.62	18" / 684.91				99
DS 10-1	ML 543+75.43	CLEAN STRUCTURE	693.00	693.00	S. / 18" / 688.81				1	
DP 10-1		CLEAN PIPE FROM DS 10-1 TO OUTLET			18" / 688.81	18" / 687.67				96
DS 11-1	ML 548+05.32	CLEAN STRUCTURE	694.57	694.57	S. / 18" / 691.08				1	
DP 11-1		CLEAN PIPE FROM DS 11-1 TO OUTLET			18" / 691.08	18" / 690.50				72
DS 12-1	ML 551+75.36	CLEAN STRUCTURE	695.75	695.75	S. / 18" / 692.78				1	
DP 12-1		CLEAN PIPE FROM DS 12-1 TO OUTLET			18" / 692.78	18" / 691.52				75
DS 13-1	ML 555+73.57	CLEAN STRUCTURE	696.91	696.91	S. / 18" / 693.27				1	
DP 13-1		CLEAN PIPE FROM DS 13-1 TO OUTLET			18" / 693.27	18" / 692.85				74
DS 14-1	ML 562+74.49	CLEAN STRUCTURE	697.25	697.25	S. / 18" / 693.38				1	
DP 14-1		CLEAN PIPE FROM DS 14-1 TO OUTLET			18" / 693.38	18" / 692.44				83
DS 15-1	ML 566+73.80	CLEAN STRUCTURE	696.03	696.03	S. / 18" / 692.37				1	
DP 15-1		CLEAN PIPE FROM DS 15-1 TO OUTLET			18" / 692.37	18" / 690.98				78
DS 16-1	ML 570+74.84	CLEAN STRUCTURE	694.84	694.84	S. / 18" / 690.31				1	
DP 16-1		CLEAN PIPE FROM DS 16-1 TO OUTLET			18" / 690.31	18" / 689.29				85
DS 17-1	ML 574+74.08	CLEAN STRUCTURE	693.68	693.68	N. / 18" / 688.29	S. / 24" / 688.29			1	
DP 17-1		CLEAN PIPE FROM DS 17-1 TO OUTLET			24" / 688.29	24" / 687.76				80
DP 17-2		CLEAN PIPE FROM DS 17-1 TO OUTLET			18" / 688.29	18" / 688.98				80
DS 18-1	ML 578+74.34	CLEAN STRUCTURE	694.79	694.79	S. / 24" / 690.45				1	
DP 18-1		CLEAN PIPE FROM DS 18-1 TO OUTLET			24" / 690.45	24" / 689.71				81
DS 19-1	ML 582+37.13	CLEAN STRUCTURE	695.95	695.95	N. / 36" / 689.94	S. / 36" / 689.94			1	
DP 19-1		CLEAN PIPE FROM DS 19-1 TO OUTLET			36" / 689.94	36" / 690.67				91
DP 19-2		CLEAN PIPE FROM DS 19-1 TO OUTLET			36" / 689.94	36" / 689.43				80
SHEET 2 TOTALS									14	1467

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AFFIXED ON: 08/21/2025

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REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL



Thruway
Authority

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK

LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
DRAINAGE TABLE

CONTRACT NUMBER:
TAB 25-31

DATE:
08/20/2025


DRAWING NUMBER:
DRT-2

DRAINAGE TABLE -- SHEET 3 OF 3										
DS #	STATION	DESCRIPTION OF WORK	EXIST T.G. ELEV.	PROP T.G. ELEV.	SIDE / SIZE / INVERT ELEV.	SIDE / SIZE / INVERT ELEV.	SIDE / SIZE / INVERT ELEV.	SIDE / SIZE / INVERT ELEV.	621.04	621.03
									(EA)	(LF)
DS 20-1	ML 598+73.71	CLEAN STRUCTURE	695.08	695.08	S. / 18" / 691.27				1	
DP 20-1		CLEAN PIPE FROM DS 20-1 TO OUTLET			18" / 691.27	18" / 690.64				86
DS 21-1	ML 594+73.80	CLEAN STRUCTURE	696.33	696.33	N. / 24" / 693.28	S. / 24" / 693.12			1	
DP 21-1		CLEAN PIPE FROM DS 21-1 TO OUTLET			24" / 693.28	24" / 684.04				74
DP 21-2		CLEAN PIPE FROM DS 21-1 TO OUTLET			24" / 693.12	24" / 690.75				83
DS 21-2	ML 595+43.91	CLEAN STRUCTURE							1	
DS 22-1	ML 598+72.51	CLEAN STRUCTURE	695.05	695.05	S. / 24" / 692.58				1	
DP 22-1		CLEAN PIPE FROM DS 22-1 TO OUTLET			24" / 692.58	24" / 694.48				61
DP 22-2		CLEAN PIPE FROM INTLET TO OUTLET			24" / 692.09	24" / 691.68				90
DS 23-1	ML 602+72.31	CLEAN STRUCTURE	696.37	696.37	N. / 24" / 693.09	S. / 24" / 693.09			1	
DP 23-1		CLEAN PIPE FROM DS 23-1 TO OUTLET			24" / 693.45	24" / 693.09				93
DP 23-2		CLEAN PIPE FROM DS 23-1 TO OUTLET			24" / 693.09	24" / 692.94				72
DS 24-1	ML 604+97.78	CLEAN STRUCTURE	696.92	696.92	S. / 18" / 694.60				1	
DP 24-1		CLEAN PIPE FROM DS 24-1 TO OUTLET			18" / 694.60	18" / 694.11				90
DS 25-1	ML 608+58.19	CLEAN STRUCTURE	698.08	698.08	N. / 36" / 693.93	S. / 36" / 693.93			1	
DP 25-1		CLEAN PIPE FROM DS 25-1 TO OUTLET			36" / 694.01	36" / 693.93				83
DP 25-2		CLEAN PIPE FROM DS 25-1 TO OUTLET			36" / 693.93	36" / 693.79				82
DP 25-3		CLEAN PIPE FROM INLET TO OUTLET			36" / 694.00	36" / 693.75				167
DS 26-1	ML 614+24.31	CLEAN STRUCTURE	708.50	708.50	S. / 18" / 698.84				1	
DP 26-1		CLEAN PIPE FROM DS 26-1 TO OUTLET			18" / 698.84	18" / 6697.81				91
DS 27-1	ML 620+92.78	CLEAN STRUCTURE	718.59	718.59	S. / 12" / 714.00				1	
DP27-1		CLEAN PIPE FROM DS 27-1 TO DS 27-2			12" / 714.00	12" / 713.14				52
DS 27-2	ML 620+76.50	CLEAN STRUCTURE	717.81	717.81	N. / 12" / 713.14	S. / 12" / 713.15			1	
DP 27-2		CLEAN PIPE FROM DS 27-2 TO DS 27-3			12" / 713.15	12" / 712.59				68
DS 27-3	ML 621+08.62	CLEAN STRUCTURE	718.36	718.36	N. / 12" / 712.59	S. / 12" / 712.31			1	
DP 27-3		CLEAN PIPE FROM DS 27-3 TO OUTLET			12" / 712.31	12" / 700.91				41
SHEET 3 TOTALS									11	1233
SHEET 2 TOTALS									14	1467
SHEET 1 TOTALS									16	2478
TOTALS									41	5178

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK
LOCATION OF PROJECT
MP 416.90 TO MP 419.40
TITLE OF DRAWING
DRAINAGE TABLE

CONTRACT NUMBER:
TAB 25-31
DATE:
08/20/2025
DRAWING NUMBER:
DRT-3

PW/

CHECKED BY: K. GAWRONSKI

DRAFTED BY: J. QUINN

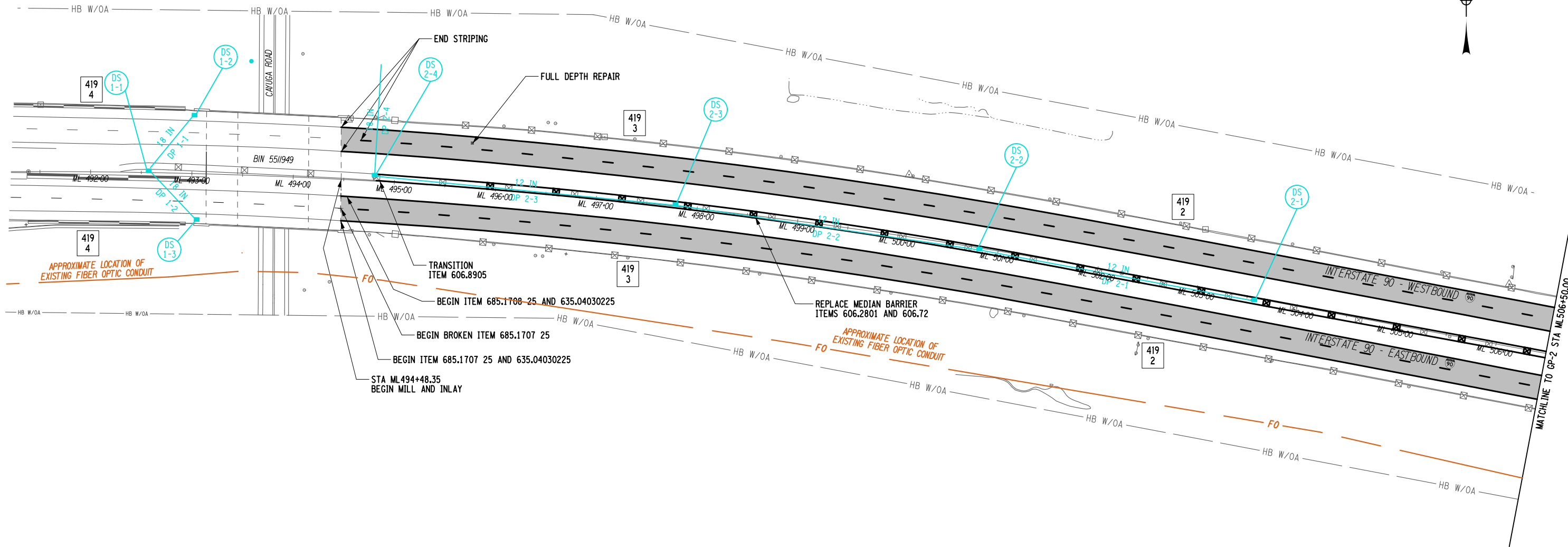
CHECKED BY: K. GAWRONSKI

DESIGNED BY: J. QUINN

DESIGN SUPERVISOR: K. GAWRONSKI

43

58



ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES:
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	621.04	CLEANING DRAINAGE STRUCTURES	EACH	1. FULL DEPTH REPAIRS ARE NOT TO SCALE, SEE MST-1 FOR ASPHALT REPAIR LOCATIONS.
203.24	SHOULDER BACKUP MATERIAL	TON	627.50	CUTTING PAVEMENT	LF	
304.12	SUBBASE COURSE, TYPE 2	CY	635.04	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF	
404.09	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON	685.17	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
404.19	19 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION	TON	685.17	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
404.37	37.5 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION	TON				
407.01	NON-TRACKING TACK COAT	TON				
418.76	ASPHALT PAVEMENT JOINT ADHESIVE	GAL				
490.10	PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE	SY				
606.28	HPBO (MOD.) CORRUGATED BEAM MEDIAN BARRIER	LF				
606.72	REMOVING AND DISPOSING CORRUGATED BEAM MEDIAN BARRIER	LF				
606.89	TRANSITION - HPBO (MOD.) CORRUGATED BEAM MEDIAN BARRIER TO WEAKE POST COR.	EACH				
621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF				

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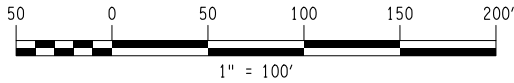
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SIGNATURE: [Signature]

STAMP: [Professional Engineer Seal for Keith R. Gawronski, State of New York, License No. 089722]

- PRODUCTION MILL AND INLAY
- MISCELLANEOUS MILL AND INLAY



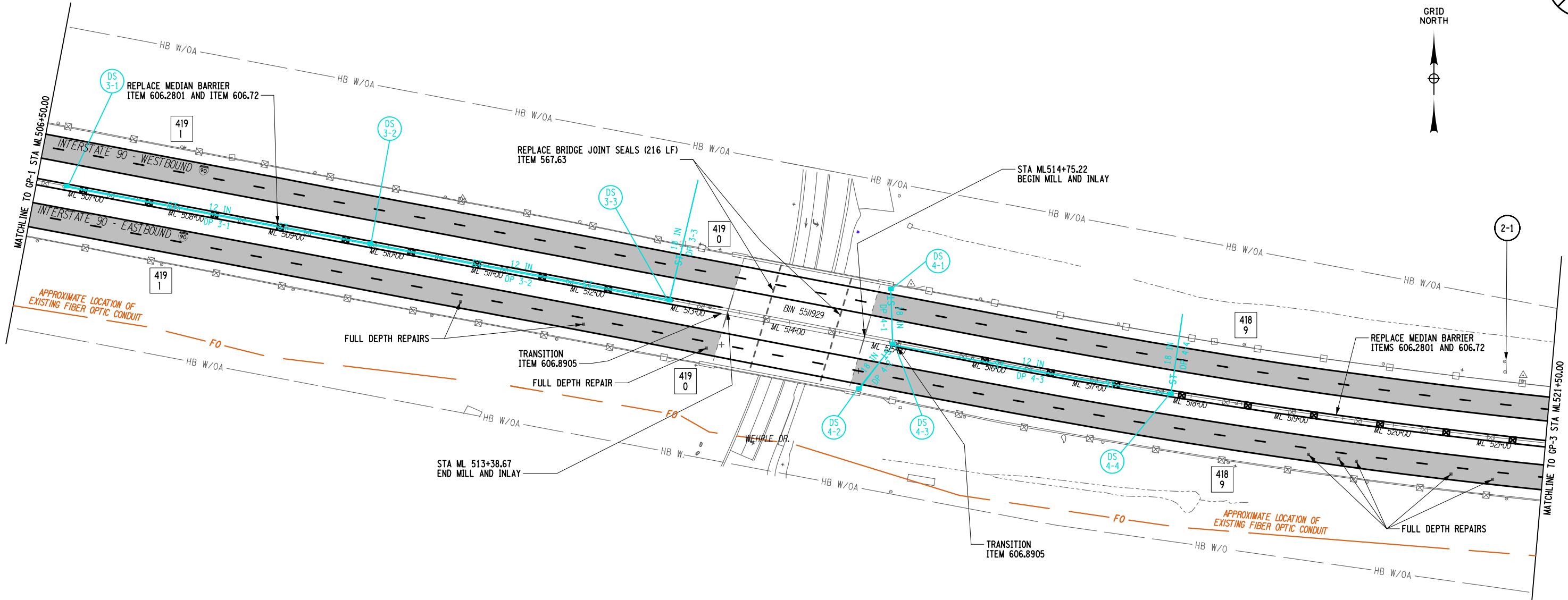
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



Thruway
Authority

TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING GENERAL PLAN	DRAWING NUMBER: GP -1



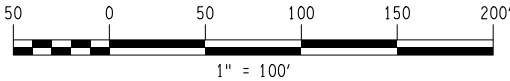
ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES:
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF	1. FULL DEPTH REPAIRS ARE NOT TO SCALE, SEE MST-1 FOR ASPHALT REPAIR LOCATIONS.
203.2400025	SHOULDER BACKUP MATERIAL	TON	621.04	CLEANING DRAINAGE STRUCTURES	EACH	
304.12	SUBBASE COURSE, TYPE 2	CY	627.5014 08	CUTTING PAVEMENT	LF	
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON	635.04030225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF	
404.1979	19 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION	TON	685.1707 25	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
404.3779	37.5 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION	TON	685.1708 25	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
407.01040009	NON-TRACKING TACK COAT	GAL				
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF				
490.10	PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE	SY				
567.63	BRIDGE JOINT SEAL	LF				
606.2801	HPBD (MOD.) CORRUGATED BEAM MEDIAN BARRIER	LF				
606.72	REMOVING AND DISPOSING CORRUGATED BEAM MEDIAN BARRIER	LF				
606.8905	TRANSITION - HPBD (MOD.) CORRUGATED BEAM MEDIAN BARRIER TO WEAK POST COR.	EACH				

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- PRODUCTION MILL AND INLAY
- MISCELLANEOUS MILL AND INLAY



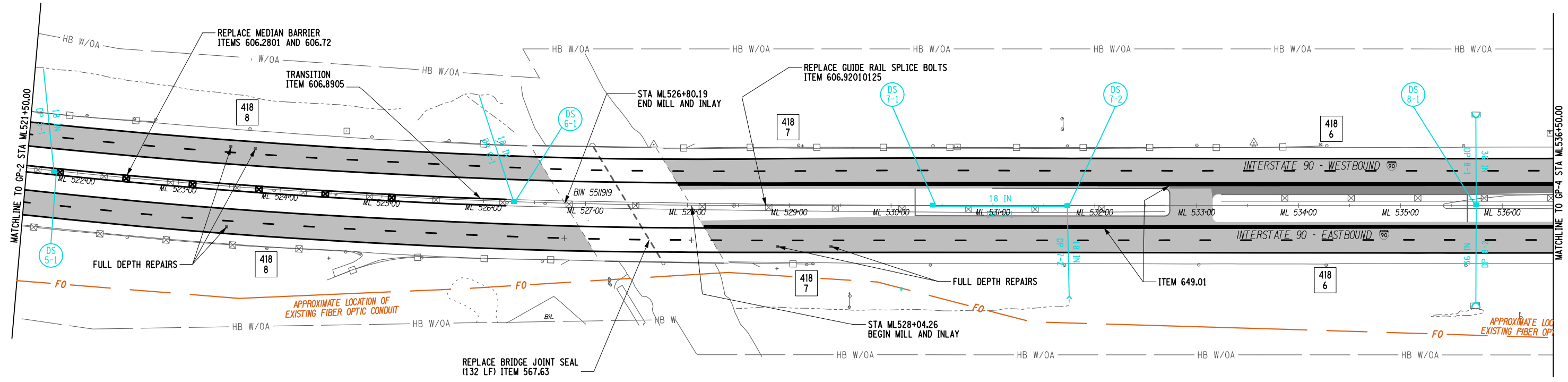
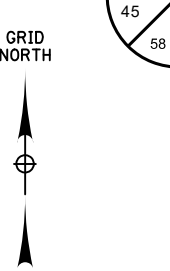
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



Thruway
Authority

TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING GENERAL PLAN	DRAWING NUMBER: GP-2



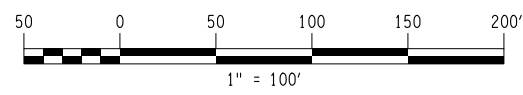
ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES:
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	606.8905	TRANSITION - HBPO (MOD.) CORRUGATED BEAM MEDIAN BARRIER TO WEAK POST COR.	EACH	1. FULL DEPTH REPAIRS ARE NOT TO SCALE, SEE MST-1 FOR ASPHALT REPAIR LOCATIONS.
203.24000025	SHOULDER BACKUP MATERIAL	TON	606.92010125	REPLACE GUIDE RAIL SPLICE BOLTS	EACH	
304.12	SUBBASE COURSE, TYPE 2	CY	621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF	
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON	621.04	CLEANING DRAINAGE STRUCTURES	LF	
404.1979	19 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION	TON	627.5014 08	CUTTING PAVEMENT	LF	
404.3779	37.5 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION	TON	635.04030225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF	
407.010400009	NON-TRACKING TACK COAT	GAL	649.01	MILLED-IN AUDIBLE ROADWAY DELINEATORS (MILARDS)	LF	
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF	685.1106 25	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
490.10	PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE	LF	685.1206 25	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
490.30	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	SY			LF	
567.63	BRIDGE JOINT SEAL	LF			LF	
606.2801	HBPO (MOD.) CORRUGATED BEAM MEDIAN BARRIER	LF			LF	
606.72	REMOVING AND DISPOSING CORRUGATED BEAM MEDIAN BARRIER	LF			LF	

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AFFIXED ON: 08/21/2025

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- PRODUCTION MILL AND INLAY
- MISCELLANEOUS MILL AND INLAY



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK

LOCATION OF PROJECT
MP 416.90 TO MP 419.40

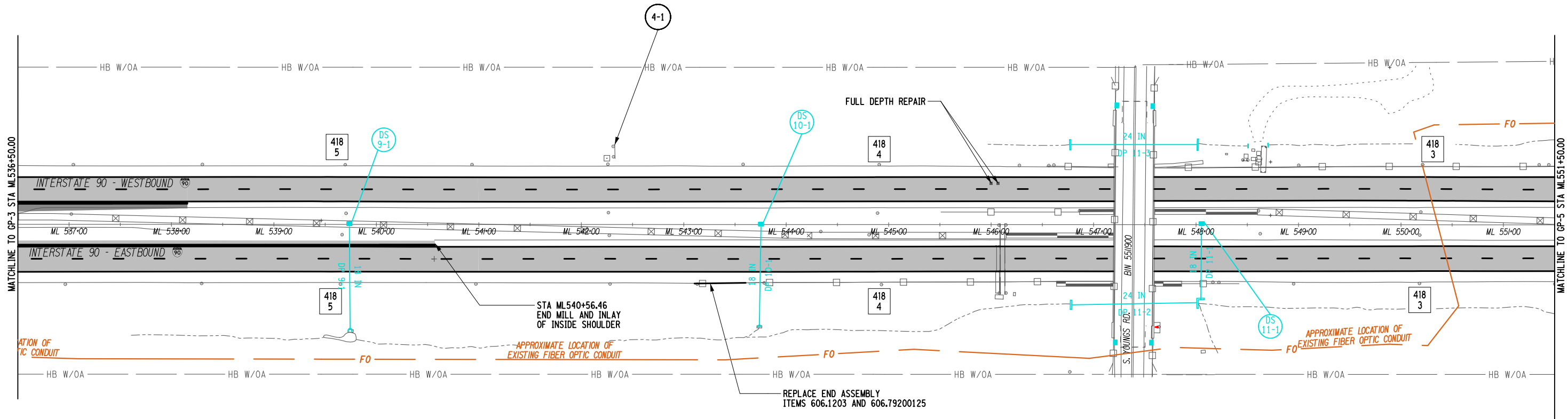
TITLE OF DRAWING
GENERAL PLAN

CONTRACT NUMBER:
TAB 25-31

DATE:
08/20/2025

DRAWING NUMBER:
GP-3

GRID NORTH



ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES:
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	627.5014	CUTTING PAVEMENT	LF	1. FULL DEPTH REPAIRS ARE NOT TO SCALE, SEE MST-1 FOR ASPHALT REPAIR LOCATIONS.
203.24000025	SHOULDER BACKUP MATERIAL	TON	635.04030225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF	
304.12	SUBBASE COURSE, TYPE 2	CY	649.01	MILLED-IN AUDIBLE ROADWAY DELINEATORS (MARDS)	LF	
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON	685.1707	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
404.1979	37.5 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION	TON	685.1708	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
404.3779	NON-TRACKING TACK COAT	TON				
407.01040009	ASPHALT PAVEMENT JOINT ADHESIVE	GAL				
418.7603	PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE	LF				
490.10	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	SY				
490.30	BOX BEAM END ASSEMBLY, TYPE III	SY				
606.1203	REMOVE AND DISPOSE END TERMINAL FOR BOX BEAM GUIDE RAIL AND MEDIAN BA.	EACH				
606.79200125	CLEANING CLOSED DRAINAGE SYSTEMS	EACH				
621.03	CLEANING DRAINAGE STRUCTURES	LF				
621.04		EACH				

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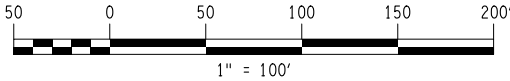
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- PRODUCTION MILL AND INLAY
- MISCELLANEOUS MILL AND INLAY



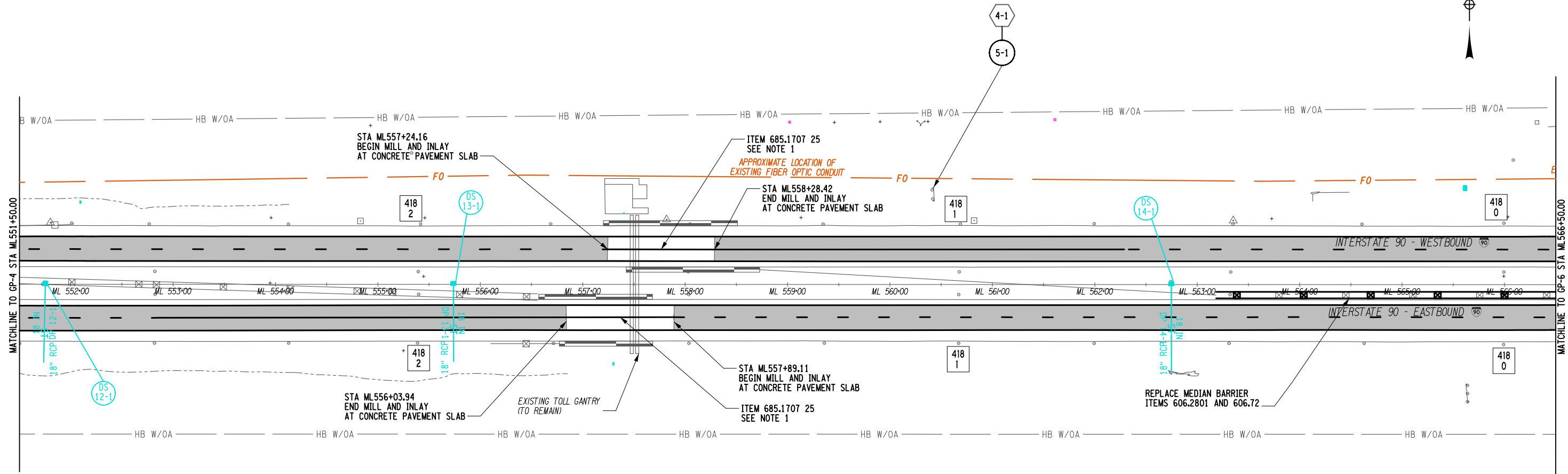
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



Thruway Authority

TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING GENERAL PLAN	DRAWING NUMBER: GP-4



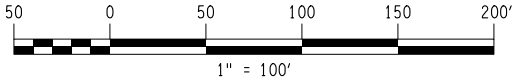
ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES:
203.24000025	SHOULDER BACKUP MATERIAL	TON	635.04030225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF	1. CONCRETE SECTION UNDER TOLL GANTRY IS ONLY TO BE STRIPED NOT MILLED OR RECESSED. SOLID WHITE PAVEMENT STRIPE ON CONCRETE SECTION UNDER TOLL GANTRY IS APPROXIMATELY 105'. THE SOLID WHITE PAVEMENT STRIPE SHALL BE EXTENDED 400' PRIOR TO THE CONCRETE SECTION WITH RECESSING.
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON	685.1707 25	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
407.01040009	NON-TRACKING TACK COAT	GAL	685.1708 25	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF			---	2. REFER TO SPECIAL NOTES IN THE PROPOSAL FOR WORKING NEAR AND UNDER TOLL GANTRIES.
490.10	PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE	SY			---	
606.2801	HPBO (MOD.) CORRUGATED BEAM MEDIAN BARRIER	LF			---	
606.72	REMOVING AND DISPOSING CORRUGATED BEAM MEDIAN BARRIER	LF			---	3. EXISTING ALIGNMENT PUCKS IN ASPHALT PAVEMENT NEAR GANTRY WILL BE REMOVED DURING MILLING. CONTACT NYSTA ITSM REPRESENTATIVE ROGER SHEPARD 2 WEEKS PRIOR TO MILLING. NYSTA WILL BE RELOCATING THE PUCKS TO CONCRETE PAVEMENT.
621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF			---	
621.04	CLEANING DRAINAGE STRUCTURES	EACH			---	

ALTERED ON:

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STAMP:

- PRODUCTION MILL AND INLAY
- MISCELLANEOUS MILL AND INLAY



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK

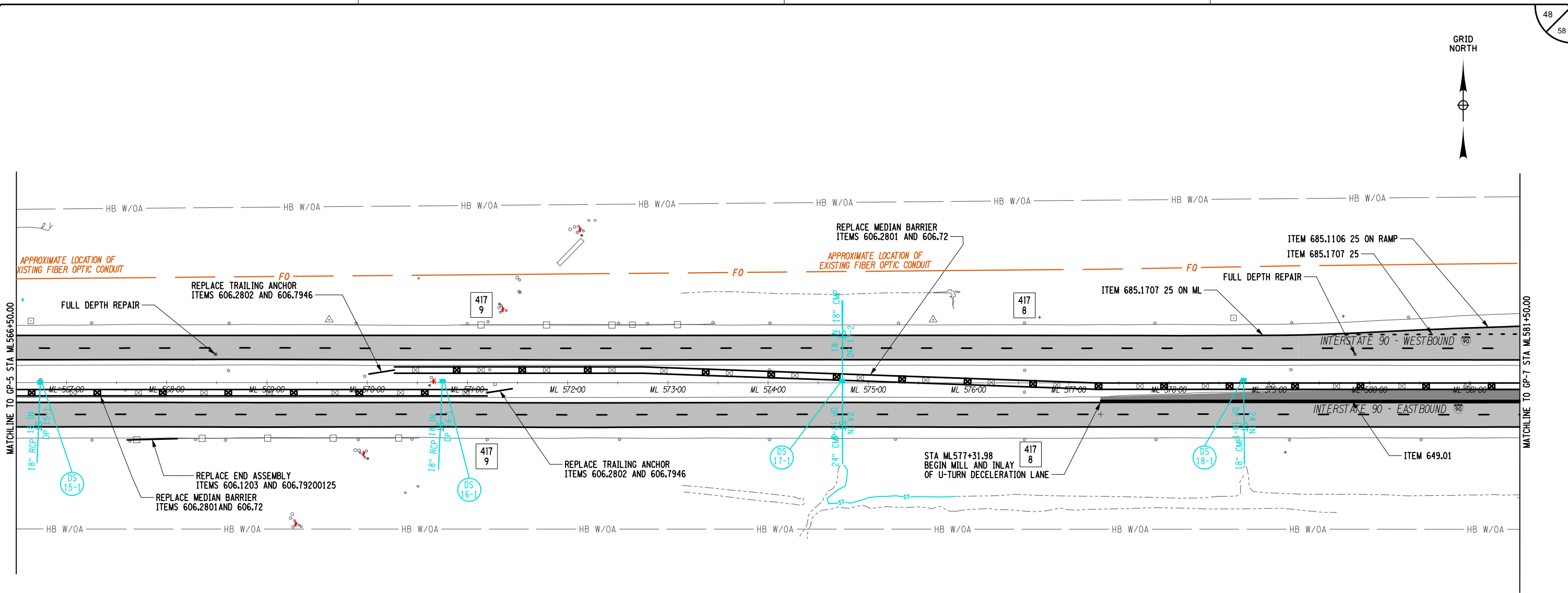
LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
GENERAL PLAN

CONTRACT NUMBER:
TAB 25-31

DATE:
08/20/2025

DRAWING NUMBER:
GP-5



ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES:
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	606.2802	ANCHORAGE UNIT FOR HPBO (MOD.) CORRUGATED BEAM MEDIAN BARRIER	EACH	1. FULL DEPTH REPAIRS ARE NOT TO SCALE, SEE MST-1 FOR ASPHALT REPAIR LOCATIONS.
203.24000025	SHOULDER BACKUP MATERIAL	TON	606.72	REMOVING AND DISPOSING CORRUGATED BEAM MEDIAN BARRIER	LF	
304.12	SUBBASE COURSE TYPE 2	CY	606.79200125	REMOVE AND DISPOSE END TERMINAL FOR BOX BEAM GUIDE RAIL AND MEDIAN BA.	EACH	
404.0952	9.5 F2 TOP COURSE ASPHALT, 70 SERIES COMPACTION	TON	606.7946	REMOVE AND DISPOSE ANCHORAGE UNIT FOR HPBO (MOD.) CORRUGATED	LF	
404.0972	9.5 F2 TOP COURSE ASPHALT, 70 SERIES COMPACTION	TON	621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF	
404.1979	19 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION	TON	621.04	CLEANING DRAINAGE STRUCTURES	EACH	
404.3779	37.5 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION	TON	635.04030225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF	
407.01040009	NON-TRACKING TACK COAT	GAL	649.01	MILLED-IN AUDIBLE ROADWAY DELINEATORS (MIARDS)	LF	
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF	627.5014 08	CUTTING PAVEMENT	LF	
490.10	PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE	SY	685.1106 25	WHITE EPOXY PAVEMENT STRIPES 6" x 20 MILS	LF	
490.15	PRODUCTION COLD MILL SURFACE PLANING OF BITUMINOUS CONCRETE	SY	685.1707 25	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
490.30	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	SY	685.1708 25	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
606.1203	BOX BEAM END ASSEMBLY TYPE III	EACH				
606.2801	HPBO (MOD.) CORRUGATED BEAM MEDIAN BARRIER	LF				

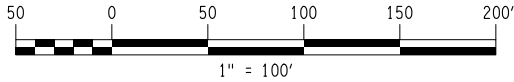
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AFFIXED ON: 08/21/2025

SIGNATURE:
STAMP:

STATE OF NEW YORK
KEITH R. GAWRONSKI
089722
LICENSED PROFESSIONAL ENGINEER

- PRODUCTION MILL AND INLAY
- MISCELLANEOUS MILL AND INLAY



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DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE
Thruway Authority

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK

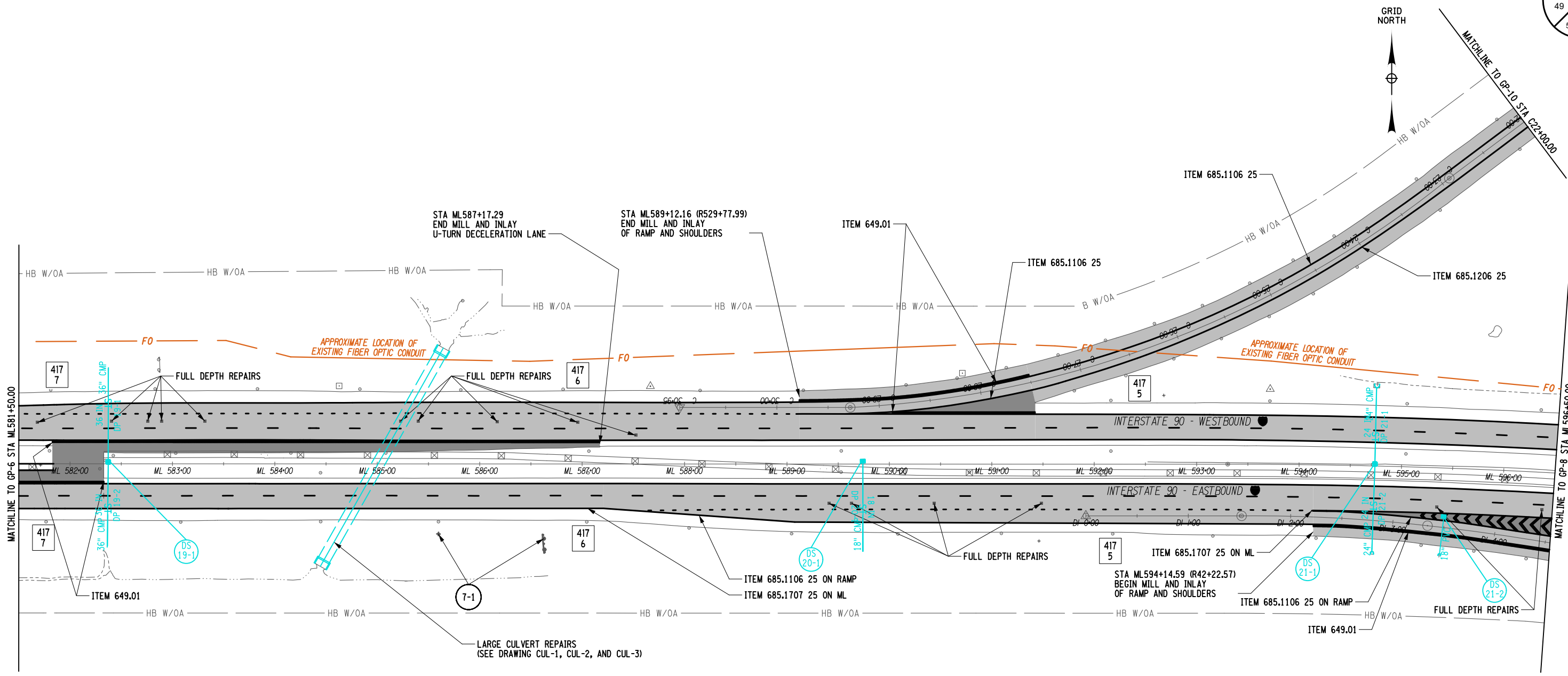
LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
GENERAL PLAN

CONTRACT NUMBER:
TAB 25-31

DATE:
08/20/2025

DRAWING NUMBER:
GP-6



ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES:
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	553.030001	TEMPORARY WATERWAY DIVERSION STRUCTURE	EACH	685.1707 25 - WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES - LF 685.1708 25 - YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES - LF 1. FULL DEPTH REPAIRS ARE NOT TO SCALE, SEE MST-1 FOR ASPHALT REPAIR LOCATIONS.
203.24000025	SHOULDER BACKUP MATERIAL	TON	556.0202	EPOXY COATED BAR REINFORCEMENT FOR STRUCTURES	LB	
203.61000X25	TEMPORARY HAUL ROAD (S)	LS	582.0051	REMOVAL AND REPLACEMENT OF STRUCTURAL CONCRETE	CY	
209.13	SILT FENCE - TEMPORARY	LF	617.10 24	DISPOSAL OF MATERIAL CONTAINING INVASIVE PLANT SPECIES	CY	
304.12	SUBBASE COURSE TYPE 2	TON	617.11 24	EQUIPMENT CLEANING FOR INVASIVE PLANT SPECIES	LS	
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON	621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF	
404.0972	9.5 F2 TOP COURSE ASPHALT, 70 SERIES COMPACTION	TON	621.04	CLEANING DRAINAGE STRUCTURES	LF	
404.1979	19 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION	TON	635.04030225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	EACH	
404.3779	37.5 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION	TON	645.81	TYPE A SIGN POSTS	EACH	
407.01040009	NON-TRACKING TACK COAT	GAL	647.31	RELOCATE SIGN PANEL, SIGN PANEL ASSEMBLY SIZE 1, (UNDER 30 SQUARE FEET)	EACH	
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF	649.01	MILLED-IN AUDIBLE ROADWAY DELINEATORS (MIARDS)	LF	
490.10	PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE	SY	627.5014 08	CUTTING PAVEMENT	LF	
490.15	PRODUCTION COLD MILL SURFACE PLANING OF BITUMINOUS CONCRETE	SY	685.1106 25	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES 6" x MILS	LF	
490.30	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	SY	685.1206 25	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES 6" x MILS	LF	

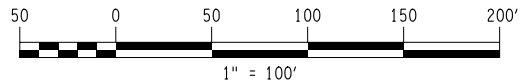
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AFFIXED ON:
09/09/2025

SIGNATURE:
STAMP:

STATE OF NEW YORK
KEITH R. GAWRONSKI
089722
LICENSED PROFESSIONAL ENGINEER

- PRODUCTION MILL AND INLAY
- MISCELLANEOUS MILL AND INLAY



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE
Thruway Authority

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK

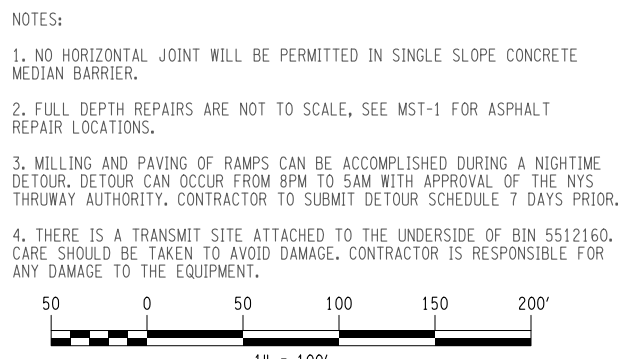
LOCATION OF PROJECT
MP 416.90 TO MP 419.40


TITLE OF DRAWING
GENERAL PLAN

CONTRACT NUMBER:
TAB 25-31

DATE:
08/20/2025

DRAWING NUMBER:
GP-7



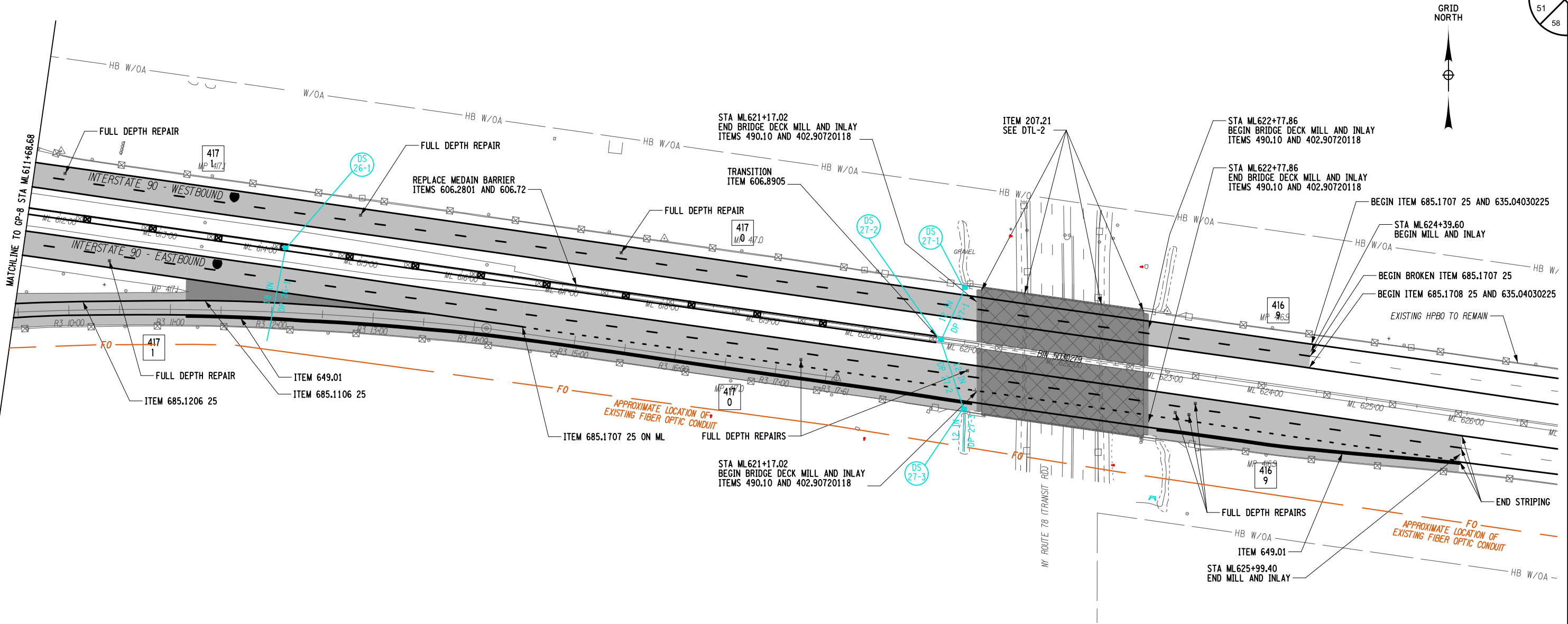
ALTERED ON:	08/21/2025
SIGNATURE: STAMP:	SIGNATURE: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING GENERAL PLAN	DRAWING NUMBER: GP-8



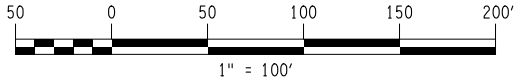
ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES:
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	606.2801	HPBO (MOD.) CORRUGATED BEAM MEDIAN BARRIER	LF	1. FULL DEPTH REPAIRS ARE NOT TO SCALE, SEE MST-1 FOR ASPHALT REPAIR LOCATIONS.
203.24000025	SHOULDER BACKUP MATERIAL	TON	606.72	REMOVING AND DISPOSING CORRUGATED BEAM MEDIAN BARRIER	LF	
207.21	GEOTEXTILE SEPARATION	SY	606.8905	TRANSITION - HPBO (MOD.) CORRUGATED BEAM MEDIAN BARRIER TO WEAK POST CO.	EACH	2. DO NOT MILL JOINT HEADERS.
304.12	SUBBASE COURSE, TYPE 2	CY	621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF	
402.90720118	WATERPROOFING BRIDGE DECK OVERLAY	TON	621.04	CLEANING DRAINAGE STRUCTURES	EACH	
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON	621.08	CUTTING PAVEMENT	LF	
404.0972	9.5 F2 TOP COURSE ASPHALT, 70 SERIES COMPACTION	TON	635.04030225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF	
404.1972	19 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION	TON	649.01	MILLED-IN AUDIBLE ROADWAY DELINEATORS (MARS)	LF	
404.3779	37.5 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION	TON	685.1106 25	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES 6" x 20 MILS	LF	
407.01040009	NON-TRACKING TACK COAT	GAL	685.1206 25	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES 6" x 20 MILS	LF	
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF	685.1707 25	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
490.10	PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE	CY	685.1708 25	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES	LF	
490.15	PRODUCTION COLD MILL SURFACE PLANING OF BITUMINOUS CONCRETE	CY				
490.30	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	CY				

ALTERED ON:

AFFIXED ON:
08/21/2025

SIGNATURE:
STAMP:

- PRODUCTION MILL AND INLAY
- MISCELLANEOUS MILL AND INLAY
- MISCELLANEOUS MILL AND WATERPROOF ASPHALT INLAY



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



Thruway
Authority

TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING GENERAL PLAN	DRAWING NUMBER: GP-9

PW/

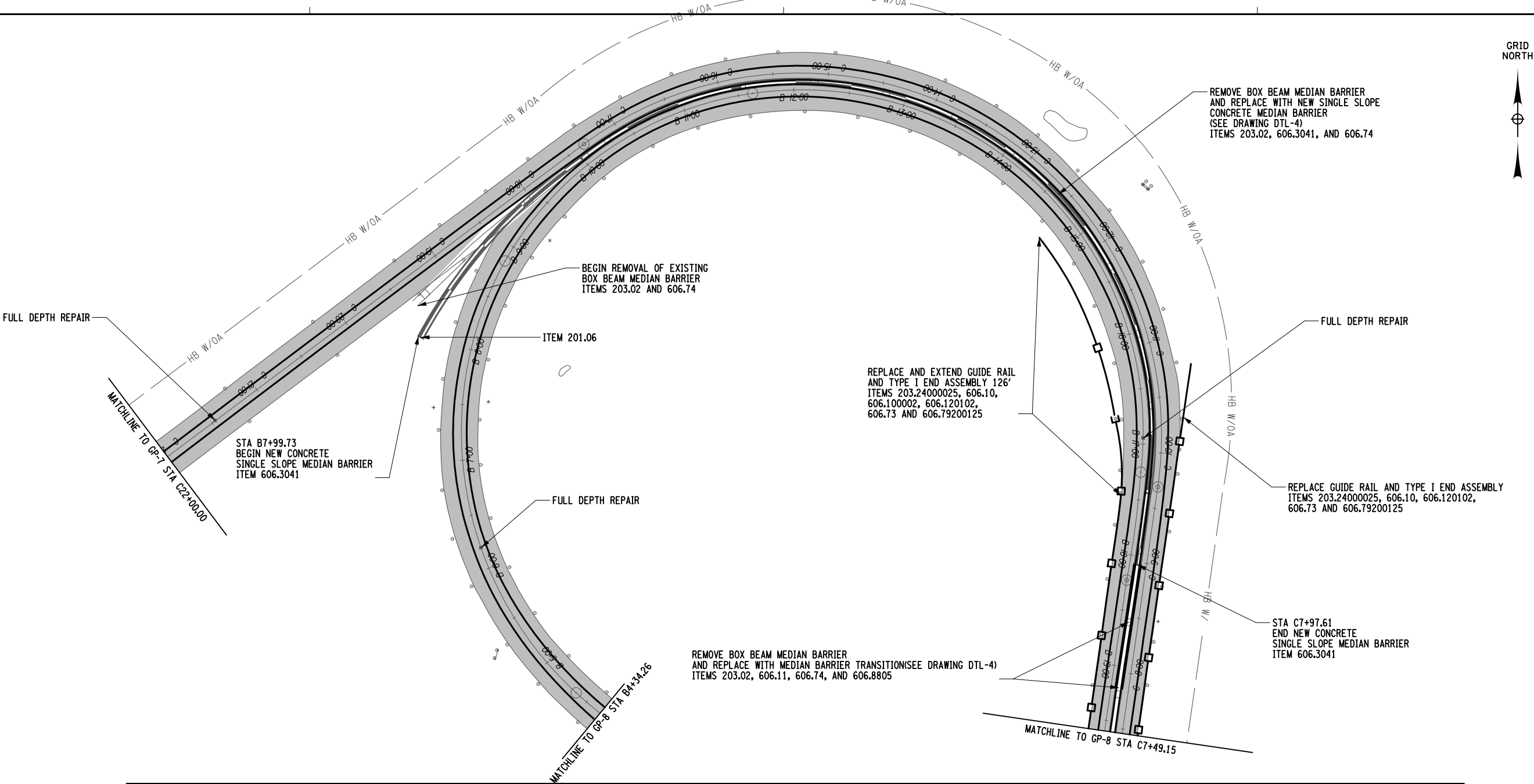
CHECKED BY: K. GAWRONSKI

DRAFTED BY: J. QUINN

CHECKED BY: K. GAWRONSKI

DESIGNED BY: J. QUINN

DESIGN SUPERVISOR: K. GAWRONSKI



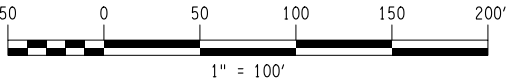
ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES:
201.06	CLEARING AND GRUBBING	LS	606.3041	SINGLE-SLOPE CONCRETE MEDIAN BARRIER (OPTIONAL)	LF	1. NO HORIZONTAL JOINT WILL BE PERMITTED IN SINGLE SLOPE CONCRETE MEDIAN BARRIER. 2. FULL DEPTH REPAIRS ARE NOT TO SCALE, SEE MST-1 FOR ASPHALT REPAIR LOCATIONS. 3. MILLING AND PAVING OF RAMPS CAN BE ACCOMPLISHED DURING A NIGHTTIME DETOUR. DETOUR CAN OCCUR FROM 8PM TO 5AM WITH APPROVAL OF THE NYS THRUWAY AUTHORITY. CONTRACTOR TO SUBMIT DETOUR SCHEDULE 7 DAYS PRIOR.
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	606.73	REMOVING AND DISPOSING BOX BEAM GUIDE RAILING	LF	
203.24000025	SHOULDER BACKUP MATERIAL	TON	604.74	REMOVING AND DISPOSING BOX BEAM MEDIAN BARRIER	LF	
304.12	SUBBASE COURSE, TYPE 2	CY	606.79200125	REMOVE AND DISPOSE END TERMINAL FOR BOX BEAM GUIDE RAIL AND MEDIAN BAR.	EACH	
404.0972	9.5 F2 TOP COURSE ASPHALT, 70 SERIES COMPACTION	TON	606.8805	TRANSITION BETWEEN BOX BEAM MEDIAN BARRIER AND SINGLE SLOPE	LF	
404.1979	19 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION	TON	627.5014 08	CUTTING PAVEMENT	LF	
404.3779	37.5 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION	TON	685.1106 25	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES 6" x 20 MILS	LF	
407.01040009	NON-TRACKING TACK COAT	GAL	685.1206 25	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES 6" x 20 MILS	LF	
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF				
490.15	PRODUCTION COLD MILL SURFACE PLANING OF BITUMINOUS CONCRETE	SY				
606.10	BOX BEAM GUIDE RAILING	LF				
606.100002	BOX BEAM GUIDE RAILING (SHOP BENT OR SHOP MITERED)	LF				
606.11	BOX BEAM MEDIAN BARRIER	LF				
606.120102	BOX BEAM GUIDE RAILING END ASSEMBLY TYPE 1	EACH				

ALTERED ON:

AFFIXED ON:
08/21/2025

SIGNATURE:
STAMP:

- PRODUCTION MILL AND INLAY
- MISCELLANEOUS MILL AND INLAY



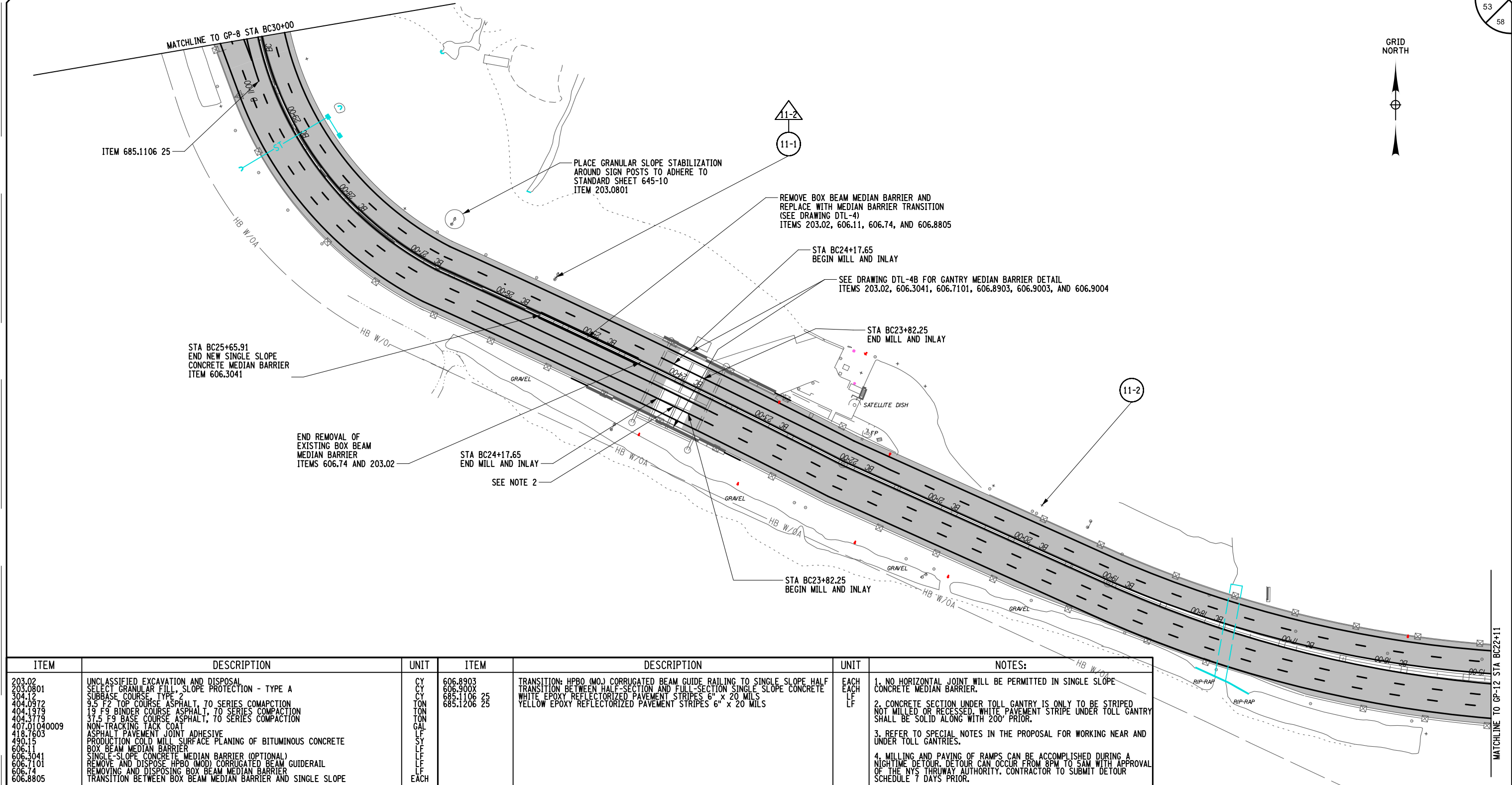
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



Thruway
Authority

TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING GENERAL PLAN	DRAWING NUMBER: GP-10



ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT	NOTES:
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	606.8903	TRANSITION: HPBO (MO) CORRUGATED BEAM GUIDE RAILING TO SINGLE SLOPE HALF	EACH	1. NO HORIZONTAL JOINT WILL BE PERMITTED IN SINGLE SLOPE CONCRETE MEDIAN BARRIER. 2. CONCRETE SECTION UNDER TOLL GANTRY IS ONLY TO BE STRIPED NOT MILLED OR RECESSED. WHITE PAVEMENT STRIPE UNDER TOLL GANTRY SHALL BE SOLID ALONG WITH 200' PRIOR. 3. REFER TO SPECIAL NOTES IN THE PROPOSAL FOR WORKING NEAR AND UNDER TOLL GANTRIES. 4. MILLING AND PAVING OF RAMPS CAN BE ACCOMPLISHED DURING A NIGHTTIME DETOUR. DETOUR CAN OCCUR FROM 8PM TO 5AM WITH APPROVAL OF THE NYS THRUWAY AUTHORITY. CONTRACTOR TO SUBMIT DETOUR SCHEDULE 7 DAYS PRIOR.
203.0801	SELECT GRANULAR FILL, SLOPE PROTECTION - TYPE A	CY	606.900X	TRANSITION BETWEEN HALF-SECTION AND FULL-SECTION SINGLE SLOPE CONCRETE	EACH	
404.12	SUBBASE COURSE, TYPE 2	CY	685.1106 25	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES 6" x 20 MILS	LF	
404.0972	9.5 F2 TOP COURSE ASPHALT, 70 SERIES COMPACTION	TON	685.1206 25	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES 6" x 20 MILS	LF	
404.1979	19 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION	TON				
404.3779	37.5 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION	TON				
407.01040009	NON-TRACKING TACK COAT	GAL				
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF				
490.15	PRODUCTION COLD MILL SURFACE PLANING OF BITUMINOUS CONCRETE	SY				
606.11	BOX BEAM MEDIAN BARRIER	LF				
606.3041	SINGLE-SLOPE CONCRETE MEDIAN BARRIER (OPTIONAL)	LF				
606.7101	REMOVE AND DISPOSE HPBO (MOD) CORRUGATED BEAM GUIDERAIL	LF				
606.74	REMOVING AND DISPOSING BOX BEAM MEDIAN BARRIER	LF				
606.8805	TRANSITION BETWEEN BOX BEAM MEDIAN BARRIER AND SINGLE SLOPE	EACH				

ALTERED ON:

AFFIXED ON: 08/21/2025

SIGNATURE:
STAMP:

- PRODUCTION MILL AND INLAY

- MISCELLANEOUS MILL AND INLAY

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK

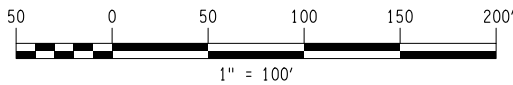
LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
GENERAL PLAN

CONTRACT NUMBER:
TAB 25-31

DATE:
08/20/2025

DRAWING NUMBER:
GP-11





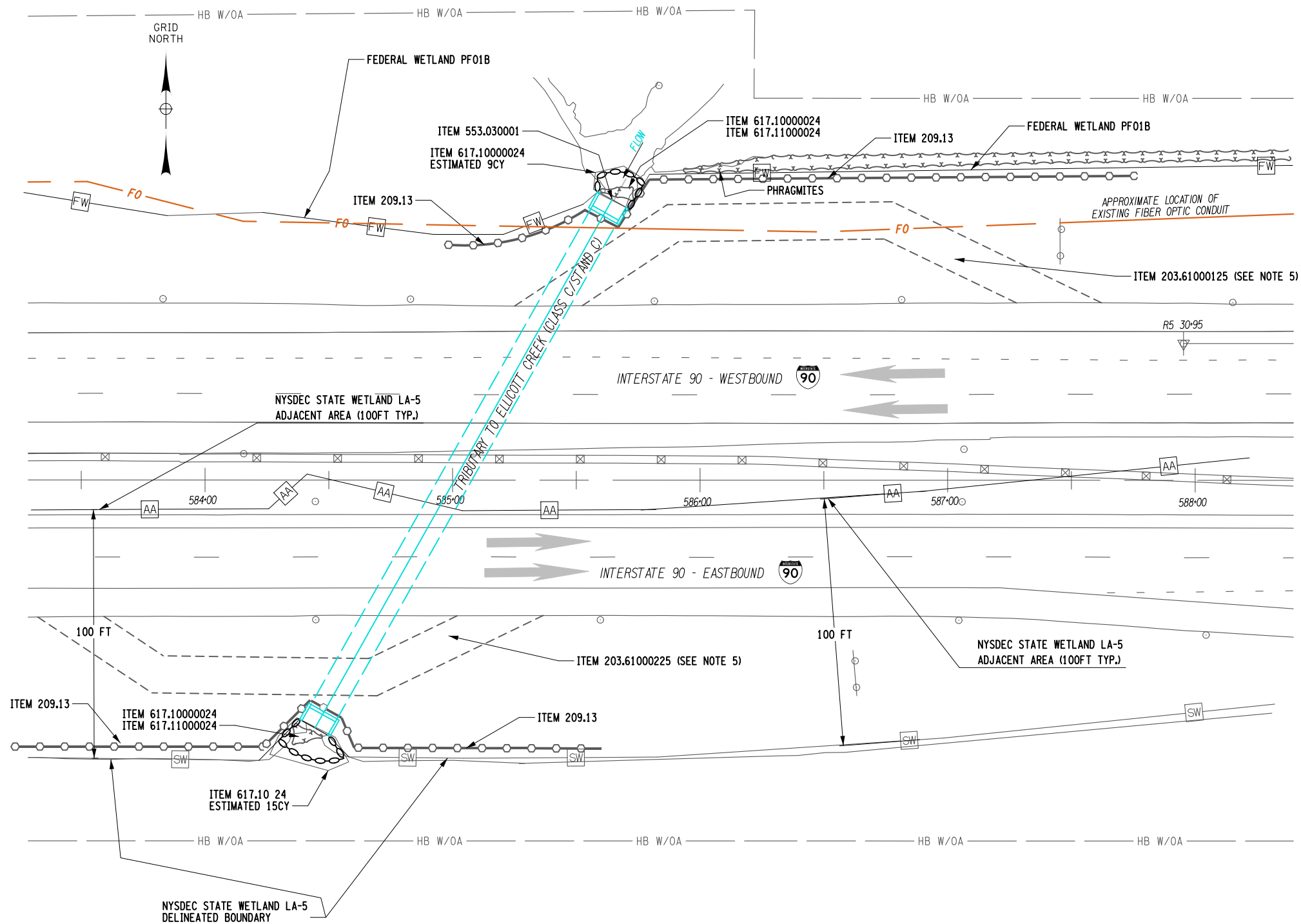
1. MILLING AND PAVING OF RAMPS CAN BE ACCOMPLISHED DURING A NIGHTTIME DETOUR. DETOUR CAN OCCUR FROM 8PM TO 5AM WITH APPROVAL OF THE NYS THRUWAY AUTHORITY. CONTRACTOR TO SUBMIT DETOUR SCHEDULE 7 DAYS PRIOR

☐ - PRODUCTION MILL AND INLAY

☐ - MISCELLANEOUS MILL AND INLAY



DRAWING NUMBER:
GP-12



FIBER OPTIC NOTE:

THE CONTRACTOR IS TO USE EXTREME CAUTION WITHIN FIVE FEET IN PROXIMITY TO WHERE THE EXISTING FIBER OPTIC LINE MAY TRAVERSE ABOVE OR BELOW THE EXISTING CULVERT OR DITCH (SEE FIBER OPTIC NOTE ON DRAWING GN-1)

NOTES:

1. REFER TO DRAWING CUL-2 AND CUL-3 FOR REPAIR DETAILS
2. WATERWAY DIVERSION SHALL BE PERFORMED IN A METHOD TO PREVENT DEWATERING OF THE AREA AT THE OUTLET OF THE CULVERT.
3. THE USE OF THE WATERWAY DIVERSION IS TEMPORARY AND IS ONLY TO FACILITATE WORK WITHIN THE CULVERT OR AT THE INLET/OUTLET. THE DIVERSION STRUCTURE SHALL REMOVED WHEN CONCRETE HAS CURED.
4. ALL LOOSE CONCRETE CHIPPED FROM THE ROOF/WALLS OF THE CULVERT TO FACILITATE REPAIRS SHALL BE REMOVED FROM THE CULVERT AND WATERWAY.
5. HAUL ROAD LOCATIONS SHOWN ON PLANS ARE FOR REFERENCE ONLY. CONTRACTOR IS TO PROPOSE LOCATION AND HAVE APPROVED BY E.I.C.
6. ACTIVITIES WITHIN WATERS OF THE US AND NYSDEC FRESHWATER WETLANDS LA-5 SHALL COMPLY WITH USACE NATIONWIDE PERMIT #3 - MAINTENANCE AND NYSDEC ARTICLE 24 GENERAL PERMIT *GP-0-21-005.

ALTERED ON:

AFFIXED ON:

08/21/2025

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STAMP:

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STAMP:



ITEM	DESCRIPTION	UNIT
203.61000X25	TEMPORARY HAUL ROAD	LS
209.13	SILT FENCE - TEMPORARY	LF
553.030001	WATERWAY DIVERSION	EA
617.1000024	DISPOSAL OF MATERIAL CONTAINING INVASIVE PLANTS	CY
617.11000024	EQUIPMENT CLEANING FOR INVASIVE PLANT SPECIES	LS

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REVISIONS

DATE	DESCRIPTION	BY	SYMBOL



**Thruway
Authority**

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK

LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
CIN 417.63011XX
CULVERT REPAIR PLAN

CONTRACT NUMBER:

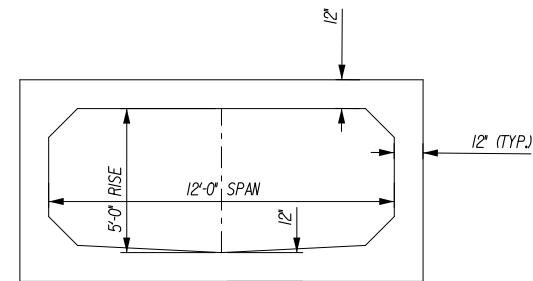
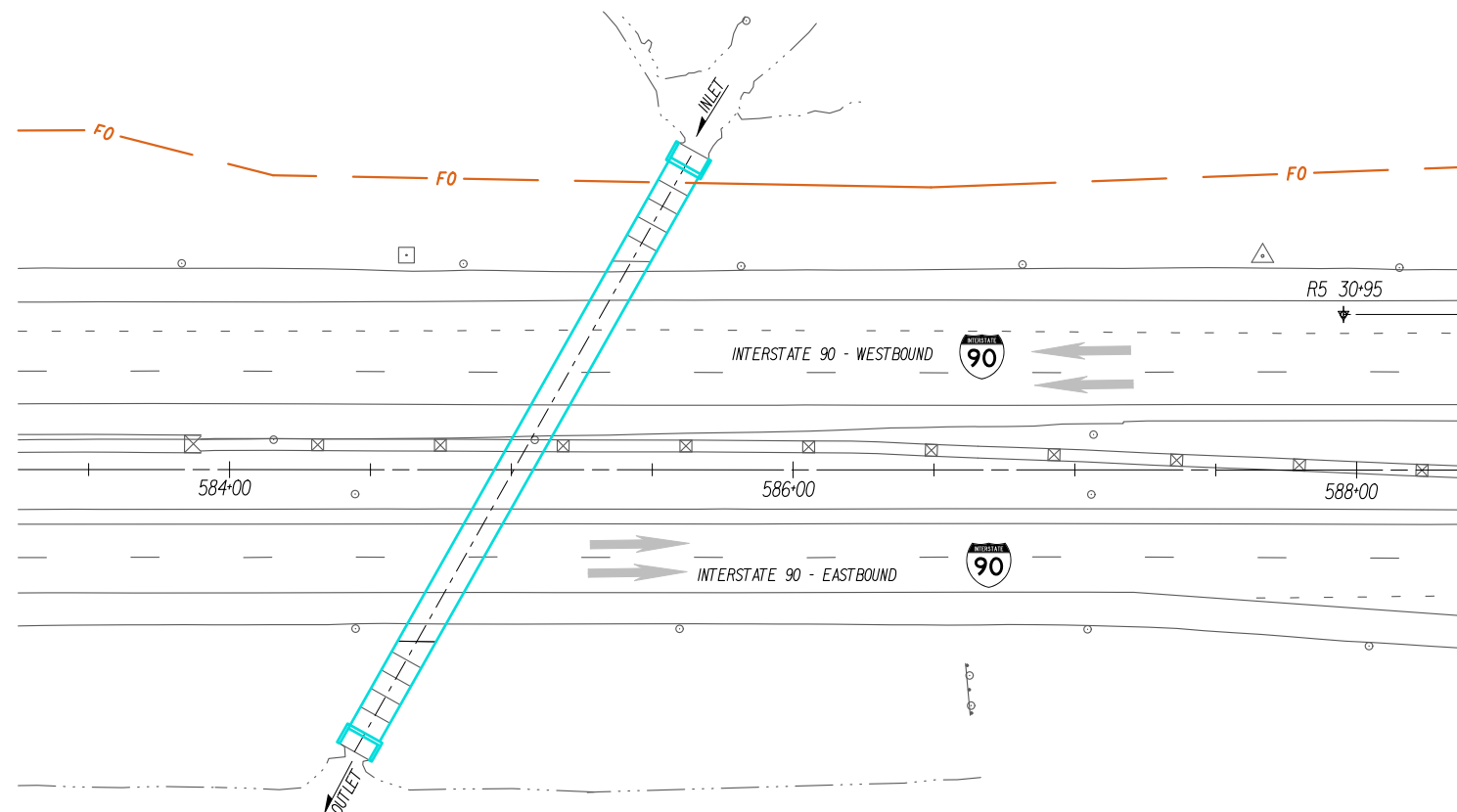
TAB 25-31

DATE:

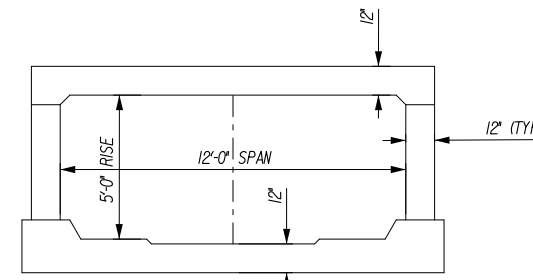
08/20/2025

DRAWING NUMBER:

CUL-1

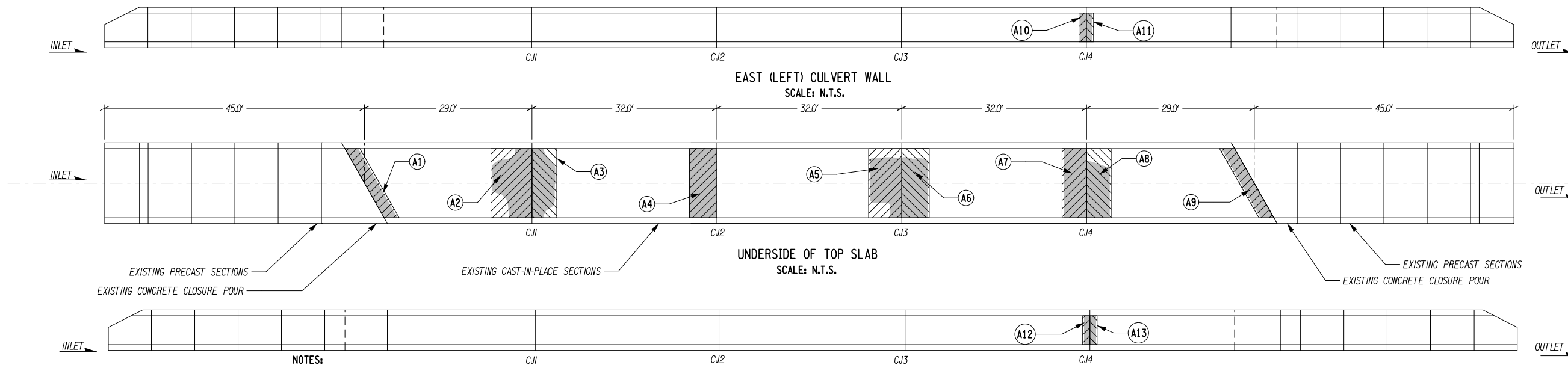


EXISTING PRECAST ELEVATION



EXISTING CAST IN PLACE ELEVATION

ITEM 582.0051 (CY)				
REMOVAL AND REPLACEMENT OF STRUCTURAL CONCRETE				
DESCRIPTION / LOCATION	LENGTH (FT)	DEPTH (FT)	WIDTH (FT)	CY
A1	13.78	0.25	2.25	0.29
A2	12.00	0.25	7.13	0.79
A3	12.00	0.25	4.25	0.47
A4	12.00	0.25	4.50	0.50
A5	12.00	0.25	5.75	0.64
A6	12.00	0.25	4.50	0.50
A7	12.00	0.25	4.00	0.44
A8	12.00	0.25	4.25	0.47
A9	13.78	0.25	2.25	0.29
A10	5.00	0.25	1.25	0.06
A11	5.00	0.25	1.25	0.06
A12	5.00	0.25	1.25	0.06
A13	5.00	0.25	1.25	0.06
TOTAL:				5 CY



NOTES:

- ALL REPAIR LIMITS SHOWN ARE APPROXIMATE AND SHALL BE CONFIRMED BY PHYSICAL SOUNDING AND LIMITS SHALL BE APPROVED BY E.I.C. PRIOR TO CONCRETE REMOVAL.
- BEFORE REMOVAL OF CONCRETE BEGINS, CONTRACTOR SHALL SUPPLY REMOVAL/DEMO PLAN TO E.I.C. FOR APPROVAL. APPROVAL SHALL BE ACCOMPANIED BY STAMPED ENGINEERED REMOVAL PLANS SUPPLIED BY CONTRACTOR AND INCLUDED UNDER ITEM 582.0051.
- DETERIORATED REINFORCEMENT SHALL BE REPLACED OF LIKE SIZED AND UNCOATED REINFORCEMENT (ITEM 556.0201), A MINIMUM LAP OF 30 BAR DIMAETERS OR A WELDED SPLICE SHALL BE PROVIDED. (SEE WELDED SPLICE DETAILS ON DTL-2) IF WELDED, WELDERS SHALL BE QUALIFIED IN ACCORDANCE WITH THE "NEW YORK STATE STEEL CONSTRUCTION MANUAL".
- CONCRETE SURFACE REPAIR DETAILS CAN BE FOUND ON DRAWING DTL-2.

WEST (RIGHT) CULVERT WALL
SCALE: N.T.S.

LEGEND

- EXISTING SPALLED AREAS
- XX - PROPOSED REPAIR LOCATION DESIGNATION
- PROPOSED REPAIR LIMITS
ITEM 582.0051

ITEM	DESCRIPTION	UNIT
556.0202	EPOXY COATED BAR REINFORCEMENT FOR STRUCTURES	LB
582.0051	REMOVAL AND REPLACEMENT OF STRUCTURAL CONCRETE	CY

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

Thruway
Authority

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1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK

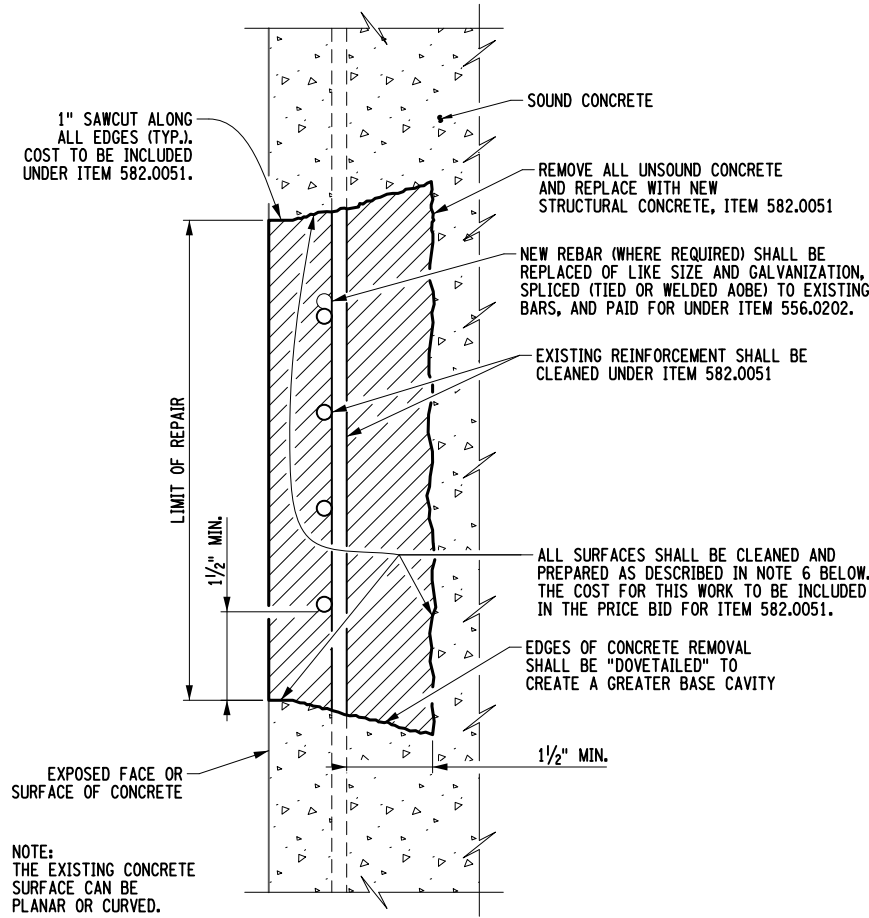
LOCATION OF PROJECT
MP 416.90 TO MP 419.40

TITLE OF DRAWING
CIN 417.63011XX
CULVERT REPAIR DETAILS

CONTRACT NUMBER:
TAB 25-31

DATE:
08/20/2025

DRAWING NUMBER:
CUL-2

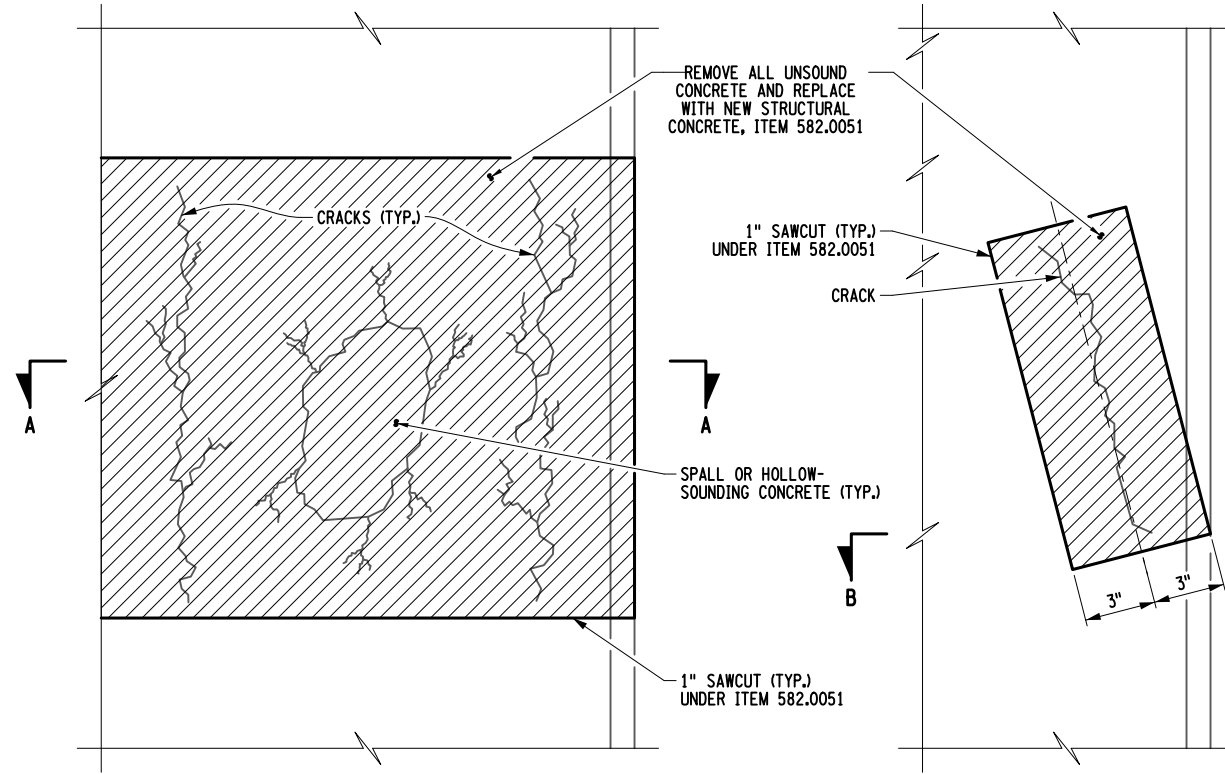


TYPICAL CONCRETE SURFACE REPAIR
SECTION
SCALE: NTS

CONCRETE REPAIR NOTES:

- FOR GENERAL NOTES, SEE DRAWING ST-1.
- FOR LOCATION OF CONCRETE REPAIRS, SEE DRAWINGS CUL-1 THROUGH CUL-2.
- SUBSTRUCTURE DETERIORATION REQUIRING REPAIR AS INDICATED ON THE PLANS HAS BEEN DETERMINED BY A FIELD INSPECTION. ALL OF THE MAJOR AREAS KNOWN TO EXIST AT THE TIME HAVE BEEN SHOWN TO INDICATE THE APPROXIMATE EXTENT OF DETERIORATION THAT WILL HAVE TO BE REPAIRED BY THE CONTRACTOR.
- THE ANTICIPATED EXTENT OF CONCRETE REPAIR HAS BEEN INDICATED ON THE CONTRACT PLANS. THE PROJECT ENGINEER SHALL EXAMINE THE SUBSTRUCTURE CONCRETE AND ESTABLISH THOSE AREAS OF REPAIR TO BE MADE WITH NEW CONCRETE. THE EXTENT OF CONCRETE REPAIR TO BE MADE SHALL BE APPROVED BY THE PROJECT ENGINEER.
- DETERIORATED REINFORCEMENT SHALL BE REPLACED OF LIKE SIZE AND COATING (ITEM 556.0202), AND A MINIMUM LAP OF 30 BAR DIAMETERS OR A WELDED SPICE SHALL BE PROVIDED. (SEE WELDED SPICE DETAILS ON THIS DRAWING.) IF WELDED, WELDERS SHALL BE QUALIFIED IN ACCORDANCE WITH THE "NEW YORK STATE STEEL CONSTRUCTION MANUAL."
- WHEN JOINING FRESH CONCRETE TO NEW CONCRETE WHICH HAS ALREADY BEEN SET, OR TO EXISTING CONCRETE, THE CONCRETE IN PLACE SHALL HAVE ITS SURFACE SCOURED OR ABRADED WITH A SUITABLE TOOL TO REMOVE ALL LOOSE AND FOREIGN MATERIALS. THIS SURFACE SHALL BE SCRUBBED WITH WIRE BROOMS. AFTER THE SURFACE PREPARATION HAS BEEN ACCEPTED, THIS SURFACE SHALL BE THOROUGHLY WET AND KEPT SO FOR A PERIOD OF 12 HOURS IMMEDIATELY PRIOR TO PLACING THE NEW CONCRETE. THIS MAY BE ACCOMPLISHED BY CONTINUOUS WETTING WITH SOAKER HOSES OR THE USE OF BURLAP/BURLENE/ETC. SO THAT MOISTURE CAN BE MAINTAINED. IF, IN THE OPINION OF THE ENGINEER, CONDITIONS OR THE SITUATION PROHIBITS THIS, THEN THE SURFACE SHALL BE WETTED FOR AS LONG AS POSSIBLE. THE CONTRACTOR SHALL REMOVE ANY PUDDLES OR FREE-STANDING WATER WITH OIL-FREE COMPRESSED AIR AND PROTECT THE SURFACE FROM DRYING SO THAT THE EXISTING CONCRETE REMAINS IN A CLEAN, SATURATED, SURFACE-DRY CONDITION UNTIL PLACEMENT OF THE NEW CONCRETE. IMMEDIATELY BEFORE PLACING THE NEW CONCRETE, THE FORMS SHALL BE DRAWN TIGHT AGAINST THE CONCRETE ALREADY IN PLACE.
- AT ALL CONCRETE REMOVAL LOCATIONS, THE MINIMUM DEPTH OF REMOVAL SHALL BE NO LESS THAN 1/2" BEHIND THE INNERMOST LAYER OF EXPOSED REINFORCING BARS, OR TO SOUND CONCRETE, WHICHEVER IS GREATER.
- WHERE CONCRETE REMOVAL AND REPLACEMENT NECESSITATES ADJACENT SEPARATE CONCRETE PLACEMENTS, CONCRETE REMOVAL SHALL NOT BE ALLOWED WITHIN 1 FOOT OF ADJACENT REPAIR AREAS. ADJACENT AREAS WHERE THIS 1 FOOT BUFFER IS NOT ATTAINABLE SHALL BE COMBINED INTO A SINGLE REPAIR AREA.
- IN SEQUENCING WORK, THE CONTRACTOR SHALL NOT BEGIN CONCRETE REMOVAL AT A LOCATION ADJACENT TO A COMPLETED REPAIR AREA UNTIL A MINIMUM OF 7 CURING DAYS HAVE PASSED.
- A PROTECTIVE OR PENETRATING SEALER FOR CONCRETE SHALL BE APPLIED TO ALL PROPOSED SUBSTRUCTURE CONCRETE SURFACE REPAIRS AND NO MORE THAN 2' BEYOND THE EXTENT OF THE REPAIRS (ITEM 559.02).
- THE EXTENT OF CONCRETE REMOVAL FOR ANY COMPONENT SHALL NOT JEOPARDIZE ITS STRUCTURAL INTEGRITY. THE CONTRACTOR SHALL SUBMIT, PRIOR TO COMMENCEMENT OF THIS WORK, A REMOVAL PLAN AND SCHEDULE FOR PROJECT ENGINEER APPROVAL.
- ALTHOUGH THE DEPTH OF REMOVAL TO SOUND CONCRETE MAY, AT SOME LOCATIONS, EXTEND TO DEPTHS GREATER THAN 5 INCHES, IT IS THE INTENT OF THIS PROJECT THAT THE CONTRACTOR SHALL MAKE ALL NECESSARY REPAIRS WITH NEW CONCRETE PER ITEM 582.0051.

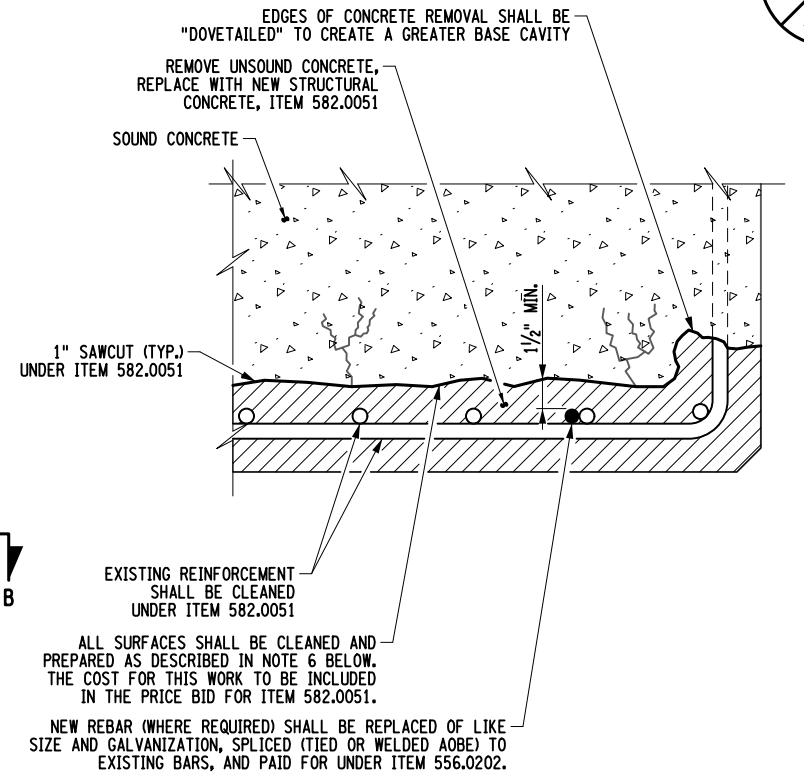
ITEM	DESCRIPTION	UNIT
556.0202	EPOXY COATED BAR REINFORCEMENT FOR STRUCTURES	LB
559.02	PROTECTIVE SEALING OF STRUCTURAL CONCRETE	SF
582.0051	REMOVAL AND REPLACEMENT OF STRUCTURAL CONCRETE	CY



CONCRETE DETERIORATION REPAIR

CONCRETE SURFACE REPAIR
SCALE: NTS

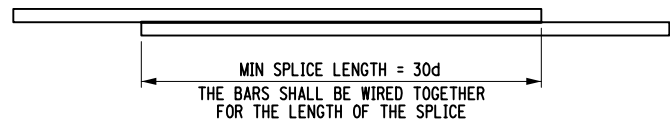
CONCRETE CRACK REPAIR



SECTION A-A
SECTION B-B SIMILAR
SCALE: NTS

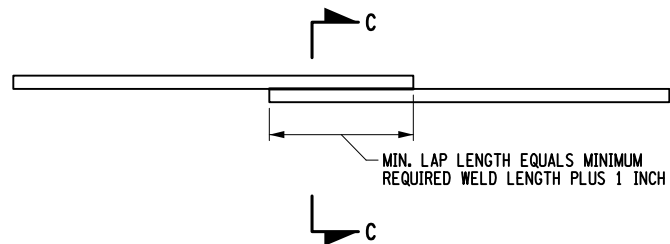
MINIMUM WELD LENGTHS

BAR SIZE	SINGLE FLARE L1 V-GROOVE WELD	DOUBLE FLARE L2 V-GROOVE WELD
5	4"	2 1/2"
6	4 1/2"	2 3/4"
7	5"	3"
8	5 1/2"	3 1/4"
9	6"	3 1/2"

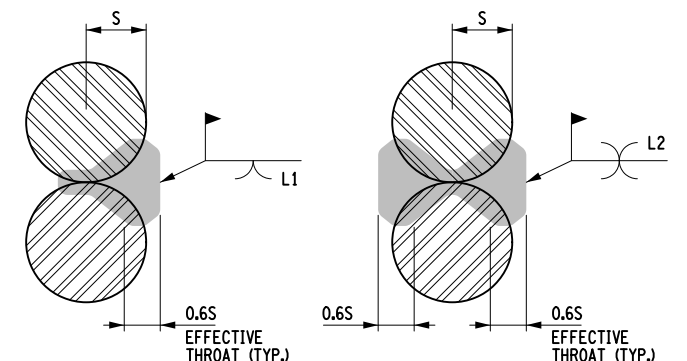


TIED SPICE LAP
PLAN
SCALE: NTS

d = BAR SIZE/DIAMETER



WELDED SPICE LAP
PLAN
SCALE: NTS



SINGLE FLARE V-GROOVE WELD **DOUBLE FLARE V-GROOVE WELD**

SECTION C-C
SCALE: NTS

S = REINFORCING BAR NOMINAL RADIUS (TYP.)

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



Thruway Authority

TITLE OF PROJECT 1R MILL AND INLAY, SAFETY IMPROVEMENTS, AND MISC. WORK	CONTRACT NUMBER: TAB 25-31
LOCATION OF PROJECT MP 416.90 TO MP 419.40	DATE: 08/20/2025
TITLE OF DRAWING CIN 417.63011XX CONCRETE REPAIR DETAILS	DRAWING NUMBER: CUL-3

SIGNING NOTES:

1. SIGN LOCATIONS AS SHOWN ON PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL INSTALL NEW SIGNS AND RELOCATE EXISTING SIGNS IN ACCORDANCE WITH THE MUTCD AND NYS SUPPLEMENT.
2. THE COLOR IS ONLY SHOWN WHEN THERE IS AN OPTION THAT MUST BE SPECIFIED.
3. THE AREA AND PAYMENT AREA FOR SIGNS ARE FROM THE APPLICABLE STANDARD SHEETS OR SIGN FACE LAYOUTS.

SIGN REMOVAL			
MP	SIDE	DESCRIPTION	ITEM NUMBER
418.10	WB	REMOVE TYPE B SIGN POSTS AND SIGN. NEW AMHERST TONAWANDA SIGN AT MP 418.46.	645.51000125 645.830202 647.52 647.65

SIGNING SUMMARY TABLE (PANELS AND POSTS)	
ITEM NUMBER	QUANTITY
645.51000125	765 SF
645.81	1 EA
645.8106	4 EA
645.830202	2 EA
645.830402	2 EA
645.830702	4 EA
647.31	1 EA
647.32	2 EA
647.52	1 EA
647.65	2 EA

DESIGNATION & COLOR (SEE NOTE 2)	LOCATION	TEXT	ITEM	SIZE	PAYMENT AREA (SEE NOTE 3)
				AREA (SEE NOTE 3)	TOTAL PAYMENT AREA
WHITE ON GREEN	2-1 (MP 418.87)		645.830702 645.51000125	9'-6"x2'-6" 20'-0"x13'-0"	284 SF
				23.75 SF 260 SF	
WHITE ON GREEN	5-1 (MP 418.10)		645.830702 645.51000125	9'-6"x2'-6" 20'-0"x13'-0"	284 SF
				23.75 SF 260 SF	
WHITE ON GREEN	4-1 (MP 418.46)		645.51000125 645.830202 647.52 647.65	11'-0"x7'-0"	77 SF
				77 SF	
WHITE ON GREEN	11-1		645.830402 645.51000125	12'-0"x10'-0"	120 SF
				120 SF	
M1-1, M1-1, M3-2P, M3-4P, M6-2P, M6-3P	8-1		645.8106 647.32	36"x18" (2) 36"x36" (2) 30"x21" (2)	36 SF
				4.5 SF (2) 9 SF (2) 4.375 SF (2)	
WHITE ON GREEN	7-1		645.81 647.31	36"x36"	9 SF
				9 SF	
M1-1, M1-1, M3-2P, M3-4P, M5-4P, M5-6P	11-2		645.8106 647.32	36"x18" (2) 36"x36" (2) 36"x24" (2)	39 SF
				4.5 SF (2) 9 SF (2) 6 F (2)	

ITEM	DESCRIPTION	UNIT	NOTES:
645.51000125 645.81 645.8106 645.830202 645.830402 645.830702 647.31 647.32 647.52 647.65	INSTALL GROUND-MOUNTED SIGN PANELS (AUTHORITY SUPPLIED) TYPE A SIGN POSTS HIGH CAPACITY TYPE A SIGN POSTS TYPE B SIGN POST, GALVANIZED, W6x9 SECTION, BI-DIRECTIONAL BREAKAWAY TYPE B SIGN POST, GALVANIZED, W8x15 SECTION, BI-DIRECTIONAL BREAKAWAY TYPE B SIGN POST, GALVANIZED, W12x26 SECTION, BI-DIRECTIONAL BREAKAWAY RELOCATE SIGN PANEL, SIGN PANEL ASSEMBLY SIZE I, (UNDER 30 SF) RELOCATE SIGN PANEL, SIGN PANEL ASSEMBLY SIZE II, (30-100 SF) REMOVE AND DISPOSE SIGN PANEL, SIGN PANEL ASSEMBLY SIZE II, (30-100 SF) REMOVE AND DISPOSE TYPE B GROUND MOUNTED SIGN SUPPORT AND FOUNDATION	SF EACH EACH EACH EACH EACH EACH EACH EACH EACH	1. REMOVAL AND DISPOSAL OF TYPE A SIGN POSTS SHALL BE INCLUDED IN RELOCATION ITEM. ----- ----- ----- ----- ----- ----- ----- ----- ----- -----

ALTERED ON:

AFFIXED ON: 09/09/2025

SIGNATURE:
STAMP:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT
1R MILL AND INLAY, SAFETY IMPROVEMENTS,
AND MISC. WORK
LOCATION OF PROJECT
MP 416.90 TO MP 419.40
TITLE OF DRAWING
SIGN DATA SHEET

CONTRACT NUMBER:
TAB 25-31
DATE:
08/20/25
DRAWING NUMBER:
SDS-1