



NEW YORK STATE OF OPPORTUNITY | Thruway Authority

BUFFALO DIVISION  
PLANS FOR  
REHABILITATION PAVEMENT RESTORATION  
FROM MILEPOST 900.7 TO 904.2  
AND  
REHABILITATION OF INTERCHANGES 1-5  
AND  
BRIDGE STEEL REPAIRS AND JOINT REPLACEMENTS  
IN  
ERIE COUNTY  
TAN 24-12 D214948  
274 SHEETS

TYPE OF CONSTRUCTION:

FULL DEPTH PAVEMENT REPAIR, PORTLAND CEMENT CONCRETE PAVEMENT REHABILITATION, MILL AND ASPHALT INLAY, OVERHEAD SIGN STRUCTURE CONSTRUCTION AND LIGHTING REPLACEMENT, BRIDGE STEEL REPAIRS AND JOINT REPLACEMENTS, GUIDE RAIL REPLACEMENT, PAVEMENT MARKINGS.

STANDARD SHEETS:

THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY NYS DOT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) LISTED ON DWG. GN-1 UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

THE LATEST REVISIONS OF THE NYSTA STANDARD SHEETS MAINTAINED BY THE AUTHORITY, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS. SEE GENERAL NOTES DRAWINGS FOR LISTING.

NOTES:

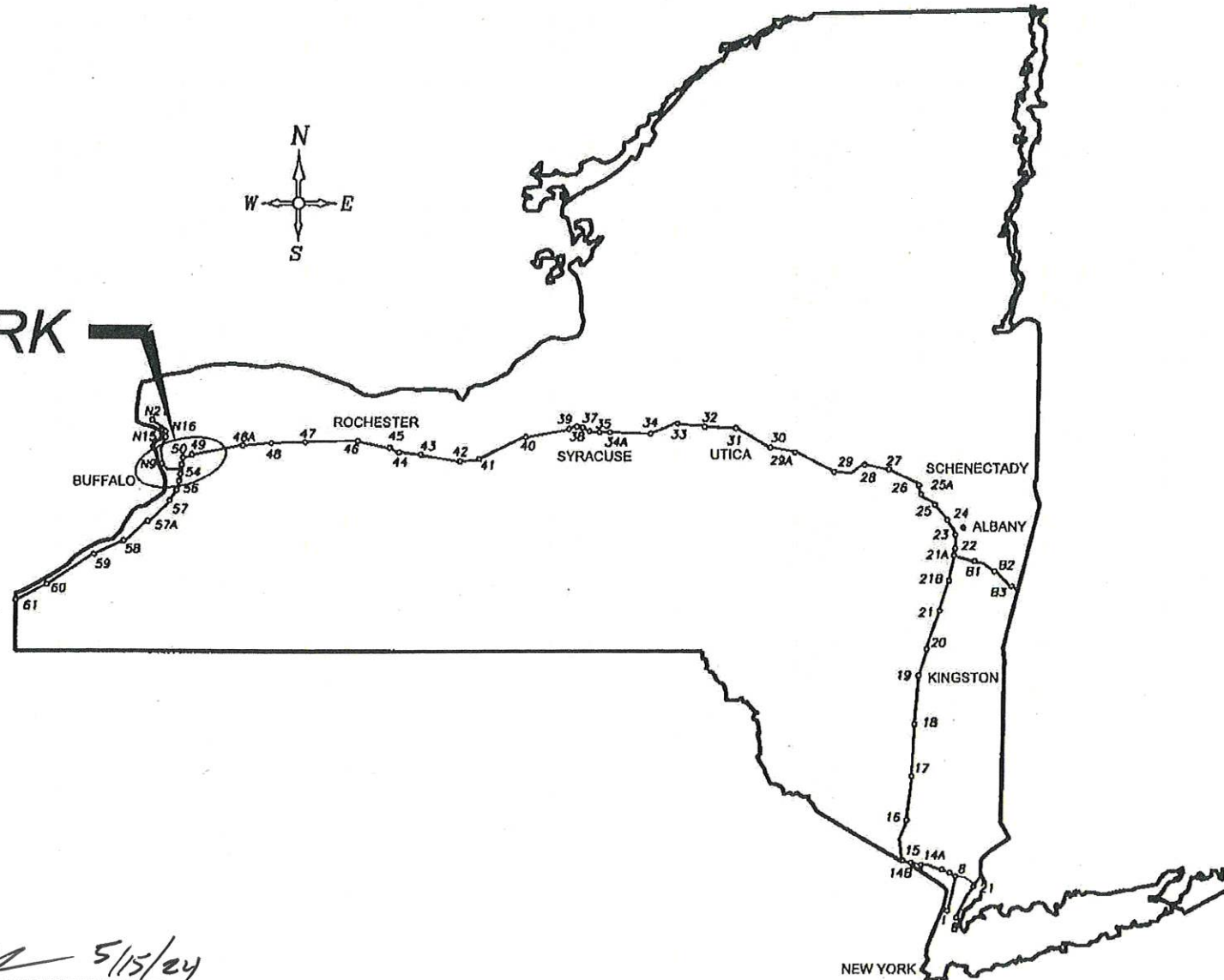
ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (US CUSTOMARY) REFERENCED IN THE CONTRACT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR CHANGES SET FORTH IN THE CONTRACT "PROPOSAL".

REVIEWED BY

PROJECT MANAGER

BUREAU DIRECTOR

SITE OF WORK



DIG SAFELY NEW YORK UNDERGROUND FACILITIES PROTECTION ORGANIZATION CALL 811

CONTRACTOR'S NAME: .....

AWARD DATE: .....

COMPLETION DATE: .....

FINAL ACCEPTANCE DATE: .....

INSPECTION FIRMS NAME: .....

RESIDENT ENG./E/C: .....

FINAL COST TOTAL: .....

FISCAL SHARE	COST (\$)

INSPECTION FIRM CONSULTANT'S STAMP:

RECOMMENDED BY: [Signature] 5/15/24  
DIRECTOR, OFFICE OF DESIGN DATE

RECOMMENDED BY: [Signature] 5/14/24  
DIVISION DIRECTOR DATE

RECOMMENDED BY: [Signature] 05/14/24  
TRAFFIC ENGINEER DATE

RECOMMENDED BY: [Signature] 05/14/24  
DIRECTOR, OFFICE OF CONSTRUCTION MANAGEMENT DATE

RECOMMENDED BY: [Signature] 5/16/24  
DIRECTOR OF MAINTENANCE AND OPERATIONS DATE

APPROVED BY: [Signature] 05/16/2024  
CHIEF ENGINEER DATE

PREPARED AND RECOMMENDED BY: [Signature] DATE 5-10-24  
CHA CONSULTING INC.  
JOSEPH KLEMMANN JR., P.E.  
N.Y.S. LICENSE NUMBER - 092577



CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ALIGNMENT			LANDSCAPE			ROADWAY			TRAFFIC WORK ZONE		
STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION	STYLE	NAME	DESCRIPTION	STYLE	TWZBT.P	DESCRIPTION
	AC	CONTROL (CENTERLINE)		LABL	AREA, BRUSH LINE		RCZ.P	CLEAR ZONE		TWZBTWL.P	BARRIER, TEMPORARY, W/ WARNING LIGHTS
	AD.P	DETOUR		LAHR	AREA, HEDGE ROW		RG	GUIDE RAIL, MISCELLANEOUS		TWZCD.P	CHANNELIZING DEVICE
	AT.P	TRANSITION CONTROL		LAPB	AREA, PLANTING BED		RGB	GUIDE RAIL, BOX BEAM		TWZPMRC.P	PAVEMENT MARKING REMOVAL OR COVERING
<b>BRIDGE</b>				LAWA	AREA, WOODED AREA OUTLINE		RGBM	GUIDE RAIL, BOX BEAM, MEDIAN	<b>UTILITIES</b>		
	BR	RAIL		LAWE	AREA, WATERS EDGE		RGC	GUIDE RAIL, CABLE	<b>STYLE</b>	<b>NAME</b>	<b>DESCRIPTION</b>
	BSHT	SHEET PILING		LCUT.P	CUT LIMIT		RGCB	GUIDE RAIL, CONCRETE BARRIER		UC	CONDUIT, UNDERGROUND
<b>CONTROL</b>				LFILL.P	FILL LIMIT		RGP.P	GUIDE POST		UCH	CONDUIT, HANGING
	CB	BASELINE		LFNC	FENCE		RGW	GUIDE RAIL, W BEAM		UCO	CONDUIT, OVERHEAD
	CBPR	BASELINE, PROJECTION		LTRC	TREE ROW, CONIFEROUS		RGWM	GUIDE RAIL, W BEAM, MEDIAN		UE	ELECTRIC LINE, UNDERGROUND
<b>DRAINAGE</b>				LTRD	TREE ROW, DECIDUOUS		RPB	PARKING BUMPER		UEH	ELECTRIC LINE, HANGING
	DCP	CULVERT PIPE		LWR	WALL, H PILE		RRC	RAIL ROAD, CATENARY		UEO	ELECTRIC LINE, OVERHEAD
	DCP.P	CULVERT PIPE (DIR)		LWS	WALL, STONE		RRER	RAIL ROAD, 3RD RAIL		UETO	ELECTRIC TRANSMISSION, OVERHEAD
	DDG.P	DITCH, GRASS LINED	<b>ROW MAPPING</b>				RRPLS.P	RAIL, PHOTO, LARGE SCALE		UESS	ELECTRIC, SUBSTATIONS
	DDP.P	DITCH, PAVED INVERT		MDL	DEED LINE		RRPSS	RAIL, PHOTO, SMALL SCALE		UFO	FIBER OPTIC, UNDERGROUND
	DDS.P	DITCH, STONE LINED		MEE	EASEMENT, EXISTING		RRS	RUMBLE STRIP		UFOH	FIBER OPTIC, HANGING
	DFL.P	FLOW LINE		MEP.P	EASEMENT, PERMANENT		RRSL.S.P	RAIL, SURVEY, LARGE SCALE		UFOO	FIBER OPTIC, OVERHEAD
	DSSD	SLOTTED DRAIN		MEPA.P	EASEMENT, PERMANENT, APPROX.		RRSSS	RAIL, SURVEY, SMALL SCALE		UG	GAS, UNDERGROUND
	DUD.P	UNDERDRAIN		MEPA.P	EASEMENT, PERMANENT, APPROX.		RRSSS	RAIL, SURVEY, SMALL SCALE		UGH	GAS, HANGING
<b>ENVIRONMENTAL</b>				MET.P	EASEMENT, TEMPORARY	<b>SIGNS</b>				UGO	GAS, OVERHEAD
	EFNS	FENCE, SILT		META.P	EASEMENT, TEMPORARY, APPROX.		SBLB	BILLBOARDS		UIC	INFORM CABLE, UNDERGROUND
	EFNSV	FENCE, SILT & VEGETATION		MF.P	FEE ACQUISITION, W/ ACCESS		SM	MULTIPLE POST		UICH	INFORM CABLE, HANGING
	EFNV	FENCE, VEGETATION		MF.A.P	FEE ACQUISITION, APPROXIMATE		SSO	STRUCTURE, OVERHEAD		UO	OIL LINE, UNDERGROUND
				MFS.P	FEE ACQUISITION, SHAPE		SSOC	STRUCTURE, OVHD. CANTILEVER		UOH	OIL LINE, HANGING
				MFWOA.P	FEE ACQUISITION, W/O ACCESS	<b>STRIPING</b>				UPBP	POLE, BRACE, PUSH BRACE
				MHA	HISTORICAL, ACQUISITION		STB*	BROKEN LINE		UPGW	POLE, GUY WIRE
				MHB	HIGHWAY BOUNDARY		STDB*	DOUBLE BROKEN LINE		USA	SANITARY SEWER, UNDERGROUND
				MHBA	HIGHWAY BOUNDARY, APPROX.		STDL*	DOTTED LINE LONG		USAH	SANITARY SEWER, HANGING
				MHBW	Hwy BOUNDARY, FACE OF WALL		STDS*	DOTTED LINE SHORT		USAF	SANITARY SEWER, FORCE MAIN, UGND
				MHBWOA	HIGHWAY BOUNDARY, W/O ACCESS		STFB*	FULL BARRIER LINE		USAFH	SANITARY SEWER, FORCE MAIN, HANG
				MJC	JURISDICTION, CITY		STH*	HATCH LINE		UT	TELEPHONE, UNDERGROUND
				MJCY	JURISDICTION, COUNTY		STPB*	PARTIAL BARRIER LINE		UTH	TELEPHONE, HANGING
				MJHD	JURISDICTION, HISTORIC DISTRICT		STRCT	ROUNDABOUT, CAT TRACKS		UTO	TELEPHONE, OVERHEAD
				MJLL	JURIS., (GREAT, MILITARY) LOT LINE		STRYL	ROUNDABOUT, YIELD LINE		UTV	CABLE TV, UNDERGROUND
				MJN	JURISDICTION, NATION		STSB	STOP BAR		UTVH	CABLE TV, HANGING
				MJPB	JURISDICTION, PUBLIC LANDS		STSE*	SOLID, EDGE		UTVO	CABLE TV, OVERHEAD
				MJS	JURISDICTION, STATE		STXL	X WALK, LADDER LINE		UUU	UNKNOWN, UNDERGROUND
				MJT	JURISDICTION, TOWN		STXLB	X WALK, LADDER BAR LINE		UUH	UNKNOWN, HANGING
				MJV	JURISDICTION, VILLAGE					UUU	UNKNOWN, OVERHEAD
				MPL	PROPERTY LOT LINE	<b>TRAFFIC CONTROL</b>				UW	WATER LINE, UNDERGROUND
				MPLA	PROPERTY LOT LINE, APPROXIMATE		TCSW	SIGNAL, SPAN WIRE		UWH	WATER LINE, HANGING
				MSL	SUB LOT LINE					UWO	WATER LINE, OVERHEAD

1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).
2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.).
3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES.
4. PROPOSED FEATURE SYMBOLOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 in ON B SIZE DRAWINGS).
5. MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS.
6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY

Thruway Authority

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2023 Mid-Range Tower  
Suite W-14023-3723  
516.452.4897 - www.thruwayauthority.com

TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
LEGEND AND ABBREVIATION

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
LEG-1

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON




DESIGN SUPERVISOR: D. MARSH

ALIGNMENT			DRAINAGE			ITS			ROW MAPPING			SIGNS			UTILITIES		
CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION	CELL	NAME	DESCRIPTION
⊗	ACC	CENTER OF CURVATURE	+	DINV	INVERT	⊗	IANT P	ANTENNAS	⊗	MDL1P	DEED LINE, TYPE 1	⊗	S	SINGLE POST	⊗	UEB	ELECTRIC, BOX
+	ACOGO	COGO	▬	DS	STRUCTURE, RECTANGULAR	⊗	IASCTS	ACCOU. SPEED/COUNT SNSR.S	⊗	MDL2P	DEED LINE, TYPE 2	⊗	S.P	SINGLE POST, PROPOSED	⊗	UEM	ELECTRIC, METER
⊗	ACS	CURVE TO SPIRAL	+	DSI	STRUCTURE, INVERT	⊗	ICABPAD	CABINET & PAD	⊗	MDL3P	DEED LINE, TYPE 3	⊗	SB.P	BACK TO BACK, PROPOSED	⊗	UEMH	ELECTRIC, MANHOLE
△	ADPL.P	DETOUR, POINT OF INTERSECT.	⊗	DSM	STRUCTURE, MANHOLE	⊗	ICCTV	CCTV SITE	⊗	MDL4P	DEED LINE, TYPE 4	⊗	SDEL	DELINEATORS	⊗	UEPT	ELECTRIC, POLE, TRANS.
⊗	ADPL.P	DETOUR, POINT ON LINE	⊗	DSMTXX.P	STRUCTURE, MANHOLE, TYPE "XX" "XX" = 48, 60, 72, 96	⊗	ICDPD	CDPD TRANSCEIVER	⊗	MDL5P	DEED LINE, TYPE 5	⊗	SPM	PARKING METER	⊗	UGM	GAS, METER
⊗	AEQN	EQUATION	⊗	DSR	STRUCTURE, ROUND	⊗	ICELLT	CELL PHONE TOWER	⊗	MEEP	EASEMENT, EXISTING	⊗	SRM	REFERENCE MARKERS	⊗	UGMH	GAS, MANHOLE
⊗	AEQNAHD	EQUATION AHEAD	⊗	DST"X"CB P	STRUCTURE, RECT., WITH CURB TYPE "X" "X" = F, G, N, O, P, R	⊗	ICJB	CONDUIT JACK OR BORING	⊗	MEPAP.P	EASEMENT, PERM., APPROX.	⊗	SRSC3	SHLD, CTY, 123 DIG.	⊗	UGLM	GAS, LINE MARKER
⊗	AEQNBK	EQUATION BACK	⊗	DST"X" P	STRUCTURE, RECT., TYPE "X" "X" = I, K, L, M, O, P, U	⊗	ICNTLCAB	CONTROLLER CABINET	⊗	MEPP.P	EASEMENT, PERM., BACK LINE	⊗	SRSC4	SHLD, CTY, 4 DIG.	⊗	UGP	GAS/FUEL PUMP
⊗	AEVT	EVENT STATION	ENVIRONMENTAL			⊗	ICPB	COMMUNICATION PULL BOX	⊗	MEPSP.P	EASEMENT, PERM., SHAPE	⊗	SRSC2	SHLD, CTY TOUR, 1-2 DIG.	⊗	UGV	GAS, VALVE
⊗	APC	POINT OF CURVATURE	⊗	EIOP.P	STR., INLET, OUTLET PROT.	⊗	ICTD	CONDUIT TURNING DOWN	⊗	MFAP.P	FEE ACQUISITION, APPROX.	⊗	SRSC4	SHLD, CTY TOUR, 3-4 DIG.	⊗	UGVT	GAS, VENT
⊗	APCC	POINT OF COMPOUND CURVATURE	⊗	EIPHS.P	STR., INLET PROT., HAY/STRAW	⊗	ICTU	CONDUIT TURNING UP	⊗	MFP.P	FEE ACQUISITION, BACK LINE	⊗	SRS1	SHLD, INTERSTATE	⊗	ULP	LIGHTING, POLE
△	API	POINT OF INTERSECTION	⊗	EIPGB.P	STR., INLET PROT., GRAVEL BAG	⊗	ICVTRT	COMM. VEH. ROAD TRANSCEIVER	⊗	MFSP.P	FEE ACQUISITION, SHAPE	⊗	SRSN2	SHLD, NATIONAL, 2 DIG.	⊗	ULPM	LIGHTING, POLE, MEDIAN
△	APOB	POINT OF BEGINNING	⊗	EIPHS.P	STR., INLET PROT., HAY/STRAW	⊗	IDFAULT	DEFAULT	⊗	MHBAP	HIGHWAY BNDRY., APPROX.	⊗	SRSN3	SHLD, NATIONAL, 3 DIG.	⊗	ULPP	LIGHTING, POLE, PED.
⊗	APOC	POINT OF CURVATURE	⊗	EIPPS.P	STR., INLET PROT., PREFAB.	⊗	IEZR	E-ZPASS READER	⊗	MHBCP	HISTORICAL, BLDG. CORNERS	⊗	SRSS2	SHLD, STATE, 2 DIG.	⊗	UMFC	MISC. FILLER CAP
△	APOE	POINT OF END	⊗	EIPPS.P	STR., INLET PROT., PREFAB.	⊗	IEZTR	TRANSMITTAL READER	⊗	MHBP	HIGHWAY BNDRY, PT.	⊗	SRSS3	SHLD, STATE, 3 DIG.	⊗	UOLM	OIL, LINE MARKER
⊗	APOL	POINT ON LINE	⊗	EIPSF.P	STR., INLET PROT., SILT FENCE	⊗	IFXCAB	FIBER OPTIC X-CONNECT CABINET	⊗	MJCP	PT., JURIS. CITY	⊗	SRSS4	SHLD, STATE, 4 DIG.	⊗	UP	POLE, WITH UTILITY
⊗	APOS	POINT ON SPIRAL	⊗	ERCB	RISER, CONCRETE BOX	⊗	IFUSSPL	FUSION SPLICE	⊗	MPBC	PT., BUILDING CORNER	BRIDGE			⊗	UPD	POLE, DEAD (NO UTILITY)
⊗	APOT	POINT ON TANGENT	⊗	ETRS.P	TRAP, SEDIMENT	⊗	IHARADV	HAR ADVISORY SIGN	⊗	MPCC	PT., CROSS CUT	⊗	BSC	BRIDGE, SCUPPER	⊗	UPL	POLE, WITH LIGHT
△	APOVC	POINT ON VERTICAL CURVE	⊗	ETWFG	WETLAND FLAG	⊗	IHARST	HAR SITE	⊗	MPDH	PT., DRILL HOLE	CONTROL			⊗	USMH	SANITARY SEWER MANHOLE
△	APOVT	POINT ON VERTICAL TANGENT	⊗	GDH	DRILL HOLE	⊗	ILC	LOAD CENTER	⊗	MPF	PT., FENCE LOCATION	⊗	CBP	BASELINE, POINT	⊗	UTB	TELEPHONE, BOOTH
Y	APORC	POINT ON REVERSE CURVE	GEOTECHNICAL			⊗	IMECSPL	MECHANICAL SPLICE	⊗	MPIP	PT., IRON PIPE	⊗	CBPOL	BASELINE, POINT ON LINE	⊗	UTLM	TELEPHONE, LINE MARKER
⊗	APT	POINT OF TANGENCY	⊗	IPVMS	PERM. VMS	⊗	IMSCS	PORT. SPEED & COUNT SENSOR	⊗	MPIR	PT., IRON ROD	⊗	CBSPP	BASELINE, SPUR POINT	⊗	UTMH	TELEPHONE, MANHOLE
⊗	APVC	POINT OF VERTICAL CURVATURE	⊗	IPVMS	PERM. VMS	⊗	IMSCCTS	MICRO SPEED & COUNT SENSOR	⊗	MPM	PT., MONUMENT	⊗	CBSP	BASELINE, SPUR POINT	⊗	UTVLM	CABLE TV, LINE MARKER
⊗	APVCC	POINT OF VERT. CMPND CURVE	⊗	IPVMS	PERM. VMS	⊗	IMT	MICROWAVE TRANSCEIVER	⊗	MPMM	PT., MONUMENT, MISC.	⊗	CBTP	BASELINE, TIE POINT	⊗	UTVPB	CABLE TV, PULL BOX
⊗	APVI	POINT OF VERT. INTERSECTION	⊗	IPVMS	PERM. VMS	⊗	IOVHMS	PERM. OVERHEAD VMS	⊗	MPN	PT., NAIL	⊗	CPBM	BENCHMARK	⊗	UUB	UNKNOWN, BOX
△	APVRC	POINT OF VERT. REVERSE CURVE	⊗	IPVMS	PERM. VMS	⊗	IPASCS	PORT. ACCOU. SPD & CNT. SENSOR	⊗	MPRS	PT., RAILROAD SPIKE	⊗	CPH	POINT, HORIZ. PHOTOGRAMMETRY	⊗	UUB	UNKNOWN, BOX
⊗	APVT	POINT OF VERTICAL TANGENCY	LANDSCAPE			⊗	IPEDS	PEDESTRIAN SIGNAL HEAD	⊗	MPSP	PT., SPIKE	⊗	CPSM	POINT, SURVEY MARKER, PERM.	⊗	UUB	UNKNOWN, BOX
⊗	ASC	SPIRAL TO CURVE	+	LELS	ELEVATION, SPOT	⊗	IPSS	PAVEMENT SURFACE SENSOR	⊗	MPST	PT., STAKE	⊗	CPSV	POINT, VERT., PHOTOGRAMMETRY	⊗	UUB	UNKNOWN, BOX
△	ASPI	SPIRAL POINT OF INTERSECTION	⊗	LPST	POST, SINGLE	⊗	IPVMS	PERM. VMS	⊗	MPTW	PT., TREE W/ WIRE	ROADWAY			⊗	UUB	UNKNOWN, BOX
⊗	ASTS	SPIRAL TO SPIRAL	⊗	LPST	POST, SINGLE	⊗	IRM	RAMP METER	⊗	MPWL	PT., WALL LOCATION	⊗	RES P	ELEVATION, SPOT	⊗	UUB	UNKNOWN, BOX
⊗	AST	SPIRAL TO TANGENT	⊗	LRB	ROCK, BOULDER	⊗	IRWIS	RDWY WEATHER INFO. SENSOR	ROW ACQUISITION			⊗	RGA	GUIDE RAIL, ANCHOR	⊗	UUB	UNKNOWN, BOX
⊗	ATS	TANGENT TO SPIRAL	⊗	LSHC	SHRUB, CONIFEROUS	⊗	ISP	SOLAR PANEL	⊗	MFS.P.T	FEE ACQUISITION	⊗	RGP	GUIDE POST, SINGLE	⊗	UUB	UNKNOWN, BOX
△	AVEVT	VERTICAL EVENT POINT	⊗	LSHD	SHRUB, DECIDUOUS	⊗	ISST	SPREAD SPECT. TRANSCEIVER	⊗	MEFS.P.T	EASEMENT, PERMANENT	ROADWAY			⊗	UUB	UNKNOWN, BOX
⊗	AVHIGH	VERTICAL HIGH POINT	⊗	LTC	TREE, CONIFEROUS	⊗	ITDB	TELEPHONE DEMARCATION BLK	⊗	METS.P.T	EASEMENT, TEMPORARY	⊗	RES P	ELEVATION, SPOT	⊗	UUB	UNKNOWN, BOX
⊗	AVLOW	VERTICAL LOW POINT	⊗	LTD	TREE, DECIDUOUS	⊗	ITP	SUBSURFACE TEMP. PROBE	⊗	METS.P.T	OCCUPANCY, TEMPORARY	⊗	RGA	GUIDE RAIL, ANCHOR	⊗	UUB	UNKNOWN, BOX
			⊗	LTS	TREE, STUMP	⊗	IVTRT	VEHICLE TO RDWY TRANSCEIVER	⊗	MFS.P.T	FEE ACQUISITION W/O ACCESS	⊗	RGP	GUIDE POST, SINGLE	⊗	UUB	UNKNOWN, BOX
			⊗	LTW.P	TREE, WELL OR WALL	⊗	IWIMD	WEIGHT IN MOTION DETECTOR				⊗	UUB	UNKNOWN, BOX	⊗	UUB	UNKNOWN, BOX
			+	LUKP	UNKNOWN POINT	⊗	IWVR	WIRELESS VIDEO REPEATER				⊗	UUB	UNKNOWN, BOX	⊗	UUB	UNKNOWN, BOX
						⊗	IWVRC	WIRELESS VIDEO RECEIVER				⊗	UUB	UNKNOWN, BOX	⊗	UUB	UNKNOWN, BOX
						⊗	IWVTT	WIRELESS VIDEO TRANSMITTER				⊗	UUB	UNKNOWN, BOX	⊗	UUB	UNKNOWN, BOX

1. THE LEGEND ILLUSTRATES MAPPING FEATURES (EXISTING AND PROPOSED).
2. FEATURES ARE SHOWN AS EITHER LINEAR (ROADWAY GUIDERAIL, ROADWAY SIDEWALK, UTILITY LINES, ETC.) OR POINT (SIGN, UTILITY POLE, ETC.).
3. FEATURES SHOWN ON THE LEGEND AS EXISTING FEATURES ALSO HAVE CORRESPONDING PROPOSED FEATURES.
4. PROPOSED FEATURE SYMBOLOGY IS IDENTICAL TO EXISTING FEATURE SYMBOLOGY EXCLUDING LINE WEIGHT. LINE WEIGHT FOR PROPOSED FEATURES IS THICKER (0.015 in ON B SIZE DRAWINGS).
5. MAPPING FEATURES NOT INCLUDED ON THE LEGEND SHEET DO NOT HAVE A UNIQUE SYMBOLOGY (SUCH AS THE PAVEMENT EDGE, PAVEMENT EDGE OF TRAVEL WAY) AND SHOULD BE LABELED ON THE PLANS.
6. FEATURES SHOWN AT THE HEAVIER WEIGHT ARE PROPOSED ONLY AND DO NOT HAVE CORRESPONDING EXISTING FEATURES.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING LEGEND AND ABBREVIATION	DRAWING NUMBER: LEG-2



**GENERAL NOTES**

1. ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE CURRENT VERSION NEW YORK STATE DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS (US CUSTOMARY) REFERENCED IN THE CONTRACT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR CHANGES SET FORTH IN THE CONTRACT "PROPOSAL."
2. THE CONTRACTOR WILL BE REQUIRED TO COORDINATE THEIR WORK WITH OTHER CONTRACTORS AND AUTHORITY MAINTENANCE FORCES AND THEY SHALL SCHEDULE THEIR OPERATIONS SO AS TO CAUSE A MINIMUM DISRUPTION TO TRAFFIC.
3. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT NO EMBANKMENT AREA FOR SURPLUS MATERIAL IS AVAILABLE FOR THIS CONTRACT WITHIN THE AUTHORITY'S RIGHT-OF-WAY. THEREFORE, ALL MATERIAL TO BE REMOVED FROM THE JOB SITE SHALL BE DISPOSED BY THE CONTRACTOR OFF THE AUTHORITY'S PROPERTY. THE CONTRACTOR SHALL COMPLY WITH ALL LOCAL AND STATE REGULATIONS THAT APPLY TO THE AREA CHOSEN FOR THE DISPOSAL OF THIS MATERIAL. ALL COST ASSOCIATED WITH THE SPOIL AREA AND REMOVAL OF SPOIL MATERIAL SHALL BE INCLUDED IN THE VARIOUS ITEMS OF THE CONTRACT.

**REHABILITATION NOTES**

1. THE CONTRACTOR SHALL EXAMINE AND VERIFY, IN THE FIELD, ALL CONDITIONS AND DIMENSIONS. DIMENSIONS OF THE EXISTING STRUCTURES SHOWN ON THESE PLANS ARE FOR GENERAL REFERENCE ONLY. THEY HAVE BEEN TAKEN FROM THE ORIGINAL CONSTRUCTION OR SUBSEQUENT REHABILITATION DRAWING AND ARE NOT GUARANTEED. THE CONTRACTOR SHALL TAKE ALL SUCH FIELD MEASUREMENTS TO ASSURE PROPER FIT OF THE FINISHED WORK, AND THE CONTRACTOR SHALL ASSUME FULL RESPONSIBILITY FOR THEIR ACCURACY. IF FIELD CONDITIONS AND DIMENSIONS DIFFER FROM THOSE ON THE PLANS, THE CONTRACTOR SHALL USE THE FIELD CONDITIONS AND DIMENSIONS AND MAKE THE APPROPRIATE CHANGES TO THOSE SHOWN ON THE PLANS AS APPROVED BY THE ENGINEER. WHEN SHOP DRAWING BASED ON THE FIELD MEASUREMENTS ARE SUBMITTED FOR APPROVAL, THE FIELD MEASUREMENTS MADE SHALL BE INDICATED ON THE SHOP DRAWINGS SUBMITTED FOR REFERENCE OF THE REVIEWER.
2. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THAT FACT THAT, DUE TO THE NATURE OF REHABILITATION PROJECTS, THE EXACT EXTENT OF WORK CANNOT ALWAYS BE ACCURATELY DETERMINED PRIOR TO THE COMMENCEMENT OF WORK. THESE CONTRACT DOCUMENTS HAVE BEEN PREPARED BASED ON FIELD INSPECTION AND OTHER INFORMATION AVAILABLE AT THE TIME. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATIONS TO CONSTRUCTION DETAIL AND WORK QUANTITIES. THE CONTRACTOR SHALL PERFORM THE WORK IN ACCORDANCE WITH FIELD CONDITIONS.
3. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ALL DAMAGES TO THE EXISTING FACILITY CAUSED BY THEIR OPERATIONS WHICH IS NOT INCLUDED AS PART OF THE INTENDED WORK. ALL DAMAGES TO THE EXISTING FACILITY WHICH IS NOT PART OF THE INTENDED WORK SHALL BE REPAIRED BY THE CONTRACTOR WITHOUT COST TO THE AUTHORITY, AND TO THE SATISFACTION OF THE ENGINEER.
4. THE CONTRACTOR SHALL CONDUCT THEIR REMOVAL OPERATIONS TO THE SATISFACTION OF THE ENGINEER SO AS NOT TO UNDULY DISTURB UNDERLYING MATERIAL WHICH ARE TO REMAIN IN PLACE. THE CONTRACTOR SHALL PERFORM ALL WORK WITH CARE SO THAT ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE AUTHORITY, WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE AUTHORITY, THE DAMAGED MATERIAL SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.

**REMOVAL, EXCAVATION AND BACKFILL NOTES**

1. THE CONTRACTOR SHALL PROVIDE ALL TEMPORARY SUPPORTS, BRACING AND OTHER DEVICES REQUIRED OR DIRECTED BY THE ENGINEER TO PROTECT THE SAFETY OF THE ADJACENT STRUCTURES, ROADWAY AND UTILITIES.
2. SHOULDER AREAS DISTURBED BY THE CONTRACTOR, AS PART OF THE WORK TO BE PERFORMED UNDER THIS CONTRACT, SHALL BE RESTORED AS SPECIFIED AND TO THE SATISFACTION OF THE ENGINEER. ALL THE DISTURBED GRASS AREAS SHALL BE GRADED IN A MANNER APPROVED BY THE ENGINEER AND SEEDED AS SPECIFIED IN THE STANDARD SEEDING ITEM. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS ITEMS IN THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
3. SURPLUS MATERIAL: SURPLUS MATERIAL FROM EXCAVATION AND MISCELLANEOUS SURPLUS MATERIAL: WASTE, SPOILS AND DRAINAGE WORK SHALL BE REMOVED FROM THE PROJECT SITE.

**MISCELLANEOUS NOTES**

1. THE CONTRACTOR IS ADVISED THAT ADDITIONAL "NOTES" WILL BE FOUND ON SUBSEQUENT SHEETS OF THE CONTACT PLANS AND SUCH "NOTES", WHILE PERTAINING TO THE SPECIFIC SHEETS THEY ARE PLACED ON, ALSO SUPPLEMENT THE GENERAL NOTES LISTED HEREIN.
2. NO ADDITIONAL PAYMENTS WILL BE MADE FOR WORK CALLED FOR BY NOTES ON THE PLANS OR IN THE SPECIFICATION UNLESS PAYMENT IS SPECIFICALLY INDICATED BY ITEM NUMBERS. THE COST OF WORK FOR WHICH NO PAYMENT ITEM IS INDICATED, SHALL BE INCLUDED IN THE UNIT PRICES BID FOR VARIOUS ITEMS OF THE CONTRACT.

**UTILITY NOTES**

1. LOCATION OF EXISTING UTILITIES, PUBLIC AND/OR PRIVATE, AS SHOWN IN THE PLANS OR INDICATED IN THE PROPOSAL ARE APPROXIMATE ONLY. THE EXACT LOCATION OF EACH UTILITY SHALL BE DETERMINED IN THE FIELD. ADDITIONAL UTILITY LINES NOT SHOWN ON THE PLANS, WHETHER ABANDONED OR IN SERVICE, MAY EXIST. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONDUCT THEIR OPERATIONS AND TAKE NECESSARY PRECAUTIONS TO PREVENT INTERFERENCE WITH OR DAMAGE TO THESE OR OTHER FACILITIES DURING THE COURSE OF CONSTRUCTION. THE CONTRACTOR IS ADVISED TO CONTACT DIG SAFELY NEW YORK AT 811 OR 1-800-962-7962 PRIOR TO ANY EXCAVATION.
2. IN THE EVENT THE CONTRACTOR DAMAGES AN EXISTING UTILITY SERVICE CAUSING AN INTERRUPTION IN SAID SERVICE, THEY SHALL IMMEDIATELY COMMENCE WORK TO RESTORE SERVICE AND MAY NOT CEASE THEIR WORK OPERATION UNTIL SERVICE IS RESTORED.
3. THE METHOD OR REMOVAL OF EXISTING ROAD WAY OR SHOULDER PAVEMENT IN THE IMMEDIATE VICINITY OF UNDERGROUND UTILITIES SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.

**WARNING: UNDERGROUND FIBER OPTIC CONDUIT**

1. THE THRUWAY'S FIBER OPTIC "BACKBONE" IS LOCATED WITHIN THE WORK LIMITS OF THIS PROJECT. THE APPROXIMATE LOCATION IS SHOWN ON THE DRAWINGS. THE CONTRACTOR IS ADVISED TO CONTACT DIG SAFELY NY AT 1-800-962-7962 PRIOR TO ANY EXCAVATION. FURTHERMORE, PURSUANT TO N.Y.S. CODE RULE 753, THE CONTRACTOR MUST BE PREPARED TO VERIFY THE LOCATION OF THE FIBER OPTIC LINE THROUGH HAND DUG TEST HOLES AT ONE OR MORE LOCATIONS WITHIN THE WORK AREA PRIOR TO ANY EXCAVATION. HAND DUG TEST HOLES SHALL BE PAID FOR UNDER ITEM 206.05 - TEST PIT EXCAVATION (EACH).

**EXISTING MILE MARKERS / TENTH-MILE MARKERS**

1. APPROXIMATE LOCATION OF EXISTING MILE MARKERS AND TENTH-MILE MARKERS ARE SHOWN ON THE PLANS.

**MILL AND INLAY NOTES**

1. COMPLETE ALL FULL-DEPTH REPAIRS PRIOR TO PRODUCTION COLD MILLING OPERATIONS.
2. ALL MILLED MATERIAL SHALL BECOME THE PROPERTY OF THE CONTRACTOR. TEMPORARY STOCK PILING WILL NOT BE ALLOWED WITHIN THE RIGHT-OF-WAY OF THE THRUWAY AUTHORITY. MILLINGS MAY BE USED AS SHOULDER BACKUP MATERIAL AS APPROVED BY THE ENGINEER.
3. ALL DEBRIS ON THE MILLED SURFACE SHALL BE REMOVED IN ACCORDANCE WITH SECTION 490 (COLD MILLING) OF THE CURRENT STANDARD SPECIFICATIONS.

**EROSION & SEDIMENT CONTROL NOTES:**

1. EROSION CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 209 OF THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION SPECIFICATIONS AND STANDARD SHEETS. ADDITIONAL GUIDANCE AND SIZING CRITERIA CAN BE FOUND IN THE MOST CURRENT EDITION OF THE NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL (THE BLUE BOOK) PUBLISHED BY THE NYS DEPARTMENT OF ENVIRONMENTAL CONSERVATION. ADDITIONAL SOIL EROSION AND SEDIMENT CONTROL MEASURES MAY BE REQUIRED AS PER SECTION 107-12 OF THE STANDARD SPECIFICATIONS.
2. THE CONTRACTOR WILL BE REQUIRED TO PERFORM ALL CONSTRUCTION OPERATIONS IN A MANNER SO AS TO MINIMIZE SOIL EROSION AND ENSURE SEDIMENT CONTROL.
3. ALL NECESSARY PRECAUTIONS SHALL BE TAKEN TO PREVENT DIRECT OR INDIRECT CONTAMINATION OF ALL WATER BODIES (INCLUDING WETLANDS) BY SILT, SEDIMENT, FUELS, SOLVENTS, LUBRICANTS, EPOXY COATING CONCRETE LEACHATE, OR ANY OTHER POLLUTANT ASSOCIATED WITH CONSTRUCTION PROCEDURES. DURING CONSTRUCTION, NO WET OR FRESH CONCRETE OR LEACHATE SHALL BE ALLOWED TO ESCAPE DIRECTLY OR INDIRECTLY INTO ANY GROUND SURFACES OR WATER BODIES (INCLUDING WETLANDS), NOR SHALL WASHINGS FROM CONCRETE TRUCKS, MIXERS, OR OTHER DEVICES BE ALLOWED TO ESCAPE DIRECTLY OR INDIRECTLY INTO ANY GROUND SURFACES OR WATER BODIES (INCLUDING WETLANDS). CONTRACTOR SHALL PROVIDE A CONCRETE WASHOUT PIT IN ACCORDANCE WITH STANDARD SPECIFICATIONS SHOWN WITHIN NYSDEC'S 2016 BLUE BOOK (SECTION 2 - RESOURCE PLANNING). SEE CONCRETE WASHOUT DETAIL ON DWG. MD-13. COST SHALL BE INCLUDED IN THE BID PRICE FOR ALL CONCRETE ITEMS.
4. ANY DEBRIS OR EXCESS MATERIAL FROM CONSTRUCTION OF THIS PROJECT SHALL BE IMMEDIATELY AND COMPLETELY REMOVED FROM THE BED AND BANKS OF ALL WATER BODIES (INCLUDING WETLANDS) AND SHALL BE DISPOSED OF AWAY FROM WETLANDS, WATER COURSES, OR OTHER BODIES OF WATER.
5. ALL EXCAVATED MATERIAL SHALL BE DISPOSED OF AND BE PROTECTED SO THAT IT CAN NOT DIRECTLY OR INDIRECTLY RE-ENTER ANY WATER BODY OR WETLAND AREA.
6. TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED AS PER SPECIFICATIONS AND IN ACCORDANCE WITH NYSDOT STANDARD SHEETS, SECTION 209 AND WITHIN THE CONTRACT DOCUMENTS. THE COST OF MAINTAINING AND REMOVING TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INCLUDED IN THE BID PRICE OF THE APPROPRIATE ITEM USED FOR THE INSTALLATION OF THE MEASURE. ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSPECTED BY THE CONTRACTOR DAILY AFTER EACH STORM EVENT THAT GENERATES RUNOFF. NOTED DEFICIENCIES SHALL BE CORRECTED WITHIN ONE BUSINESS DAY.

**CONSTRUCTION QUANTITY NOTES**

1. CONTRACTOR SHALL TAKE NOTE THAT BID QUANTITIES FOR CONCRETE PAVEMENT REPAIRS ARE HIGHER THAN THE ASSOCIATED WORK SHOWN IN CONTRACT PLANS. IT IS ANTICIPATED THAT FURTHER DETERIORATION OF THE CONCRETE PAVEMENT WILL HAVE OCCURRED OVER THE COURSE OF THE WINTER, BETWEEN BIDDING PHASE AND THE START OF CONSTRUCTION. ANY NEW DAMAGE FOUND WILL BE ADDED TO THE PROJECT WITH THE APPROVAL OF THE ENGINEER DURING THE EVALUATION AND MARK-OUT PHASE.

**WORK TO BE DONE:**

THE FOLLOWING IS A GENERAL DESCRIPTION OF THE WORK TO BE DONE UNDER THIS CONTRACT. THIS LIST IS INTENDED TO GIVE THE CONTRACTOR A GENERAL DESCRIPTION OF THE WORK INVOLVED IN THIS CONTRACT AND IS NOT A COMPLETE LISTING OF ALL WORK TO BE DONE. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS EVEN THOUGH NOT SPECIFICALLY MENTIONED IN THIS LIST.

THE CONTRACT COMPLETION DATE IS: OCTOBER 30, 2026

FOR ALL WORK EXCEPT WHERE NOTED ON THE PLANS, THE CONTRACTOR SHALL ESTABLISH THE WORK ZONE TRAFFIC CONTROL (WZTC) USING STANDARD NYSTA WORK ZONE TRAFFIC CONTROL DETAILS AND NOTES AND AS ORDERED BY THE ENGINEER (AOBE).

INSTALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES PRIOR TO DISTURBING AREAS. COMPLETE CLEARING AND GRUBBING OPERATIONS.

COMPLETE TEST PITS ON ALL POTENTIAL UTILITY CONFLICTS. (AOBE)

**MAINLINE MP 900.7 TO MP 904.2 CONCRETE REPAIR/MISCELLANEOUS HIGHWAY WORK**

SETUP WORK ZONE TRAFFIC CONTROL (WZTC), AS REQUIRED TO COMPLETE CONCRETE REPAIR WORK INCLUDING RAMP CLOSURES AND DETOURS AS NECESSARY AND APPROVED BY EIC.

COMPLETE PORTLAND CEMENT CONCRETE (PCC) REPAIRS TO THE NORTHBOUND AND SOUTHBOUND MAINLINE TRAVEL AND AUXILIARY LANES.

COMPLETE MAINLINE FULL DEPTH ASPHALT RECONSTRUCTION AND INSTALLATION OF PRE-CAST CONCRETE PANELS AT ALL BRIDGE OVERPASSES. (REFER TO THE PLANS FOR LIMITS OF RECONSTRUCTION).

INSTALL ALL GUIDE RAILING AS SHOWN ON THE PLANS.

MILL THE EXISTING 1 1/2" TOP COURSE ON ALL MAINLINE ASPHALT SHOULDERS TO THE LIMITS SHOWN ON THE PLANS AND AOB.

RESTORE ANY PAVEMENT MARKINGS REMOVED DURING THE PCC REPAIRS AND MILLING OPERATIONS AND AOB, USING INTERIM PAVEMENT MARKINGS.

COMPLETE DRAINAGE WORK, FRAME AND GRATE ADJUSTMENTS.

**BRIDGE REHABILITATION AND STRENGTHENING**

COMPLETE ALL BRIDGE STRENGTHENING AND BRIDGE DECK REPAIR WORK AS SHOWN ON PLANS PRIOR TO PLACING MAINLINE ASPHALT OVERLAY. REPLACE BRIDGE JOINTS AT LOCATIONS NOTED ON PLANS. JOINT WORK TO BE COMPLETED AFTER PLACING THE MAINLINE ASPHALT OVERLAY.

**MP 900.7 TO MP 904.2 TWO -COURSE ASPHALT OVERLAY**

SETUP WZTC AS NECESSARY TO COMPLETE THE MAINLINE TWO COURSE OVERLAY.

INSTALL THE 3 1/2" TWO COURSE ASPHALT OVERLAY TO THE LIMITS SHOWN ON PLANS

INSTALL NEW MIARDS.

INSTALL ALL PROPOSED PAVEMENT MARKINGS ON THE MAINLINE.

**MP 900.7 TO MP 904.2 OVERHEAD SIGN STRUCTURES AND NEW HIGHWAY SIGNS**

INSTALL ALL OVERHEAD SIGN STRUCTURES AND SIGN PANELS. COVER ANY OVERHEAD SIGN PANELS THAT CONFLICT WITH THE NEW OVERHEAD SIGNS.

REMOVE EXISTING OVERHEAD SIGN STRUCTURES AND PANELS.

REMOVE EXISTING SIGNS AND INSTALL NEW ROADSIDE SIGNS AS SHOWN ON THE CONTRACT PLANS.

**MP 900.7 TO MP 904.2 INTERCHANGE RAMP WORK**

SETUP WZTC TEMPORARY DETOURS SHOWN ON THE PLANS.

COMPLETE ALL DRAINAGE WORK AS SHOWN ON THE CONTRACT PLANS.

COMPLETE ALL GUIDE RAIL AND CONCRETE BARRIER MODIFICATIONS AS SHOWN ON THE PLANS.

MILL AND PAVE THE INTERCHANGE RAMPS TO THE LIMITS SHOWN ON THE PLANS.

INSTALL FINAL PAVEMENT MARKINGS ON THE RAMPS.

**MP 900.7 TO MP 904.2 HIGHWAY LIGHTING**

SETUP WZTC AS NECESSARY TO INSTALL THE NEW LIGHT POLES, CONDUIT AND PULL BOXES.

INSTALL NEW LIGHTING CONDUIT, PULL BOXES AND LIGHT POLES.

ENERGIZE NEW LIGHTING SYSTEM.


CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN



DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING GENERAL NOTES	DRAWING NUMBER: GN-1

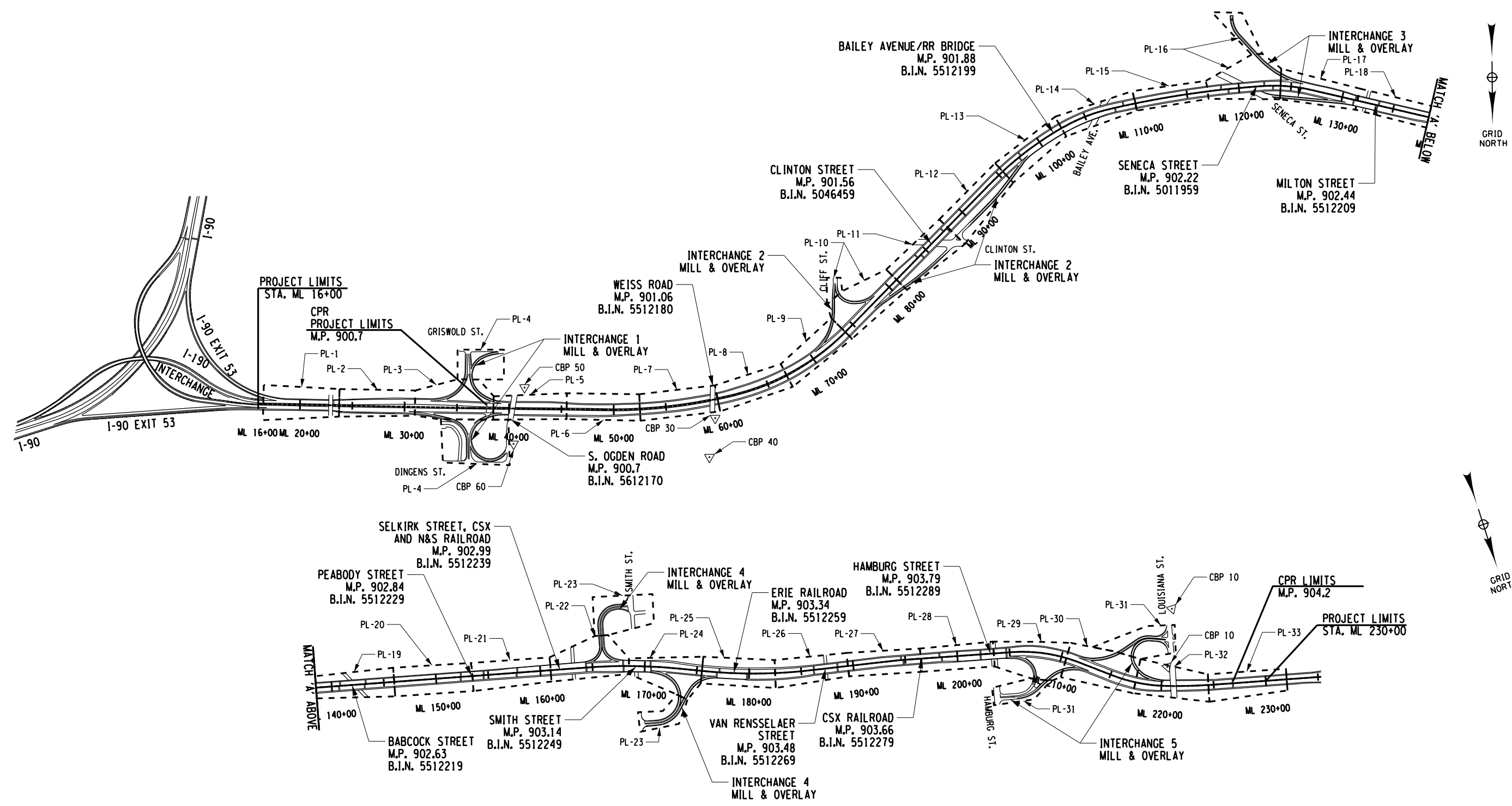
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DRAFTED BY: N. HOY

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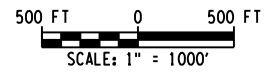
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



NOTES:  
1. SEE BL-1 FOR BASELINE TIE AND CONTROL POINT INFORMATION.

**MAINLINE I-190**  
M.P. 900.7 TO M.P. 904.2



ALTERED ON:	AFFIXED ON: 5/10/2024
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK**  
STATE OF  
OPPORTUNITY.

**Thruway  
Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING KEY PLANS	DRAWING NUMBER: KP-1

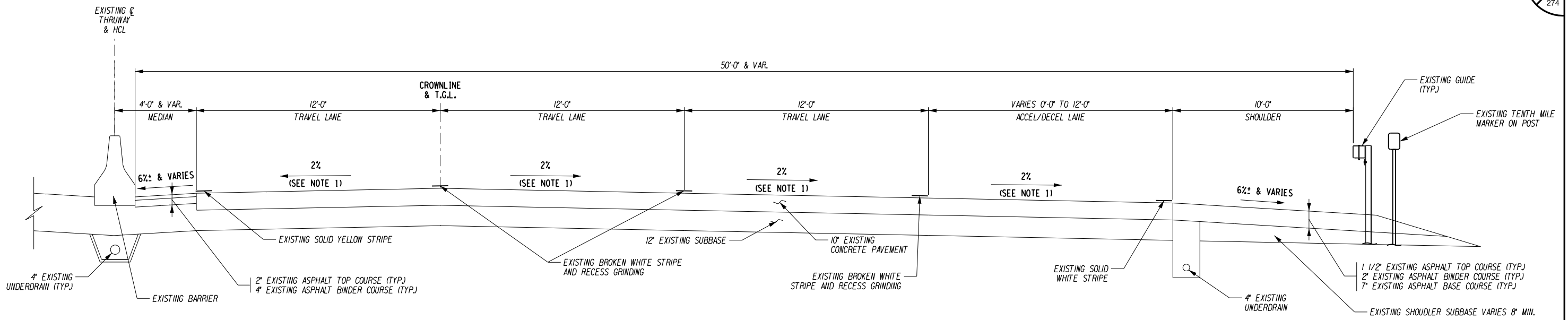
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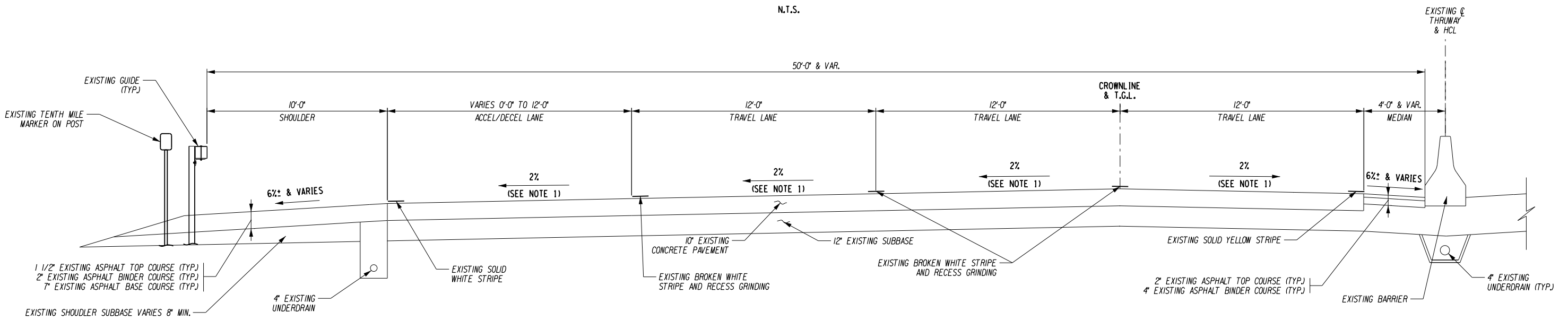
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DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



**EXISTING TYPICAL HALF SECTION I-190 MAINLINE**  
M.P. 900.07 TO M.P. 904.2  
NORTHBOUND  
N.T.S.



**EXISTING TYPICAL HALF SECTION I-190 MAINLINE**  
M.P. 900.7 TO M.P. 904.2  
SOUTHBOUND  
N.T.S.

**NOTES:**  
1. NORMAL CROWN SHOWN, SIMILAR FOR BANKED SECTION.

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TYPICAL SECTIONS	DRAWING NUMBER: TYP-1

ALTERED ON: \_\_\_\_\_

AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_

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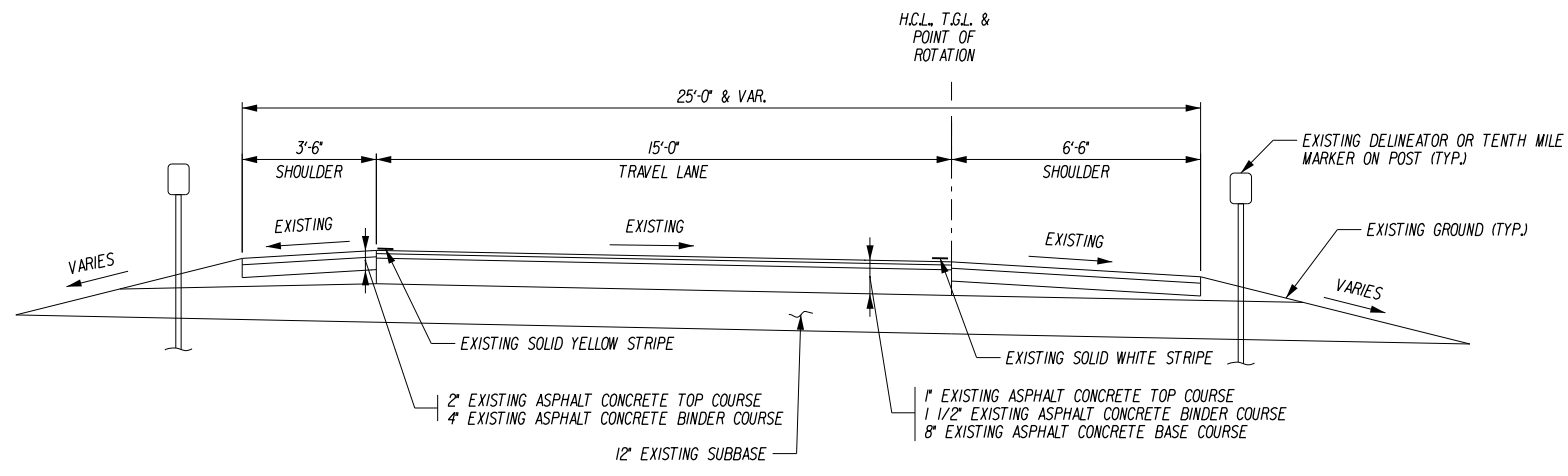
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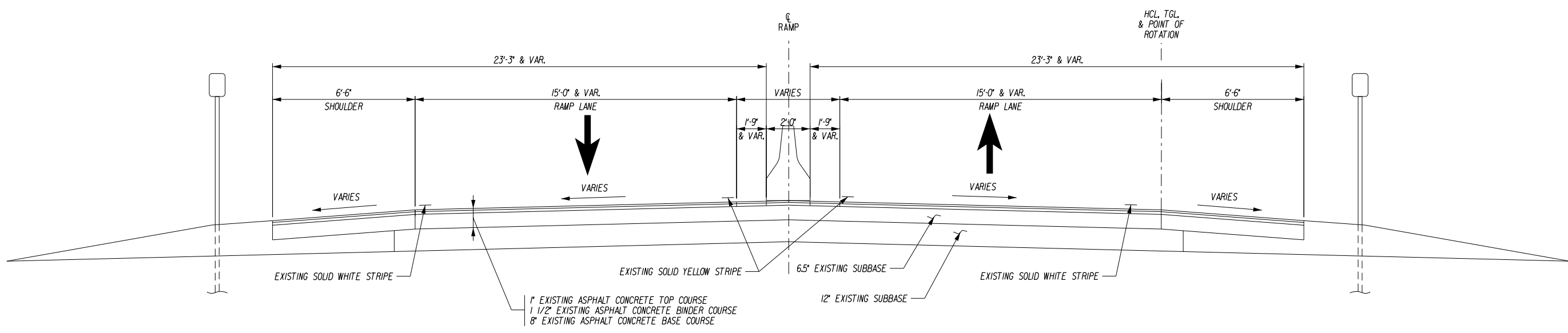
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



**EXISTING RAMP SECTION**

RAMPS A, B, C, D, E, F, G, J AND K  
N.T.S.



**EXISTING TYPICAL TWO WAY RAMP - NORMAL CROWN**

RAMPS C1, D1, L1, O1, AND R1  
N.T.S.

ALTERED ON:	AFFIXED ON: 5/10/2024
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TYPICAL SECTIONS	DRAWING NUMBER: TYP-2

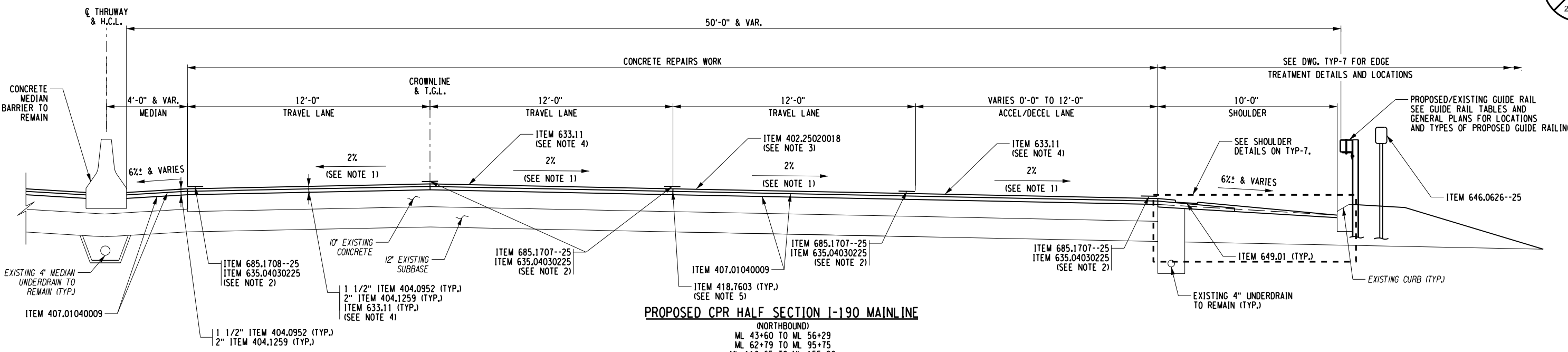
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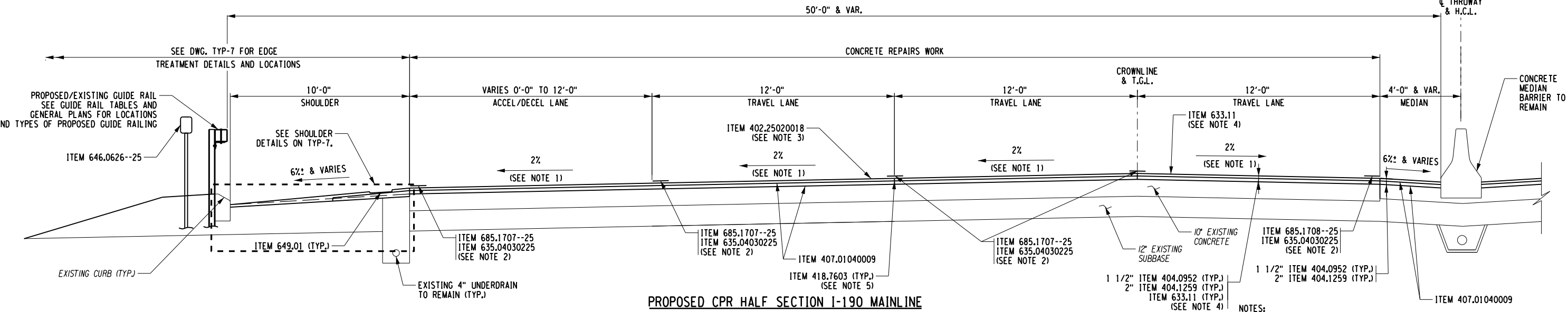
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



**PROPOSED CPR HALF SECTION I-190 MAINLINE**

(NORTHBOUND)  
ML 43+60 TO ML 56+29  
ML 62+79 TO ML 95+75  
ML 110+65 TO ML 155+90  
ML 166+20 TO ML 217+95  
SEE NOTE 6  
N.T.S.



**PROPOSED CPR HALF SECTION I-190 MAINLINE**

(SOUTHBOUND)  
ML 43+60 TO ML 56+29  
ML 62+79 TO ML 95+75  
ML 110+65 TO ML 155+90  
ML 166+20 TO ML 217+95  
SEE NOTE 6  
N.T.S.

**NOTES:**

1. NORMAL CROWN CROSS SLOPE SHOWN. CROSS SLOPES VARY FROM NORMAL CROWN TO FULL SUPERELEVATION (BOTH BANKED LEFT AND RIGHT). THE PROPOSED TRAVEL LANES SHALL MATCH EXISTING CROSS SLOPES IN ALL CASES. SHOULDER CROSS SLOPES SHALL VARY AND BE AS DEFINED IN THE EDGE TREATMENT DETAILS SHOWN ON DWG. TYP-7.
2. THE MAINLINE I-190 FINAL PAVEMENT MARKING STRIPES SHALL CONSIST OF HIGHLY REFLECTORIZED TRIPLE DROP WHITE AND YELLOW EPOXY PAVEMENT STRIPES. PAVEMENT MARKING STRIPES SHALL BE INSTALLED IN RECESS ITEM 635.04030225 (RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS).
3. SAW CUT AND SEAL THE FINISHED ASPHALT PAVEMENT ABOVE. SAW CUTS SHALL OCCUR DIRECTLY OVER AND IN LINE WITH THE TRANSVERSE JOINTS IN THE UNDERLYING PCC PAVEMENT.
4. CONTRACTOR SHALL CLEAN THE EXISTING PAVEMENT SURFACE. CLEANING SHALL BE PAID FOR UNDER ITEM 633.11.
5. ITEM 418.7603 SHALL BE APPLIED ON ALL LONGITUDINAL AND TRANSVERSE JOINT FACES OF THE TOP COURSE ASPHALT PAVEMENT.
6. SEE BR-1 FOR LOCATIONS OF BRIDGE DECK CPR WORK INCLUDED IN THE STATIONING LIMITS LISTED.

ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT
402.25020018	SAW AND SEALING JOINTS IN NEW ASPHALT OVERLAYS	LF	635.04030225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON	646.0626--25	MILE MARKERS	EA
404.1259	12.5 F9 BINDER COURSE ASPHALT, 50 SERIES COMPACTION	TON	649.01	MILLED-IN AUDIBLE ROADWAY DELINEATORS (MIARDS)	LF
407.01040009	NON-TRACKING TACK COAT	GAL	685.1707--25	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH X 20 MILS	LF
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF	685.1708--25	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH X 20 MILS	LF
633.11	CLEANING EXISTING PAVEMENT AND/OR SHOULDER	SY			

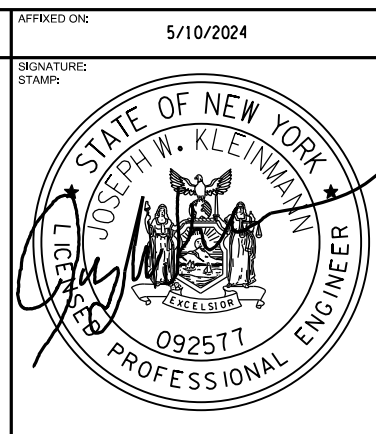
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF SPORTS OPPORTUNITY**

**Thruway Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TYPICAL SECTIONS	DRAWING NUMBER: TYP-3



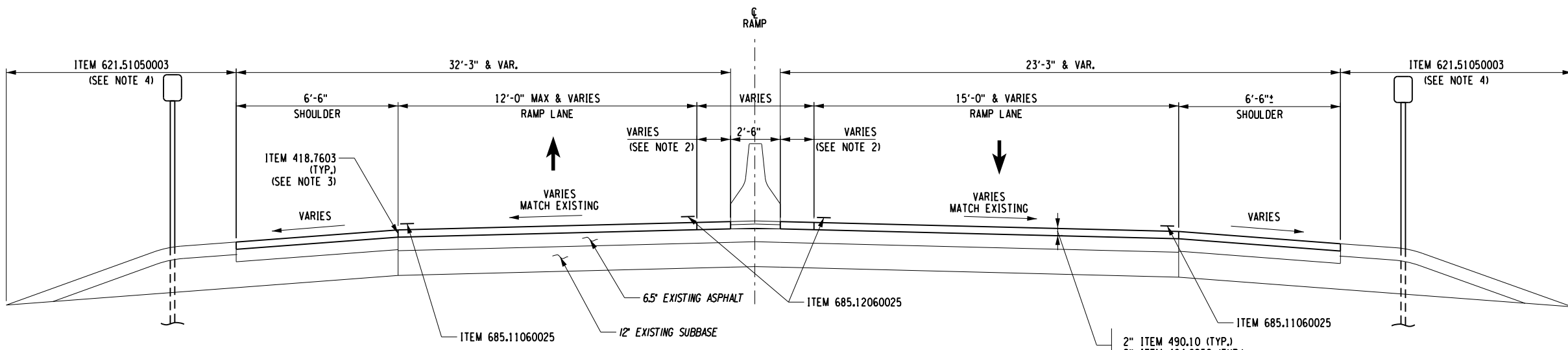
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

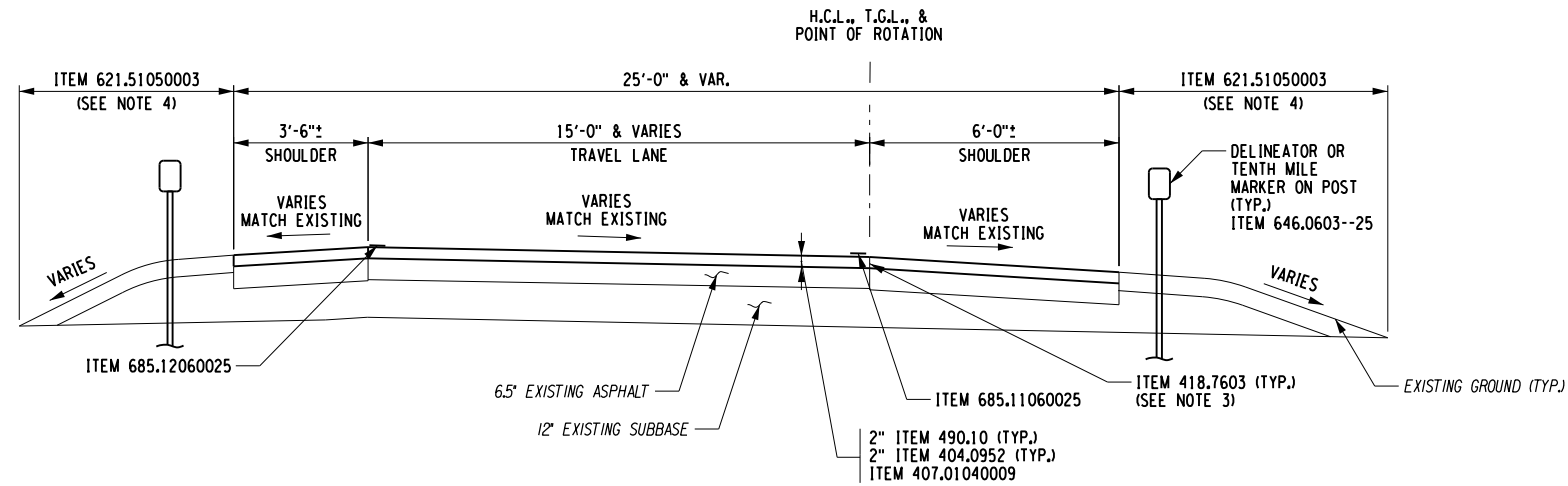
CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

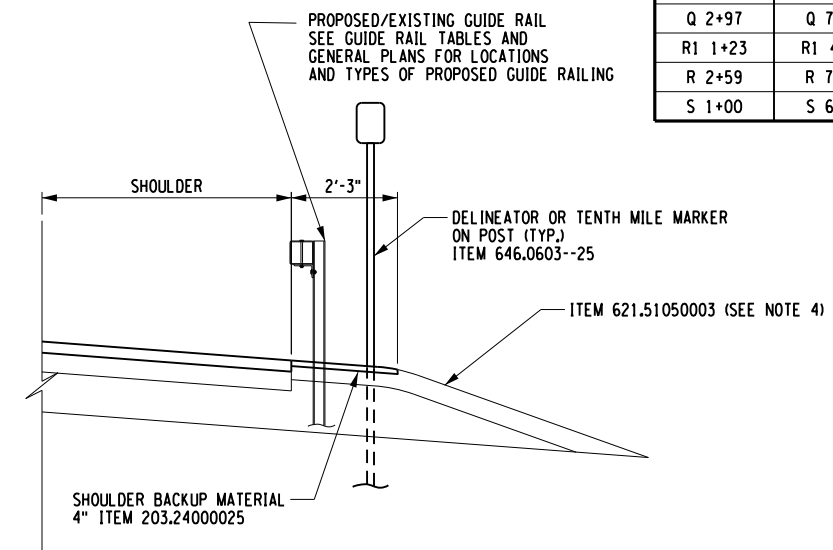
DESIGN SUPERVISOR: D. MARSH



**PROPOSED TYPICAL TWO WAY RAMP - ①**  
NORMAL CROWN  
RAMPS C1, D1, L1, O1, Q1 AND R1  
N.T.S.



**PROPOSED TYPICAL ONE WAY RAMP - ②**  
NORMAL CROWN  
RAMPS A-H AND J-S  
N.T.S.



**RAMP GUIDE RAILING EDGE TREATMENT DETAIL**  
NTS

RAMP STATION LOCATION		RAMP TYPICAL SECTION NUMBER	
FROM	TO	1	2
A 9+81	A 14+00	X	
B 3+14	B 6+14	X	
C1 10+00	C1 12+17		X
C 1+43	C 5+87	X	
C 6+52	C 10+00	X	
D1 1+28	D1 3+42		X
D 1+86	D 5+71	X	
D 6+51	D 9+41	X	
E 0+50	E 6+86	X	
F 1+67	F 6+05	X	
G 2+17	G 11+28	X	
H 1+04	H 10+96	X	
J 2+28	J 11+52	X	
K 1+00	K 7+39	X	
L 1+13	L 3+33	X	
L1 1+18	L1 7+10		X
M 1+66	M 3+61	X	
N 1+83	N 6+04	X	
O1 1+20	O1 5+44		X
O 1+32	O 5+29	X	
P 1+70	P 6+70	X	
Q1 3+40	Q1 4+90		X
Q 2+97	Q 7+58	X	
R1 1+23	R1 4+35		X
R 2+59	R 7+80	X	
S 1+00	S 6+58	X	

- NOTES:**
- NORMAL CROWN CROSS SLOPE SHOWN. CROSS SLOPES VARY FROM NORMAL CROWN TO FULL SUPERELEVATION (BOTH BANKED LEFT AND RIGHT). THE PROPOSED TRAVEL LANES SHALL MATCH EXISTING CROSS SLOPES IN ALL CASES. SHOULDER CROSS SLOPES SHALL VARY AND BE AS DEFINED IN THE EDGE TREATMENT DETAILS SHOWN ON DWG. TYP-7.
  - FOR RAMPS C1 AND L1, THE DIMENSION SHALL BE 1'-9" AND FOR RAMPS O1, R/R1 AND S, THE DIMENSION SHALL BE 3'-3". FOR RAMPS D1, P, AND Q/Q1 THE DIMENSION SHALL VARY FROM 2'-9" TO 11'-9" A.O.B.E.
  - ITEM 418.7603 SHALL BE APPLIED ON ALL LONGITUDINAL AND TRANSVERSE JOINT FACES OF THE TOP COURSE ASPHALT PAVEMENT.
  - THE CONTRACTOR SHALL TRIM, GRADE AND RESHAPE EXISTING SLOPES FROM THE EDGE OF PAVED SHOULDER TO WIDTH SPECIFIED OR ABOVE. SEE TABLE ON DWG. MT-4 FOR LOCATION.

ITEM	DESCRIPTION	UNIT
203.24000025	SHOULDER BACKUP MATERIAL	TON
404.0952	9.5 F1 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
418.7603	ASPHALT PAVEMENT JOINT ADHESIVE	LF
490.10	PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE	SY
646.0603--25	MILE MARKERS	EA
621.51050003	GRADING MAINTENANCE, CLEANING, TRIMMING & RESHAPING EXISTING ROADSIDE SECTION	LF
685.11060025	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES 6" X 20 MILS	LF
685.12060025	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES 6" X 20 MILS	LF

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TYPICAL SECTIONS	DRAWING NUMBER: TYP-4

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

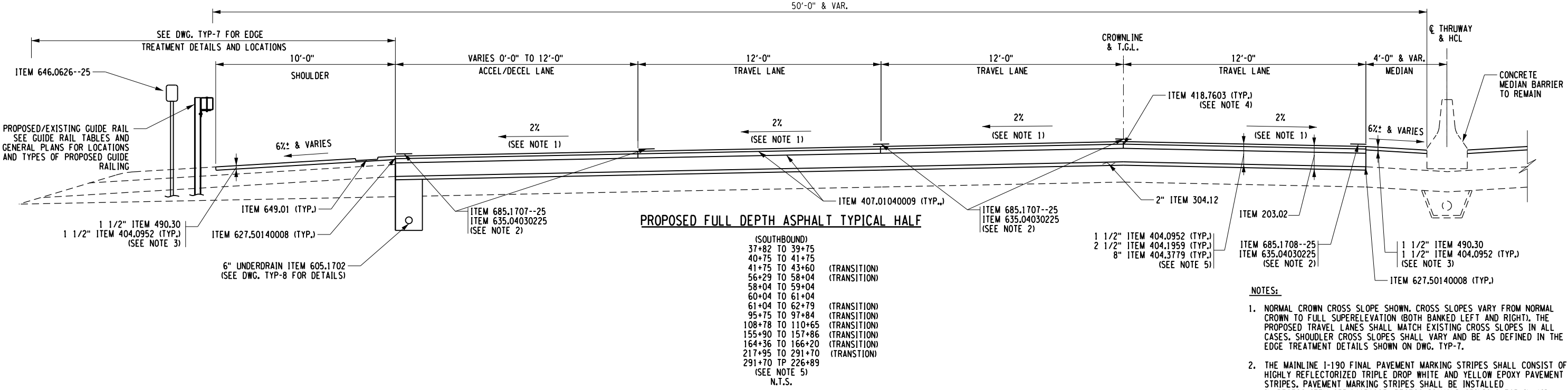
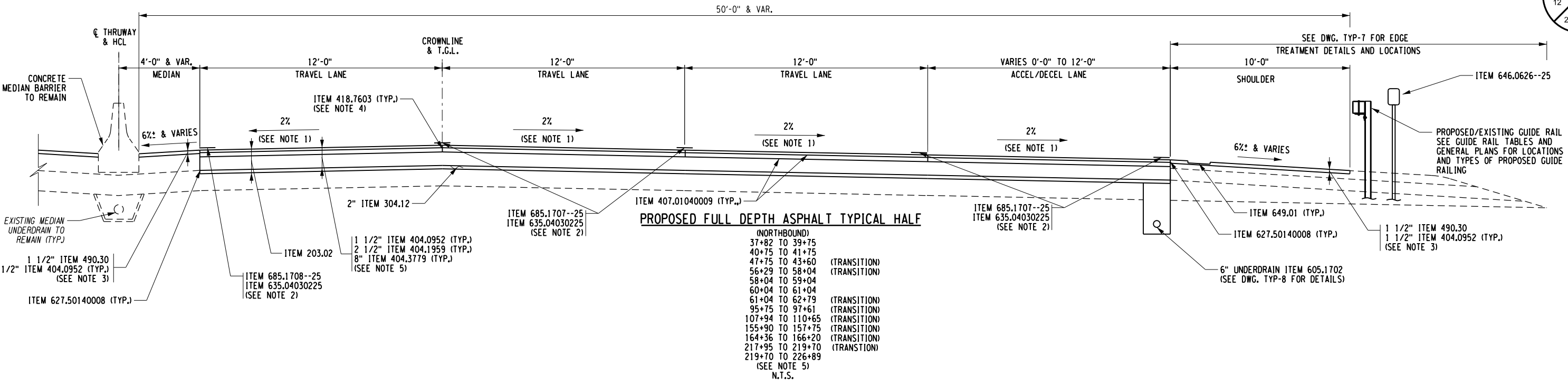
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



- NOTES:**
- NORMAL CROWN CROSS SLOPE SHOWN. CROSS SLOPES VARY FROM NORMAL CROWN TO FULL SUPERELEVATION (BOTH BANKED LEFT AND RIGHT). THE PROPOSED TRAVEL LANES SHALL MATCH EXISTING CROSS SLOPES IN ALL CASES. SHOULDER CROSS SLOPES SHALL VARY AND BE AS DEFINED IN THE EDGE TREATMENT DETAILS SHOWN ON DWG. TYP-7.
  - THE MAINLINE I-190 FINAL PAVEMENT MARKING STRIPES SHALL CONSIST OF HIGHLY REFLECTORIZED TRIPLE DROP WHITE AND YELLOW EPOXY PAVEMENT STRIPES. PAVEMENT MARKING STRIPES SHALL BE INSTALLED IN RECESS ITEM 635.04030225 (RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS).
  - SHOULDER PAVEMENT THICKNESS WILL VARY BETWEEN THE TRANSITION FROM FULL DEPTH PAVEMENT SECTION AND THE 3 1/2" OVERLAY. SEE DWG. TYP-3 FOR OVERLAY SECTION AND DWG. MD-7 FOR THE DETAIL SHOWING THE TRANSITION FROM FULL DEPTH PAVEMENT TO THE 3 1/2" OVERLAY.
  - ITEM 418.7603 SHALL BE APPLIED ON ALL LONGITUDINAL AND TRANSVERSE JOINT FACES OF THE TOP COURSE ASPHALT PAVEMENT.
  - THE TRANSITION STATIONS DEFINE THE LENGTH NEEDED TO ADJUST THE PROFILE BETWEEN THE FULL DEPTH RECONSTRUCTION SECTION AND THE 3 1/2" OVERLAY. A PORTION OF THE FULL DEPTH TOP AND BINDER COURSE MUST BE MILLED OUT TO TRANSITION TO THE OVERLAY FINISHED GRADE. THE CONTRACTOR HAS THE OPTION TO INSTALL 4" OF ITEM 404.1959 BINDER COURSE WITH IN THE DEFINED TRANSITION LENGTHS IN LUE OF PLACING A 1 1/2" TOP COURSE. SEE DWG. MD-7 FOR PAVEMENT TRANSITION DETAILS.

ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	605.1702	OPTIONAL UNDERDRAIN PIPE, 6 INCH DIAMETER	LF
304.12	SUBBASE COURSE TYPE II	CY	627.50140008	CUTTING PAVEMENT	LF
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON	635.04030225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF
404.1959	19 F9 BINDER COURSE ASPHALT, 50 SERIES COMPACTION	TON	646.0626--26	MILE MARKERS	EA
404.3779	37.5 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION	TON	649.01	MILLED-IN AUDIBLE ROADWAY DELINEATORS (MIARDS)	LF
407.01040009	ASPHALT PAVEMENT JOINT ADHESIVE	LF	685.1707--25	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH X 20 MILS	LF
418.7603	NON-TRACKING TACK COAT	GAL	685.1708--25	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH X 20 MILS	LF
490.30	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	SY			

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY**

**Thruway Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TYPICAL SECTIONS	DRAWING NUMBER: TYP-5

ALTERED ON: \_\_\_\_\_

AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_

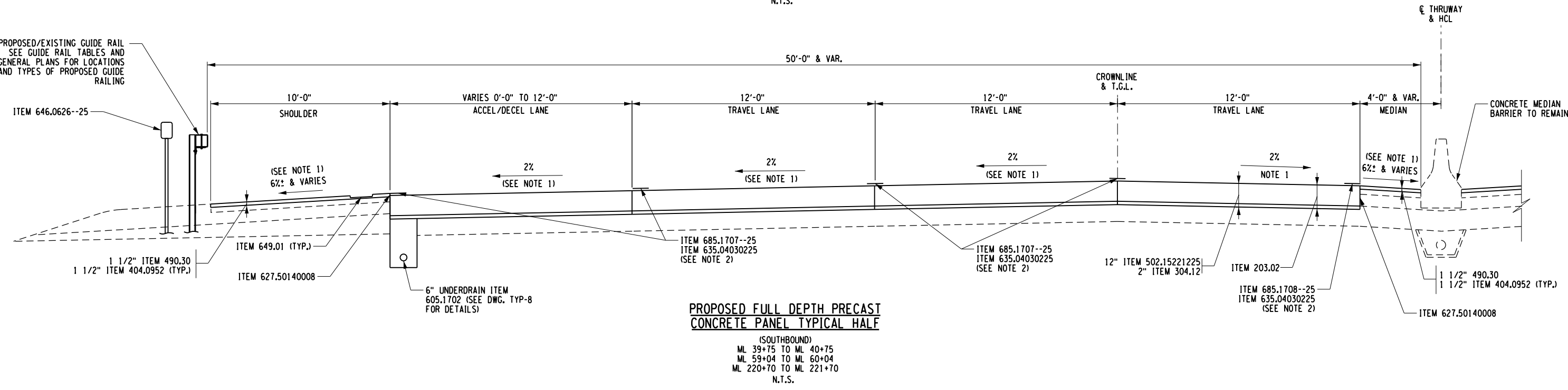
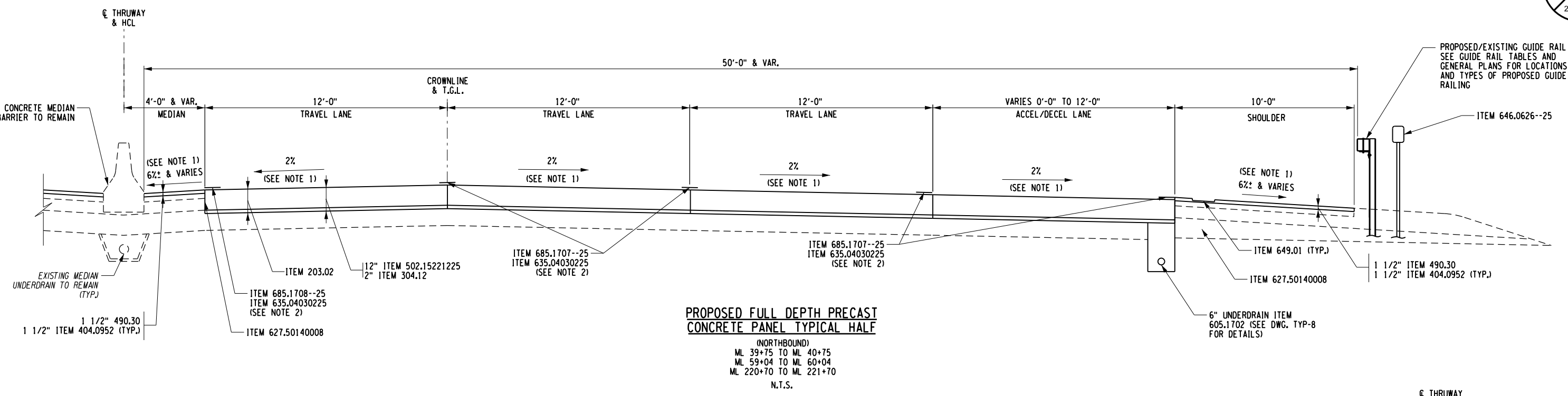
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	635.04030225	RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS	LF
304.12	SUBBASE COURSE TYPE II	CY	646.0626--25	MILE MARKER	EA
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON	649.01	MILLED-IN AUDIBLE ROADWAY DELINEATORS (MIARDS)	LF
407.01040009	NON-TRACKING TACK COAT	GAL	685.1707--25	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH X 20 MILS	LF
490.30	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	SY	685.1708--25	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH X 20 MILS	LF
502.15221225	PRE-CAST CONCRETE PAVEMENT SLABS	CY			
605.1702	OPTIONAL UNDERDRAIN PIPE, 6 INCH DIAMETER	LF			

- NOTES:**
- NORMAL CROWN CROSS SLOPE SHOWN. CROSS SLOPES VARY FROM NORMAL CROWN TO FULL SUPERELEVATION (BOTH BANKED LEFT AND RIGHT). THE PROPOSED TRAVEL LANES SHALL MATCH EXISTING CROSS SLOPES IN ALL CASES.
  - THE MAINLINE I-190 FINAL PAVEMENT MARKING STRIPES SHALL CONSIST OF HIGHLY REFLECTORIZED TRIPLE DROP WHITE AND YELLOW EPOXY PAVEMENT STRIPES. PAVEMENT MARKING STRIPES SHALL BE INSTALLED IN RECESS ITEM 635.04030225 (RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS).

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY

Thruway Authority

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TYPICAL SECTIONS	DRAWING NUMBER: TYP-6

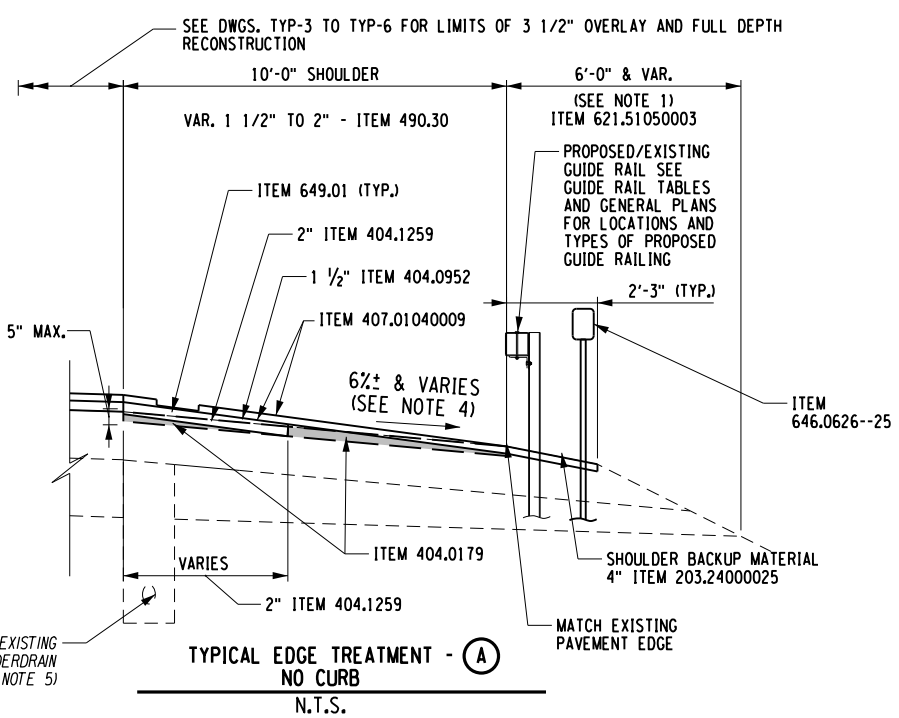
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

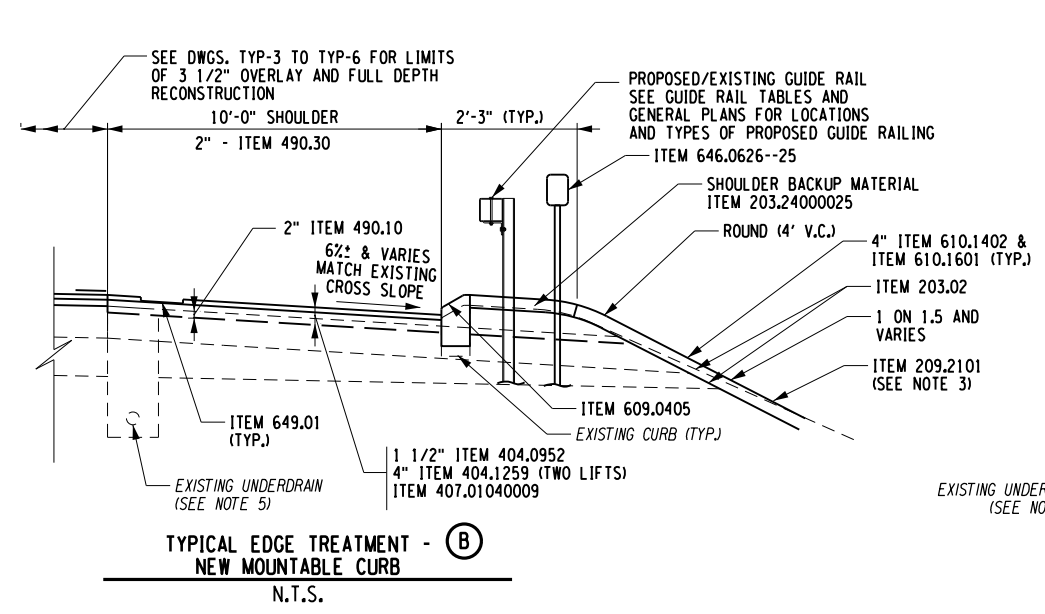
CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

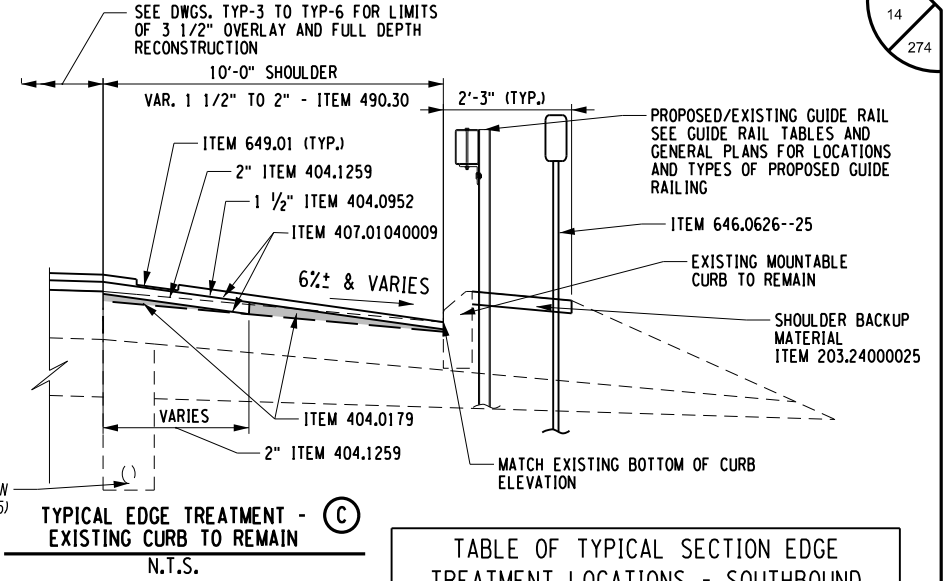
DESIGN SUPERVISOR: D. MARSH



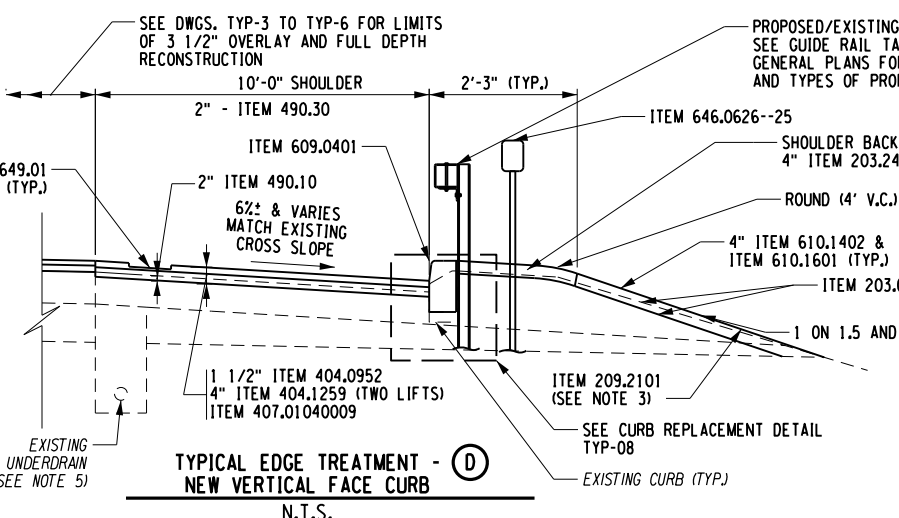
TYPICAL EDGE TREATMENT - **(A)**  
NO CURB  
N.T.S.



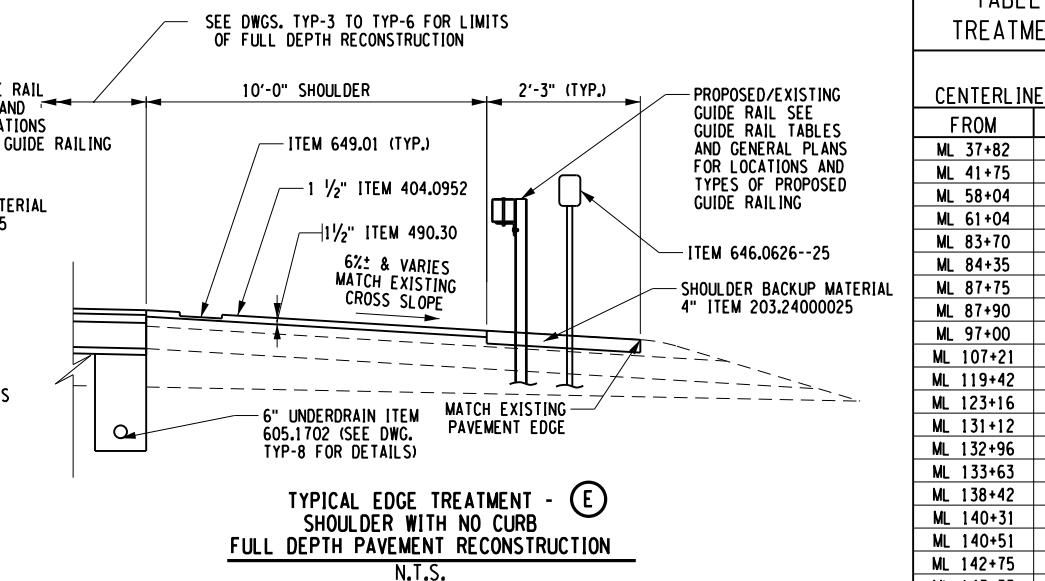
TYPICAL EDGE TREATMENT - **(B)**  
NEW MOUNTABLE CURB  
N.T.S.



TYPICAL EDGE TREATMENT - **(C)**  
EXISTING CURB TO REMAIN  
N.T.S.



TYPICAL EDGE TREATMENT - **(D)**  
NEW VERTICAL FACE CURB  
N.T.S.



TYPICAL EDGE TREATMENT - **(E)**  
SHOULDER WITH NO CURB  
FULL DEPTH PAVEMENT RECONSTRUCTION  
N.T.S.

NOTES:

1. THE CONTRACTOR SHALL TRIM, GRADE AND RESHAPE EXISTING SLOPES FROM THE EDGE OF PAVED SHOULDER TO WIDTH SPECIFIED OR ABOVE. SEE TABLE ON DWG. MT-4 FOR LOCATION.
2. EXISTING CONCRETE BARRIER ADJACENT TO SHOULDER SHALL REMAIN IN ALONG THIS EDGE TREATMENT SEGMENT.
3. SEE DWG. TYP-8 FOR SOIL STABILIZATION LOCATIONS.
4. SHOULDER CROSS SLOPE VARIES, MATCH EXISTING EDGE OF SHOULDER PAVEMENT OR BOTTOM OF CURB ELEVATIONS. SHOULDER CROSS SLOPE SHALL NOT EXCEED 8% ROLLOVER BETWEEN TRAVEL LANES AND SHOULDER OR ABOVE.
5. EXISTING UNDERDRAIN TO REMAIN IN PAVEMENT AREAS RECEIVING A 2 COURSE OVERLAY. NEW UNDERDRAIN WILL BE INSTALLED IN FULL DEPTH PAVEMENT SECTIONS. SEE DWG. TYP-8 FOR LOCATIONS NEW UNDERDRAIN.

ITEM	DESCRIPTION	UNIT
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY
203.03	EMBANKMENT IN PLACE	CY
203.24000025	SHOULDER BACKUP MATERIAL	TON
209.2101	SOIL STABILIZATOIN CLASS IV TYPE A	SY
404.0179	TRUING & LEVELING F9, ASPHALT, 70 SERIES	TON
404.0952	9.5 F2 TOP COURSE WMA, 50 SERIES COMPACTION	TON
404.1259	12.5 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON
404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
490.30	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	SY
605.1702	OPTIONAL UNDERDRAIN PIPE, 6 INCH DIAMETER	LF
609.0401	CAST-IN-PLACE CONCRETE CURB TYPE VF6	LF
609.0405	CAST-IN-PLACE CONCRETE CURB TYPE MA	LF
610.1402	TOPSOIL - ROADSIDE	SY
610.1601	TURF ESTABLISHMENT - ROADSIDE	SY
621.51050003	GRADING MAINTENANCE, CLEANING, TRIMMING & RESHAPING EXISTING ROADSIDE SECTION	LF
627.51050003	CUTTING PAVEMENT	LF
646.0626--25	MILE MARKERS	EA
649.01	MILLED-IN AUDIBLE ROADWAY DELINEATORS (MIARDS)	LF

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CENTERLINE STATION		SIDE	EDGE TREATMENT DETAIL
FROM	TO		
ML 37+82	ML 41+75	NB	E
ML 41+75	ML 58+04	NB	A
ML 58+04	ML 61+04	NB	E
ML 61+04	ML 83+70	NB	A
ML 83+70	ML 84+35	NB	B
ML 84+35	ML 85+05	NB	D
ML 87+75	ML 87+90	NB	D
ML 87+90	ML 97+00	NB	A
ML 97+00	ML 97+78	NB	C
ML 107+21	ML 119+42	NB	C
ML 119+42	ML 119+98	NB	B
ML 123+16	ML 131+12	NB	A
ML 131+12	ML 131+47	NB	D
ML 132+96	ML 133+63	NB	D
ML 133+63	ML 138+42	NB	A
ML 138+42	ML 140+31	NB	C
ML 140+31	ML 140+51	NB	B
ML 140+51	ML 141+15	NB	D
ML 142+75	ML 143+33	NB	D
ML 143+33	ML 143+53	NB	B
ML 143+53	ML 150+50	NB	C
ML 150+50	ML 152+14	NB	B
ML 152+14	ML 152+75	NB	D
ML 154+29	ML 158+14	NB	B
ML 164+17	ML 164+47	NB	D
ML 164+47	ML 167+17	NB	B
ML 167+17	ML 168+48	NB	D
ML 170+35	ML 170+55	NB	D
ML 170+55	ML 171+20	NB	B
ML 171+20	ML 176+00	NB	A
ML 176+00	ML 177+02	NB	B
ML 177+02	ML 177+22	NB	D
ML 179+20	ML 179+60	NB	D
ML 179+60	ML 186+38	NB	A
ML 186+38	ML 186+50	NB	D
ML 188+09	ML 188+47	NB	D
ML 188+47	ML 190+40	NB	B
ML 190+40	ML 194+83	NB	C
ML 194+83	ML 195+03	NB	B
ML 195+03	ML 195+30	NB	D
ML 204+23	ML 204+65	NB	D
ML 204+65	ML 205+67	NB	B
ML 205+67	ML 219+70	NB	A
ML 219+70	ML 226+89	NB	E

TABLE OF TYPICAL SECTION EDGE TREATMENT LOCATIONS - SOUTHBOUND

CENTERLINE STATION	SIDE	EDGE TREATMENT DETAIL
ML 37+88	SB	E
ML 41+75	SB	A
ML 58+04	SB	E
ML 61+04	SB	A
ML 83+75	SB	D
ML 86+66	SB	D
ML 87+00	SB	A
ML 97+30	SB	B
ML 108+70	SB	B
ML 121+43	SB	A
ML 131+10	SB	D
ML 133+17	SB	D
ML 133+70	SB	A
ML 140+25	SB	B
ML 140+87	SB	D
ML 140+87	SB	D
ML 142+98	SB	D
ML 143+46	SB	B
ML 144+78	SB	C
ML 150+50	SB	B
ML 152+45	SB	D
ML 152+75	SB	D
ML 152+75	SB	SEE NOTE 2
ML 163+32	SB	B
ML 164+50	SB	A
ML 164+50	SB	A
ML 168+00	SB	B
ML 169+75	SB	D
ML 169+95	SB	B
ML 169+95	SB	B
ML 171+57	SB	C
ML 177+80	SB	B
ML 178+18	SB	D
ML 179+75	SB	D
ML 179+95	SB	D
ML 179+95	SB	B
ML 180+34	SB	C
ML 186+05	SB	B
ML 186+30	SB	D
ML 186+30	SB	D
ML 188+12	SB	D
ML 188+39	SB	B
ML 188+39	SB	B
ML 190+40	SB	C
ML 194+36	SB	B
ML 194+67	SB	D
ML 194+67	SB	D
ML 197+07	SB	D
ML 197+27	SB	B
ML 197+61	SB	C
ML 197+61	SB	C
ML 200+50	SB	B
ML 202+40	SB	D
ML 204+38	SB	D
ML 204+60	SB	B
ML 206+60	SB	C
ML 206+60	SB	B
ML 212+20	SB	A
ML 212+20	SB	A
ML 219+70	SB	E

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE THRUWAY AUTHORITY

2025 Mark Road Tower  
Suite, WI 10228-3723  
616.463.6997 www.thruwayauthority.com

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TYPICAL SECTIONS	DRAWING NUMBER: TYP-7

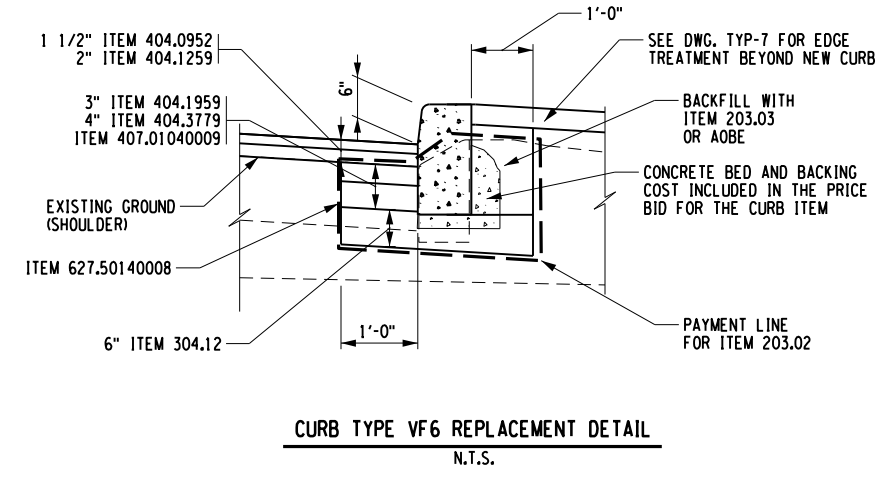
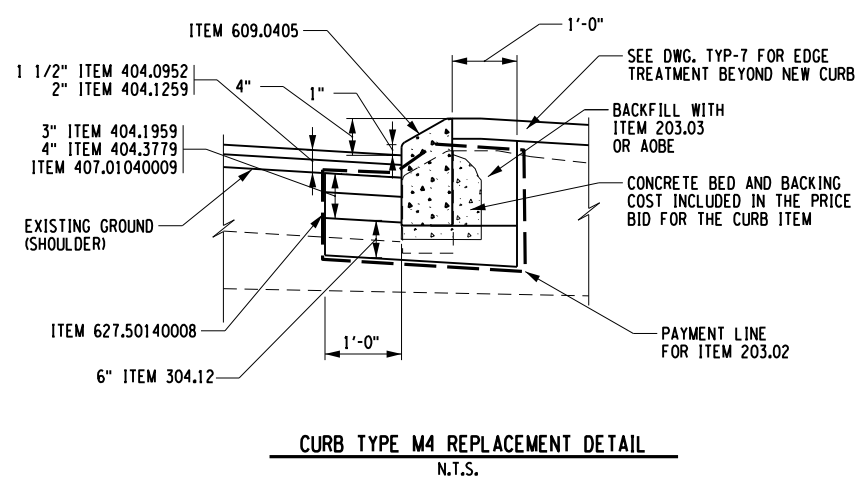
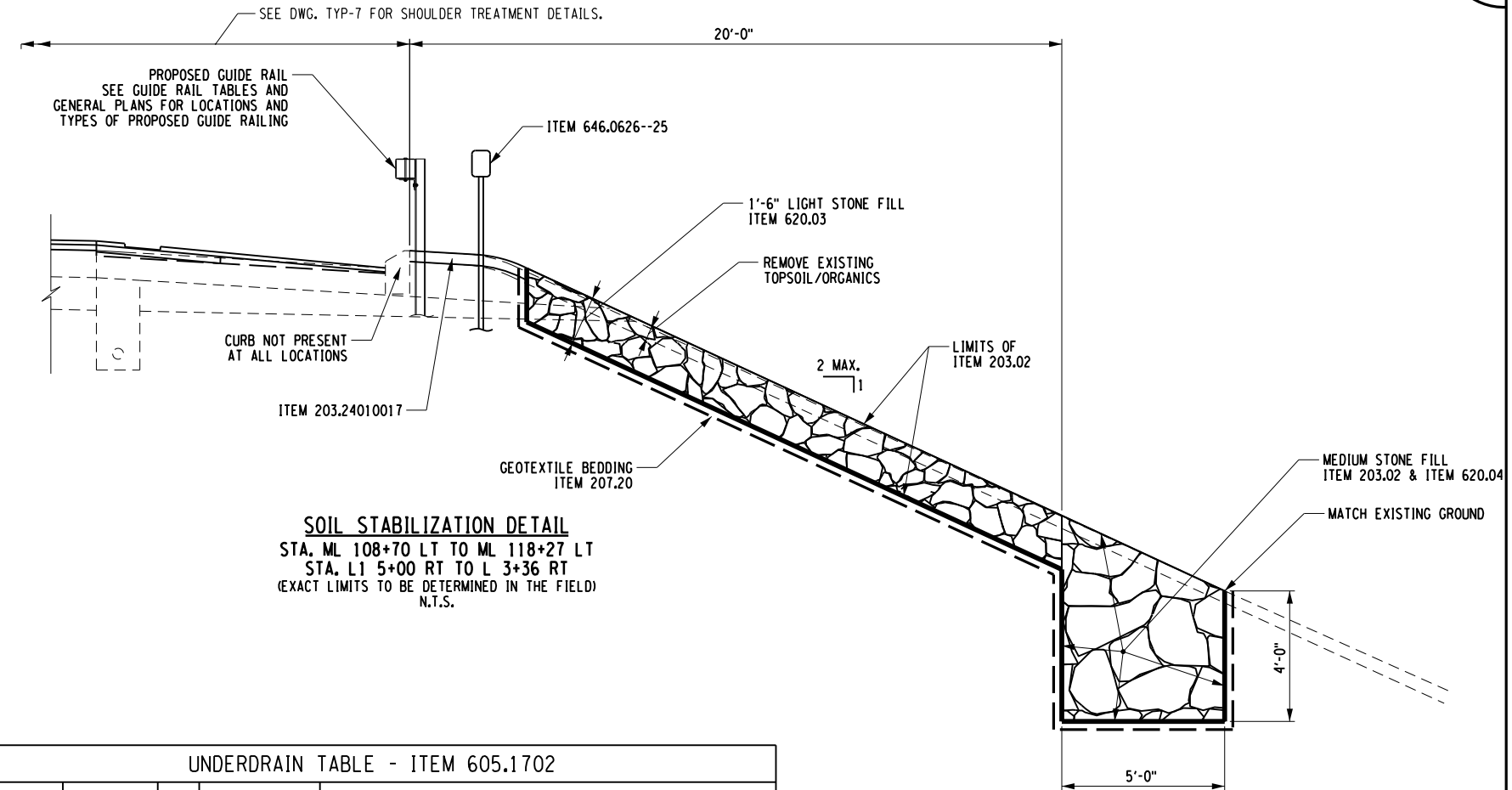
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DRAFTED BY: N. HOY

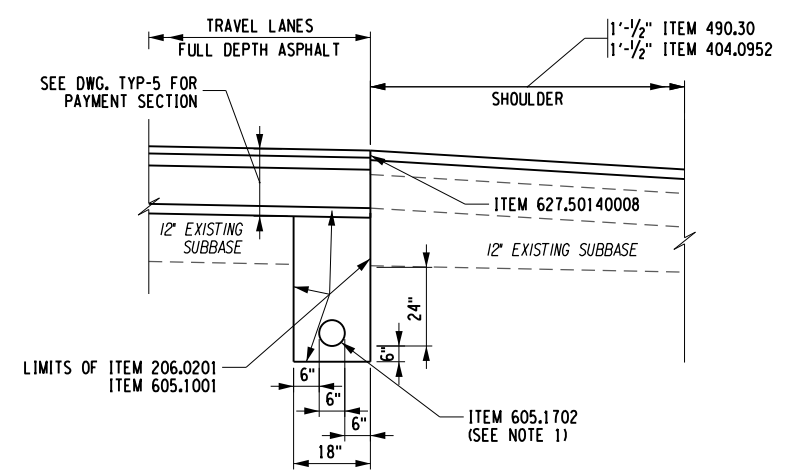
CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



UNDERDRAIN TABLE - ITEM 605.1702				
FROM STATION	TO STATION	SIDE	LENGTH (FT)	COMMENT
ML 37+82	ML 43+60	NB	578.0	CONNECT TO EXISTING UNDERDRAIN AT BEGIN AND END STATIONS
ML 37+88	ML 43+60	SB	572.0	CONNECT TO EXISTING UNDERDRAIN AT BEGIN AND END STATIONS
ML 56+29	ML 62+79	NB	650.0	CONNECT TO EXISTING UNDERDRAIN AT BEGIN AND END STATIONS
ML 56+29	ML 62+79	SB	650.0	CONNECT TO EXISTING UNDERDRAIN AT BEGIN AND END STATIONS
ML 95+75	ML 97+61	NB	186.0	CONNECT TO EXISTING UNDERDRAIN AT BEGIN STATION
ML 95+75	ML 97+84	SB	209.0	CONNECT TO EXISTING UNDERDRAIN AT BEGIN STATION
ML 107+94	ML 110+65	NB	271.0	CONNECT TO EXISTING UNDERDRAIN AT END STATION
ML 108+78	ML 110+65	SB	187.0	CONNECT TO EXISTING UNDERDRAIN AT END STATION
ML 155+90	ML 157+75	NB	185.0	CONNECT TO EXISTING UNDERDRAIN AT BEGIN STATION
ML 155+90	ML 157+86	SB	196.0	CONNECT TO EXISTING UNDERDRAIN AT BEGIN STATION
ML 164+36	ML 166+20	NB	184.0	CONNECT TO EXISTING UNDERDRAIN AT END STATION
ML 164+37	ML 166+20	SB	183.0	CONNECT TO EXISTING UNDERDRAIN AT END STATION
ML 217+95	ML 226+89	NB	894.0	CONNECT TO EXISTING UNDERDRAIN AT BEGIN STATION
ML 217+95	ML 226+89	SB	894.0	CONNECT TO EXISTING UNDERDRAIN AT BEGIN STATION
TOTAL			5839.0	



ITEM	DESCRIPTION	UNIT	ITEM	DESCRIPTION	UNIT
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	490.30	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	SY
203.03	EMBANKMENT IN PLACE	CY	605.1001	UNDERDRAIN FILTER TYPE 2	CY
203.24000025	SHOULDER BACKUP MATERIAL	TON	605.1702	OPTIONAL UNDERDRAIN PIPE, 6 INCH DIAMETER	LF
206.0201	TRENCH AND CULVERT EXCAVATION	CY	609.0401	CAST-IN-PLACE CONCRETE CURB TYPE VF6	LF
207.20	GEOTEXTILE BEDDING	SY	609.0405	CAST-IN-PLACE CONCRETE CURB TYPE MA	LF
304.12	SUBBASE COURSE, TYPE 2	CY	610.1402	TOPSOIL - ROADSIDE	SY
404.0952	9.5 F2 TOP COURSE WMA, 50 SERIES COMPACTION	TON	610.1601	TURF ESTABLISHMENT - ROADSIDE	SY
404.1259	12.5 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	620.03	STONE FILLING (LIGHT)	CY
404.1959	19 F9 BINDER COURSE ASPHALT, 50 SERIES COMPACTION	TON	620.04	STONE FILLING (MEDIUM)	CY
404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	627.50140008	CUTTING PAVEMENT	LF
407.01040009	NON-TRACKING TACK COAT	GAL	646.0626--25	MILE MARKERS	EA

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TYPICAL SECTIONS	DRAWING NUMBER: TYP-8

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

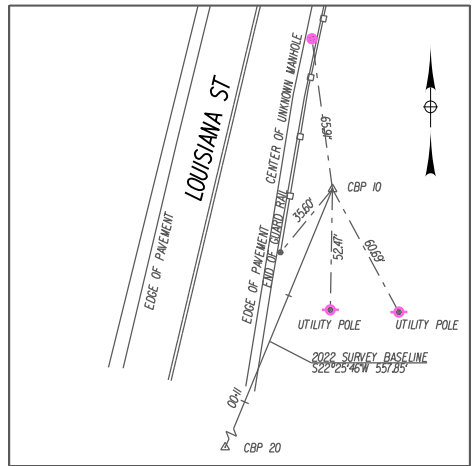
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

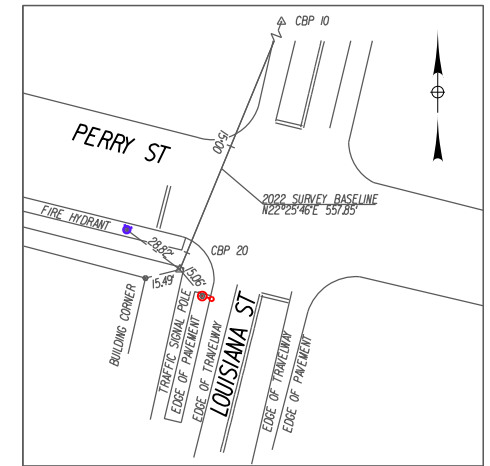
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



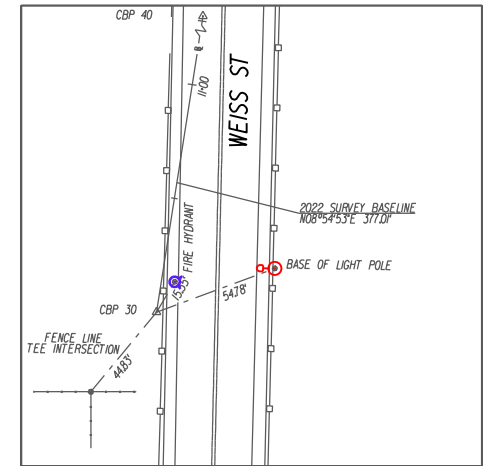
**CBP 10 (STA. 10+00.00)**  
 N.A.D. '83(2011) N.Y.S.P.C.S. WEST ZONE  
 NAVD 1988 GEOID 18  
 N: 1047662.713  
 E: 1073252.954  
 ELEV: 591.65'

CBP 10 IS A REBAR WITH PLASTIC CAP STAMPED "GDB SURVEY" SET ON THE EAST SIDE OF LOUISIANA STREET, 526 +/- NORTH OF THE INTERSECTION OF PERRY STREET AND LOUISIANA STREET.



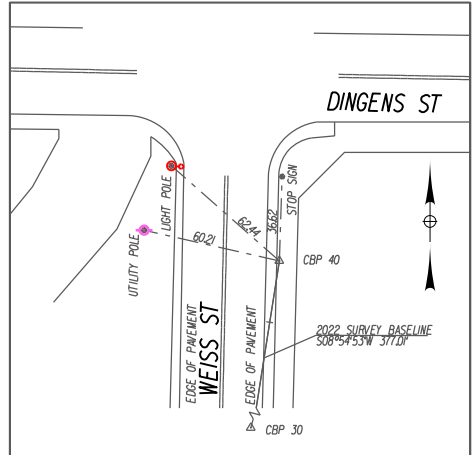
**CBP 20 (STA. 15+57.85)**  
 N.A.D. '83(2011) N.Y.S.P.C.S. WEST ZONE  
 NAVD 1988 GEOID 18  
 N: 1047147.068  
 E: 1073040.111  
 ELEV: 580.93'

CBP 20 IS A REBAR WITH PLASTIC CAP STAMPED "GDB SURVEY" SET ON THE EAST SIDE OF LOUISIANA STREET, 36 +/- SOUTH OF THE INTERSECTION OF PERRY STREET AND LOUISIANA STREET.



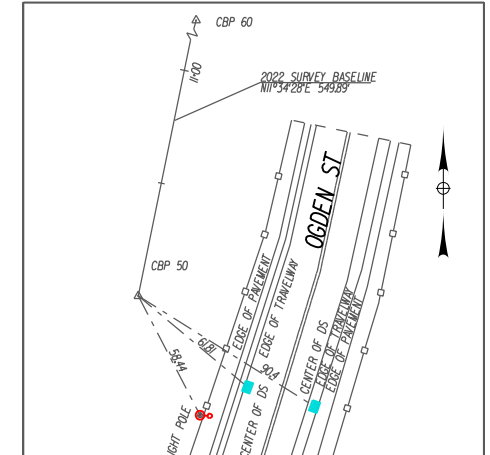
**CBP 30 (STA. 10+00.00)**  
 N.A.D. '83(2011) N.Y.S.P.C.S. WEST ZONE  
 NAVD 1988 GEOID 18  
 N: 1048170.409  
 E: 1087910.644  
 ELEV: 604.91'

CBP 30 IS A REBAR WITH PLASTIC CAP STAMPED "GDB SURVEY" SET ON THE WEST SIDE OF WEISS STREET, 460 +/- SOUTH OF THE INTERSECTION OF DINGENS STREET AND WEISS STREET.



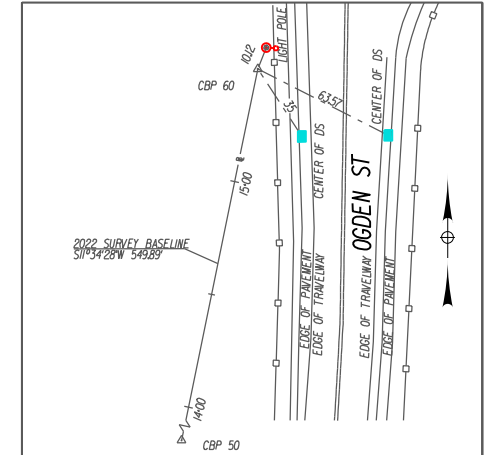
**CBP 40 (STA. 13+77.01)**  
 N.A.D. '83(2011) N.Y.S.P.C.S. WEST ZONE  
 NAVD 1988 GEOID 18  
 N: 1048542.868  
 E: 1087969.067  
 ELEV: 597.55'

CBP 40 IS A REBAR WITH PLASTIC CAP STAMPED "GDB SURVEY" SET ON THE EAST SIDE OF WEISS STREET, 84 +/- SOUTH OF THE INTERSECTION OF DINGENS STREET AND WEISS STREET.



**CBP 50 (STA. 10+00.00)**  
 N.A.D. '83(2011) N.Y.S.P.C.S. WEST ZONE  
 NAVD 1988 GEOID 18  
 N: 1047859.872  
 E: 1089730.563  
 ELEV: 596.03'

CBP 50 IS A REBAR WITH PLASTIC CAP STAMPED "GDB SURVEY" SET ON THE WEST SIDE OF OGDEN STREET, 739 +/- SOUTH OF THE INTERSECTION OF DINGENS STREET AND OGDEN STREET.



**CBP 60 (STA. 15+49.89)**  
 N.A.D. '83(2011) N.Y.S.P.C.S. WEST ZONE  
 NAVD 1988 GEOID 18  
 N: 1048398.584  
 E: 1089840.895  
 ELEV: 600.99'

CBP 60 IS A REBAR WITH PLASTIC CAP STAMPED "GDB SURVEY" SET ON THE WEST SIDE OF OGDEN STREET, 192 +/- SOUTH OF THE INTERSECTION OF DINGENS STREET AND OGDEN STREET.

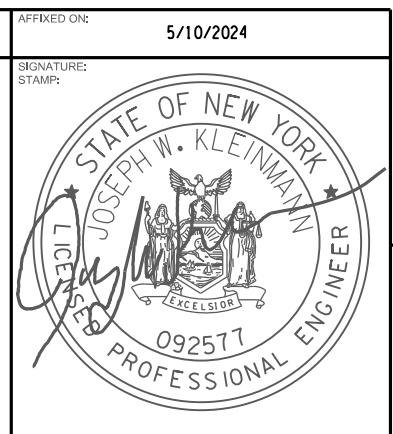
**SURVEY NOTES:**

1. NORTH ORIENTATION AND COORDINATES SHOWN HEREON ARE REFERENCED TO THE NEW YORK STATE PLANE COORDINATE SYSTEM, WESTERN ZONE, N.A.D. '83(2011) EPOCH 2010.00 THROUGH A COMBINATION OF STATIC GPS OBSERVATIONS AND TRIGONOMETRIC MEASUREMENTS. A LEAST SQUARES ADJUSTMENT WAS PERFORMED UTILIZING STARNET VERSION 9.2.6.556 SOFTWARE.
2. ELEVATIONS SHOWN HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (N.A.V.D. 88) GEOID 18 THROUGH TIES TO N.Y.S. C.O.R.S. GPS. DIFFERENTIAL LEVELING WAS PERFORMED THROUGH ALL PRIMARY CONTROL POINTS.
3. UNITS OF MEASURE = U.S. SURVEY FEET
4. SCALE FACTOR : 0.99994222
5. COMBINED SCALE FACTOR : 0.99991927

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING SURVEY BASELINE TIES	DRAWING NUMBER: BLT-1





CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

INTERSTATE I-190 ML HORIZONTAL ALIGNMENT DATA	
P.O.B. STA. ML 16+00.00 N. 1047993.02 E. 1092278.84	
CURVE ML 1	
P.C. STA. ML 49+06.16 N. 1048079.30 E. 1088973.82	
P.I. STA. ML 55+30.24 N. 1048093.52 E. 1088349.91	
P.C.C. STA. ML 61+47.89 N. 1047953.02 E. 1087741.64	
DELTA= 14°13'45"	E= 38.80 L= 1241.72 T= 624.07
RADIUS=5000.00	
BACK TANGENT AZ 271°18'19"	
AHEAD TANGENT AZ 257°04'34"	
CURVE ML 2	
P.C. STA. ML 61+47.89 N. 1047953.94 E. 1087741.64	
P.I. STA. ML 67+99.55 N. 1047808.19 E. 1087106.48	
P.T. STA. ML 74+16.73 N. 1047346.37 E. 1086646.72	
DELTA= 32°12'16"	E= 92.18 L= 1268.84 T= 651.67
RADIUS= 2257.43	
BACK TANGENT AZ 257°04'34"	
AHEAD TANGENT AZ 224°52'19"	
CURVE ML 3	
P.C. STA. ML 94+09.42 N. 1045934.17 E. 1085240.82	
P.I. STA. ML 98+74.57 N. 1045604.52 E. 1084912.65	
P.C.C. STA. ML 103+32.42 N. 1045389.32 E. 1084500.28	
DELTA= 17°34'10"	E= 35.73 L= 922.99 T= 465.15
RADIUS= 3010.00	
BACK TANGENT AZ 224°52'19"	
AHEAD TANGENT AZ 242°26'28"	

INTERSTATE I-190 ML HORIZONTAL ALIGNMENT DATA	
CURVE ML 4	
P.C.C. STA. ML 103+32.42 N. 1045389.32 E. 1084500.28	
P.I. STA. ML 105+24.78 N. 1045300.32 E. 1084329.74	
P.C.C. STA. ML 107+15.06 N. 1045257.23 E. 1084142.27	
DELTA= 14°36'57"	E= 12.28 L= 382.64 T= 192.36
RADIUS=1500.00	
BACK TANGENT AZ 242°26'28"	
AHEAD TANGENT AZ 257°03'25"	
CURVE ML 5	
P.C.C. STA. ML 107+15.06 N. 1045257.23 E. 1084142.27	
P.I. STA. ML 114+72.62 N. 1045087.55 E. 1082410.03	
P.C.C. STA. ML 122+26.62 N. 1045043.68 E. 1082647.66	
DELTA= 9°37'23"	E= 31.83 L= 1511.56 T= 757.56
RADIUS= 9000.00	
BACK TANGENT AZ 257°03'25"	
AHEAD TANGENT AZ 266°40'47"	
CURVE ML 6	
P.C.C. STA. ML 122+26.62 N. 1045043.68 E. 1082647.66	
P.I. STA. ML 124+64.64 N. 1045029.89 E. 1082647.66	
P.T. STA. ML 126+99.20 N. 1045085.87 E. 1082178.69	
DELTA= 16°55'23"	E= 17.61 L= 472.58 T= 238.02
RADIUS= 1600.00	
BACK TANGENT AZ 266°40'47"	
AHEAD TANGENT AZ 283°36'10"	

INTERSTATE I-190 ML HORIZONTAL ALIGNMENT DATA	
CURVE ML 7	
P.C. STA. ML 164+58.33 N. 1045969.98 E. 1078525.01	
P.I. STA. ML 168+90.31 N. 1046071.58 E. 1078105.14	
P.T. STA. ML 173+20.16 N. 1046243.68 E. 1077708.92	
DELTA= 9°52'33"	E= 18.63 L= 861.83 T= 431.98
RADIUS=5000.00	
AHEAD TANGENT AZ 283°36'10"	
BACK TANGENT AZ 293°28'43"	
CURVE ML 8	
P.C. STA. ML 175+71.25 N. 1046343.72 E. 1077478.62	
P.I. STA. ML 180+80.85 N. 1047908.16 E. 1073099.54	
P.T. STA. ML 185+86.64 N. 1046647.11 E. 1076511.59	
DELTA= 12°07'13"	E= 26.98 L= 1015.40 T= 509.60
RADIUS= 4800.00	
BACK TANGENT AZ 293°28'43"	
AHEAD TANGENT AZ 281°21'30"	
CURVE ML 9	
P.C. STA. ML 193+33.11 N. 1046794.12 E. 1075779.74	
P.I. STA. ML 194+39.32 N. 1046815.04 E. 1075675.60	
P.T. STA. ML 195+45.51 N. 1046840.36 E. 1075572.45	
DELTA= 2°26'2"	E= 1.13 L= 212.40 T= 106.21
RADIUS= 5000.00	
BACK TANGENT AZ 281°21'30"	
AHEAD TANGENT AZ 283°47'31"	
CURVE ML 10	
P.C. STA. ML 205+16.30 N. 1047071.80 E. 1074629.65	
P.I. STA. ML 208+52.44 N. 1047151.93 E. 1074303.21	
P.T. STA. ML 211+77.64 N. 1047363.71 E. 1074042.18	
DELTA= 25°15'41"	E= 37.20 L= 661.34 T= 336.13
RADIUS=1500.00	
AHEAD TANGENT AZ 283°47'31"	
BACK TANGENT AZ 309°03'12"	

INTERSTATE I-190 ML HORIZONTAL ALIGNMENT DATA	
CURVE ML 11	
P.C. STA. ML 215+18.88 N. 1047578.70 E. 1073777.19	
P.I. STA. ML 217+84.27 N. 1047745.91 E. 1073571.10	
P.C.C. STA. ML 220+44.21 N. 1047832.25 E. 1073320.16	
DELTA= 20°03'58"	E= 23.30 L= 525.33 T= 265.38
RADIUS= 1500.00	
BACK TANGENT AZ 309°3'12"	
AHEAD TANGENT AZ 288°59'14"	
CURVE ML 12	
P.C.C. STA. ML 220+44.21 N. 1047832.25 E. 1073320.16	
P.I. STA. ML 222+77.53 N. 1047908.16 E. 1073099.54	
P.T. STA. ML 225+10.51 N. 1047963.20 E. 1072872.80	
DELTA= 5°20'36"	E= 5.44 L= 466.29 T= 233.32
RADIUS= 5000.00	
BACK TANGENT AZ 288°59'14"	
AHEAD TANGENT AZ 283°38'38"	
CURVE ML 13	
P.C. STA. ML 243+60.46 N. 1048399.58 E. 1071075.05	
P.I. STA. ML 246+70.82 N. 1048472.79 E. 1070773.45	
P.T. STA. ML 249+72.54 N. 1048659.67 E. 1070525.67	
DELTA= 23°22'46"	E= 31.77 L= 612.08 T= 310.36
RADIUS=1500.00	
AHEAD TANGENT AZ 283°38'38"	
BACK TANGENT AZ 307°01'25"	
CURVE ML 14	
P.C. STA. ML 253+58.09 N. 1048891.83 E. 1070217.85	
P.I. STA. ML 255+83.33 N. 1049027.45 E. 1070038.03	
P.T. STA. ML 258+05.22 N. 1049104.28 E. 1069826.30	
DELTA= 17°04'44"	E= 16.82 L= 447.13 T= 225.23
RADIUS= 1500.00	
BACK TANGENT AZ 307°1'25"	
AHEAD TANGENT AZ 289°56'41"	


INTERSTATE I-190 ML HORIZONTAL ALIGNMENT DATA	
CURVE ML 15	
P.C. STA. ML 261+12.96 N. 1049209.25 E. 1069537.01	
P.I. STA. ML 264+84.06 N. 1049335.84 E. 1069188.18	
P.T. STA. ML 268+40.55 N. 1049610.47 E. 1068938.60	
DELTA= 27°47'30"	E= 45.22 L= 727.58 T= 371.10
RADIUS= 1500.00	
BACK TANGENT AZ 289°56'40"	
AHEAD TANGENT AZ 317°44'10"	
P.O.E. STA. ML 268+40.55 N. 1049610.47 E. 1068938.60	

INTERSTATE I-190 NB HORIZONTAL ALIGNMENT DATA	
P.O.B. STA. NB 16+00.00 N. 1048013.51 E. 1092279.41	
CURVE NB 1	
P.C. STA. NB 49+04.92 N. 1048099.77 E. 1088975.63	
P.I. STA. NB 55+25.68 N. 1048113.91 E. 1088355.03	
P.C.C. STA. NB 61+40.20 N. 1047976.74 E. 1087749.61	
DELTA= 14°04'15"	E= 38.16 L= 1235.28 T= 620.76
RADIUS=5030.00	
AHEAD TANGENT AZ 271°18'19"	
BACK TANGENT AZ 257°14'04"	
CURVE NB 2	
P.C.C. STA. NB 61+40.20 N. 1047976.74 E. 1087749.61	
P.I. STA. NB 68+00.88 N. 1047830.76 E. 1087105.26	
P.T. STA. NB 74+26.19 N. 1047362.33 E. 1086639.33	
DELTA= 32°32'16"	E= 93.99 L= 1285.99 T= 660.68
RADIUS= 2275.00	
BACK TANGENT AZ 257°14'04"	
AHEAD TANGENT AZ 224°50'48"	

INTERSTATE I-190 NB HORIZONTAL ALIGNMENT DATA	
CURVE NB 3	
P.C. STA. NB 94+34.88 N. 1045986.53 E. 1085222.78	
P.I. STA. NB 98+88.01 N. 1045616.92 E. 1084903.23	
P.C.C. STA. NB 103+34.27 N. 1045405.04 E. 1084502.69	
DELTA= 17°16'30"	E= 34.22 L= 899.39 T= 453.13
RADIUS= 3983.00	
BACK TANGENT AZ 224°50'48"	
AHEAD TANGENT AZ 242°07'18"	
CURVE NB 4	
P.C.C. STA. NB103+34.27 N. 1045405.04 E. 1084502.69	
P.I. STA. NB 105+34.73 N. 1045311.30 E. 1084325.49	
P.C.C. STA. NB 107+34.78 N. 1045267.98 E. 1084129.76	
DELTA= 15°23'47"	E= 13.49 L= 398.51 T= 200.46
RADIUS=1483.00	
AHEAD TANGENT AZ 242°07'18"	
BACK TANGENT AZ 257°31'06"	
CURVE NB 5	
P.C.C. STA. NB 107+32.78 N. 1045237.98 E. 1084129.76	
P.I. STA. NB 114+82.53 N. 1045105.93 E. 1083397.73	
P.T. STA. NB 122+29.37 N. 1045057.13 E. 1082349.57	
DELTA= 8°44'59"	E= 28.64 L= 1496.59 T= 749.75
RADIUS= 9800.00	
BACK TANGENT AZ 257°31'05"	
AHEAD TANGENT AZ 266°16'05"	
CURVE NB 6	
P.C.C. STA. NB 122+29.37 N. 1045057.13 E. 1082649.57	
P.I. STA. NB 124+55.69 N. 1045042.40 E. 1082423.72	
P.C.C. STA. NB 126+78.55 N. 1045095.70 E. 1082203.77	
DELTA= 17°21'15"	E= 17.17 L= 449.18 T= 226.32
RADIUS= 1483.00	
BACK TANGENT AZ 266°16'04"	
AHEAD TANGENT AZ 283°37'20"	

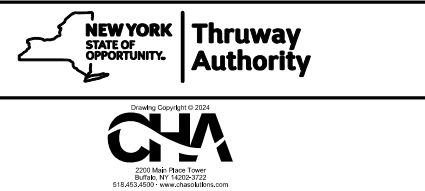
INTERSTATE I-190 NB HORIZONTAL ALIGNMENT DATA	
CURVE NB 7	
P.C.C. STA. NB 164+60.92 N. 1045986.53 E. 1078527.80	
P.I. STA. NB 168+94.62 N. 1046088.67 E. 1078106.30	
P.T. STA. NB 173+26.14 N. 1046262.10 E. 1077708.78	
DELTA= 9°56'55"	E= 18.84 L= 865.22 T= 433.70
RADIUS= 4983.00	
BACK TANGENT AZ 283°37'20"	
AHEAD TANGENT AZ 293°34'15"	
CURVE NB 8	
P.C. STA. NB 175+73.28 N. 1046360.93 E. 1077482.26	
P.I. STA. NB 180+98.71 N. 1046571.04 E. 1077000.67	
P.T. STA. NB 186+20.00 N. 1046672.45 E. 1076485.12	
DELTA= 12°26'33"	E= 28.55 L= 1046.72 T= 525.43
RADIUS=4820.00	
AHEAD TANGENT AZ 293°34'15"	
BACK TANGENT AZ 281°07'42"	
CURVE NB 9	
P.C. STA. NB 192+90.92 N. 1046801.94 E. 1075826.82	
P.I. STA. NB 193+95.75 N. 1046822.17 E. 1075723.96	
P.T. STA. NB 195+00.55 N. 1046846.88 E. 1075622.08	
DELTA= 2°30'09"	E= 1.14 L= 209.62 T= 104.83
RADIUS=4800.00	
AHEAD TANGENT AZ 281°07'42"	
BACK TANGENT AZ 283°37'50"	
CURVE NB 10	
P.C. STA. NB 205+10.47 N. 1047084.88 E. 1074640.60	
P.I. STA. NB 208+44.11 N. 1047163.50 E. 1074316.36	
P.T. STA. NB 211+66.82 N. 1047373.41 E. 1074057.03	
DELTA= 25°21'29"	E= 37.07 L= 656.35 T= 333.64
RADIUS= 1483.00	
BACK TANGENT AZ 283°37'50"	
AHEAD TANGENT AZ 308°59'18"	

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ SIGNATURE: JOSEPH W. KLEINMANN  
STAMP: \_\_\_\_\_ STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 90.7 TO MP 90.42	DATE: MAY 2024
TITLE OF DRAWING ALIGNMENT DATA SHEET	DRAWING NUMBER: AGL-1



CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

INTERSTATE I-190 NB HORIZONTAL ALIGNMENT DATA	
CURVE NB 11	
P.C. STA. NB 215+14.55 N. 1047592.19 E. 1073786.75	
P.I. STA. NB 124+55.69 N. 1045042.40 E. 1082423.72	
P.C.C. STA. NB 126+78.55 N. 1045095.70 E. 1082203.77	
DELTA= 17°21'15"	E= 17.17 L= 449.18 T= 226.32
RADIUS= 1483.00	
BACK TANGENT AZ 308°59'18"	
AHEAD TANGENT AZ 289°14'05"	
CURVE NB 12	
P.C.C. STA. NB 200+38.59 N. 1047845.88 E. 1073331.17	
P.I. STA. NB 222+79.22 N. 1047925.15 E. 1073103.98	
P.T. STA. NB 225+19.45 N. 1047981.45 E. 1072870.03	
DELTA= 5°42'14.89"	E= 5.99 L= 480.86 T= 240.63
RADIUS= 4830.00	
BACK TANGENT AZ 289°15' 05"	
AHEAD TANGENT AZ 283°31'50"	
P.O.E. STA. NB 233+60.50 N. 1048178.23 E. 1072052.32	

INTERSTATE I-190 SB HORIZONTAL ALIGNMENT DATA	
P.O.B. STA. SB 16+00.00 N. 1047972.53 E. 1092278.26	
CURVE SB 1	
P.C. STA. SB 29+25.73 N. 1047998.12 E. 1090952.88	
P.I. STA. SB 31+45.69 N. 1047998.70 E. 1090732.92	
P.T. STA. SB 33+65.57 N. 1048010.03 E. 1090513.24	
DELTA= 2°48'00"	E= 2.69 L= 439.84 T= 219.96
RADIUS= 9000.00	
BACK TANGENT AZ 270°09'07"	
AHEAD TANGENT AZ 272°57'07"	

INTERSTATE I-190 SB HORIZONTAL ALIGNMENT DATA	
CURVE SB 2	
P.C. STA. SB 36+64.84 N. 1048025.44 E. 1090214.37	
P.I. STA. SB 38+39.18 N. 1048034.42 E. 1090040.26	
P.T. STA. SB 40+13.49 N. 1048038.34 E. 1089865.97	
DELTA= 1°39'53"	E= 1.27 L= 348.65 T= 174.34
RADIUS= 12000.00	
BACK TANGENT AZ 272°57'07"	
AHEAD TANGENT AZ 271°17'14"	
CURVE SB 3	
P.C. STA. SB 49+02.37 N. 1048058.31 E. 1088977.32	
P.I. STA. SB 55+21.37 N. 1048072.21 E. 1088358.47	
P.C.C. STA. SB 61+34.15 N. 1047935.37 E. 1087754.79	
DELTA= 14°03'32"	E= 38.02 L= 1231.78 T= 619.00
RADIUS= 5020.00	
BACK TANGENT AZ 271°17'14"	
AHEAD TANGENT AZ 283°31'42"	
CURVE SB 4	
P.C.C. STA. SB 61+34.15 N. 1047935.37 E. 1087754.79	
P.I. STA. SB 67+95.52 N. 1047789.16 E. 1087109.78	
P.T. STA. SB 74+21.56 N. 1047320.49 E. 1086643.13	
DELTA= 32°21'08"	E= 93.99 L= 1287.41 T= 661.37
RADIUS= 2280.00	
BACK TANGENT AZ 257°13'42"	
AHEAD TANGENT AZ 224°52'34"	
CURVE SB 5	
P.C. STA. SB 94+02.84 N. 1045916.49 E. 1085245.19	
P.I. STA. SB 98+59.90 N. 1045592.61 E. 1084922.70	
P.C.C. STA. SB 103+10.07 N. 1045378.65 E. 1084518.82	
DELTA= 17°12'43"	E= 34.39 L= 907.22 T= 457.05
RADIUS= 3020.00	
BACK TANGENT AZ 224°52'34"	
AHEAD TANGENT AZ 242°05'17"	

INTERSTATE I-190 SB HORIZONTAL ALIGNMENT DATA	
CURVE SB 6	
P.C.C. STA. SB 103+10.07 N. 1045378.65 E. 1084518.82	
P.I. STA. SB 105+16.93 N. 1045281.81 E. 1084336.02	
P.C.C. STA. SB 107+21.27 N. 1045237.35 E. 1084133.98	
DELTA= 15°30'01"	E= 14.01 L= 411.21 T= 206.87
RADIUS= 1520.00	
BACK TANGENT AZ 242°05'17"	
AHEAD TANGENT AZ 257°35'18"	
CURVE SB 7	
P.C.C. STA. SB 107+21.27 N. 1045237.35 E. 1084133.98	
P.I. STA. SB 114+69.98 N. 1045076.43 E. 1083402.77	
P.C.C. STA. SB 122+15.92 N. 1045025.96 E. 1082655.77	
DELTA= 8°32'48"	E= 27.93 L= 1494.64 T= 748.71
RADIUS= 10020.00	
BACK TANGENT AZ 257°35'18"	
AHEAD TANGENT AZ 283°08'05"	
CURVE SB 8	
P.C.C. STA. SB 122+15.92 N. 1045025.96 E. 1082655.77	
P.I. STA. SB 124+49.40 N. 1045010.22 E. 1082422.81	
P.T. STA. SB 126+79.26 N. 1045065.12 E. 1082195.88	
DELTA= 17°27'56"	E= 17.83 L= 463.34 T= 233.48
RADIUS= 1520.00	
BACK TANGENT AZ 257°13'42"	
AHEAD TANGENT AZ 283°36'02"	
CURVE SB 9	
P.C. STA. SB 164+57.44 N. 1045953.56 E. 1078523.64	
P.I. STA. SB 168+94.38 N. 1046056.31 E. 1078098.95	
P.T. STA. SB 173+29.13 N. 1047817.54 E. 1077698.39	
DELTA= 9°56'57"	E= 18.98 L= 871.70 T= 436.95
RADIUS= 5020.00	
BACK TANGENT AZ 283°36'02"	
AHEAD TANGENT AZ 293°32'58"	

INTERSTATE I-190 SB HORIZONTAL ALIGNMENT DATA	
CURVE SB 10	
P.C. STA. SB 175+78.38 N. 1046330.47 E. 1077469.90	
P.I. STA. SB 181+01.17 N. 1046539.34 E. 1076990.66	
P.T. STA. SB 186+19.85 N. 1046640.15 E. 1076477.69	
DELTA= 12°25'54"	E= 28.39 L= 1041.46 T= 522.78
RADIUS= 4800.00	
BACK TANGENT AZ 293°32'58"	
AHEAD TANGENT AZ 281°07'05"	
CURVE SB 11	
P.C. STA. SB 192+82.71 N. 1046767.97 E. 1075827.26	
P.I. STA. SB 193+94.59 N. 1046789.55 E. 1075717.49	
P.T. STA. SB 195+06.43 N. 1046815.99 E. 1075608.78	
DELTA= 2°33'12"	E= 1.25 L= 223.72 T= 111.88
RADIUS= 5020.00	
BACK TANGENT AZ 281°07'05"	
AHEAD TANGENT AZ 283°40'17"	
CURVE SB 12	
P.C. STA. SB 205+09.67 N. 1047053.11 E. 1074633.96	
P.I. STA. SB 208+49.22 N. 1047133.36 E. 1074304.03	
P.T. STA. SB 211+77.81 N. 1047346.39 E. 1074039.61	
DELTA= 25°11'07"	E= 37.47 L= 668.14 T= 339.56
RADIUS= 1520.00	
BACK TANGENT AZ 283°40'17"	
AHEAD TANGENT AZ 308°51'24"	
CURVE SB 13	
P.C. STA. SB 215+33.43 N. 1047569.50 E. 1073762.68	
P.I. STA. SB 217+93.90 N. 1047732.91 E. 1073559.85	
P.C.C. STA. SB 220+49.13 N. 1047817.54 E. 1073313.51	
DELTA= 19°53'49"	E= 22.67 L= 515.69 T= 260.47
RADIUS= 1485.00	
BACK TANGENT AZ 308°51'24"	
AHEAD TANGENT AZ 288°57'35"	

INTERSTATE I-190 SB HORIZONTAL ALIGNMENT DATA	
CURVE SB 14	
P.C.C. STA. SB 220+49.13 N. 1047817.54 E. 1073313.51	
P.I. STA. SB 222+86.62 N. 1047894.70 E. 1073088.90	
P.T. STA. SB 225+23.76 N. 1047950.32 E. 1072858.01	
DELTA= 5°25'02"	E= 5.61 L= 474.63 T= 237.49
RADIUS= 5020.00	
BACK TANGENT AZ 288°57'35"	
AHEAD TANGENT AZ 283°32'33"	
P.O.E. STA. SB 233+60.08 N. 1048146.15 E. 1072044.94	

INTERSTATE I-190 RAMP A ALIGNMENT DATA	
P.O.B. STA. A 1+00.00 N. 1048073.60 E. 1091481.01	
CURVE RAMP A 1	
P.C. STA. A 6+59.43 N. 1048089.41 E. 1090921.81	
P.I. STA. A 9+08.63 N. 1048096.45 E. 1090672.70	
P.T. STA. A 11+54.88 N. 1048168.82 E. 1090434.24	
DELTA= 15°15'41"	E= 16.62 L= 495.45 T= 249.20
RADIUS= 1860.00	
BACK TANGENT AZ 271°37'11"	
AHEAD TANGENT AZ 286°52'52"	
CURVE RAMP A 2	
P.C.C. STA. A 11+54.88 N. 1048168.82 E. 1090434.24	
P.I. STA. A 13+11.58 N. 1048214.32 E. 1090284.29	
P.T. STA. A 14+24.13 N. 1048371.02 E. 1090285.23	
DELTA= 73°27'38"	E= 52.02 L= 269.25 T= 156.70
RADIUS= 210.00	
BACK TANGENT AZ 286°52'52"	
AHEAD TANGENT AZ 0°20'30"	
P.O.E. STA. A 16+19.28 N. 1048566.17 E. 1090286.39	

INTERSTATE I-190 RAMP B ALIGNMENT DATA	
P.O.B. STA. B 1+00.00 N. 1047511.43 E. 1090284.47	
CURVE RAMP B 1	
P.C. STA. B 2+82.21 N. 1047693.55 E. 1090290.44	
P.I. STA. B 5+06.12 N. 1047917.34 E. 1090297.76	
P.C.C. STA. B 6+58.06 N. 1047955.52 E. 1090518.39	
DELTA= 78°18'22"	E= 79.63 L= 375.84 T= 223.91
RADIUS= 275.00	
BACK TANGENT AZ 1°52'32"	
AHEAD TANGENT AZ 80°10'54"	
CURVE RAMP B 2	
P.C.C. STA. B 6+58.06 N. 1047955.52 E. 1090518.39	
P.I. STA. B 7+14.28 N. 1047955.52 E. 1090518.39	
P.T. STA. B 7+70.12 N. 1047963.29 E. 1090629.99	
DELTA= 11°40'27"	E= 2.87 L= 112.06 T= 56.23
RADIUS= 550.00	
BACK TANGENT AZ 80°10'54"	
AHEAD TANGENT AZ 88°08'40"	
P.O.E. STA. B 7+70 N. 1047963.29 E. 1090629.99	

INTERSTATE I-190 RAMP C ALIGNMENT DATA	
P.O.B. STA. C 1+00.00 N. 1048384.52 E. 1089899.49	
CURVE RAMP C 1	
P.C. STA. C 1+00.00 N. 1048384.52 E. 1089899.49	
P.I. STA. C 3+52.87 N. 1048637.29 E. 1089906.52	
P.C.C. STA. C 4+27.58 N. 1048529.05 E. 1090135.05	
DELTA= 113°45'03"	E= 136.94 L= 327.58 T= 252.87
RADIUS= 165.00	
BACK TANGENT AZ 1°35'32"	
AHEAD TANGENT AZ 115°20'35"	

INTERSTATE I-190 RAMP C ALIGNMENT DATA	
CURVE RAMP C 2	
P.C.C. STA. C 4+27.58 N. 1048529.05 E. 1090135.05	
P.I. STA. C 7+80.78 N. 1048377.87 E. 1090454.26	
P.C.C. STA. C 8+49.80 N. 1048181.91 E. 1090160.41	
DELTA= 120°57'30"	E= 205.89 L= 422.22 T= 353.20
RADIUS= 200.00	
BACK TANGENT AZ 115°20'33"	
AHEAD TANGENT AZ 236°18'02"	
CURVE RAMP C 3	
P.C.C. STA. C 8+49.80 N. 1048181.91 E. 1090160.41	
P.I. STA. C 9+75.94 N. 1048111.92 E. 1090055.47	
P.T. STA. C 10+94.18 N. 1048114.79 E. 1089929.37	
DELTA= 35°00'17"	E= 19.42 L= 244.38 T= 126.14
RADIUS= 400.00	
BACK TANGENT AZ 236°18'01"	
AHEAD TANGENT AZ 271°18'18"	
P.O.E. STA. C 10+94.18 N. 1048114.79 E. 1089929.37	

INTERSTATE I-190 RAMP D ALIGNMENT DATA	
P.O.B. STA. D 1+00.00 N. 1048000.70 E. 1089910.32	
CURVE RAMP D 1	
P.C. STA. D 1+00.00 N. 1048000.70 E. 1089910.32	
P.I. STA. D 2+49.75 N. 1047997.29 E. 1090060.04	
P.C.C. STA. D 3+86.58 N. 1047896.39 E. 1090170.70	
DELTA= 41°02'60"	E= 27.11 L= 286.58 T= 149.75
RADIUS= 400.00	
BACK TANGENT AZ 91°18'19"	
AHEAD TANGENT AZ 132°21'19"	

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ALIGNMENT DATA SHEET	DRAWING NUMBER: AGL-2

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

### INTERSTATE I-190 RAMP D ALIGNMENT DATA

CURVE RAMP D 2	
P.C.C. STA. D 3+86.58 N. 1047896.39 E. 1090170.70	
P.I. STA. D 7+25.93 N. 1047667.81 E. 1090421.50	
P.C.C. STA. D 8+01.87 N. 1047559.11 E. 1090100.04	
DELTA= 118°58'20"	E= 193.90 L= 415.29 T= 339.34
RADIUS= 200.00	
BACK TANGENT AZ 132°20'47"	
AHEAD TANGENT AZ 251°19'07"	
CURVE RAMP D 3	
P.C.C. STA. D 8+01.87 N. 1047559.11 E. 1090100.04	
P.I. STA. D 8+85.80 N. 1047532.23 E. 1090020.53	
P.T. STA. D 9+68.24 N. 1047532.23 E. 1089936.61	
DELTA= 18°40'45"	E= 6.86 L= 166.36 T= 83.93
RADIUS= 510.29	
BACK TANGENT AZ 251°19'12"	
AHEAD TANGENT AZ 269°59'58"	
P.O.E. STA. D 9+68.24 N. 1047532.23 E. 1089936.61	

### INTERSTATE I-190 RAMP E ALIGNMENT DATA

CURVE RAMP E 1	
P.O.B. STA. E 1+00.00 N. 1046942.27 E. 1086772.81	
P.C. STA. E 3+23.29 N. 1047165.50 E. 1086778.17	
P.I. STA. E 5+80.25 N. 1047422.38 E. 1086784.34	
P.T. STA. E 7+98.00 N. 1047566.90 E. 1086996.80	
DELTA= 54°23'53"	E= 62.16 L= 474.71 T= 256.95
RADIUS= 500.00	
BACK TANGENT AZ 1°22'34"	
AHEAD TANGENT AZ 55°46'27"	
P.O.E. STA. E 7+98.01 N. 1047566.90 E. 1086996.80	

### INTERSTATE I-190 RAMP F ALIGNMENT DATA

CURVE RAMP F 1	
P.O.B. STA. F 1+00.00 N. 1046989.11 E. 1086364.47	
P.C. STA. F 1+00.00 N. 1046989.11 E. 1086364.47	
P.I. STA. F 2+55.18 N. 1047099.08 E. 1086473.95	
P.C.C. STA. F 3+82.74 N. 1047062.68 E. 1086624.81	
DELTA= 58°41'40"	E= 40.64 L= 282.74 T= 155.18
RADIUS= 276.00	
BACK TANGENT AZ 44°52'21"	
AHEAD TANGENT AZ 103°34'00"	
CURVE RAMP F 2	
P.C.C. STA. F 3+82.74 N. 1047062.68 E. 1086624.81	
P.I. STA. F 4+95.39 N. 1047036.26 E. 1086734.31	
P.T. STA. F 5+72.46 N. 1046923.63 E. 1086731.93	
DELTA= 77°38'35"	E= 39.69 L= 189.72 T= 112.65
RADIUS= 140.00	
BACK TANGENT AZ 103°33'58"	
AHEAD TANGENT AZ 181°12'34"	
P.O.E. STA. F 6+00.00 N. 1046896.09 E. 1086731.35	

### INTERSTATE I-190 RAMP G ALIGNMENT DATA

CURVE RAMP G 1	
P.O.B. STA. G 1+00.00 N. 1047189.56 E. 1086417.28	
P.C. STA. G 1+00.00 N. 1047189.56 E. 1086417.28	
P.I. STA. G 1+88.77 N. 1047126.65 E. 1086354.65	
P.T. STA. G 2+77.07 N. 1047075.76 E. 1086281.92	
DELTA= 10°08'44"	E= 3.93 L= 177.07 T= 88.77
RADIUS= 1000.00	
BACK TANGENT AZ 224°52'21"	
AHEAD TANGENT AZ 235°01'04"	
CURVE RAMP G 2	
P.C. STA. G 5+91.08 N. 1046895.74 E. 1086024.64	
P.I. STA. G 7+06.04 N. 1046829.83 E. 1085930.45	
P.T. STA. G 8+20.75 N. 1046753.55 E. 1085844.43	
DELTA= 6°34'47"	E= 3.30 L= 229.68 T= 114.96
RADIUS= 2000.00	
BACK TANGENT AZ 235°01'04"	
AHEAD TANGENT AZ 228°26'17"	
P.O.E. STA. G 11+41.07 N. 1046541.05 E. 1085604.75	

### INTERSTATE I-190 RAMP H ALIGNMENT DATA

CURVE RAMP H 1	
P.O.B. STA. H 1+00.00 N. 1046491.87 E. 1085600.55	
P.C. STA. H 6+05.87 N. 1046133.58 E. 1085243.43	
P.I. STA. H 7+10.73 N. 1046059.32 E. 1085169.40	
P.T. STA. H 8+15.25 N. 1047975.48 E. 1085106.43	
DELTA= 7°59'51"	E= 3.66 L= 209.37 T= 104.86
RADIUS= 1500.00	
BACK TANGENT AZ 224°54'26"	
AHEAD TANGENT AZ 216°54'35"	
CURVE RAMP H 2	
P.C. STA. H 10+64.54 N. 1045776.14 E. 1084956.71	
P.I. STA. H 11+56.48 N. 1045702.63 E. 1084901.50	
P.T. STA. H 12+47.00 N. 1045649.02 E. 1084826.80	
DELTA= 17°25'24"	E= 7.00 L= 182.46 T= 91.94
RADIUS= 600.00	
BACK TANGENT AZ 216°54'35"	
AHEAD TANGENT AZ 234°19'60"	
P.O.E. STA. H 12+47.00 N. 1045649.02 E. 1084826.80	

### INTERSTATE I-190 RAMP J ALIGNMENT DATA

CURVE RAMP J 1	
P.O.B. STA. J 1+00.00 N. 1045029.36 E. 1082189.91	
P.C. STA. J 1+69.70 N. 1045012.95 E. 1082257.65	
P.I. STA. J 4+19.07 N. 1044954.25 E. 1082500.01	
P.T. STA. J 6+53.69 N. 1044769.97 E. 1082668.03	
DELTA= 34°01'32"	E= 37.30 L= 483.99 T= 249.37
RADIUS= 815.00	
BACK TANGENT AZ 103°36'60"	
AHEAD TANGENT AZ 137°38'32"	
CURVE RAMP J 2	
P.C. STA. J 9+50.46 N. 1044550.68 E. 1082867.98	
P.I. STA. J 9+90.39 N. 1044521.17 E. 1082894.88	
P.T. STA. J 10+29.71 N. 1044484.97 E. 1082911.73	
DELTA= 17°23'46"	E= 3.04 L= 79.24 T= 39.93
RADIUS= 261.00	
BACK TANGENT AZ 137°38'32"	
AHEAD TANGENT AZ 155°02'17"	
CURVE RAMP J 3	
P.C.C. STA. J 10+29.71 N. 1044484.97 E. 1082911.73	
P.I. STA. J 11+18.66 N. 1044404.32 E. 1082949.27	
P.C.C. STA. J 11+99.57 N. 1044319.29 E. 1082923.15	
DELTA= 42°02'22"	E= 16.50 L= 169.86 T= 88.96
RADIUS= 231.50	
BACK TANGENT AZ 155°02'17"	
AHEAD TANGENT AZ 197°04'39"	
P.O.E. STA. J 11+99.57 N. 1044319.29 E. 1082923.15	

### INTERSTATE I-190 RAMP K ALIGNMENT DATA

CURVE RAMP K 1	
P.O.B. STA. K 1+00.00 N. 1045145.12 E. 1082559.78	
P.C. STA. K 7+63.88 N. 1045212.76 E. 1081899.36	
P.I. STA. K 8+04.68 N. 1045216.92 E. 1081858.76	
P.T. STA. K 8+45.36 N. 1045226.54 E. 1081819.11	
DELTA= 7°46'51"	E= 1.39 L= 81.48 T= 40.80
RADIUS= 600.00	
BACK TANGENT AZ 275°50'52"	
AHEAD TANGENT AZ 283°37'43"	
P.O.E. STA. K 8+45.36 N. 1045226.54 E. 1081819.11	

### INTERSTATE I-190 RAMP L ALIGNMENT DATA

CURVE RAMP L 1	
P.O.B. STA. L 1+00.00 N. 1045804.36 E. 1078353.96	
P.C. STA. L 1+00.00 N. 1045804.36 E. 1078353.96	
P.I. STA. L 2+49.90 N. 1045950.02 E. 1078389.36	
P.T. STA. L 3+35.52 N. 1045914.72 E. 1078535.04	
DELTA= 89°57'41"	E= 62.06 L= 235.52 T= 149.90
RADIUS= 150.00	
BACK TANGENT AZ 13°39'30"	
AHEAD TANGENT AZ 103°37'11"	
P.O.E. STA. L 3+35.52 N. 1045914.72 E. 1078535.04	

### INTERSTATE I-190 RAMP L1 ALIGNMENT DATA

CURVE RAMP L1 1	
P.O.B. STA. L1 1+00.00 N. 1045538.94 E. 1077949.85	
P.C. STA. L1 2+47.96 N. 1045504.00 E. 1078093.62	
P.I. STA. L1 4+11.91 N. 1045465.30 E. 1078252.93	
P.T. STA. L1 5+05.52 N. 1045624.60 E. 1078291.69	
DELTA= 89°58'51"	E= 67.89 L= 257.56 T= 163.94
RADIUS= 164.00	
BACK TANGENT AZ 103°39'22"	
AHEAD TANGENT AZ 13°40'31"	
P.O.E. STA. L1 6+94.90 N. 1045808.61 E. 1078336.47	

### INTERSTATE I-190 RAMP M ALIGNMENT DATA

CURVE RAMP M 1	
P.O.B. STA. M 1+00.00 N. 1046014.12 E. 1078175.97	
P.C. STA. M 1+00.00 N. 1046014.12 E. 1078175.97	
P.I. STA. M 1+48.67 N. 1045999.41 E. 1078222.36	
P.C.C. STA. M 1+96.50 N. 1045970.79 E. 1078261.72	
DELTA= 18°25'50"	E= 3.92 L= 96.50 T= 48.67
RADIUS= 300.00	
BACK TANGENT AZ 107°35'36"	
AHEAD TANGENT AZ 126°01'26"	
CURVE RAMP M 2	
P.C.C. STA. M 1+96.50 N. 1045970.79 E. 1078261.72	
P.I. STA. M 2+96.99 N. 1045911.69 E. 1078342.99	
P.T. STA. M 3+73.58 N. 1045814.04 E. 1078319.26	
DELTA= 67°38'12"	E= 30.55 L= 177.07 T= 100.49
RADIUS= 150.00	
BACK TANGENT AZ 126°01'36"	
AHEAD TANGENT AZ 193°39'48"	
P.O.E. STA. M 3+73.58 N. 1045814.04 E. 1078319.26	

ALTERED ON:

AFFIXED ON: 5/10/2024

SIGNATURE:  
STAMP:

SIGNATURE:  
STAMP:



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
ALIGNMENT DATA SHEET

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
AGL-3

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

INTERSTATE I-190 RAMP N ALIGNMENT DATA	
P.O.B. STA. N 1+00.00 N. 1046187.00 E. 1077992.18	
CURVE RAMP N 1	
P.C. STA. N 1+00.00 N. 1046187.00 E. 1077992.18	
P.I. STA. N 1+82.05 N. 1046215.35 E. 1077915.18	
P.C.C. STA. N 2+62.32 N. 1046269.05 E. 1077853.14	
DELTA= 20°40'04"      E= 7.42 RADIUS= 450.00        L= 162.32 T= 82.05	
BACK TANGENT AZ 290°12'41"	
AHEAD TANGENT AZ 310°52'45"	
CURVE RAMP N 2	
P.C.C. STA. N 2+62.32 N. 1046269.05 E. 1077853.14	
P.I. STA. N 5+06.91 N. 1046429.12 E. 1077668.21	
P.T. STA. N 6+34.51 N. 1046600.08 E. 1077843.13	
DELTA= 94°46'36"      E= 107.34 RADIUS= 225.00        L= 372.19 T= 244.59	
BACK TANGENT AZ 310°52'46"	
AHEAD TANGENT AZ 45°39'22"	
P.O.E. STA. N 6+34.51 N. 1046600.08 E. 1077843.13	

INTERSTATE I-190 RAMP O ALIGNMENT DATA	
P.O.B. STA. O 1+00.00 N. 1046627.97 E. 1077815.87	
CURVE RAMP O 1	
P.C. STA. O 2+86.16 N. 1046497.85 E. 1077682.74	
P.I. STA. O 4+65.90 N. 1046372.22 E. 1077554.20	
P.T. STA. O 6+04.55 N. 1046439.53 E. 1077387.54	
DELTA= 66°20'13"      E= 53.53 RADIUS= 275.00        L= 318.39 T= 179.74	
BACK TANGENT AZ 225°39'21"	
AHEAD TANGENT AZ 291°59'34"	
P.O.E. STA. O 6+04.55 N. 1046439.53 E. 1077387.54	

INTERSTATE I-190 RAMP O1 ALIGNMENT DATA	
P.O.B. STA. O1 1+00.00 N. 1046638.16 E. 1078216.96	
CURVE RAMP O1 1	
P.C. STA. O1 2+63.45 N. 1046676.76 E. 1078058.13	
P.I. STA. O1 3+98.99 N. 1046708.76 E. 1077926.43	
P.T. STA. O1 5+10.97 N. 1046614.03 E. 1077829.50	
DELTA= 58°00'07"      E= 35.05 RADIUS= 244.50        L= 247.51 T= 135.53	
BACK TANGENT AZ 283°39'29"	
AHEAD TANGENT AZ 225°39'22"	
P.O.E. STA. O1 5+10.97 N. 1046614.03 E. 1077829.50	

INTERSTATE I-190 RAMP P ALIGNMENT DATA	
P.O.B. STA. P 1+00.00 N. 1047117.00 E. 1074662.33	
CURVE RAMP P 1	
P.C. STA. P 1+00.00 N. 1047117.00 E. 1074662.33	
P.I. STA. P 1+68.77 N. 1047133.19 E. 1074595.49	
P.C.C. STA. P 2+35.57 N. 1047174.90 E. 1074540.82	
DELTA= 23°43'23"      E= 7.14 RADIUS= 327.42        L= 135.57 T= 68.77	
BACK TANGENT AZ 283°37'03"	
AHEAD TANGENT AZ 307°20'26"	
CURVE RAMP P 2	
P.C.C. STA. P 2+35.57 N. 1047174.90 E. 1074540.82	
P.I. STA. P 10+33.81 N. 1047659.18 E. 1073906.27	
P.T. STA. P 6+97.69 N. 1047474.99 E. 1074682.97	
DELTA= 155°59'26"      E= 646.35 RADIUS= 169.74        L= 462.13 T= 798.24	
BACK TANGENT AZ 307°21'02"	
AHEAD TANGENT AZ 103°20'28"	
P.O.E. STA. P 6+97.71 N. 1047475.00 E. 1074682.95	

INTERSTATE I-190 RAMP Q ALIGNMENT DATA	
P.O.B. STA. Q 1+00.00 N. 1047508.72 E. 1073313.01	
CURVE RAMP Q 1	
P.C. STA. Q 1+00.00 N. 1047508.72 E. 1073313.01	
P.I. STA. Q 2+04.92 N. 1047533.06 E. 1074588.94	
P.T. STA. Q 3+01.95 N. 1047488.87 E. 1074493.78	
DELTA= 38°19'50"      E= 17.71 RADIUS= 301.87        L= 201.95 T= 104.92	
BACK TANGENT AZ 283°25'07"	
AHEAD TANGENT AZ 245°05'16"	
CURVE RAMP Q 2	
P.C. STA. Q 4+43.58 N. 1047429.21 E. 1074365.34	
P.I. STA. Q 6+67.54 N. 1047334.87 E. 1074162.21	
P.T. STA. Q 8+44.06 N. 1047475.84 E. 1073988.18	
DELTA= 63°55'17"      E= 64.14 RADIUS= 358.97        L= 400.48 T= 223.97	
BACK TANGENT AZ 245°05'14"	
AHEAD TANGENT AZ 309°00'30"	
P.O.E. STA. Q 8+44.06 N. 1047475.84 E. 1073988.18	


INTERSTATE I-190 RAMP Q1 ALIGNMENT DATA	
P.O.B. STA. Q1 1+00.00 N. 1047442.95 E. 1074891.94	
CURVE RAMP Q 1	
P.C. STA. Q1 3+05.28 N. 1047490.59 E. 1074692.26	
P.I. STA. Q1 4+02.62 N. 1047513.18 E. 1074597.58	
P.T. STA. Q1 4+89.61 N. 1047461.45 E. 1074515.12	
DELTA= 45°31'25"      E= 19.59 RADIUS= 232.00        L= 184.33 T= 97.34	
BACK TANGENT AZ 76°34'52"	
AHEAD TANGENT AZ 57°53'44"	
P.O.E. STA. Q1 4+89.61 N. 1047461.45 E. 1074515.12	

INTERSTATE I-190 RAMP R ALIGNMENT DATA	
P.O.B. STA. R 1+00.00 N. 1047337.43 E. 1073304.41	
CURVE RAMP R 1	
P.C. STA. R 1+00.00 N. 1047337.43 E. 1073304.41	
P.I. STA. R 1+68.08 N. 1047310.66 E. 1073375.61	
P.T. STA. R 2+30.85 N. 1047329.33 E. 1073441.08	
DELTA= 39°04'14"      E= 11.72 RADIUS= 191.89        L= 130.85 T= 68.08	
BACK TANGENT AZ 113°09'12"	
AHEAD TANGENT AZ 74°04'58"	
CURVE RAMP R 2	
P.C. STA. R 4+05.99 N. 1047377.36 E. 1073609.51	
P.I. STA. R 6+55.77 N. 1047638.59 E. 1073849.71	
P.T. STA. R 8+68.99 N. 1047294.20 E. 1074048.18	
DELTA= 53°18'10"      E= 59.16 RADIUS= 497.68        L= 463.00 T= 249.78	
BACK TANGENT AZ 74°04'58"	
AHEAD TANGENT AZ 127°23'08"	
P.O.E. STA. R 8+68.99 N. 1047294.20 E. 1074048.18	

INTERSTATE I-190 RAMP R1 ALIGNMENT DATA	
P.O.B. STA. R1 1+00.00 N. 1047429.96 E. 1073146.22	
CURVE RAMP R 1	
P.C. STA. R1 2+89.40 N. 1047355.49 E. 1073320.37	
P.I. STA. R1 3+66.86 N. 1047325.03 E. 1073391.58	
P.T. STA. R1 4+35.15 N. 1047357.57 E. 1073461.88	
DELTA= 47°59'35"      E= 16.46 RADIUS= 174.00        L= 145.75 T= 77.46	
BACK TANGENT AZ 66°50'51"	
AHEAD TANGENT AZ 65°09'35"	
P.O.E. STA. R1 4+35.15 N. 1047357.57 E. 1073461.88	

INTERSTATE I-190 RAMP S ALIGNMENT DATA	
P.O.B. STA. S 1+00.00 N. 1047784.02 E. 1071276.58	
CURVE RAMP S 1	
P.C. STA. S 1+00.00 N. 1047784.02 E. 1071276.58	
P.I. STA. S 1+78.60 N. 1047758.36 E. 1073378.71	
P.C.C. STA. S 2+55.93 N. 1047711.13 E. 1073441.54	
DELTA= 17°52'06"      E= 6.14 RADIUS= 500.00        L= 155.93 T= 78.60	
BACK TANGENT AZ 109°03'32"	
AHEAD TANGENT AZ 126°55'38"	
CURVE RAMP S 2	
P.C.C. STA. S 2+55.93 N. 1047711.13 E. 1073441.55	
P.I. STA. S 3+76.66 N. 1047638.59 E. 1073538.06	
P.C.C. STA. S 4+80.89 N. 1047517.91 E. 1073541.30	
DELTA= 51°32'01"      E= 27.61 RADIUS= 250.11        L= 224.96 T= 120.73	
BACK TANGENT AZ 126°55'42"	
AHEAD TANGENT AZ 178°27'43"	
CURVE RAMP S 3	
P.C.C. STA. S 4+80.89 N. 1047517.91 E. 1073541.30	
P.I. STA. S 7+19.36 N. 1047279.52 E. 1073547.61	
P.T. STA. S 7+86.88 N. 1047373.28 E. 1073328.34	
DELTA= 114°40'14"      E= 130.39 RADIUS= 152.89        L= 305.99 T= 238.47	
BACK TANGENT AZ 178°28'57"	
AHEAD TANGENT AZ 293°09'11"	
P.O.E. STA. S 7+86.88 N. 1047373.28 E. 1073328.34	

INTERSTATE I-190 RAMP U ALIGNMENT DATA	
P.O.B. STA. U 1+00.00 N. 1048414.34 E. 1071276.58	
CURVE RAMP U 1	
P.C. STA. U 1+00.00 N. 1048414.34 E. 1071276.58	
P.I. STA. U 2+43.91 N. 1048448.04 E. 1071136.67	
P.C.C. STA. U 3+66.53 N. 1048580.63 E. 1071080.73	
DELTA= 53°34'58"      E= 34.27 RADIUS= 285.00        L= 266.53 T= 143.91	
BACK TANGENT AZ 283°32'36"	
AHEAD TANGENT AZ 337°07'34"	
CURVE RAMP U 2	
P.C.C. STA. U 3+66.53 N. 1048580.63 E. 1071080.73	
P.I. STA. U 5+57.58 N. 1048756.66 E. 1071006.47	
P.C.C. STA. U 7+47.47 N. 1048943.56 E. 1070966.88	
DELTA= 10°54'47"      E= 9.10 RADIUS= 2000.00      L= 380.94 T= 191.05	
BACK TANGENT AZ 337°07'34"	
AHEAD TANGENT AZ 348°02'22"	
CURVE RAMP U 3	
P.C.C. STA. U 7+47.47 N. 1048943.56 E. 1070966.88	
P.I. STA. U 7+66.24 N. 1048961.92 E. 1070962.99	
P.T. STA. U 7+85.01 N. 1048980.44 E. 1070959.91	
DELTA= 2°31'48"      E= 0.21 RADIUS= 850.00        L= 37.53 T= 18.77	
BACK TANGENT AZ 348°02'21"	
AHEAD TANGENT AZ 350°34'09"	
P.O.E. STA. U 7+85.01 N. 1048980.44 E. 1070959.91	

ALTERED ON:		AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:		SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING: ALIGNMENT DATA SHEET	DRAWING NUMBER: AGL-4

**GENERAL WORK ZONE TRAFFIC CONTROL**

1. WORK ZONE TRAFFIC CONTROL SHALL BE PROVIDED THROUGHOUT THE LENGTH OF THE CONTRACT IN ACCORDANCE WITH THE PROVISIONS OF SECTION 619 - WORK ZONE TRAFFIC CONTROL - OF THE NYSDOT STANDARD SPECIFICATIONS, THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND ANY PROVISIONS CONTAINED IN THE CONTRACT DOCUMENTS, AND AS ORDERED BY THE ENGINEER (A.O.B.E.). THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (NATIONAL MUTCD) SHALL BE DEFINED AS THE NATIONAL MUTCD, JULY 2022 EDITION WITH SUPPLEMENTS.
2. THE CONTRACT DOCUMENTS AND THE TRAFFIC MAINTENANCE SCHEMES SHOWN IN SECTION 6 OF THE NATIONAL MUTCD ARE CONSIDERED TO BE MINIMUM REQUIREMENTS. ADDITIONAL SIGNS, DELINEATION, AND/OR TRAFFIC CONTROL DEVICES MAY BE REQUIRED AS DETERMINED BY THE ENGINEER. THE COST OF ANY ADDITIONAL DEVICES SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - WORK ZONE TRAFFIC CONTROL.
3. THE CONTRACT DOCUMENTS ARE NOT INTENDED TO BE ALL INCLUSIVE, BUT RATHER SERVE AS A GUIDE FOR THE SAFE & EFFICIENT SEQUENCING OF CONSTRUCTION OPERATIONS. THE TRAFFIC CONTROL PLANS PROVIDE FOR THE BASIC REQUIREMENTS & RESTRICTIONS OF THIS CONTRACT. ANY INCIDENTAL ADDITIONS & REVISIONS TO THESE PLANS, AS DIRECTED BY THE ENGINEER, SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - WORK ZONE TRAFFIC CONTROL.
4. PRIOR TO THE START OF WORK, THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, ANY PROPOSED CHANGES TO THE TRAFFIC CONTROL PLANS FOR APPROVAL. ANY CHANGES WHICH ALTER THE BASIC CONCEPT OF THESE PLANS MUST BE APPROVED BY THE ENGINEER.
5. IF THE ENGINEER NOTIFIES THE CONTRACTOR OF ANY HAZARDOUS CONSTRUCTION PRACTICES AND/OR CONDITIONS, ALL OPERATIONS IN THAT AREA SHALL BE DISCONTINUED AND IMMEDIATE REMEDIAL ACTION SHALL BE TAKEN TO CORRECT THE SITUATION, TO THE SATISFACTION OF THE ENGINEER, BEFORE WORK IS RESUMED.
6. ON LONG LANE CLOSURES, ADDITIONAL DEVICES (DRUMS, OVERSIZED VERTICAL PANELS, ETC.) SHALL BE PLACED PERPENDICULAR TO TRAFFIC IN THE CLOSED LANE, EVERY 800 FEET, IN ADDITION, ADDITIONAL DEVICES SHALL BE SIMILARLY PLACED IN ADVANCE OF ANY TRANSVERSE DROP-OFF OF 1 1/2 INCHES OR GREATER.
7. THE CONTRACTOR SHALL PLAN AND INCORPORATE ACCESS POINTS INTO THE WORK ZONE SUCH THAT, TO THE EXTENT PRACTICAL, THE CONTRACTOR'S VEHICLES ENTERING AND LEAVING THE WORK ZONE SHALL NOT IMPEDE THE MOVEMENT OF THROUGH TRAFFIC IN THE ADJACENT OPEN LANE(S).
8. CONSTRUCTION EQUIPMENT AND MATERIALS SHALL BE REMOVED FROM THE CLEAR ROADSIDE AREA DURING THE HOURS THAT THE CONTRACTOR IS NOT WORKING. THIS REQUIREMENT SHALL NOT BE LIMITED TO THE CONTRACT LIMITS.
9. ADDITIONAL WORK ZONE TRAFFIC CONTROL NOTES ARE INCLUDED ELSEWHERE IN THE TRAFFIC CONTROL PLANS, OTHER CONTRACT DRAWINGS AND THE NYSTA CURRENT STANDARD SHEETS.
10. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT THESE HIGHWAYS WILL BE DESIGNATED AS NON-RESTRICTED HIGHWAYS UNDER THIS CONTRACT.
11. WHEN TWO OR MORE WORK AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY AS DETERMINED BY THE ENGINEER, THE CONTRACTOR SHALL ENSURE THERE IS NO CONFLICT IN SIGNING AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.

**DELINEATION AND CHANNELIZATION**

1. THE CONTRACTOR SHALL DELINEATE AREAS WHERE THERE IS A DROP-OFF NEAR THE EDGE OF THE TRAVELED WAY AND AREAS ON WHICH IT IS UNSAFE TO TRAVEL. THE PROVISIONS FOR DELINEATION SHALL BE IN ACCORDANCE WITH NYSDOT STANDARD SPECIFICATIONS 619 AND NYSTA STANDARD SHEETS TA 619-01 AND TA 619-02. THESE SECTIONS INDICATE THE MINIMUM DELINEATION REQUIREMENTS. ADDITIONAL DELINEATION MAY BE REQUIRED AS DIRECTED BY THE ENGINEER OR AS SHOWN ON THE PLANS. MUTCD COMPLIANT CHANNELIZATION DEVICES MEETING 619 SPECIFICATIONS SHALL BE USED AT HAZARDOUS LOCATIONS DETERMINED BY THE ENGINEER. DELINEATION DEVICES SHALL REMAIN IN PLACE UNTIL SATISFACTORY PROTECTION IS PROVIDED OR THE HAZARD REMOVED. THEY SHALL BE SPACED AT A DISTANCE NOT TO EXCEED 40 FEET OR AS DIRECTED BY THE ENGINEER. THE CONTRACTOR MAY ELECT, AT NO ADDITIONAL COST TO THE AUTHORITY, TO INSTALL TEMPORARY POSITIVE BARRIER TO ELIMINATE DROP-OFFS OR HAZARDS. THE COST OF ALL DELINEATION AND DROP-OFF PROTECTION SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL.
2. PAVEMENT MARKINGS PLACED DURING CONSTRUCTION SHALL BE APPLIED PER SECTION 619 OF THE NYSDOT STANDARD SPECIFICATIONS AND SHALL CONFORM TO THE PROJECT'S PERMANENT MARKINGS WITH RESPECT TO COLOR AND PATTERN. PAYMENT WILL BE MADE UNDER ITEMS 619.10010125 & 619.10010225 - INTERIM PAVEMENT MARKINGS STRIPES.
3. THE CONTRACTOR IS RESPONSIBLE FOR MONITORING THE WORK ZONE. ALL DISLODGED DELINEATION DEVICES AND/OR OBSCURED PAVEMENT MARKINGS WILL BE RESET AND/OR RE-MARKED AS REQUIRED, OR A.O.B.E.
4. THE CONTRACTOR SHALL REMOVE EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH INTERIM PAVEMENT MARKINGS REQUIRED FOR CONSTRUCTION. THE COST TO REMOVE THE EXISTING PAVEMENT MARKINGS, UNLESS REMOVED DURING EXCAVATION, WILL BE INCLUDED IN THE PRICE BID FOR ITEMS 619.08010225 - REMOVE PAVEMENT MARKING STRIPES 6 INCHES.
5. THE CONTRACTOR SHALL NOT MIX DELINEATION DEVICES IN A LINEAR CLOSURE OR TAPER, (I.E., CONES, VERTICAL PANELS, CANDLES, OR DRUMS SHALL NOT BE USED IN THE SAME TAPER OR CLOSURE). HOWEVER, DIFFERENT WORK AREAS WITHIN THE PROJECT LIMITS MAY USE DIFFERENT DELINEATION DEVICES.
6. SEE SECTION 619-3.02A OF THE NYSDOT STANDARD SPECIFICATIONS, REGARDING TRAFFIC RIDING ON A MILLED SURFACE AND SECTION 619-3.06 REGARDING TEMPORARY PAVEMENT MARKINGS.
7. THE MAXIMUM LENGTH OF ANY CONTINUOUS WORK SPACE SHALL NOT EXCEED 2 MILES EXCEPT UNDER A FOUR-LANE, TWO-WAY OPERATION (FLTWO). ALL TRAFFIC SHALL BE RE-ESTABLISHED TO ITS NORMAL LANE CONFIGURATION FOR A MINIMUM 2 MILES PRIOR TO A SUCCESSIVE LANE CLOSURE. (THE SEPARATION BETWEEN SUCCESSIVE LANE CLOSURES IS MEASURED FROM THE LAST SIGN OF THE FIRST LANE CLOSURE TO THE BEGINNING OF THE MERGING TAPER OF THE SECOND LANE CLOSURE).

**CONSTRUCTION SIGNS**

1. DIAMOND-SHAPED ADVANCE WARNING SIGNS SHALL BE USED FOR ALL ADVANCE WARNING SIGNS SHOWN IN THE NATIONAL MUTCD. COLOR REQUIREMENTS SHALL BE BLACK TEXT ON ORANGE BACKGROUND. COLOR REQUIREMENTS FOR REGULATORY & GUIDE SIGNS USED FOR CONSTRUCTION SIGNING SHALL BE AS SPECIFIED IN THE NATIONAL MUTCD FOR EACH SIGN, OR AS NOTED ON THE PLANS.
2. LONGITUDINAL PLACEMENT OF CONSTRUCTION SIGNS SHALL BE AS SHOWN ON THE TRAFFIC CONTROL PLANS AND AS DIRECTED BY THE ENGINEER. THE LATERAL CLEARANCE FROM THE EDGE OF THE TRAVEL LANE TO THE EDGE OF THE CONSTRUCTION SIGN SHALL BE A MINIMUM OF 3 FEET, OR AS A.O.B.E.. SIGNS SHALL BE PLACED TO PROVIDE OPTIMUM VISIBILITY.
3. MOUNTING HEIGHT REQUIREMENTS FOR ALL LONG DURATION CONSTRUCTION SIGNS SHALL BE 7 FEET MINIMUM MEASURED FROM THE GROUND SURFACE TO THE BOTTOM OF THE SIGN. SHORT DURATION CONSTRUCTION SIGNS MAY BE MOUNTED AT A MINIMUM HEIGHT OF 5 FEET, OR A.O.B.E.
4. DURING THE PROGRESSION OF CONSTRUCTION, SOME EXISTING SIGNS MAY REQUIRE MINOR RELOCATION DUE TO TEMPORARY PAVEMENT WIDENING, SHIFT OF TRAFFIC LANES, OR CHANGE IN THE FLOW OF TRAFFIC. THE COST OF THESE RELOCATIONS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL.
5. ALL FLAGGING OPERATIONS SHALL USE 24" (MIN.) OCTAGONAL SHAPED STOP/SLOW PADDLES HAVING A 6' STAFF.
6. THE COST OF ALL TEMPORARY SIGNS NECESSARY FOR WORK ZONE TRAFFIC CONTROL AS NOTED IN THE PLANS, STANDARD SPECIFICATIONS, NATIONAL MUTCD, AND A.O.B.E., SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL.
7. THE CONTRACTOR WILL BE REQUIRED TO COVER AND UNCOVER EXISTING AND CONSTRUCTION SIGNS DURING THE PROJECT. THIS PROCESS WILL BE DIRECTED BY THE ENGINEER-IN-CHARGE AND SHALL BE INCLUDED IN ITEM 619.01 AT NO ADDITIONAL COST.
8. THE CONTRACTOR SHALL REMOVE, STORE, AND RE-SET EXISTING SIGNS WHICH CONFLICT WITH CONSTRUCTION SIGNS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE EXISTING SIGNS DUE TO THE REMOVAL, STORAGE, AND RE-SETTING OF THE SIGNS.
9. ALL WARNING AND REGULATORY SIGNS SHALL BE POSTED ON BOTH SIDES OF MULTI-LANE DIVIDED HIGHWAYS, MULTI-LANE RAMP, AND ONE-WAY STREETS. IN CASES WHERE LANE RESTRICTIONS REDUCE THE TRAVEL LANE TO ONE LANE, SIGNS SHALL BE POSTED ON THE RIGHT SIDE OF THE ACTIVE TRAVEL LANE, UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
10. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCD. ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY THE EIC.

**CONSTRUCTION ACTIVITIES**

1. UNLESS OTHERWISE SHOWN OR APPROVED BY THE ENGINEER, WORK AREAS ON OPPOSITE SIDES OF THE ROAD SHALL NOT OVERLAP OR CO-EXIST.
2. NO MATERIAL IS TO BE STORED WITHIN THE CLEAR ROADSIDE AREA OTHER THAN THAT MATERIAL WHICH WILL BE PLACED THAT DAY, WITHOUT THE APPROVAL OF THE ENGINEER. THE CLEAR ROADSIDE AREA IS DEFINED AS THE AREA WITHIN 30 FEET OF THE EDGE OF TRAVEL LANE.
3. CONSTRUCTION EQUIPMENT SHALL BE REMOVED FROM THE CLEAR ROADSIDE AREA DURING HOURS THAT THE CONTRACTOR IS NOT WORKING. THIS REQUIREMENT SHALL NOT BE LIMITED TO THE CONTRACT LIMITS.
4. NO PRIVATE VEHICLES (OWNED BY THE CONTRACTOR'S EMPLOYEES) SHALL BE PARKED ON THRUWAY PROPERTY. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL MARSHALL AT AN OFF-SITE YARD AND TRAVEL TO THE WORK SITE IN CONTRACTOR OWNED WORK VEHICLES.
5. ALL OPERATIONS, INCLUDING PAVING, ROLLING, GRADING, SUPPLYING OF MATERIAL, AND THE TRAVEL OF SUPERVISORY PERSONNEL THROUGH THE WORK ZONE, AND THE ENTIRE LENGTH OF THE CONTRACT, WILL ALWAYS BE IN THE DIRECTION OF NORMAL TRAFFIC EXCEPT WHERE SPECIFICALLY ALLOWED OTHERWISE BY THE ENGINEER TO PERFORM A PARTICULAR OPERATION.
6. ALL UNEVEN TRANSVERSE PAVEMENT SURFACES (I.E. PAVEMENT REBATES, END OF PAVED SURFACES) SHALL BE POSTED WITH "BUMP" SIGNS (W8-1) AND EITHER A TYPE 1 OBJECT MARKER (OM1-3) OR A CONSTRUCTION DRUM WITH A FLASHING WARNING LIGHT ON BOTH SIDES OF THE ROADWAY, TO BE LOCATED A.O.B.E. COST TO BE INCLUDED IN ITEM 619.01.

**NIGHTTIME OPERATIONS NOTES**

1. WORK ZONE TRAFFIC CONTROL FOR NIGHTTIME CONSTRUCTION SHALL BE CONDUCTED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS.
2. TO THE EXTENT PROJECT AND TRAFFIC CONDITIONS PERMIT, THE CONTRACTOR SHALL TAKE ADVANTAGE OF DAYLIGHT HOURS BEFORE AND AFTER THE SHIFT FOR SETUP AND REMOVAL OF TRAFFIC CONTROLS, AS WELL AS OTHER PREPARATIONS AND CLEANUP WORK.
3. ALL ARROW BOARDS USED AT NIGHT SHALL BE PROPERLY DIMMED AS DESCRIBED BY THE MUTCD AND THE STANDARD SPECIFICATIONS TO AVOID GLARE FOR APPROACHING MOTORISTS.
4. NIGHTTIME OPERATIONS, INCLUDING LANE CLOSURES OR OBSTRUCTIONS, WILL BE PERMITTED AS NOTED IN THE "SCHEDULE AND SUSPENSION OF WORK" LOCATED IN THE PROPOSAL.
5. NIGHTTIME LANE CLOSURES SHALL BE LOCATED AWAY FROM OTHER CONFLICT POINTS SUCH AS RAMPS WHENEVER POSSIBLE.
6. THE CONTRACTOR SHALL PROVIDE LIGHTING FOR ALL NIGHTTIME OPERATIONS IN ACCORDANCE WITH ITEM 619.24.
7. THE CONTRACTOR IS ADVISED THAT ILLUMINATION IS TO BE PROVIDED THROUGHOUT THE ENTIRE AREA OF CONSTRUCTION OPERATIONS, WHICH INCLUDES ALL WORK AREAS OCCUPIED BY THE CONTRACTOR'S PERSONNEL (TRUCK STAGING AREAS, CLEAN OUT AREAS, LAYOUT AND MEASUREMENT AHEAD OF ACTUAL WORK, ETC.). ALL SUCH WORK AREAS ARE TO BE SHOWN ON THE LIGHTING PLAN SUBMITTED TO THE ENGINEER.

**CONES, DRUMS, BARRICADES, AND MARKERS**

1. ALL CHANNELIZING/DELINEATION DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM OF ONE FOOT CLEARANCE TO THE TRAVELED WAY UNLESS OTHERWISE SHOWN ON THE PLANS. THE CONTRACTOR SHALL MAKE CERTAIN THAT PLACEMENT OF CONES, DRUMS AND MARKERS OR BARRICADES SHALL NOT INTERFERE WITH SIGHT DISTANCE.
2. LONGITUDINAL SPACING SHALL BE 40 FT. OTHER SPACING MAY BE REQUIRED AS SHOWN ON THE PLANS OR A.O.B.E..
3. WHERE POSSIBLE A LATERAL BUFFER SPACE OF ONE FOOT SHOULD BE PROVIDED BETWEEN THE WORK ZONE AND THE LINE OF DELINEATION DEVICES.
4. THE COST OF ANY DELINEATION AND GUIDING DEVICES (CONES, DRUMS, VERTICAL PANELS, ETC.) SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.
5. DRUMS ARE THE PREFERRED CHANNELIZING DEVICES TO BE USED DURING THE HOURS OF DARKNESS. DRUMS MUST BE USED ON TAPERS BOTH DAY AND NIGHT.

**LOCAL ROADS - WORK ZONE TRAFFIC CONTROL**

1. WHEN THE VISIBILITY OF THE TRAVELING PUBLIC IS RESTRICTED DUE TO WEATHER CONDITIONS AND/OR THE WORK OPERATIONS COMMENCE PRIOR TO DAWN OR CONTINUE BEYOND DUSK, FLASHING BEACONS (TYPE 'A' LIGHTS) SHALL BE PROVIDED.
2. THE CONTRACTOR SHALL COORDINATE WITH THE CITY OF BUFFALO DEPARTMENT OF PUBLICS WORKS, (716) 851-5636, PRIOR TO THE START OF ANY CONSTRUCTION THAT MAY IMPACT LOCAL CITY ROADS, CITY SERVICES AND OPERATIONS.

**LANE CLOSURES**

1. LANE CLOSURES REQUIRED FOR ANY CONSTRUCTION OPERATIONS MUST BE CONDUCTED IN ACCORDANCE WITH THE 'LANE CLOSURE CHARTS' ON NYSTA STANDARD SHEET TA 619-36.
2. THE CONTRACTOR SHALL PROVIDE FLAGGERS A.O.B.E. TO CONTROL TRAFFIC MOVEMENT ALONG RAMP WORK OPERATIONS REQUIRING TEMPORARY SHIFTING OF TRAFFIC FLOW ON PORTIONS OF THE TRAVEL WAY. FLAGGING IS NOT PERMITTED ON THRUWAY MAINLINE.
3. RAMP CLOSURES WILL BE ALLOWED TO COMPLETE THE CPR AND PAVING OPERATIONS ON ALL INTERCHANGE RAMPS. CONTRACTOR SHALL SUBMIT FOR APPROVAL ALL INTERCHANGE RAMP CLOSURES 7 DAYS PRIOR TO THE DESIRED CLOSURE. SEE DWGS, TCP-4 TO TCP-20 FOR RAMP DETOURS.

**PAVEMENT MARKINGS**

1. THE COST OF REMOVAL OR COVERING OF EXISTING PAVEMENT MARKINGS, WHERE REQUIRED, SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.08010225, REMOVE PAVEMENT MARKING STRIPES (6 INCH).
2. THE CONTRACTOR SHALL PROVIDE PAVEMENT MARKINGS AT ALL TIMES ON ALL PAVEMENT, WHETHER EXISTING, TEMPORARY OR NEW, UNTIL PERMANENT MARKINGS ARE INSTALLED OR RESTORED. THIS SHALL INCLUDE AT ALL APPROPRIATE LOCATIONS: EDGE LINES, LANE LINES (SOLID OR BROKEN), CHANNELIZING LINES, DOTTED LINES, PLUS ANY MARKINGS ORDERED BY THE ENGINEER. ALL MARKINGS SHALL BE APPLIED IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS, CONTRACT PLANS AND THE NEW YORK STATE MUTCD SUPPLEMENT AND SHALL INDICATE ACTUAL CONDITIONS AT ALL TIMES. THESE MARKINGS SHALL BE PAID FOR AS SHOWN ON THE CONTRACT PLANS.
3. WORK ZONE PAVEMENT MARKINGS AT 6" WIDTH, SHALL BE USED AS FOLLOWS:
  - MAINLINE - USE ITEM 619.10010125
  - RAMPS - USE ITEM 619.10010125
  - OVER WINTER SHUTDOWN - USE ITEM 619.10010225 OR AS DIRECTED BY ENGINEER.

SEE TEMPORARY AND INTERIM PAVEMENT MARKING DETAILS ON NYSTA STANDARD SHEET TA 685-04.

TEMPORARY OR INTERIM PAVEMENT MARKINGS COMPRISED OF TRAFFIC MARKING PAINT SHALL BE REPAINTED AT NO ADDITIONAL COST WHEN 10% OF THE UNDERLYING PAVEMENT IS VISIBLE THROUGH THE PAVEMENT MARKING OR WHEN THE ENGINEER DEEMS REFLECTIVITY IS INADEQUATE.

CHECKED BY: J. KLEINMANN

DRAFTED BY: P. THOMPSON

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ALTERED ON:

SIGNATURE: STAMP:

AFFIXED ON:

5/10/2024

SIGNATURE: STAMP:



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TRAFFIC CONTROL NOTES	DRAWING NUMBER: TCN-1

**PORTABLE VARIABLE MESSAGE DISPLAY UNITS**

1. THE CONTRACTOR SHALL PROVIDE UP TO 9 PORTABLE VARIABLE MESSAGE SIGNS (PVMS) WITH COMMUNICATION ABILITY (ITEM 619.110513) TO BE USED FOR THE DURATION OF THIS CONTRACT. PVMS ARE INTENDED TO SUPPLEMENT FIXED SIGNS AND ARE NOT INTENDED TO REPLACE THEM.
2. TYPICAL APPLICATIONS FOR VARIABLE MESSAGE DISPLAY UNITS INCLUDE LOCATIONS WHERE:
  - A. TRAFFIC SPEED IS EXPECTED TO DROP SUBSTANTIALLY.
  - B. SIGNIFICANT QUEUING OR DELAYS ARE EXPECTED.
  - C. CHANGES IN ALIGNMENT OR SURFACE CONDITIONS OCCUR.
  - D. LANES, RAMPS OR ROADWAYS ARE CLOSED.
  - E. SIGNIFICANT ACCIDENTS OR INCIDENTS HAVE OCCURRED.
  - F. NIGHTTIME WORK IS PERFORMED. PERIOD OF ADVANCED NOTIFICATION TO BE DETERMINED BY THE ENGINEER.
3. LOCATIONS:
  - A. VARIABLE MESSAGE SIGNS ARE TO BE PLACED BEYOND THE SHOULDER WHEREVER PRACTICAL.
  - B. VARIABLE MESSAGE SIGNS SHALL BE PLACED WHERE THEY ARE VISIBLE TO TRAFFIC FROM AT LEAST 2800 FEET AWAY BOTH DAY AND NIGHT OR APPROVED BY ENGINEER.
4. PORTABLE VARIABLE MESSAGE SIGNS SHALL BE LOCATED AS DEEMED NECESSARY BY THE AUTHORITY'S PROJECT ENGINEER AND BY THE NYSTA DIVISION TRAFFIC SUPERVISOR. ALTHOUGH PAYMENT IS PER SIGN, THE CONTRACTOR MAY BE REQUIRED TO MOVE THE SIGNS AS OFTEN AS REQUESTED BY EITHER THE ENGINEER OR THE NYSTA DIVISION TRAFFIC SUPERVISOR. NO ADDITIONAL PAYMENT WILL BE MADE FOR SIGN MOVEMENTS.
5. ALL MESSAGES DISPLAYED BY THE VARIABLE MESSAGE SIGNS SHALL BE APPROVED BY THE NYSTA DIVISION TRAFFIC SUPERVISOR PRIOR TO USE.

**EMERGENCY VEHICLE ACCESS**

1. THE CONTRACTOR MUST PROVIDE SAFE AND CONVENIENT EMERGENCY ACCESS AT ALL TIMES DURING CONSTRUCTION. PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE LOCAL FIRE, POLICE, AND AMBULANCE SERVICES TO COORDINATE THE PROPOSED CONSTRUCTION SCHEDULE, AND TO DEVELOP AN EMERGENCY ACCESS PLAN FOR ALL ROADS AND STREETS WITHIN THE PROJECT LIMITS.
2. THE CONTRACTOR SHALL PROVIDE A WRITTEN LIST OF INDIVIDUALS RESPONSIBLE FOR EMERGENCY REPAIRS AND EMERGENCY COORDINATION. THIS "CALL OUT" LIST SHALL BE PROVIDED TO THE LOCAL FIRE, POLICE, AND AMBULANCE SERVICES, THE VILLAGE GOVERNMENT CENTER, AND THE ENGINEER. THIS LIST SHALL INCLUDE THE NAME, ADDRESS, AND TELEPHONE NUMBER(S) OF THE PERSONS AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS DURING NON-WORKING HOURS.
3. IF EMERGENCY ACCESS IS REQUIRED THROUGH ANY CLOSED WORK AREA, THE CONTRACTOR SHALL BE RESPONSIBLE TO ALWAYS BE PREPARED TO ALLOW EMERGENCY VEHICLES THROUGH THE CLOSED AREA. THIS MAY REQUIRE TRENCHES TO BE COVERED WITH STEEL PLATES, EXCAVATION DROP OFFS TO BE BACKED-UP AND RAMPED, OR THE ACTUAL PROGRESSION OF WORK TO BE SEQUENCED. THE COST OF ANY ADDITIONAL WORK NECESSARY TO MEET THIS REQUIREMENT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 - WORK ZONE TRAFFIC CONTROL.

**DELINEATORS**

1. SEE NYSTA STANDARD SHEET TA 646-01 FOR DELINEATOR AND MILE MARKER DETAILS.
2. EXISTING DELINEATORS AND MILE MARKERS DURING CONSTRUCTION:
  - RELOCATION AND BACK-TO-BACK DELINEATOR AND MILE MARKERS COSTS SHALL BE INCLUDED UNDER ITEM 619.01.
  - DELINEATORS AND MILE MARKERS SHALL BE CLEARLY VISIBLE AT ALL TIMES.
3. REMOVE AND REPLACE EXISTING DELINEATORS AND MILE MARKERS AT COMPLETION OF PROJECT IN ACCORDANCE WITH NYSTA STANDARD SHEET TA 646-01. PAYMENT SHALL BE UNDER DELINEATOR AND MILE MARKERS REMOVAL AND NEW DELINEATOR PAYMENT ITEMS.

**'BE PREPARED TO STOP' SIGNS**

1. THE CONTRACTOR SHALL HAVE ON HAND "BE PREPARED TO STOP" SIGNS (W3-4) TO BE INSTALLED IN ADVANCE OF LANE CLOSURE SETUPS WHERE THE ENGINEER DETERMINES THEY ARE NECESSARY. THE SIGNS AS WELL AS ADDITIONAL W20-1 "ROAD WORK" SIGNS SHALL BE PLACED AND RELOCATED AS NECESSARY IN ACCORDANCE WITH SECTION 619-3.02H.5 OF THE STANDARD SPECIFICATIONS AND AS MODIFIED BY THE CONTRACT PROPOSAL. THE CONTRACTOR SHALL HAVE THESE SIGNS (48" X 48") TRUCK MOUNTED AND READY FOR AN INCIDENT ON SHORT NOTICE.

**CONSTRUCTION INGRESS AND EGRESS**

1. VEHICLES, MATERIALS, AND/OR EQUIPMENT, INCLUDING OUT OF SERVICE SIGNS, SHALL NOT BE PARKED OR STORED WITHIN 30 FT. OF A ROADWAY USED BY THE GENERAL PUBLIC OR ANY OTHER AREAS DEEMED HAZARDOUS BY THE EIC.
2. VEHICLES PARKED BEHIND GUIDERAIL/BARRIER SHALL BE PARKED BEYOND DEFLECTION DISTANCE AND WITHIN THE POINT OF NEED COVERAGE.

**BARRIER/SHADOW VEHICLES**

1. IN ADDITION TO THE REQUIREMENTS OF SECTION 619-2.02G AND 619-3.02G OF THE NYS STANDARD SPECIFICATIONS, AND UNLESS OTHERWISE APPROVED BY THE ENGINEER, THE CONTRACTOR SHALL PROVIDE BARRIER/SHADOW VEHICLES FOR ALL ACTIVE WORK AREAS OCCUPYING ANY TRAVEL LANE OR SHOULDER CLOSED TO TRAFFIC WITH CONES, DRUMS OR OTHER CHANNELIZING DEVICES. A SEPARATE BARRIER/SHADOW VEHICLE SHALL BE PROVIDED IN EACH CLOSED LANE AND EACH CLOSED SHOULDER 8 FT OR GREATER IN WIDTH.
2. EXCEPTIONS TO THE REQUIREMENT MAY BE MADE WHERE, IN THE JUDGEMENT OF THE ENGINEER, THE BARRIER/SHADOW VEHICLE PLACEMENT WOULD BE INEFFECTIVE OR WOULD INTERFERE WITH THE SAFE OPERATION OF TRAFFIC AT OR NEAR RAMPS OR CONNECTORS.
3. THE BARRIER/SHADOW VEHICLES SHALL TYPICALLY BE LOCATED AS INDICATED ON THE PLANS, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. IF THE WORK AREAS MOVE WITHIN A STATIONARY LANE OR SHOULDER CLOSURE THE BARRIER/SHADOW VEHICLE SHALL BE RE-POSITIONED ACCORDINGLY.
4. PAYMENT FOR TYPICAL BARRIER/SHADOW VEHICLES SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.
5. THE ARROW PANEL SHALL OPERATE IN THE "CAUTION MODE" WHEN IN USE. A "4 CORNER FLASH" SHALL ONLY BE PERMITTED. A FLASHING BAR DISPLAY SHALL NOT BE PERMITTED FOR THE CAUTION MODE. USE "4 LAMPS FLASHING SIMULTANEOUSLY" OR "2 PAIRS OF LAMPS FLASHING IN A PATTERN". A DIRECTION WILL NOT BE INDICATED.
6. ANY ROADWAY DEBRIS PICKUP BY CONTRACTOR PERSONNEL SHALL BE PROTECTED BY THE A SHADOW VEHICLE WITH IMPACT ATTENUATOR.

**DRAINAGE**

1. THE CONTRACTOR MUST MAINTAIN EXISTING DRAINAGE AT ALL TIMES DURING CONSTRUCTION.

**SLOWING AND STOPPING TRAFFIC**

1. SLOWING AND STOPPING OF TRAFFIC SHALL BE USED FOR REMOVAL AND INSTALLATION OF OVERHEAD SIGN STRUCTURES, OVERHEAD SIGN PANELS AND OTHER OPERATIONS AS APPROVED BY THE ENGINEER.
2. SLOWING AND STOPPING TRAFFIC ALONG I-190 WILL BE LIMITED TO 15 MINUTES.
3. REQUESTS FOR SLOWING/STOPPING TRAFFIC SHALL BE SUBMITTED TO THE ENGINEER AT LEAST TWO (2) WEEKS PRIOR TO A PLANNED SLOWING/STOPPING OPERATION.
4. THE CONTRACTOR SHALL PERFORM SLOWDOWNS/STOPPAGES ACCORDING TO THE PROCEDURES NOTED IN THE NYSTA ADDENDUM TO THE NYSDOT STANDARD SPECIFICATIONS.
5. GENERAL ALLOWABLE HOURS OF STOPPAGE 12 A.M. TO 6 A.M. FOR NORTHBOUND AND SOUTHBOUND DIRECTIONS, SUBJECT TO APPROVAL.
6. HOLIDAY RESTRICTIONS REFER TO THE SCHEDULE AND SUSPENSION OF WORK WITHIN THE PROPOSAL FOR RESTRICTIONS WHEN SLOWDOWNS/STOPPAGES WILL NOT BE ALLOWED.
7. MINIMUM CLEARANCE TIME BETWEEN SLOWDOWNS/STOPPAGES SHALL BE 15 MINUTES.
8. EMERGENCY SERVICE PROVIDES, INCLUDING NYS POLICE, LOCAL POLICE/AMBULANCE/FIRE DEPARTMENTS, NYSTA, NITTEC, NYSDOT AND THE CITY OF BUFFALO DEPT. OF PUBLIC WORKS SHALL BE NOTIFIED AT LEAST ONE (1) WEEK PRIOR TO UTILIZATION OF TRAFFIC SLOWDOWNS/STOPPAGES.
9. ALL COSTS RELATED TO SLOWDOWNS/STOPPAGE OF TRAFFIC SHALL BE INCLUDED IN ITEM 619.01

**SUGGESTED CONSTRUCTION SEQUENCE**

**STAGE 1A: (NORTHBOUND)**

SET UP WZTC FOR A LONG-TERM LEFT LANE CLOSURE. COMPLETE ALL CONCRETE PAVEMENT REPAIRS (CPR), FULL DEPTH RECONSTRUCTION AND PRE-CAST CONCRETE PANELS INSTALLATION FOR THE INSIDE LEFT TRAVEL LANE.

REMOVE EXISTING MIARDS ON THE OUTSIDE SHOULDER FOR SUBSEQUENT CONSTRUCTION STAGES.

**STAGE 1B: (NORTHBOUND)**

SETUP INTERMEDIATE-TERM WZTC MEASURES AS NECESSARY AND AOBSE TO COMPLETE ALL CONCRETE PAVEMENT REPAIRS (CPR), FULL DEPTH RECONSTRUCTION AND PRE-CAST CONCRETE PANELS INSTALLATION FOR THE CENTER TRAVEL LANE.

**STAGE 1C: (NORTHBOUND)**

SETUP LONG-TERM WZTC MEASURES AS NECESSARY AND AOBSE TO COMPLETE ALL CONCRETE PAVEMENT REPAIRS (CPR), FULL DEPTH RECONSTRUCTION AND PRE-CAST CONCRETE PANELS INSTALLATION FOR THE RIGHT LANE AND AUXILIARY LANES. INSTALL PROPOSED GUIDE RAILING AND NEW SHOULDER CURB, COMPLETE ALL DRAINAGE WORK ALONG OUTSIDE SHOULDER AREA.

**STAGE 2A: (SOUTHBOUND)**

SET UP WZTC FOR A LONG-TERM LEFT LANE CLOSURE. COMPLETE ALL CONCRETE PAVEMENT REPAIRS (CPR), FULL DEPTH RECONSTRUCTION AND PRE-CAST CONCRETE PANELS INSTALLATION FOR THE INSIDE LEFT TRAVEL LANE.

REMOVE EXISTING MIARDS ON THE OUTSIDE SHOULDER FOR SUBSEQUENT CONSTRUCTION STAGES.

**STAGE 2B: (SOUTHBOUND)**

SETUP INTERMEDIATE-TERM WZTC MEASURES AS NECESSARY AND AOBSE TO COMPLETE ALL CONCRETE PAVEMENT REPAIRS (CPR), FULL DEPTH RECONSTRUCTION AND PRE-CAST CONCRETE PANELS INSTALLATION FOR CENTER TRAVEL LANE.

**STAGE 2C: (SOUTHBOUND)**

SETUP LONG-TERM WZTC MEASURES AS NECESSARY AND AOBSE TO COMPLETE ALL CONCRETE PAVEMENT REPAIRS (CPR), FULL DEPTH RECONSTRUCTION AND PRE-CAST CONCRETE PANELS INSTALLATION FOR THE RIGHT LANE AND AUXILIARY LANES. INSTALL PROPOSED GUIDE RAILING AND NEW SHOULDER CURB, COMPLETE ALL DRAINAGE WORK ALONG OUTSIDE SHOULDER.

**STAGE 3:**

COMPLETE BRIDGE STRENGTHENING. INSTALL ANY REMAINING GUIDE RAILING. INSTALL OVERHEAD SIGN STRUCTURES AND PROPOSED SIGNS.

**STAGE 4A:**

INSTALL TWO-COURSE OVERLAY ALONG ALL NORTHBOUND TRAVEL LANES, AUXILIARY LANES AND SHOULDERS. INSTALL MILL AND OVERLAY ON ALL RAMPS. REPLACE BRIDGE JOINTS AT LOCATIONS NOTED ON PLANS. INSTALL ALL PROPOSED PAVEMENT MARKINGS.

**STAGE 4B:**

INSTALL TWO-COURSE OVERLAY ALONG ALL SOUTHBOUND TRAVEL LANES, AUXILIARY LANES AND SHOULDERS. INSTALL MILL AND OVERLAY ON ALL RAMPS. REPLACE BRIDGE JOINTS AT LOCATIONS NOTED ON PLANS. INSTALL ALL PROPOSED PAVEMENT MARKINGS.

**STAGE 5:**

INSTALL NEW LIGHT POLES, CONDUIT AND PULL BOXES. REMOVE ALL WZTC APPROACH SIGNS, PVMS SIGNS AND ANY REMAINING MISCELLANEOUS WZTC MATERIALS AND SIGNS.

NOTE: STAGE 3 AND STAGE 5 WORK FOR THE BRIDGE STRENGTHENING, OVERHEAD SIGN STRUCTURES, AND LIGHTING INSTALLATION CAN BE COMPLETED DURING ANY STAGE, AOBSE.

CHECKED BY: J. KLEINMANN

DRAFTED BY: P. THOMPSON

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

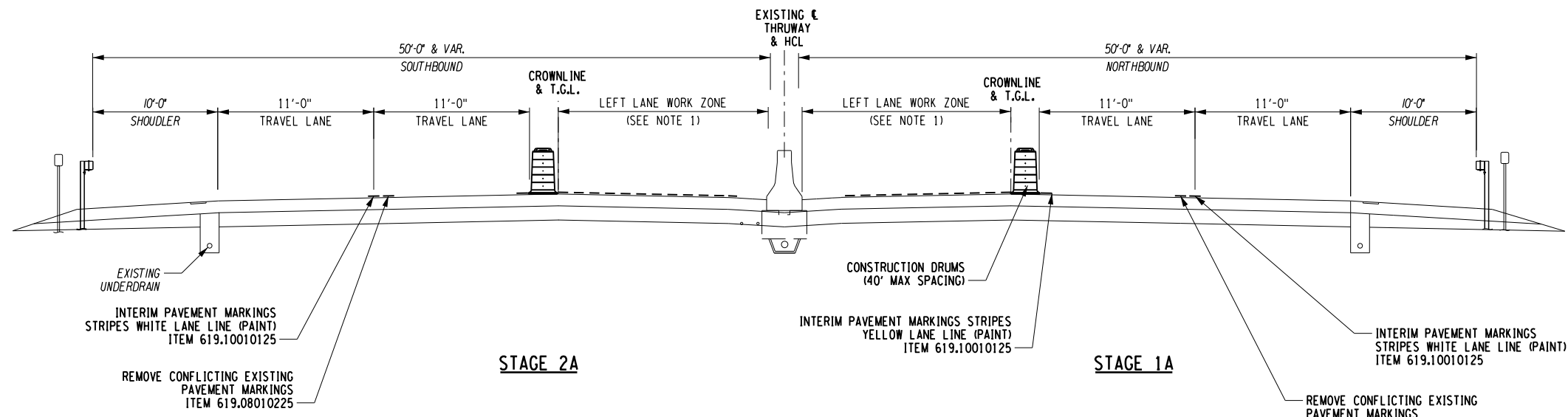
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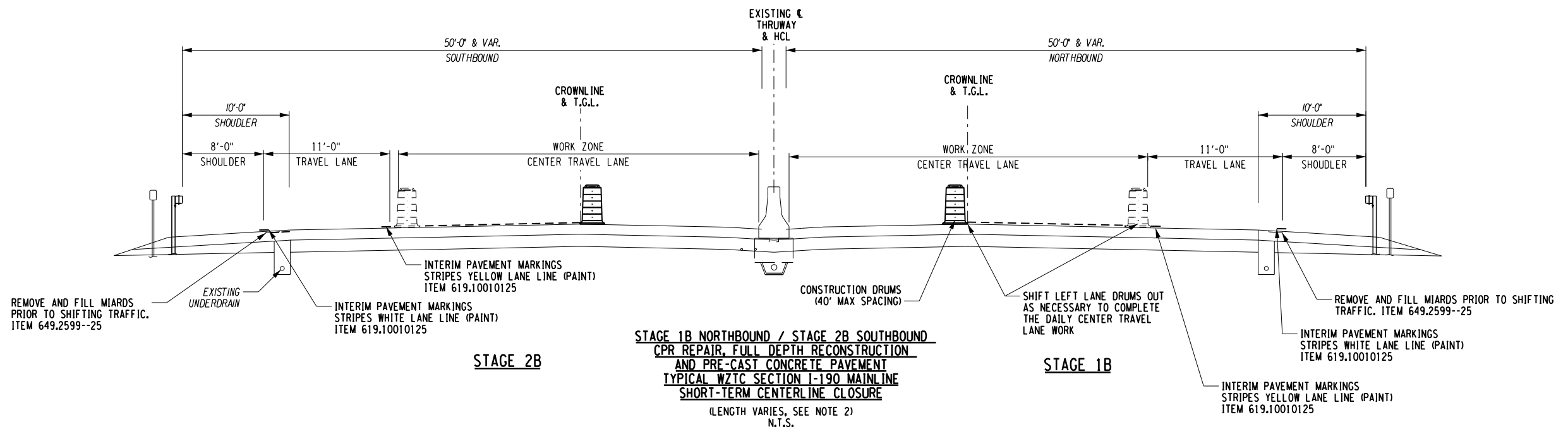
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING TRAFFIC CONTROL NOTES	DRAWING NUMBER: TCN-2



**STAGE 1A (NORTHBOUND / STAGE 2A (SOUTHBOUND)  
CPR REPAIR, FULL DEPTH PAVEMENT RECONSTRUCTION  
AND PRE-CAST CONCRETE PAVEMENT  
TYPICAL WZTC SECTION I-190 MAINLINE  
INTERMEDIATE TERM LEFT LANE CLOSURE**  
M.P. 900.7 TO M.P. 904.2  
N.T.S.



**STAGE 1B NORTHBOUND / STAGE 2B SOUTHBOUND  
CPR REPAIR, FULL DEPTH RECONSTRUCTION  
AND PRE-CAST CONCRETE PAVEMENT  
TYPICAL WZTC SECTION I-190 MAINLINE  
SHORT-TERM CENTERLINE CLOSURE**  
(LENGTH VARIES, SEE NOTE 2)  
N.T.S.

**NOTES:**

1. THE LONG-TERM LEFT LANE CLOSURE SHALL BE IN PLACE UNTIL THE COMPLETION OF STAGES 1A, 1B, 2A, AND 2B OR ABOVE UNLESS OTHERWISE APPROVED BY THE ENGINEER. WORK AREAS ON OPPOSITE SIDES OF THE ROAD SHALL NOT OVERLAP OR CO-EXIST.
2. CENTER LANE WORK AND WZTC CLOSURE SHALL BE INTERMEDIATE-TERM ONLY, AND MUST BE CONDUCTED IN ACCORDANCE WITH THE "LANE CLOSURE CHARTS" ON NYSTA STANDARD SHEET TA 619-37 AND RE-OPENED AT THE CONCLUSION OF THE WORKING SHIFT.
3. THE CONTRACTOR SHALL MEET ALL REQUIREMENTS OF SECTION 619-3.02K. ALL EQUIPMENT AND MATERIALS SHALL BE MOVED AT LEAST 30' FROM THE TRAVELED WAY AT THE END OF EACH WORK SHIFT. (UNLESS PROTECTED BY POSITIVE BARRIER).

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT	I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER:	TAN 24-12
LOCATION OF PROJECT	I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE:	MAY 2024
TITLE OF DRAWING	TRAFFIC CONTROL DETAILS	DRAWING NUMBER:	TCP-1

CHECKED BY: J. KLEINMANN

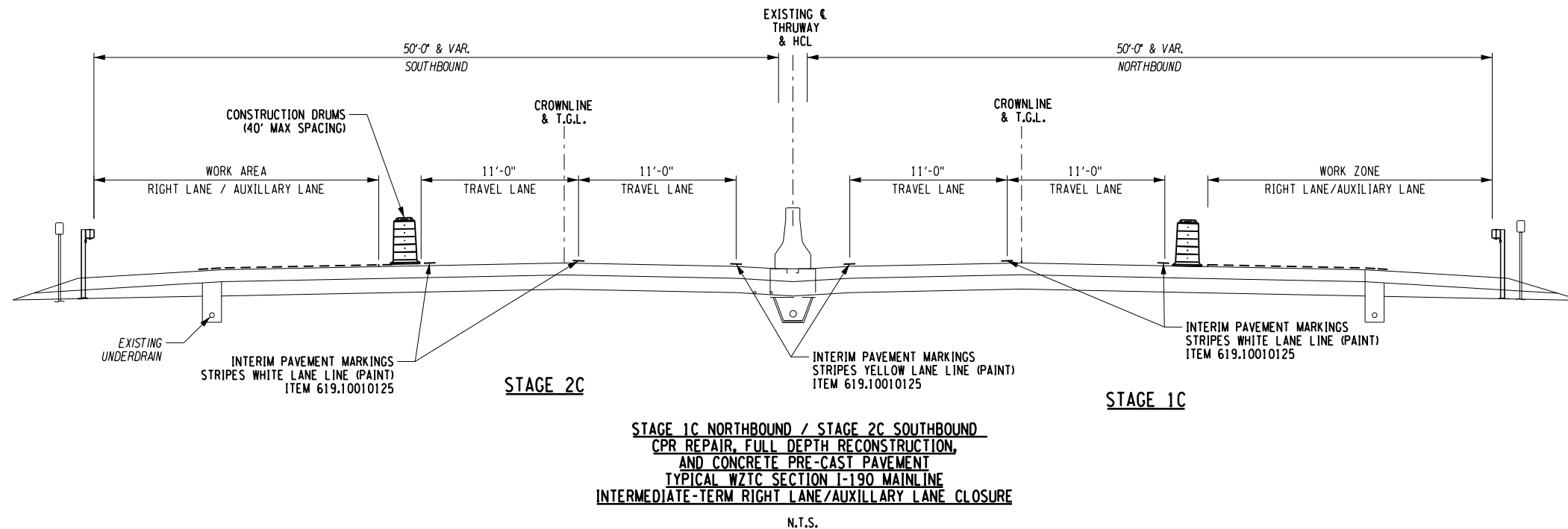
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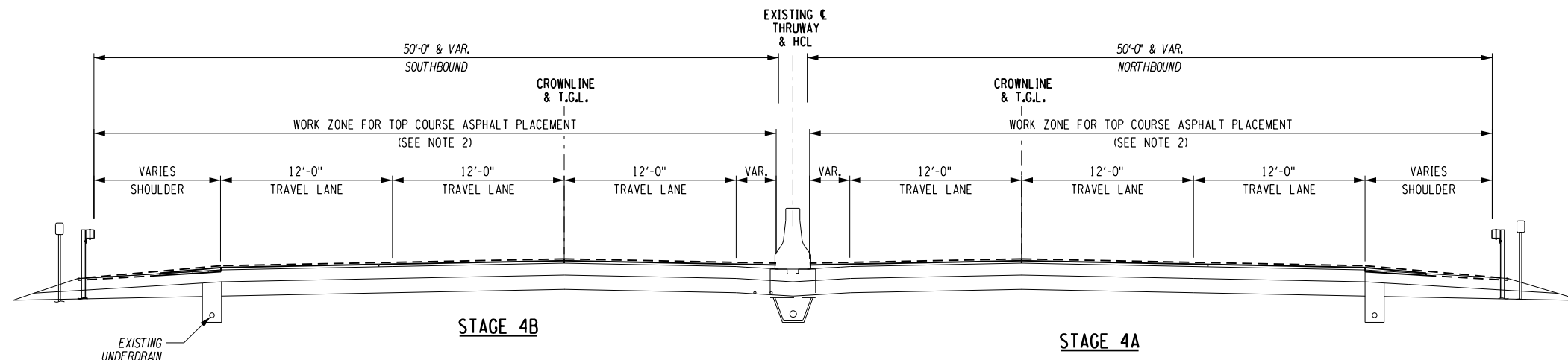
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

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**STAGE 1C NORTHBOUND / STAGE 2C SOUTHBOUND**  
**CPR REPAIR, FULL DEPTH RECONSTRUCTION,**  
**AND CONCRETE PRE-CAST PAVEMENT**  
**TYPICAL WZTC SECTION I-190 MAINLINE**  
**INTERMEDIATE-TERM RIGHT LANE/AUXILIARY LANE CLOSURE**  
 N.T.S.



**STAGE 4A (NORTHBOUND) / STAGE 4B (SOUTHBOUND)**  
**3 1/2" ASPHALT OVERLAY**  
**TYPICAL WZTC SECTION I-190 MAINLINE**  
**SHORT TERM WZTC FOR ALL LANES CLOSURES**  
 M.P. 900.7 TO M.P. 904.2  
 N.T.S.

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DRAFTED BY: P. THOMPSON

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

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**NOTES:**

1. THE LONG TERM RIGHT LANE CLOSURE SHALL BE IN PLACE UNTIL THE COMPLETION OF STAGES 1C AND 2C OR ABOVE UNLESS OTHERWISE APPROVED BY THE ENGINEER. WORK AREAS ON OPPOSITE SIDES OF THE ROAD SHALL NOT OVERLAP OR CO-EXIST.
2. STAGE 4 ASPHALT PAVEMENT SHALL BE PLACED USING SHORT-TERM LANE CLOSURES IN ACCORDANCE WITH THE "LANE CLOSURE CHARTS" ON NYSTA STANDARD SHEETS TA 619-37. ONE LANE OF TRAFFIC MUST BE OPENED AT ALL TIMES. THE CONTRACTOR MUST PLACE THE FULL TWO COURSE OVERLAY ACROSS ALL LANES FOR THE ENTIRE LENGTH OF ASPHALT PLACED IN ONE WORKING DAY.

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 TITLE OF DRAWING: TRAFFIC CONTROL DETAILS

CONTRACT NUMBER: TAN 24-12  
 DATE: MAY 2024  
 DRAWING NUMBER: TCP-2



SUMMARY OF WORK ZONE TRAFFIC CONTROL (WZTC)

STAGE/ PHASE/ LOCATION	DESCRIPTION OF PROPOSED WORK	ANTICIPATED WZTC REQUIRED	STANDARD SHEET/ WZTC DRAWING	STAGE/PHASE/LOCATION TIME RESTRICTION (SEE NOTE 3)	REMARKS
STAGE 1A & 2A I-190 MAINLINE	SETUP WZTC APPROACH SIGNING, DETOUR SIGNS, PVMS SIGNS, AND GENERAL WZTC DEVICES	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-19	SEE STD. SHEETS: TA 619-33, TA 619-37	
VARIOUS STAGES I-190 RAMPS	SETUP WZTC APPROACH SIGNING, DETOUR SIGNS, PVMS SIGNS, AND GENERAL WZTC DEVICES	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-16	SEE STD. SHEETS: TA 619-33	
STAGES 1, 2 AND 4 I-190 MAINLINE	CONCRETE PAVEMENT REPAIRS, PAVEMENT OVERLAY,	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-07, TA 619-12, TA 619-13, TA 619-14, TA 619-15, TA 619-16, TA 619-17 DRAWINGS: TCP-1 THRU TCP-13	SEE STD. SHEETS: TA 619-33, TA 619-37	
STAGE 4A AND 4B I-190 RAMPS	PAVEMENT MILL & OVERLAY,	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-12, TA 619-16, TA 619-17	SEE STD. SHEETS: TA 619-33	
STAGE 4A & 4B I-190 MAINLINE	DRAINAGE STRUCTURE ADJUSTMENTS	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-12, TA 619-16, TA 619-17	SEE STD. SHEETS: TA 619-33, TA 619-37	
STAGE 4A & 4B I-190 MAINLINE	DRAINAGE STRUCTURE ADJUSTMENTS	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-12, TA 619-16, TA 619-17	SEE STD. SHEETS: TA 619-33	
STAGE 4A & 4B I-190 MAINLINE	FINAL STRIPING	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-22, TA 619-23, TA 619-24, TA 619-25	SEE STD. SHEETS: TA 619-33, TA 619-37	
STAGE 4A & 4B I-190 RAMPS	FINAL STRIPING	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-22, TA 619-23, TA 619-24, TA 619-25	SEE STD. SHEETS: TA 619-33	
STAGE 4A & 4B I-190 MAINLINE	MIARDS, MISCELLANEOUS TASKS	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-18, TA 619-19	SEE STD. SHEETS: TA 619-33, TA 619-37	
STAGE 4A & 4B I-190 RAMPS	MIARDS, MISCELLANEOUS TASKS	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-18, TA 619-19	SEE STD. SHEETS: TA 619-33	
STAGE 3 I-190 MAINLINE	OVERHEAD SIGN STRUCTURES	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-12, TA 619-16, TA 619-27	SEE STD. SHEETS: TA 619-33, TA 619-37	
STAGE 3 I-190 RAMPS	OVERHEAD SIGN STRUCTURES	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-12, TA 619-16, TA 619-27	SEE STD. SHEETS: TA 619-33	
STAGES 1, 2 & 3 I-190 MAINLINE	GUIDE RAIL IMPROVEMENTS, GROUND-MOUNTED SIGNS	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-12, TA 619-16	SEE STD. SHEETS: TA 619-33, TA 619-37	
STAGES 1, 2, & 3 I-190 RAMPS	GUIDE RAIL IMPROVEMENTS, GROUND-MOUNTED SIGNS	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-12, TA 619-16	SEE STD. SHEETS: TA 619-33	
STAGE 5 I-190 MAINLINE	REMOVE WZTC APPROACH SIGNING, DETOUR SIGNS, PVMS SIGNS, AND GENERAL WZTC DEVICES	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-19	SEE STD. SHEETS: TA 619-33, TA 619-37	
STAGE 5 I-190 RAMPS	REMOVE WZTC APPROACH SIGNING, DETOUR SIGNS, PVMS SIGNS, AND GENERAL WZTC DEVICES	WORK BEYOND SHOULDER	SEE STD. SHEETS: TA 619-06		
		SHOULDER CLOSURE	SEE STD. SHEETS: TA 619-03, TA 619-04		
		LANE CLOSURE	SEE STD. SHEETS: TA 619-16	SEE STD. SHEETS: TA 619-33	


CHECKED BY: J. KLEINMANN

DRAFTED BY: P. THOMPSON

CHECKED BY: J. KLEINMANN



DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING TRAFFIC CONTROL DETAILS	DRAWING NUMBER: TCP-3

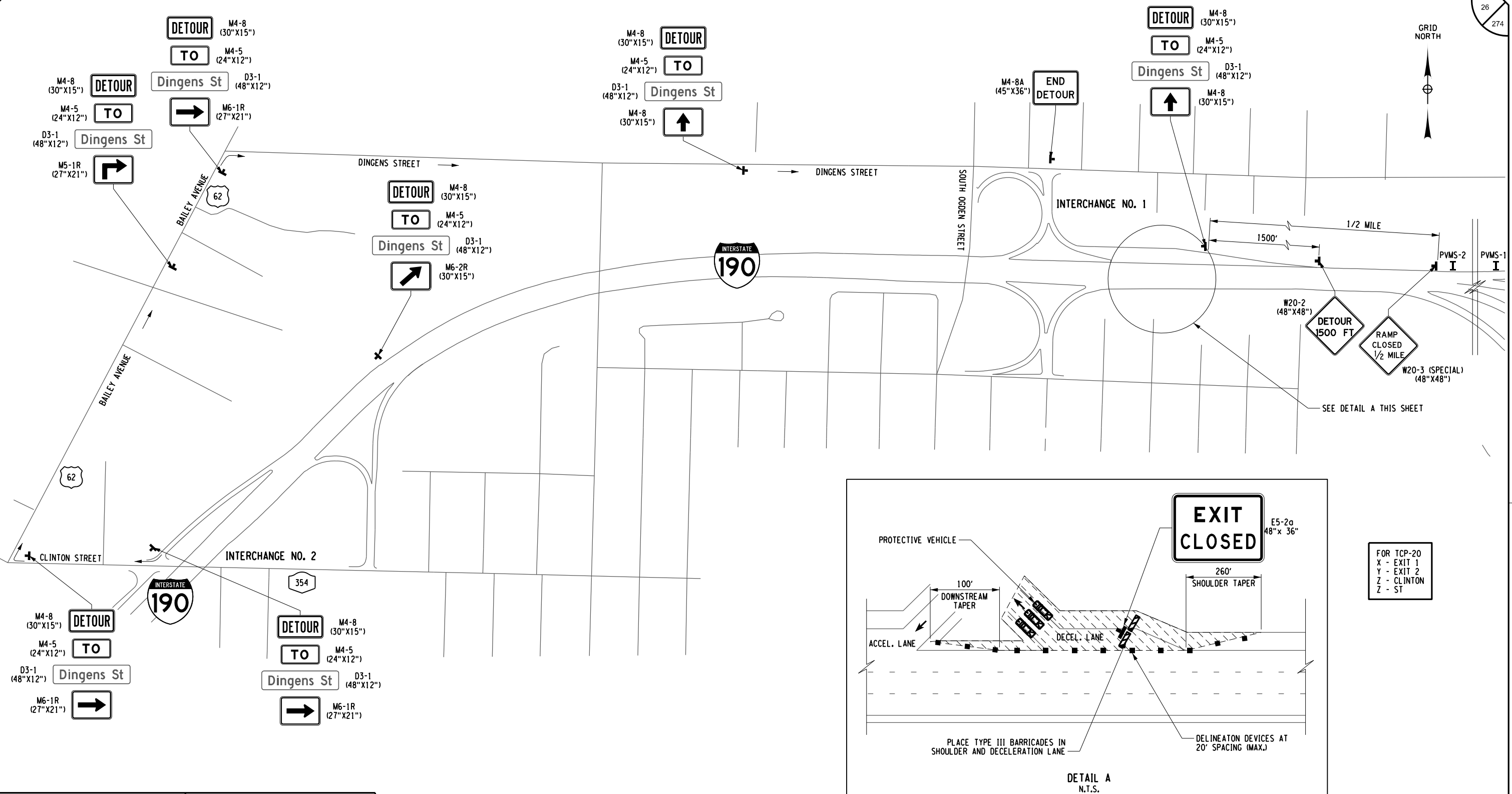
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

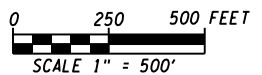
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



**DETOUR PLAN**  
**I-190, INT 1 NB OFF-RAMP CLOSURE**

N.T.S.



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TRAFFIC CONTROL PLANS EXIT DETOURS	DRAWING NUMBER: TCP-4

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

27  
274

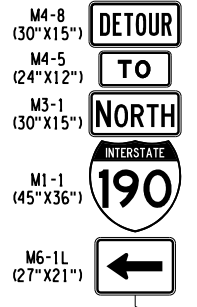
M4-8 (30" X15")  
M4-5 (24" X12")  
M3-1 (30" X15")  
M1-1 (45" X36")  
M6-1L (27" X21")



NORTH INTERSTATE 190  
DETOUR AHEAD

M3-1 (30" X15")  
M1-1 (45" X36")  
W20-2 (36" X36")

DETOUR TO NORTH INTERSTATE 190  
M4-8 (30" X15")  
M4-5 (24" X12")  
M3-1 (30" X15")  
M1-1 (45" X36")  
M5-1L (27" X21")



M4-8 (30" X15")  
M4-5 (24" X12")  
M3-1 (30" X15")  
M1-1 (45" X36")  
M6-3 (27" X21")



62  
BAILEY AVENUE

M4-8 (30" X15")  
M4-5 (24" X12")  
M3-1 (30" X15")  
M1-1 (45" X36")  
M5-1L (27" X21")



INTERSTATE 190

S. OGDEN STREET

INTERCHANGE NO. 1

DINGENS STREET

NORTH INTERSTATE 190  
DETOUR AHEAD

M3-1 (30" X15")  
M1-1 (45" X36")  
W20-2 (36" X36")

SEE DETAIL A ON LEFT OF THIS SHEET

M4-8 (30" X15")  
M4-5 (24" X12")  
M3-1 (30" X15")  
M1-1 (45" X36")  
M5-1L (27" X21")



62  
BAILEY AVENUE

354  
CLINTON STREET

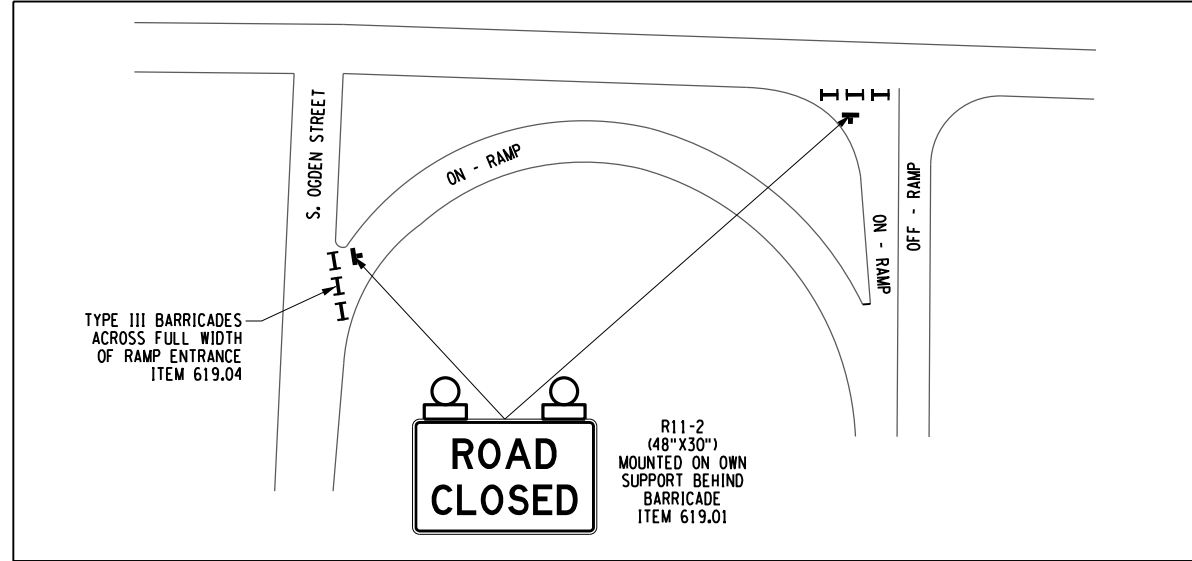
INTERCHANGE NO. 2

M4-8 (30" X15")  
M4-5 (24" X12")  
M3-1 (30" X15")  
M1-1 (45" X36")  
M6-1L (27" X21")



END DETOUR  
M4-8A (45" X36")

INTERSTATE 190



DETAIL A  
N.T.S.

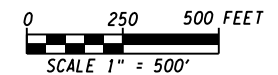
DETOUR PLAN  
I-190, INT 1, NB ON-RAMP CLOSURE  
N.T.S.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TRAFFIC CONTROL PLANS EXIT DETOURS	DRAWING NUMBER: TCP-5



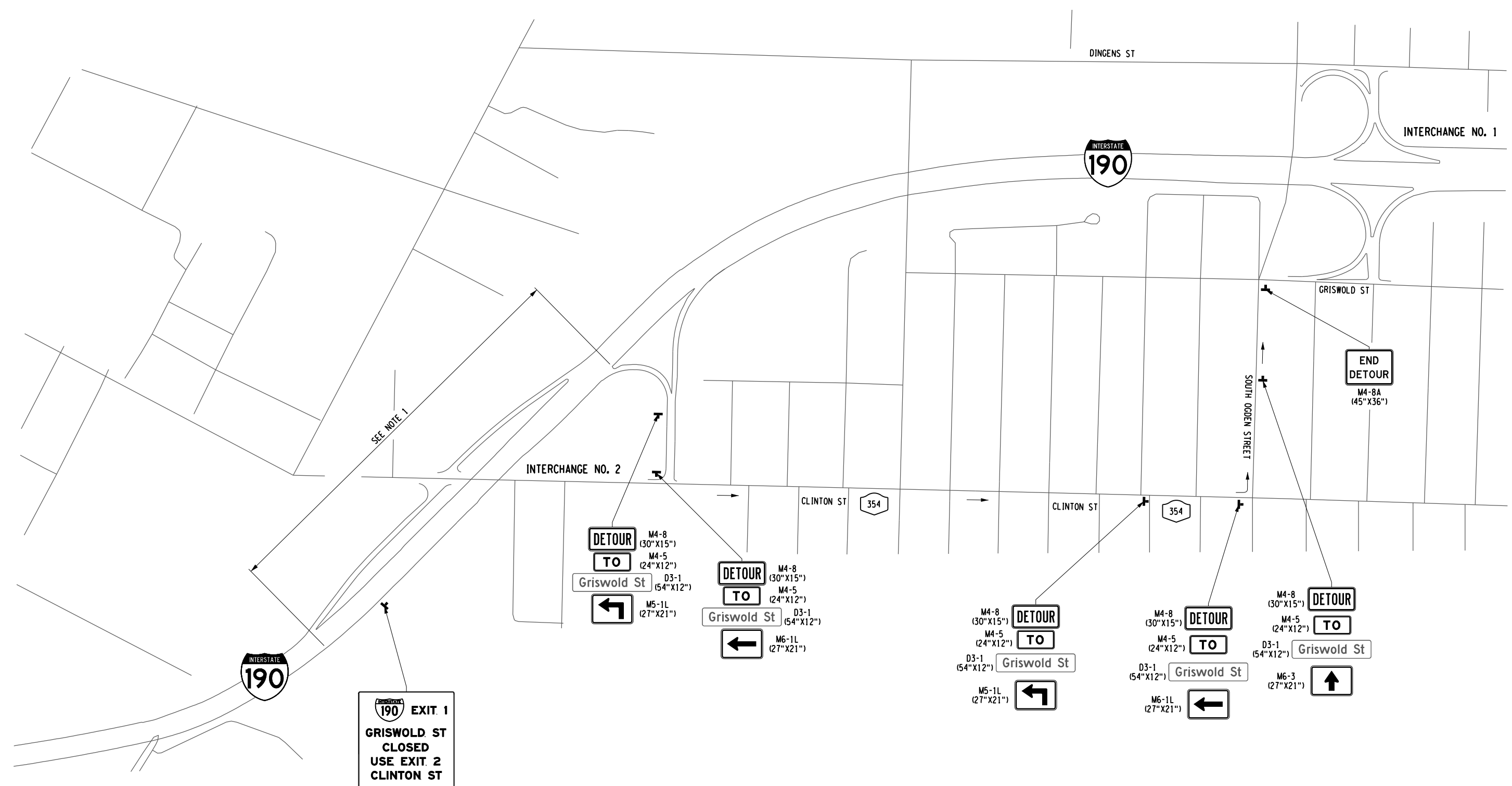
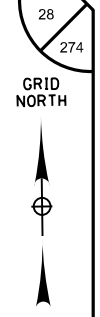
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

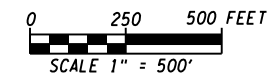
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



DETOUR PLAN  
I-190, INT 1 SB OFF-RAMP CLOSURE  
N.T.S.

FOR TCP-20  
A - GRISWOLD ST  
B - CLINTON ST  
X - EXIT 1  
Y - EXIT 2  
Z - GRISWOLD ST



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

NOTES:  
1. SEE DRAWING TCP-20 FOR I-190 ADVANCED RAMP CLOSURE DETAILS.

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY Thruway Authority  
GMA  
2024 Mark "Race" Tower  
GMA, WI 14223-3723  
616.463.4897 www.gma.com

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TRAFFIC CONTROL PLANS EXIT DETOURS	DRAWING NUMBER: TCP-6

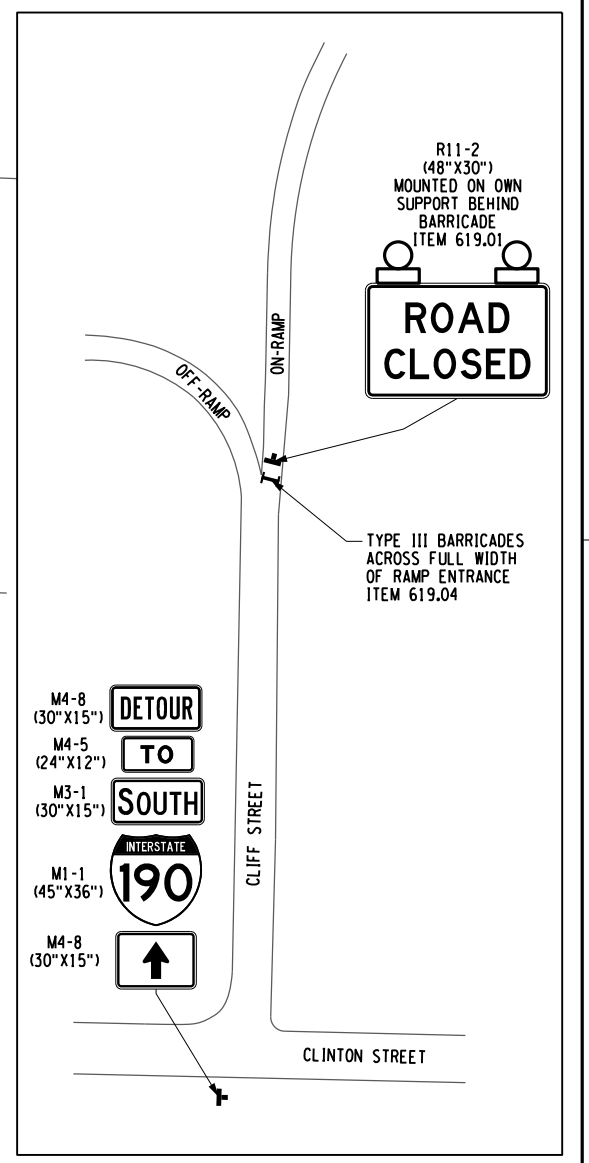
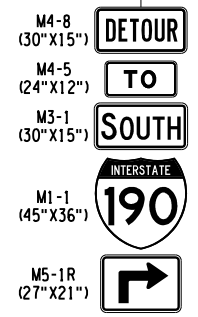
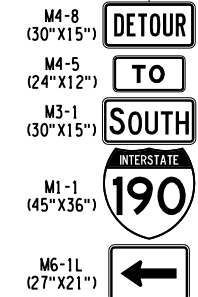
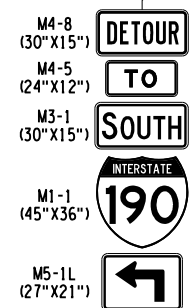
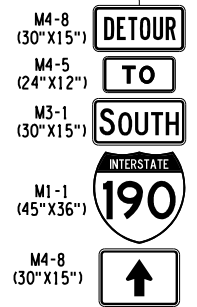
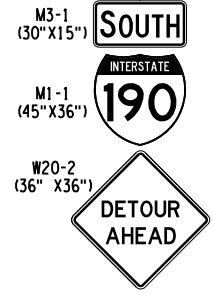
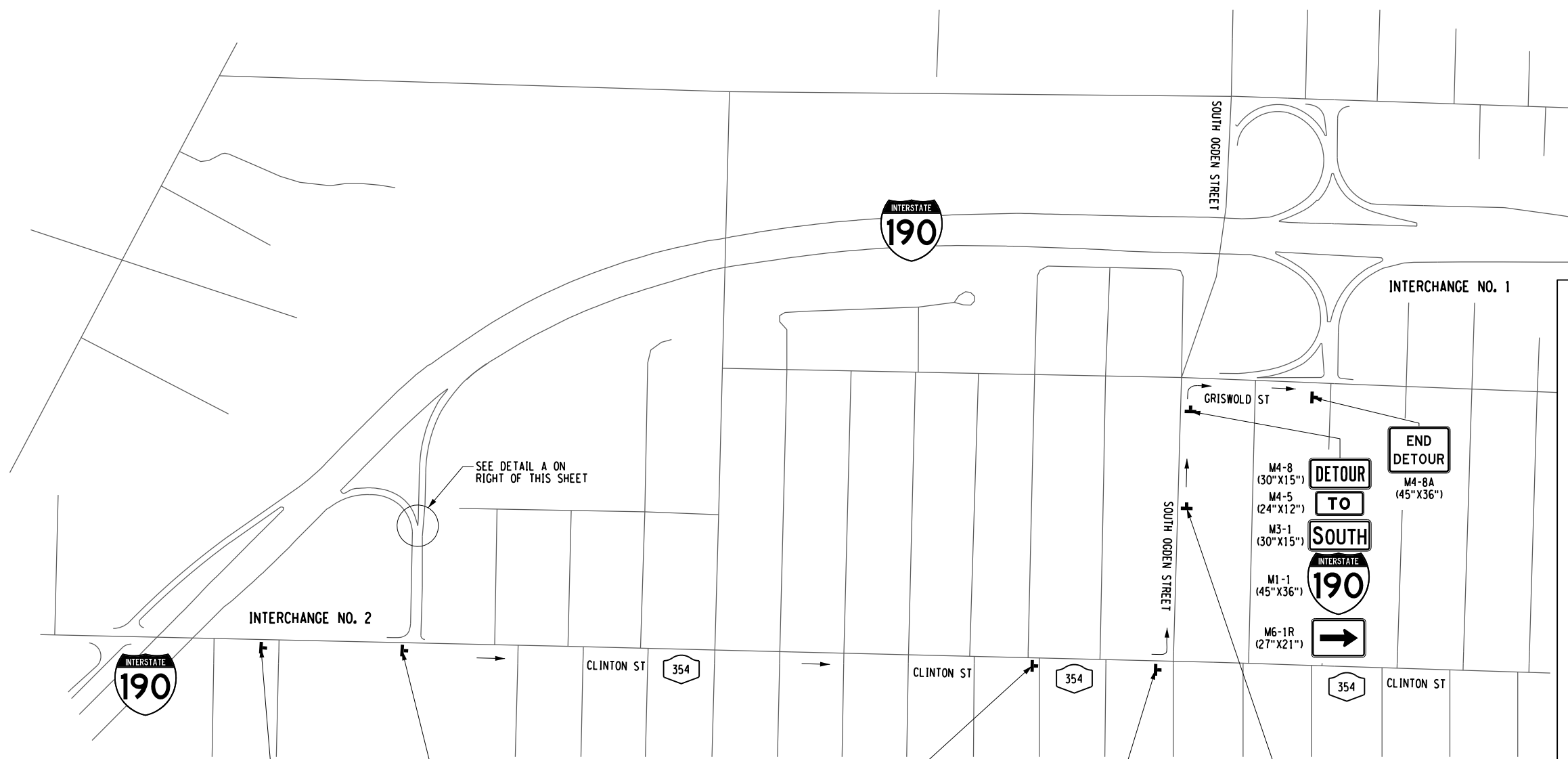
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

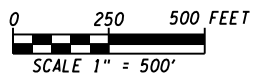
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



**DETOUR PLAN**  
**I-190, INT 2 SB ON-RAMP CLOSURE**  
N.T.S.

**DETAIL A**  
N.T.S.



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

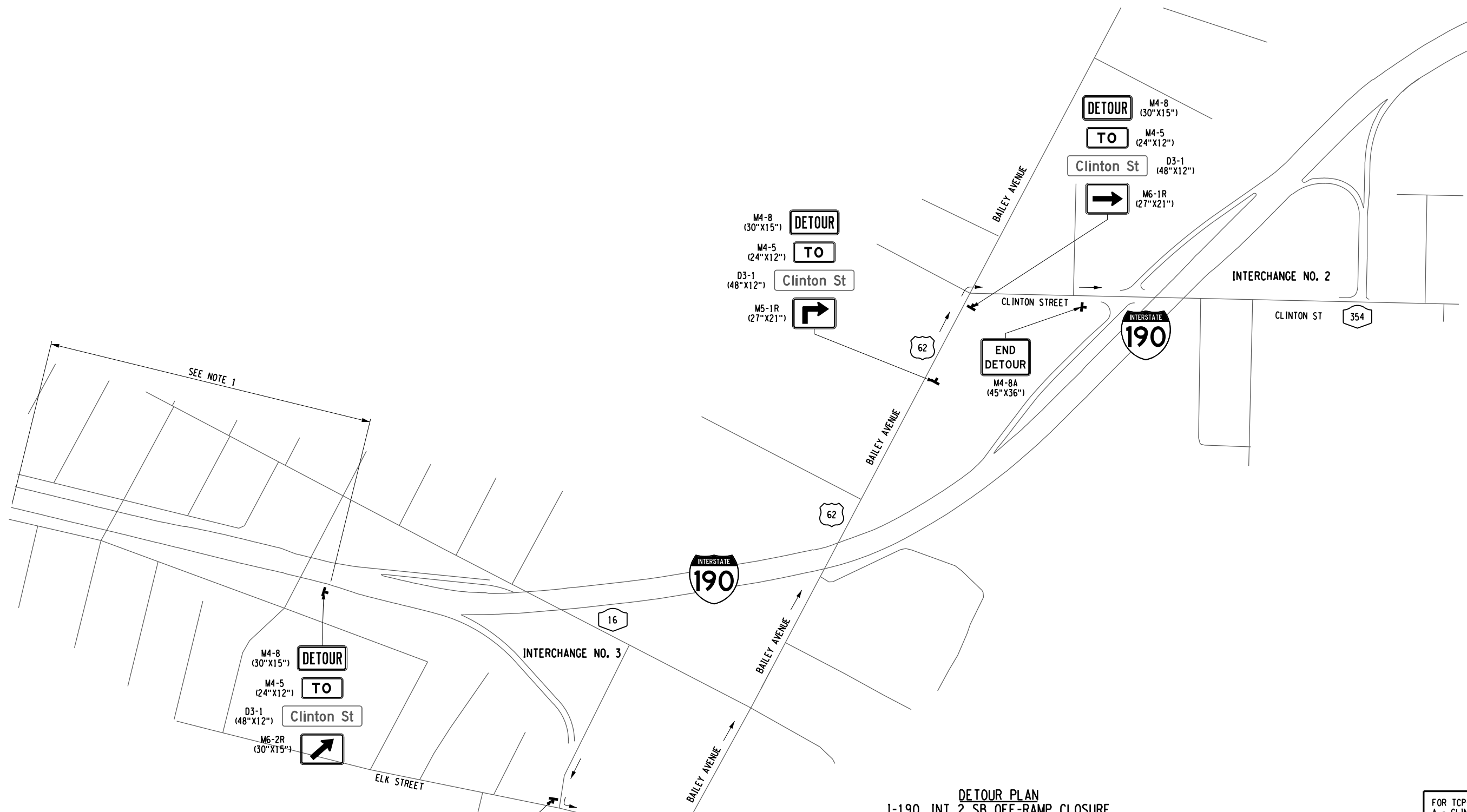
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TRAFFIC CONTROL PLANS EXIT DETOURS	DRAWING NUMBER: TCP-7



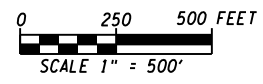
CHECKED BY: J. KLEINMANN  
 DRAFTED BY: N. HOY  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: P. THOMPSON  
 DESIGN SUPERVISOR: D. MARSH



SEE NOTE 1

**DETOUR PLAN**  
**I-190, INT 2 SB OFF-RAMP CLOSURE**  
 N.T.S.

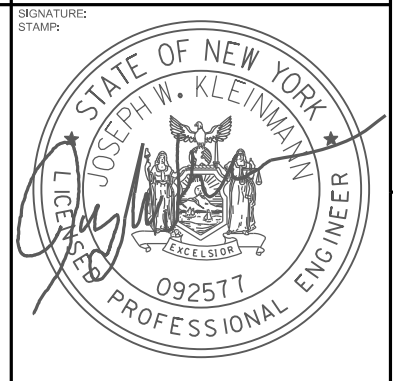
FOR TCP-20  
 A - CLINTON ST  
 B - ELK ST  
 X - EXIT 2  
 Y - EXIT 3  
 Z - CLINTON ST



NOTES:  
 1. SEE DRAWING TCP-20 FOR I-190 ADVANCED RAMP CLOSURE DETAILS.

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_  
 STAMP: \_\_\_\_\_



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 TITLE OF DRAWING: TRAFFIC CONTROL PLANS EXIT DETOURS

CONTRACT NUMBER: TAN 24-12  
 DATE: MAY 2024  
 DRAWING NUMBER: TCP-8

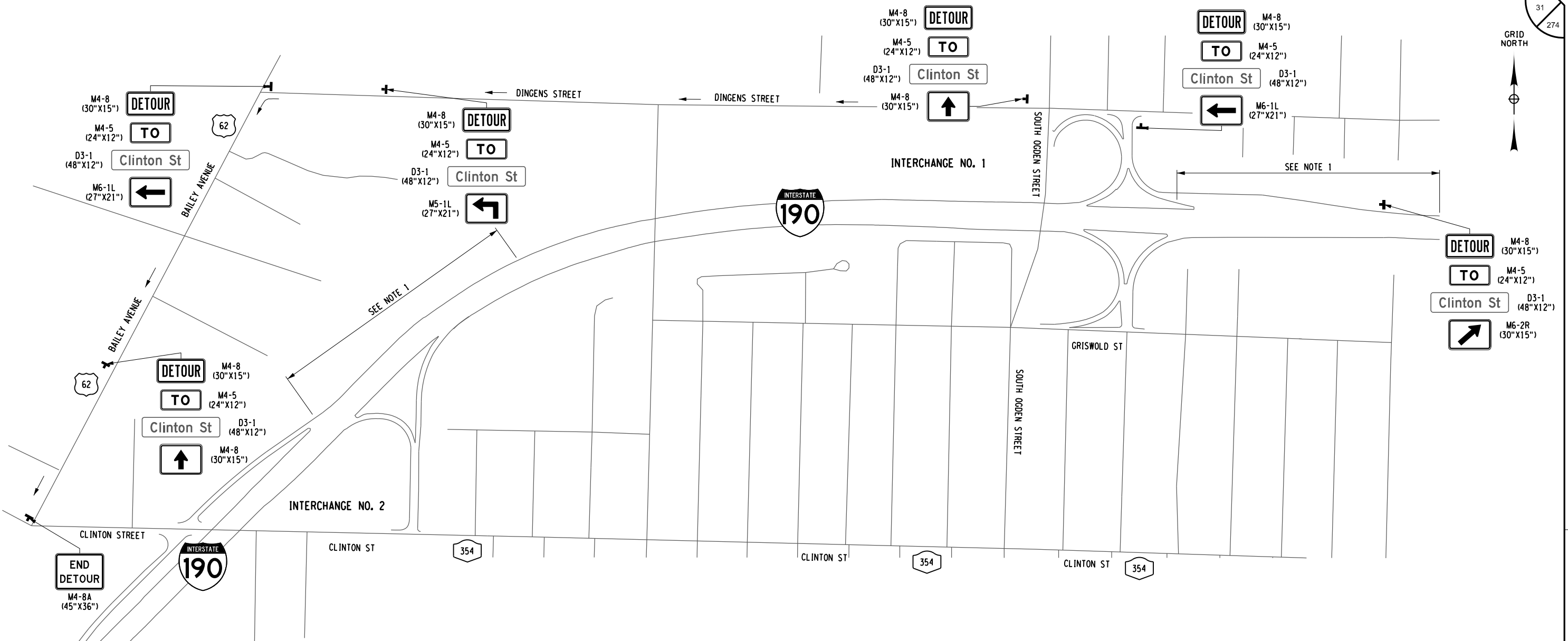
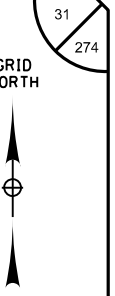
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

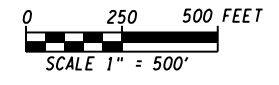
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



DETOUR PLAN  
I-190, INT 2 NB OFF-RAMP CLOSURE  
N.T.S.

FOR TCP-20  
A - CLINTON ST  
B - OGDEN ST  
X - EXIT 2  
Y - EXIT 1  
Z - CLINTON  
Z - ST



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

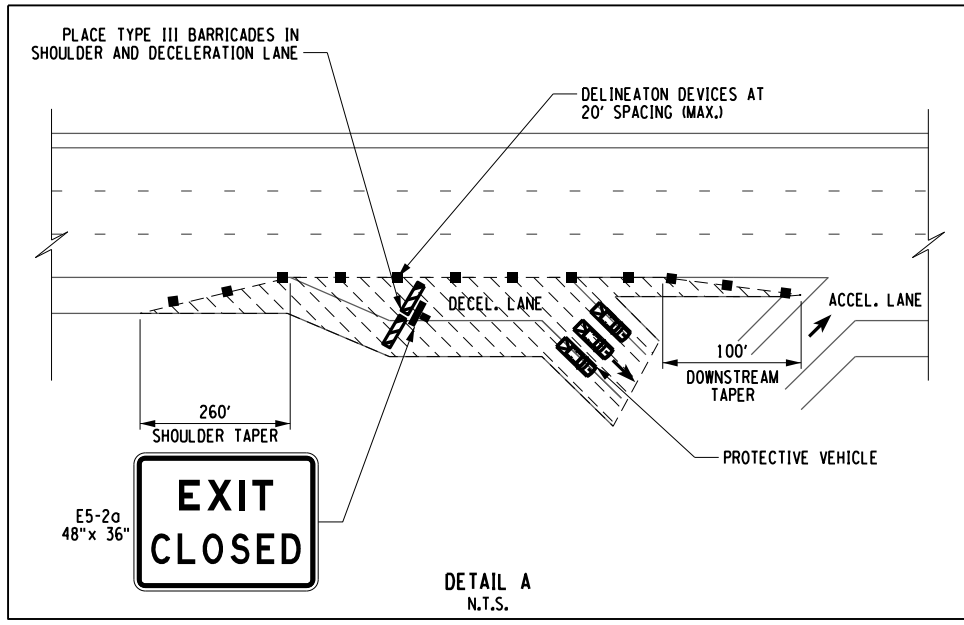
NOTES:  
1. SEE DRAWING TCP-20 FOR I-190 ADVANCED RAMP CLOSURE DETAILS.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

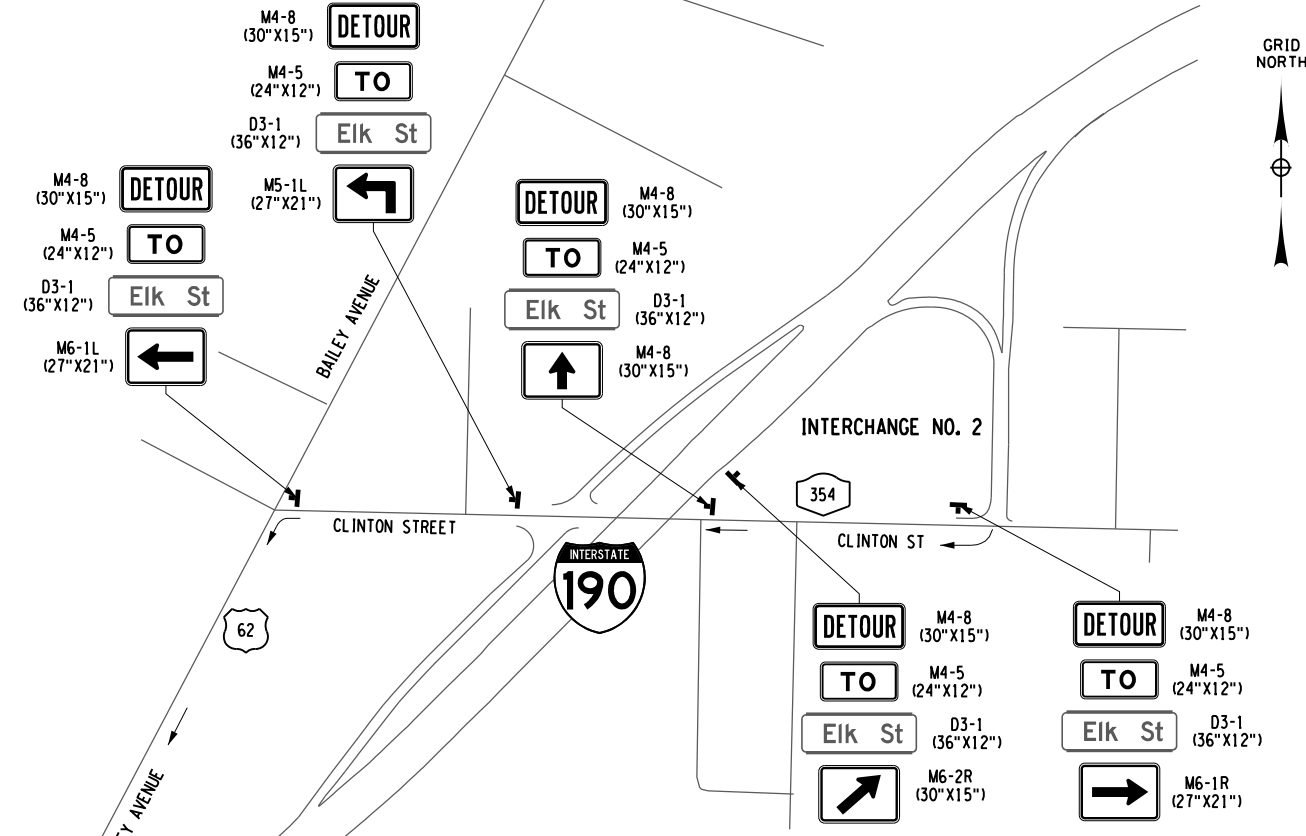
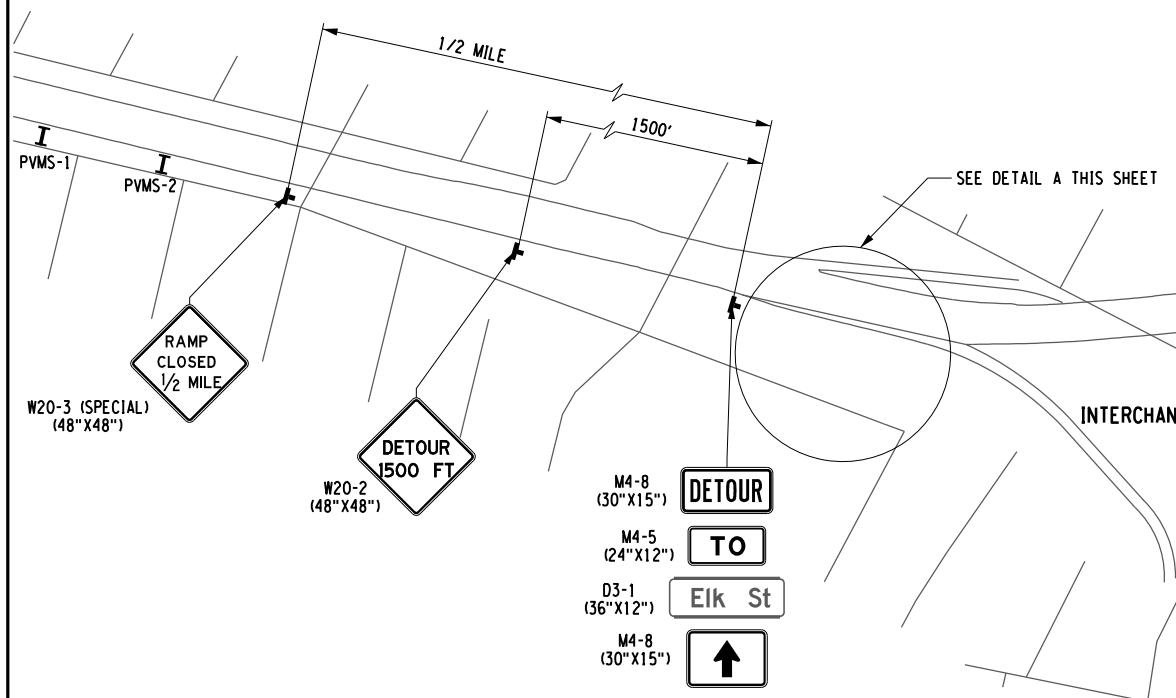
REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TRAFFIC CONTROL PLANS EXIT DETOURS	DRAWING NUMBER: TCP-9

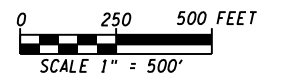
CHECKED BY: J. KLEINMANN  
DRAFTED BY: N. HOY  
CHECKED BY: J. KLEINMANN  
DESIGNED BY: P. THOMPSON  
DESIGN SUPERVISOR: D. MARSH



FOR TCP-20  
X - EXIT 3  
Y - EXIT 4  
Z - CLINTON  
Z - ST



**DETOUR PLAN**  
**I-190, INT 3 SB OFF-RAMP CLOSURE**  
N.T.S.



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TRAFFIC CONTROL PLANS EXIT DETOURS	DRAWING NUMBER: TCP-10



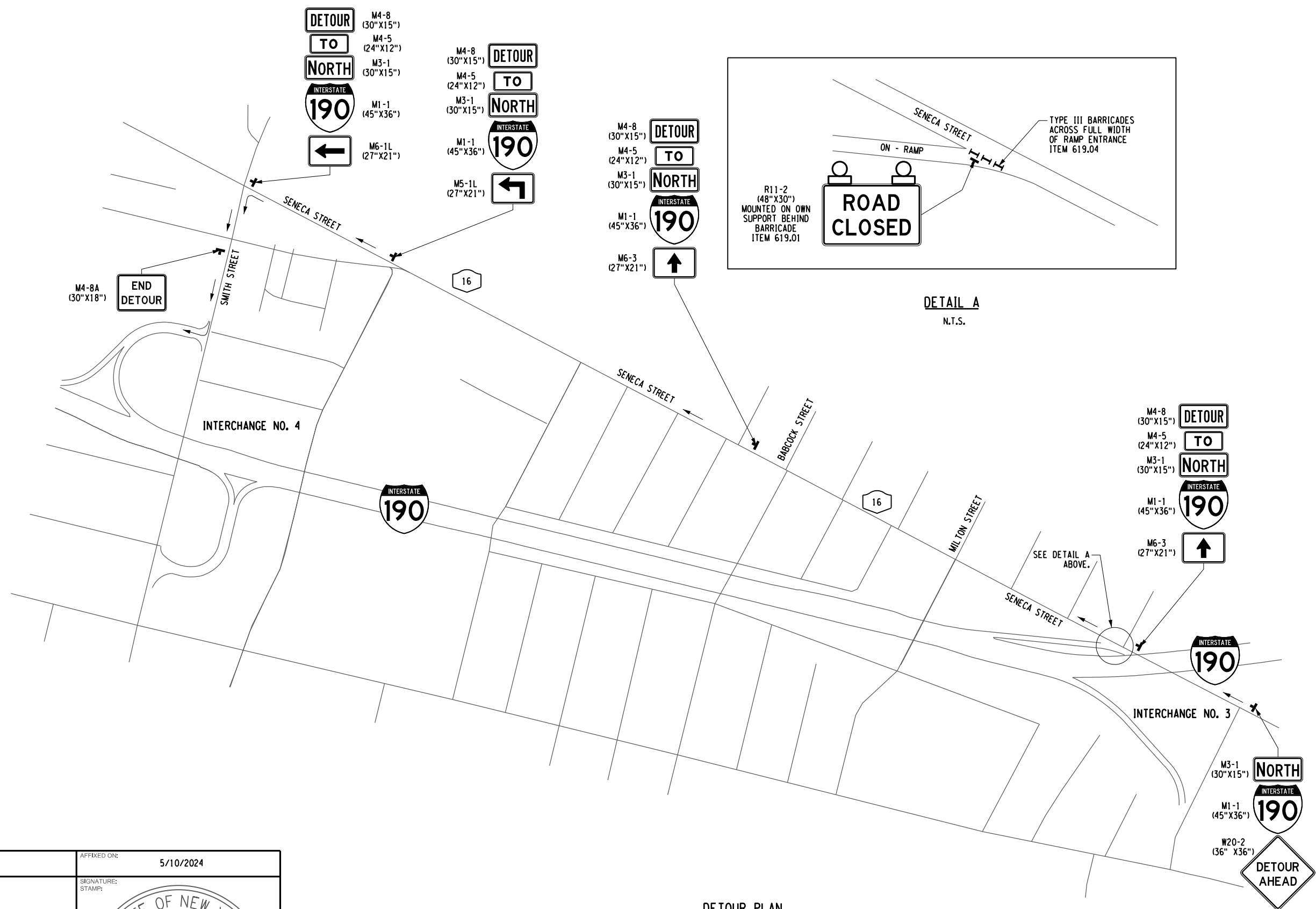
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



DETAIL A  
N.T.S.

DETOUR PLAN  
I-190, INT 3, NB ON-RAMP CLOSURE  
N.T.S.

M4-8 (30" X 15")  
M4-5 (24" X 12")  
M3-1 (30" X 15")  
M1-1 (45" X 36")  
M6-3 (27" X 21")

M3-1 (30" X 15")  
M1-1 (45" X 36")  
W20-2 (36" X 36")

0 250 500 FEET  
SCALE 1" = 500'

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TRAFFIC CONTROL PLANS EXIT DETOURS	DRAWING NUMBER: TCP-11



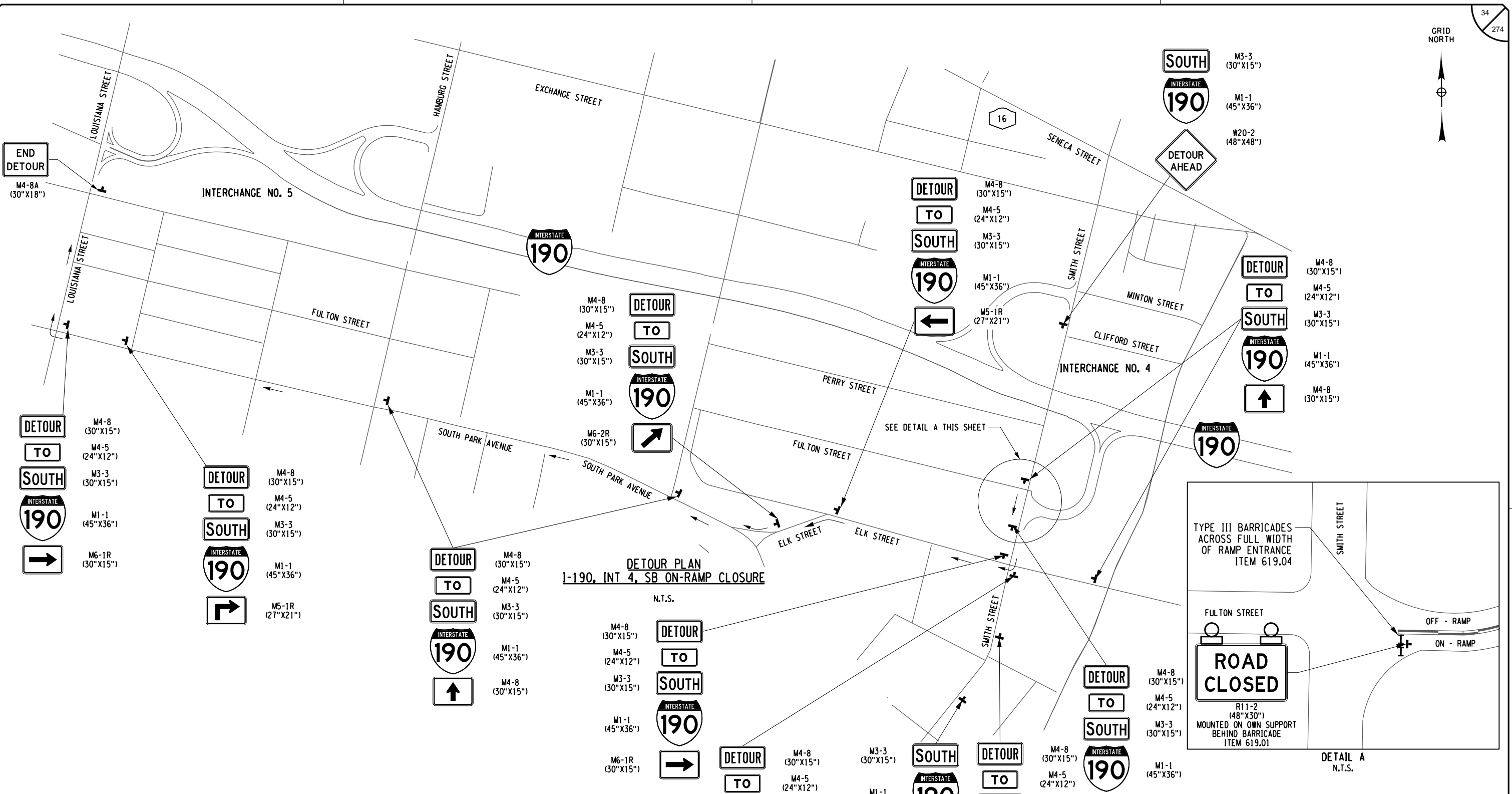
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

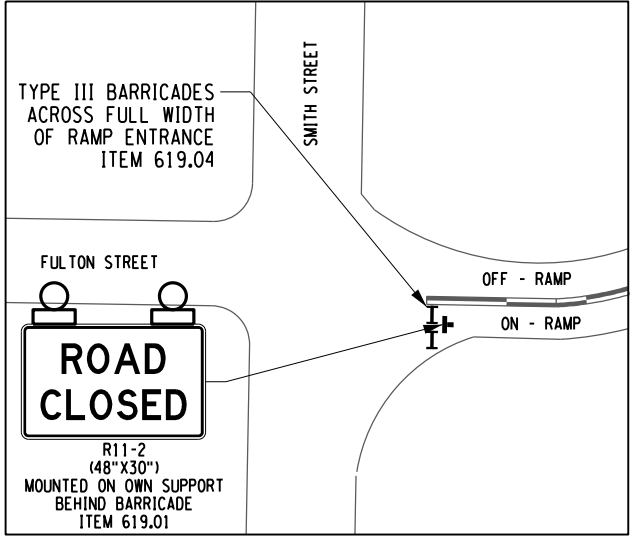
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

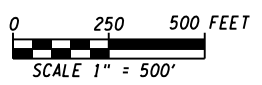


**DETOUR PLAN**  
I-190, INT 4, SB ON-RAMP CLOSURE

N.T.S.



DETAIL A  
N.T.S.



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TRAFFIC CONTROL PLANS EXIT DETOURS	DRAWING NUMBER: TCP-12



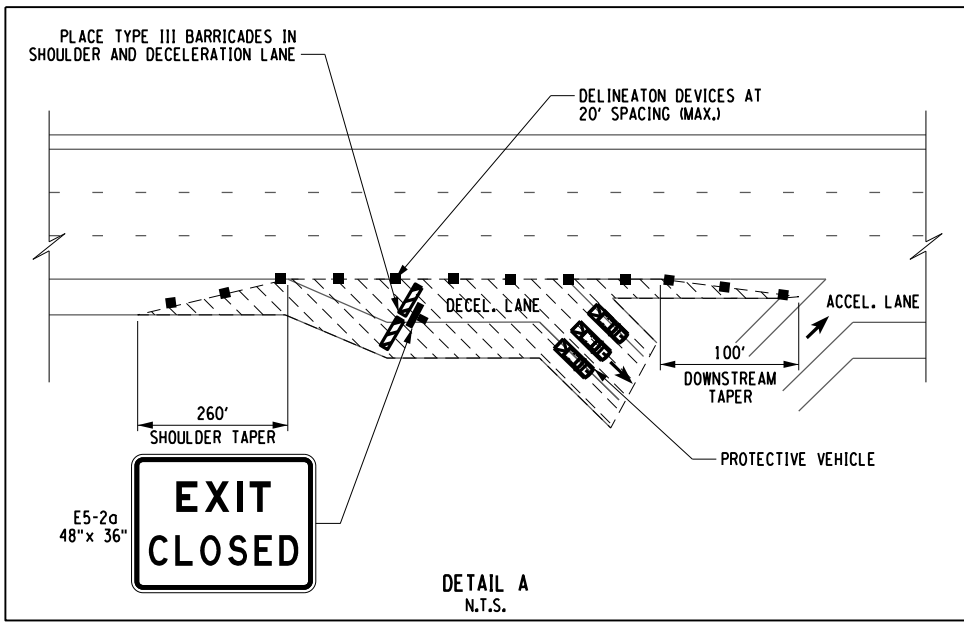
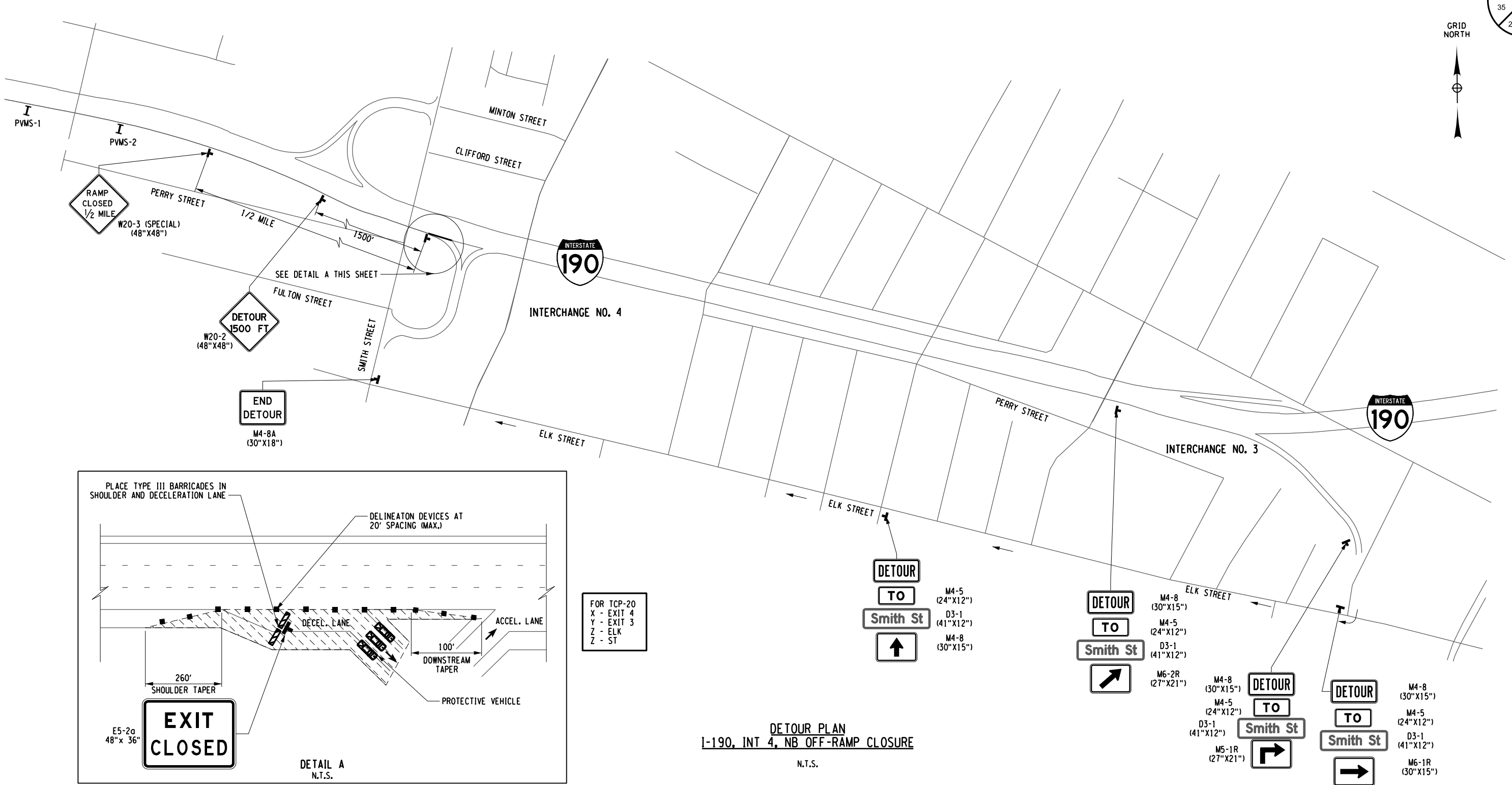
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

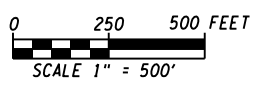
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



FOR TCP-20  
X - EXIT 4  
Y - EXIT 3  
Z - ELK  
Z - ST

DETOUR PLAN  
I-190, INT 4, NB OFF-RAMP CLOSURE  
N.T.S.



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING TRAFFIC CONTROL PLANS EXIT DETOURS	DRAWING NUMBER: TCP-13

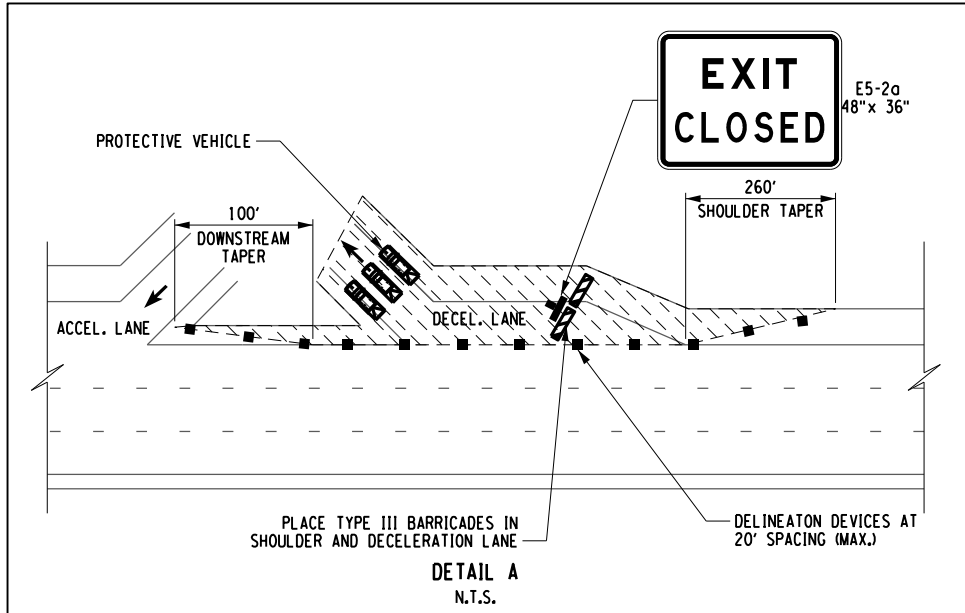
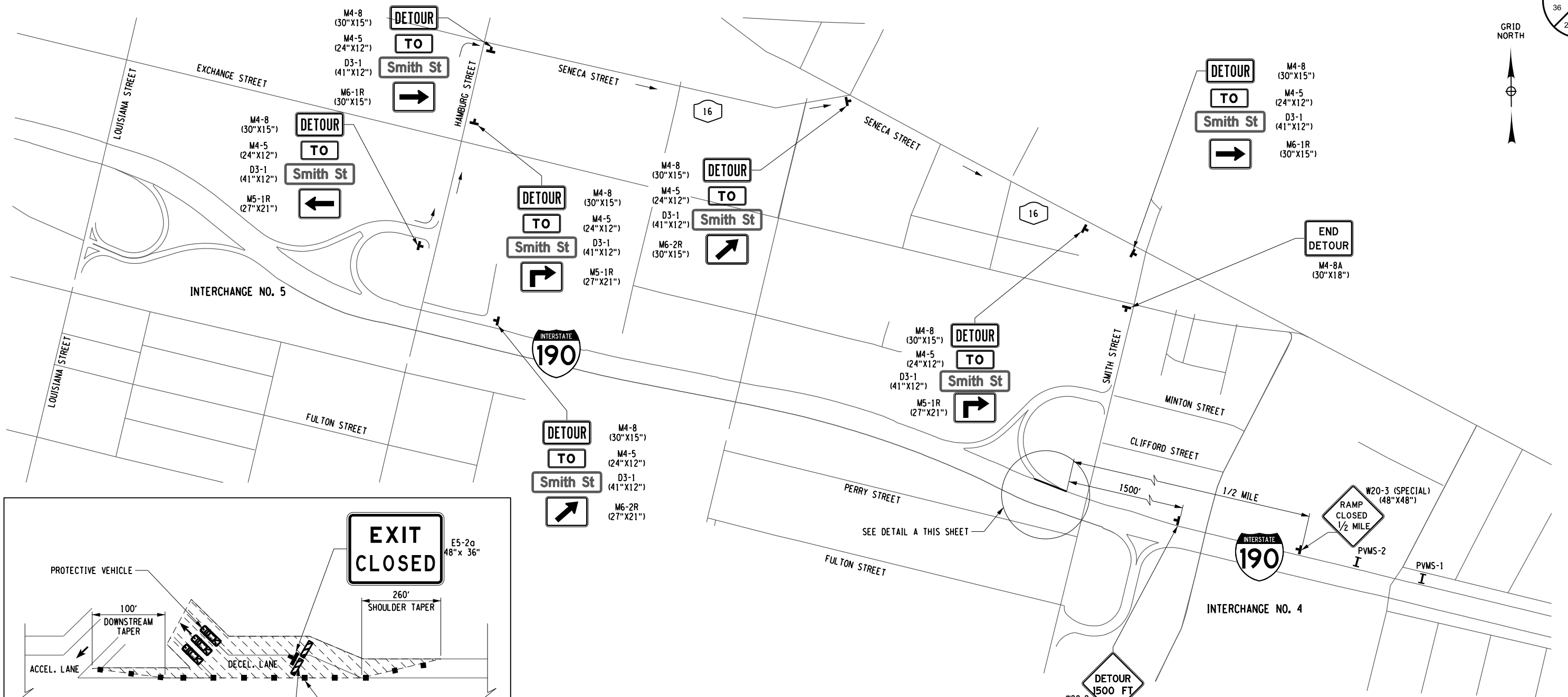
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



**DETOUR PLAN**  
**I-190, INT 4, NB OFF-RAMP CLOSURE**  
N.T.S.

FOR TCP-20  
X - EXIT 4  
Y - EXIT 5  
Z - HAMBURG  
Z - ST

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

JOSEPH W. KLEINMANN  
LICENSED PROFESSIONAL ENGINEER  
STATE OF NEW YORK  
092577

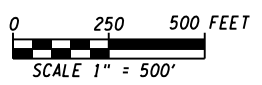
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK** STATE OF OPPORTUNITY  
**Thruway Authority**

**GMA**  
2025 Mid-Range Tower  
Suamico, WI 54233-3723  
918.462.4897 - www.gma.com

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TRAFFIC CONTROL PLANS EXIT DETOURS	DRAWING NUMBER: TCP-14



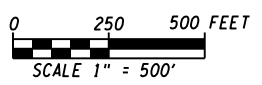
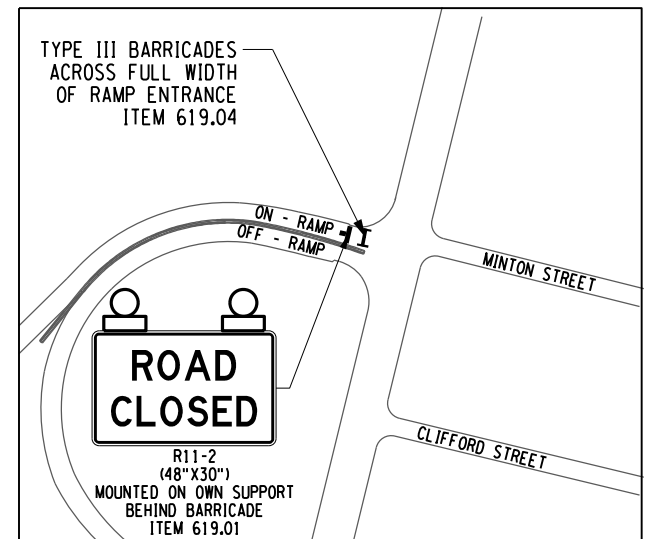
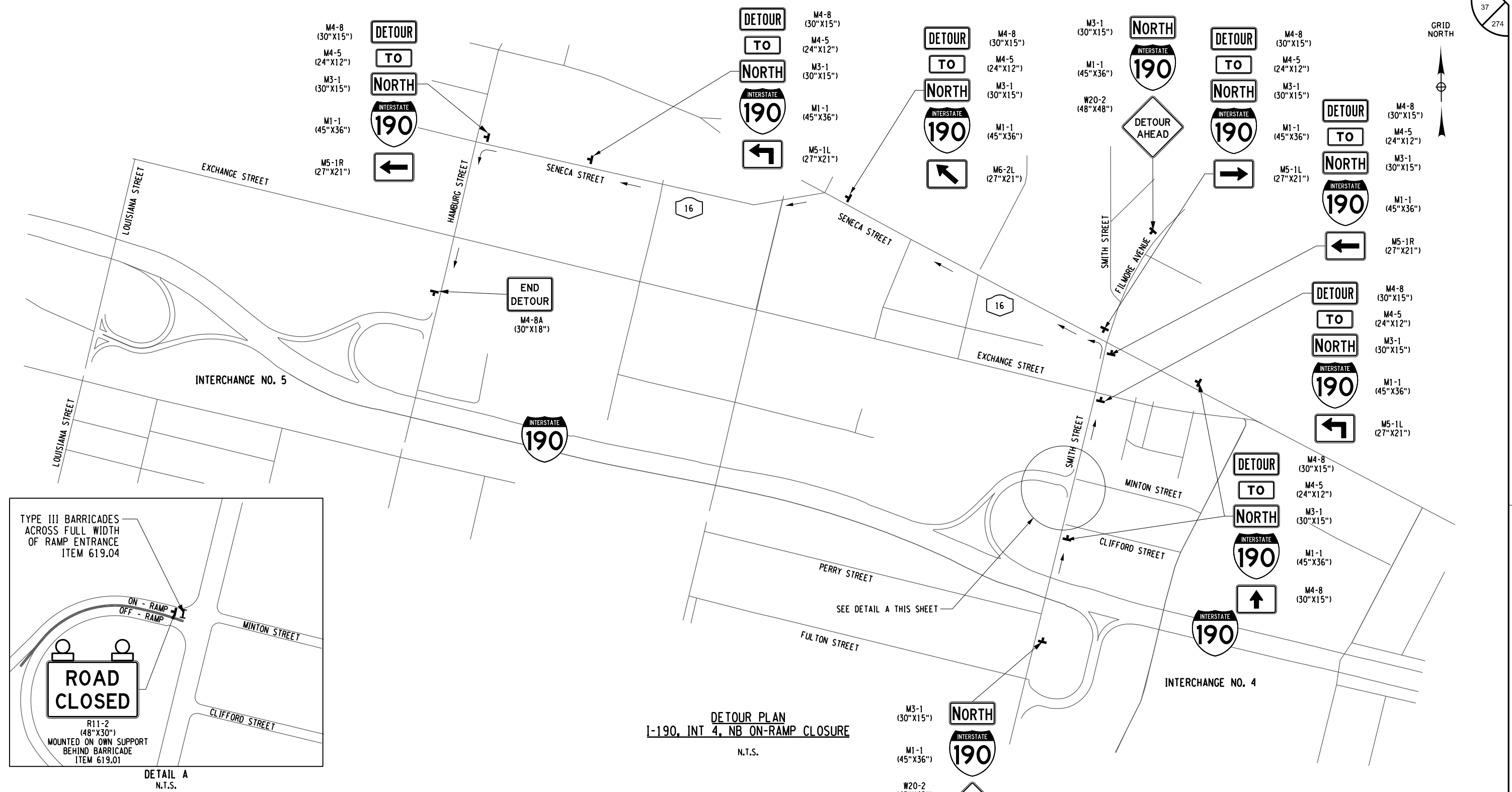
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

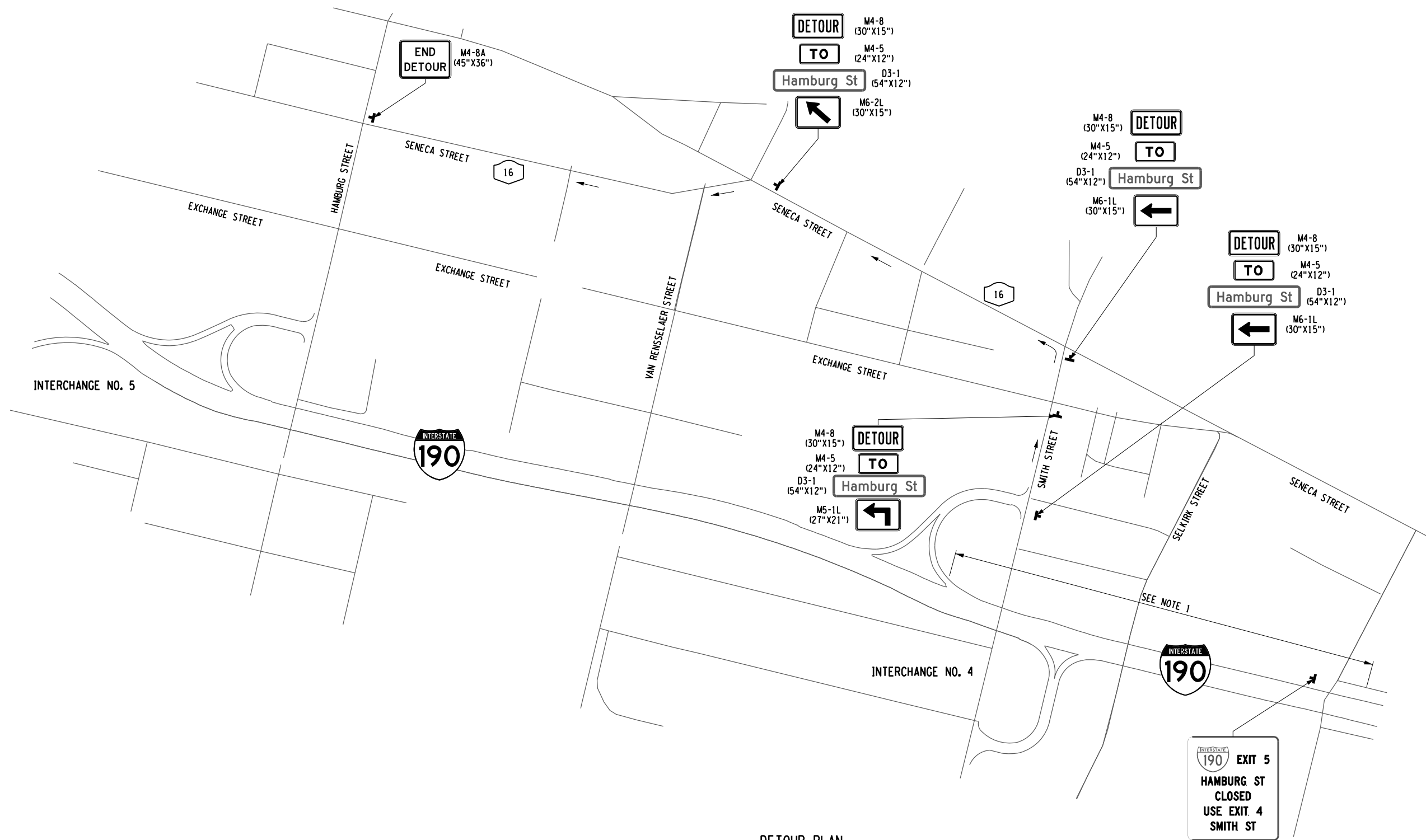


ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TRAFFIC CONTROL PLANS EXIT DETOURS	DRAWING NUMBER: TCP-15

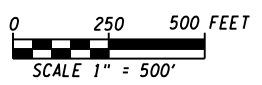


INTERSTATE 190 EXIT 5  
HAMBURG ST  
CLOSED  
USE EXIT 4  
SMITH ST

**DETOUR PLAN**  
**I-190, INT 5, NB OFF-RAMP CLOSURE**

N.T.S.

- NOTES:  
1. SEE DRAWING TCP-20 FOR I-190 ADVANCED RAMP CLOSURE DETAILS.




CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL




TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
TRAFFIC CONTROL PLANS  
EXIT DETOURS

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
TCP-16

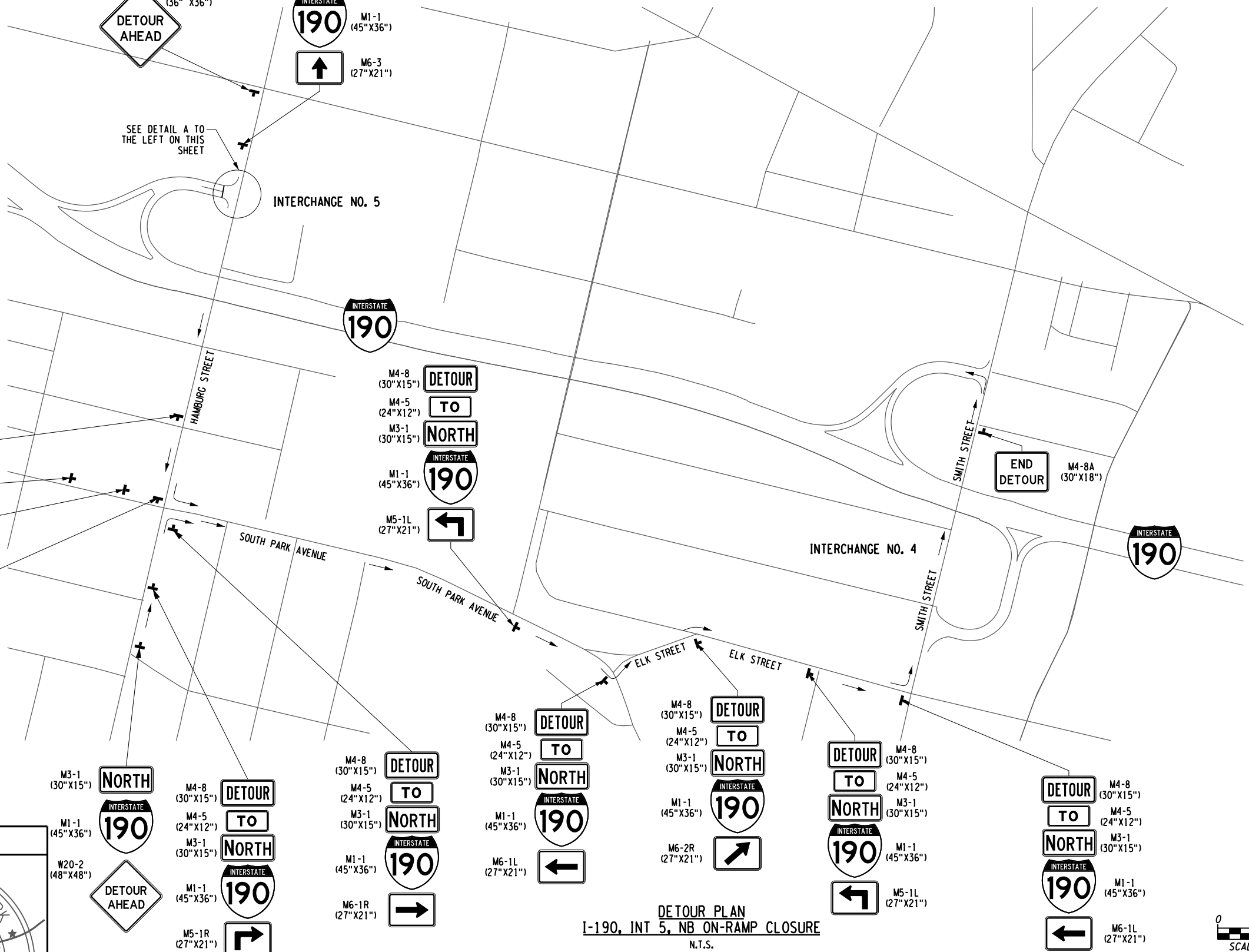
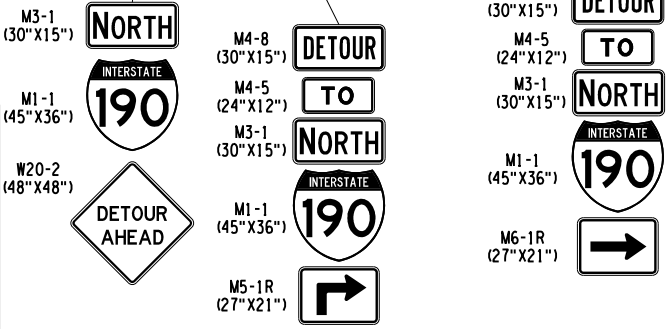
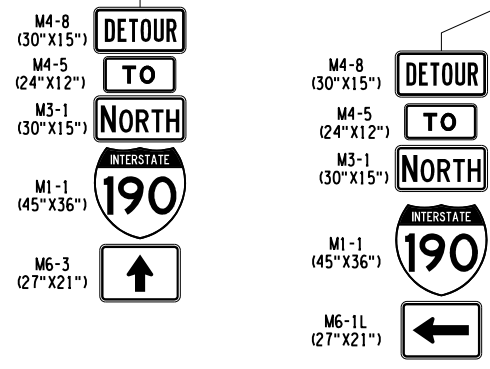
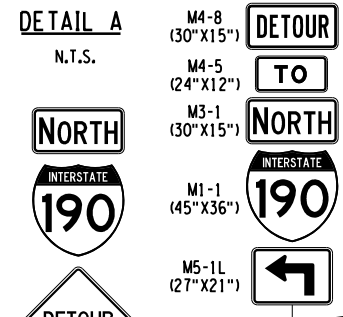
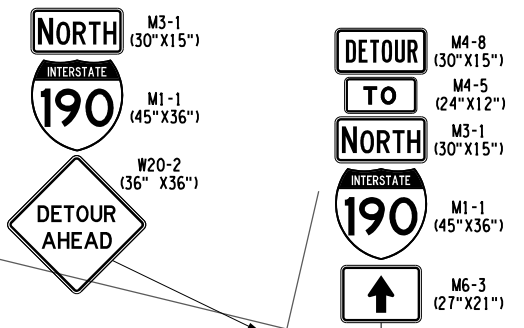
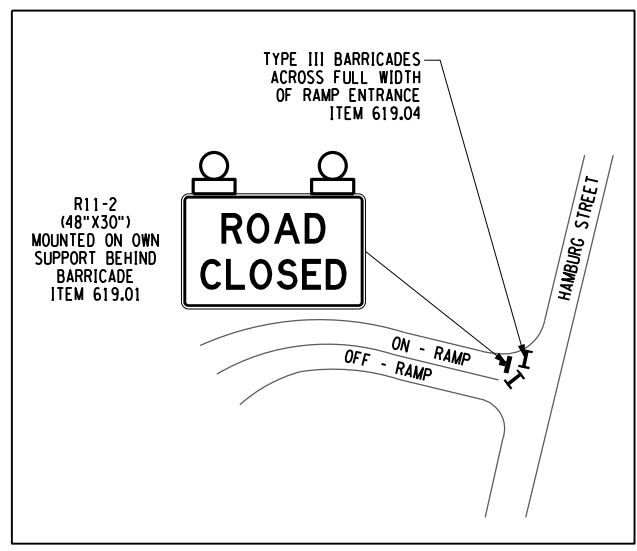
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

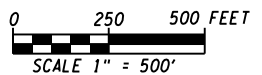
CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



DETOUR PLAN  
I-190, INT 5, NB ON-RAMP CLOSURE  
N.T.S.



ALTERED ON: \_\_\_\_\_  
SIGNATURE: \_\_\_\_\_  
STAMP: \_\_\_\_\_

AFFIXED ON: 5/10/2024  
SIGNATURE: \_\_\_\_\_  
STAMP: \_\_\_\_\_

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
TITLE OF DRAWING: TRAFFIC CONTROL PLANS EXIT DETOURS

CONTRACT NUMBER: TAN 24-12  
DATE: MAY 2024  
DRAWING NUMBER: TCP-17

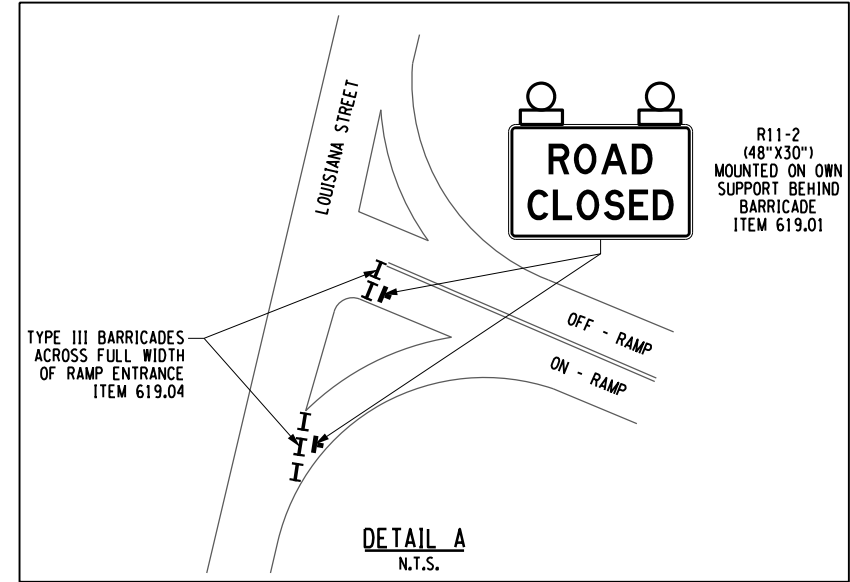
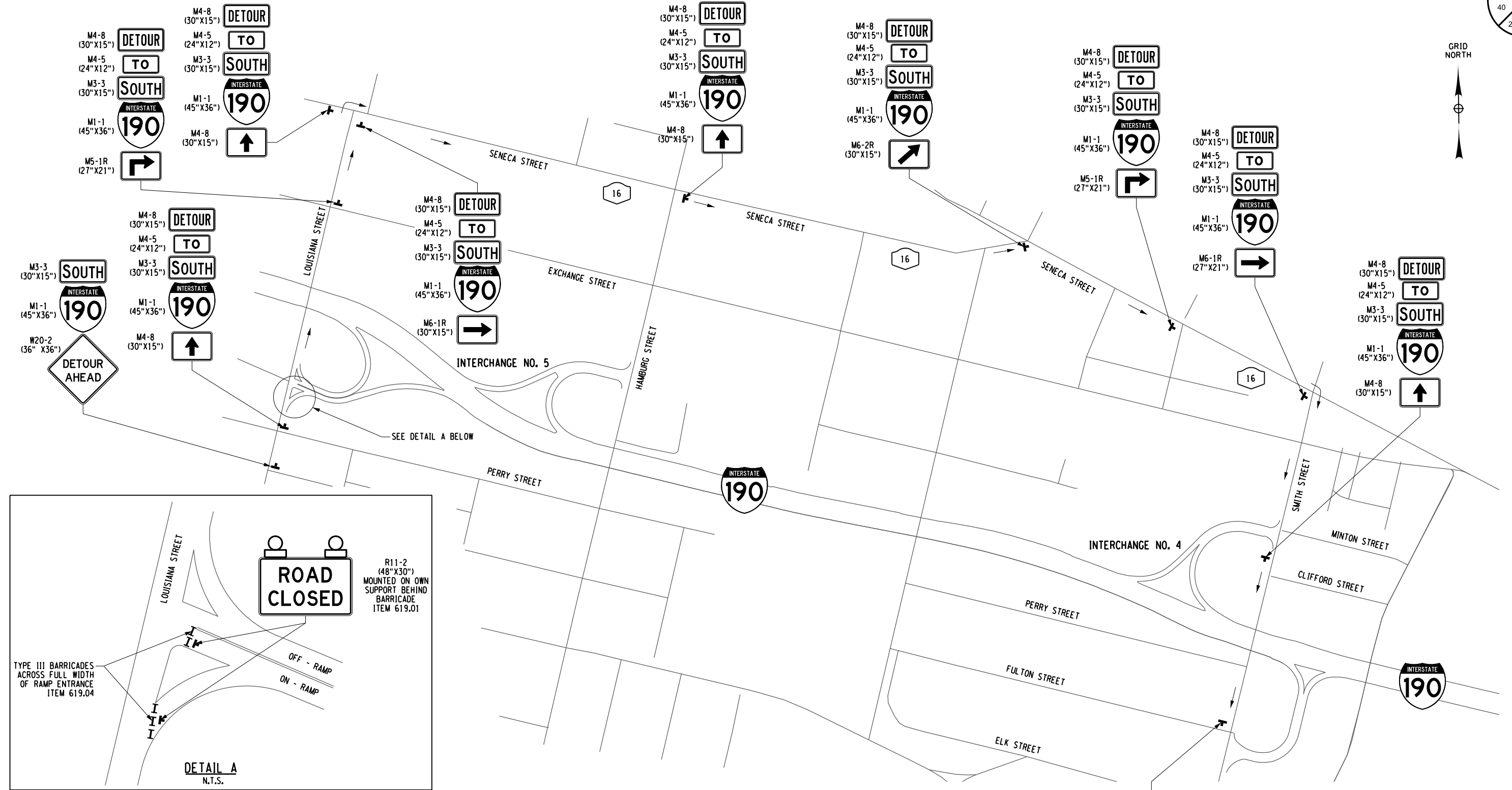
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

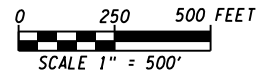
CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



**DETOUR PLAN**  
**I-190, INT 5, SB ON-RAMP CLOSURE**  
N.T.S.



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE:	SIGNATURE:
STAMP:	

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING TRAFFIC CONTROL PLANS EXIT DETOURS	DRAWING NUMBER: TCP-18



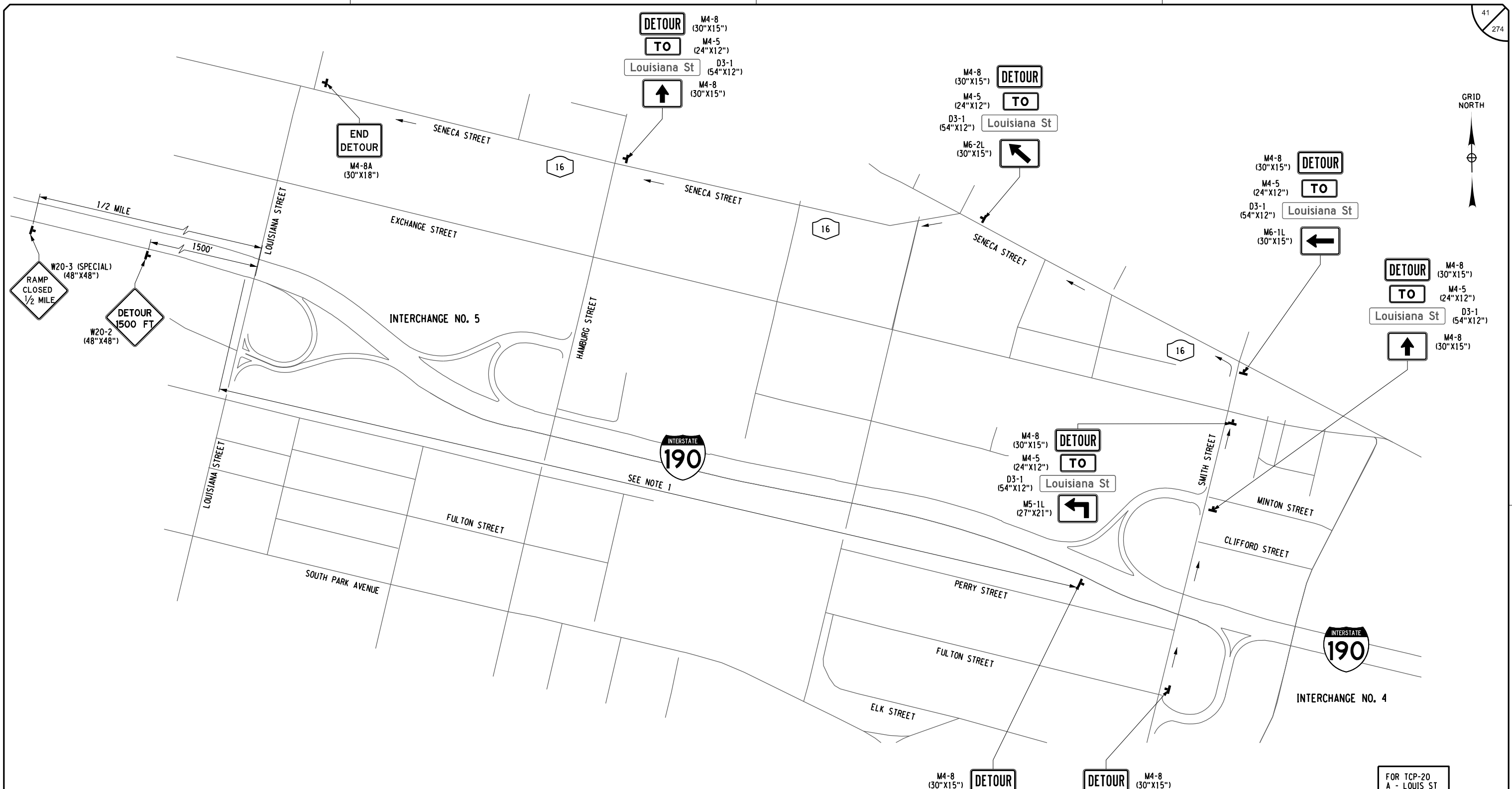
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

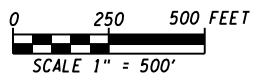
CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



FOR TCP-20  
 A - LOUIS ST  
 B - SMITH ST  
 X - EXIT 5  
 Y - EXIT 4  
 Z - LOUIS ST



**DETOUR PLAN**  
**I-190, INT 5, SB OFF-RAMP CLOSURE**  
 N.T.S.

- NOTES:  
 1. SEE DRAWING TCP-20 FOR I-190 ADVANCED RAMP CLOSURE DETAILS.

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
TRAFFIC CONTROL PLANS  
EXIT DETOURS

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
TCP-19

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

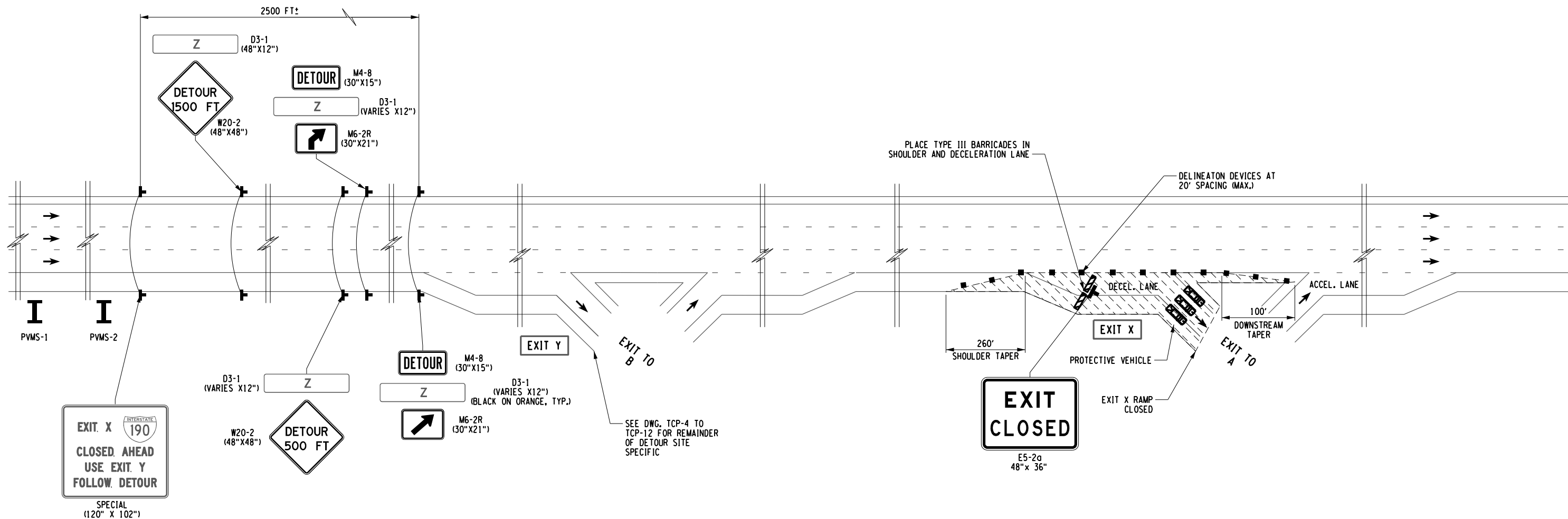
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



TYPICAL I-190 OFF-RAMP CLOSURE DETAIL  
N.T.S.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

EXIT X RAMP CLOSED MESSAGE 1	FOLLOW DETOUR AHEAD MESSAGE 2	TRAFC TO Z Z MESSAGE 1	USE EXIT Y MESSAGE 2
PVMS-1		PVMS-2	

- A - EXIT X STREET NAME
- B - EXIT Y STREET NAME
- X - APPLICABLE CLOSED EXIT NUMBER
- Y - APPLICABLE DETOUR EXIT NUMBER
- Z - APPLICABLE DETOUR STREET NAME

LEGEND	
	LANE AND RAMP CLOSURE
	DIRECTION OF TRAFFIC
	SIGN, TEMPORARY
	TYPE III BARRICADE ITEM 619.04
	CHANNELIZING DEVICE
	CHANGABLE MESSAGE SIGN (PVMS) ITEM 619.110512
	PROTECTIVE VEHICLE

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
TRAFFIC CONTROL PLANS  
OFF-RAMP CLOSURE DETAIL

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
TCP-20

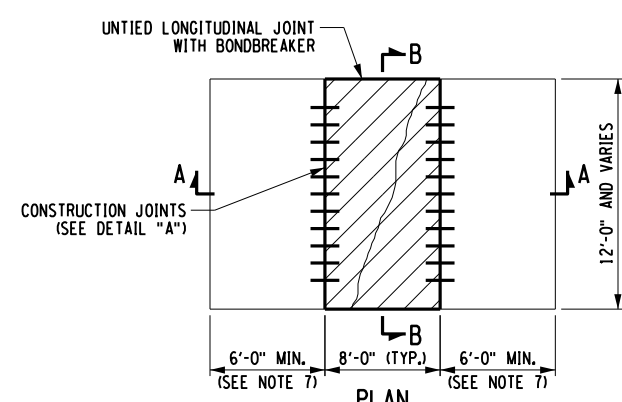
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DRAFTED BY: P. AMICONE

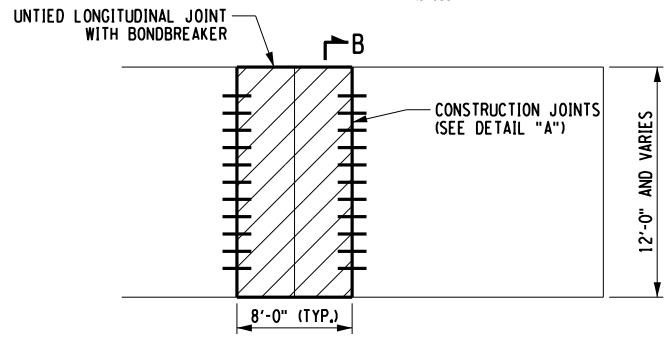
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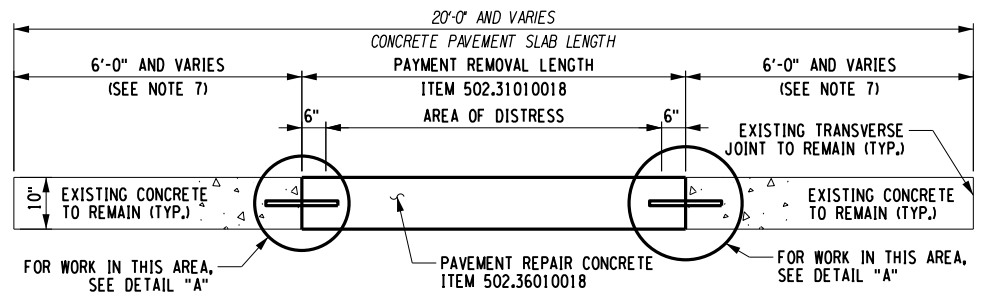
DESIGN SUPERVISOR: D. MARSH



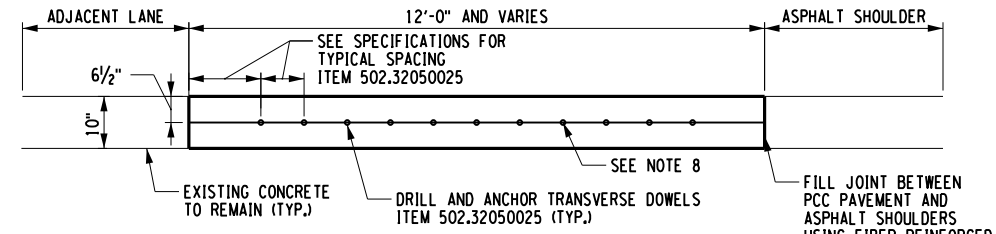
PLAN  
PARTIAL WIDTH, MID-SLAB REPAIR  
N.T.S.



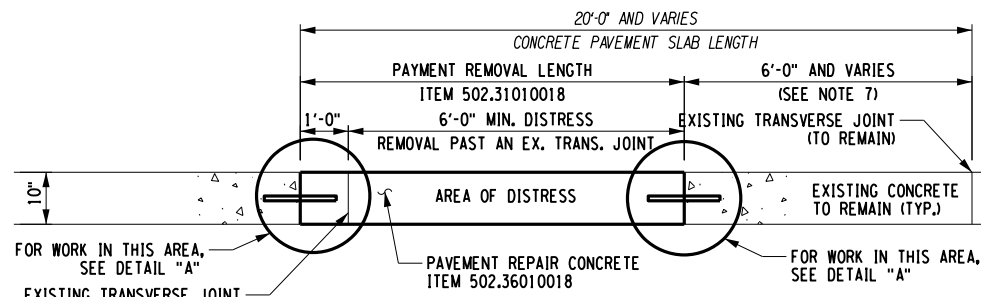
PLAN  
PARTIAL WIDTH, JOINT REPAIR  
N.T.S.



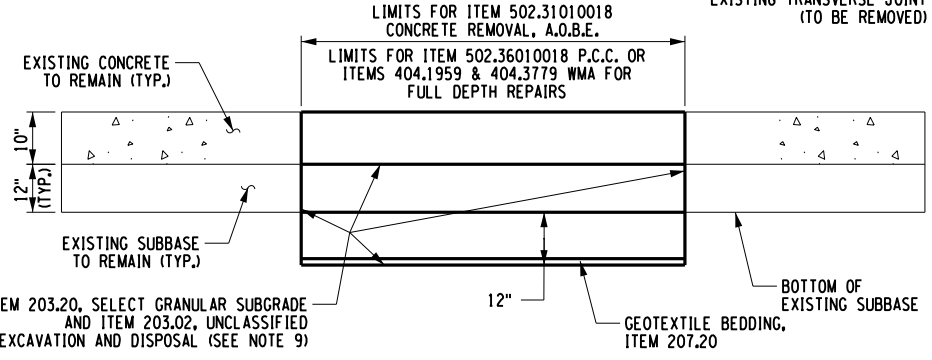
SECTION A-A  
N.T.S.  
(SEE DWG. NO. MD-2 FOR SECTION LOCATIONS)



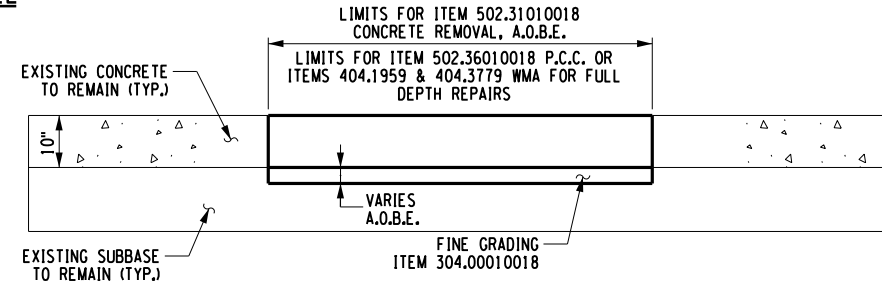
SECTION B-B  
N.T.S.  
(SEE DWG. NO. MD-2 FOR SECTION LOCATIONS)



SECTION C-C  
N.T.S.  
(SEE DWG. NO. MD-2 FOR SECTION LOCATIONS)



UNDERCUTTING DETAIL WITH GEOTEXTILE  
N.T.S.



SUBBASE FINE GRADING DETAIL  
N.T.S.

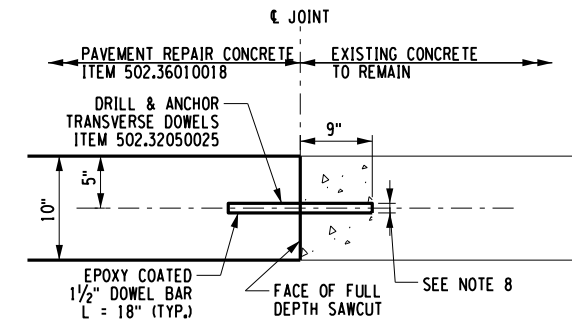
CLEAN AND FILL ALL EXISTING LONGITUDINAL AND TRANSVERSE JOINTS ON MAINLINE AND INTERCHANGE RAMPS. REFER TO NOTES "G" AND "J" FOR SAWING AND SEALING NEW LONGITUDINAL AND TRANSVERSE JOINTS IN ACCORDANCE WITH NYS DOT STANDARD SHEETS.

GENERAL NOTES:

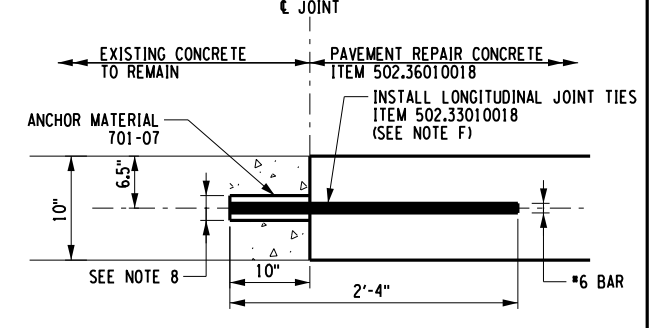
- SAW CUTTING - LAYOUT & SAWING:  
THE CONTRACTOR SHALL MARK OUT CLEARLY THE SAW CUT BOUNDARY LINE ACCORDING TO THE PAVEMENT REPAIR TABLES AND THE TYPICAL PAVEMENT REMOVAL PLANS SHOWN ON THIS SHEET.  
THE CONTRACTOR MAY MAKE FULL DEPTH SAW CUTS A MAXIMUM OF 24 HOURS IN ADVANCE OF THE REMOVAL OPERATION. PAVEMENT REMOVAL OPERATIONS SHALL BEGIN 24 HOURS AFTER THE FULL DEPTH SAW CUT. ANY PAVEMENT DAMAGE RESULTING FROM FAILING TO BEGIN REMOVAL OPERATIONS AS SPECIFIED ABOVE SHALL BE REPAIRED AT NO COST TO THE AUTHORITY AND IN A MANNER ACCEPTABLE TO THE ENGINEER.
- DAMAGE AFTER SAWCUTTING:  
ONCE THE SAWCUT IS MADE, THE CONTRACTOR SHALL PROTECT THE SAW CUT EDGE FROM DAMAGE. DAMAGE TO THE SAW CUT EDGE CAUSED BY THE CONTRACTOR'S OPERATION, AS DETERMINED BY THE ENGINEER, SHALL REQUIRE THE CONTRACTOR TO RE-CUT THE BOUNDARY LINE AT A NEW LOCATION OR AS DETERMINED BY THE ENGINEER.  
THE BOUNDARY LINE SHALL BE RE-CUT 2 INCHES FROM THE DAMAGED EDGE. IF THE DAMAGE CAN NOT BE REMOVED BY RE-CUTTING THE BOUNDARY LINE 2 INCHES AWAY, THE ENGINEER WILL DETERMINE THE LOCATION OF THE DAMAGE REMOVAL LIMITS. ALL ADDITIONAL WORK NEEDED TO REPAIR DAMAGED AREAS SHALL BE COMPLETED BY THE CONTRACTOR AT NO COST TO THE AUTHORITY.  
THE CONTRACTOR SHALL NOTE THAT THE ENGINEER MAY BE REQUIRED TO SIGNIFICANTLY INCREASE THE SIZE OF PAVEMENT REPAIR LIMITS IF DAMAGE OCCURS, TO MAINTAIN SIMILAR PAVEMENT SLAB SIZES.
- PAVEMENT LIFT-OUT METHOD:  
THE CONTRACTOR SHALL REMOVE THE PAVEMENT SLABS USING THE CONCRETE PAVEMENT LIFT-OUT METHOD. THE CONTRACTOR SHALL NOT USE PAVEMENT BREAKERS OR BREAKING METHODS TO REMOVE PAVEMENT SLABS.  
THE CONTRACTOR SHALL MAKE ADDITIONAL SAW CUTS AS NEEDED TO SAFELY REMOVE THE PAVEMENT SLAB. NO OVER SAWING SHALL BE ALLOWED FOR THESE SAW CUTS. THE CONTRACTOR SHALL MAKE ADDITIONAL CUTS AS DIRECTED BY THE ENGINEER. IF THE ENGINEER DETERMINES THAT REMOVAL OPERATIONS NEED TO BE SAFER OR IF THE SUBBASE MATERIALS ARE DISTURBED TO A POINT THAT REPAIR IS NEEDED, THE COST OF THESE ADDITIONAL SAW CUTS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 502.31010018 - FULL DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT.
- IF THE EXISTING CRACK IS LESS THAN 6 FEET FROM AN EXISTING TRANSVERSE JOINT, EXTEND THE PROPOSED REPAIR BEYOND THE TRANSVERSE JOINT BY A MINIMUM OF 12 INCHES.
- LONGITUDINAL JOINT TIES, ITEM 502.33010018, SHALL BE USED ON SLAB REPAIRS WHERE TRANSVERSE JOINTS LINE UP WITH ADJACENT SLABS ONLY.
- THE DETAILS SHOWN ON THIS SHEET ARE TYPICAL FOR ALL MULTI-LANE SECTIONS.
- IF THE MINIMUM PAVEMENT REMOVAL WIDTH FALLS WITHIN 6 FEET OF AN EXISTING TRANSVERSE JOINT, EXTEND THE PROPOSED REPAIR TO THE JOINT.
- THE HOLE DIAMETERS SHALL BE IN ACCORDANCE WITH THE ANCHORING MATERIAL MANUFACTURER'S WRITTEN RECOMMENDATIONS. PROVIDE THOSE RECOMMENDATIONS TO THE ENGINEER BEFORE DRILLING ANY HOLES.
- IN AREAS WHERE THE EXISTING SUBBASE IS DETERMINED BY THE ENGINEER TO BE UNSUITABLE, THE EXISTING SUBBASE SHALL BE UNDERCUT AND BACKFILLED AS SHOWN IN DETAIL.
- FULL DEPTH ASPHALT REPAIRS SHALL CONSIST OF 4" OF ITEM 404.1959 - BINDER COURSE ASPHALT AND 6" OF ITEM 404.3779 - BASE COURSE ASPHALT. TACK COAT SHALL BE APPLIED BETWEEN THE LIFTS AND ALONG THE VERTICAL EDGES OF THE SURROUNDING CONCRETE PAVEMENT PANELS TO REMAIN.

JOINT NOTES:

- ALL JOINTS CONSTRUCTED SHALL MATCH THE EXISTING JOINT LAYOUT AS PRACTICABLE AT THE LOCATIONS SHOWN ACCORDING TO REPAIR PLANS OR AS ORDERED BY THE ENGINEER. ADDITION OF JOINTS OTHER THAN AS OUTLINED ABOVE WILL NOT BE ALLOWED.
- ALL JOINT SAWCUTS SHALL BE PERPENDICULAR TO THE PAVEMENT SURFACE AND PARALLEL TO THE EXISTING JOINT PLAN. ANY SAWCUTS NOT CONFORMING, AS DETERMINED BY THE ENGINEER, TO THE DETAILS AND NOTES IN THESE PLANS WILL BE REJECTED BY THE ENGINEER. NO PAYMENT WILL BE MADE FOR REJECTED SAWCUTS.
- IN AREAS OF MULTIPLE SLABS THAT ARE BEING PLACED IN ONE POUR, IF IT IS NECESSARY TO CONSTRUCT TRANSVERSE OR LONGITUDINAL JOINTS, AS SHOWN IN THE DETAILS, THE COST FOR THE INITIAL SAWCUT SHALL BE INCLUDED IN THE LF COST BID FOR JOINT CONSTRUCTION.
- THE FINAL SAWCUT SHALL BE MADE AT ALL JOINT LOCATIONS TO THE SIZE SHOWN IN THE DETAIL AND THEN SEALED AND PAID FOR UNDER ITEMS 502.90010018 & 502.9310.
- THE CONTRACTOR SHALL HAVE THE RESPONSIBILITY FOR LOCATING AND ASCERTAINING WHEN TO MAKE THE REQUIRED SAW CUTS BASED ON THE CONCRETE USED TO MAKE THESE REPAIRS. ANY CRACKING OR OTHER DEFECTS IN THE CONCRETE REPAIR, AS DETERMINED BY THE ENGINEER, WILL RESULT IN THE REJECTION OF THE PAVEMENT REPAIR. REPLACEMENT OF REJECTED REPAIRS SHALL BE MADE ACCORDING TO THE SPECIFICATIONS.
- LONGITUDINAL JOINT TIES - SEE DETAIL "B" THIS DWG.  
LONGITUDINAL JOINT TIES SHALL BE USED ONLY ON PAVEMENT REPAIRS WITH FULL LENGTH SLABS, WHEN NEW TRANSVERSE JOINTS MATCH EXISTING.  
WHEN ALLOWED BY THE ENGINEER, HAND-HELD DRILLS MAY BE USED IF THE DRILL SPECIFIED IN ITEM 502.33010018 CANNOT BE POSITIONED WITHOUT BEING IN CONFLICT WITH THE WORK ZONE TRAFFIC CONTROL.  
ANCHOR MATERIAL MEETING THE REQUIREMENTS OF STANDARD SPECIFICATIONS SECTION 701-07 SHALL BE USED TO HOLD THE BAR IN PLACE.
- CONSTRUCTING NEW LONGITUDINAL JOINTS  
LONGITUDINAL JOINT TIES SHALL BE USED WHEN PAVEMENT REPAIRS ARE TO BE COMPLETED FOR MORE THAN ONE LANE OF THE PAVEMENT AND THE LANE OF THE PAVEMENT AND THE REPAIRS HAVE THE SAME TRANSVERSE JOINT LOCATION.  
NEW LONGITUDINAL JOINTS SHALL BE CONSTRUCTED USING ITEM 502.38010018 - LONGITUDINAL JOINTS. SEE NYS DOT STANDARD SHEETS 502-03, 502-04 AND 502-06 FOR DETAILS.
- LONGITUDINAL JOINT TIES - GENERAL  
THE LONGITUDINAL JOINT TIES SHALL MEET THE REQUIREMENTS OF ITEM 502.38010018 - LONGITUDINAL JOINTS. PLACEMENT AND SPACING OF THE JOINT TIES SHALL CONFORM TO THE STANDARD SPECIFICATION. THE UNIT PRICE BID FOR THIS ITEM SHALL INCLUDE THE COST OF ALL WORK TO COMPLETE THE REPAIRS, AS DETAILED IN THESE PLANS.
- LONGITUDINAL JOINT - BOND BREAKER TREATMENT  
THE CONTRACTOR SHALL APPLY A CONCRETE BOND BREAKER CONFORMING TO SECTION 711-05 OF THE STANDARD SPECIFICATIONS, OR OTHER SIMILAR MATERIAL APPROVED BY THE ENGINEER. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR CONCRETE PAVEMENT REPAIRS, FULL DEPTH.
- CONSTRUCTING NEW TRANSVERSE JOINTS  
WHEN MULTIPLE SLABS ARE BEING REPLACED FOR ONE LANE, DRILL AND ANCHOR TRANSVERSE DOWEL BARS TO TIE INTO THE EXISTING CONCRETE AT THE BEGINNING AND END, ITEM 502.32050025. THE TRANSVERSE JOINTS BETWEEN SLABS AND MID SPAN JOINTS SHALL BE CONSTRUCTED USING HINGE AND TRANSVERSE JOINTS, ITEMS 502.34010018 & 502.37010018, RESPECTIVELY. SEE NYS DOT STANDARD SHEETS 502-05 & 502-06 FOR DETAILS. NEW JOINTS SHALL BE FILLED USING ITEM 502.90010018.



DETAIL "A"  
DRILL & ANCHOR TRANSVERSE DOWELS N.T.S.



DETAIL "B"  
DRILL & ANCHOR LONGITUDINAL JOINT TIES N.T.S.

ALTERED ON:  
SIGNATURE:  
STAMP:

AFFIXED ON:  
5/10/2024  
SIGNATURE:  
STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY  
Thruway Authority  
2025  
95

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CAST-IN-PLACE FULL DEPTH REPAIR DETAILS	DRAWING NUMBER: MD-1

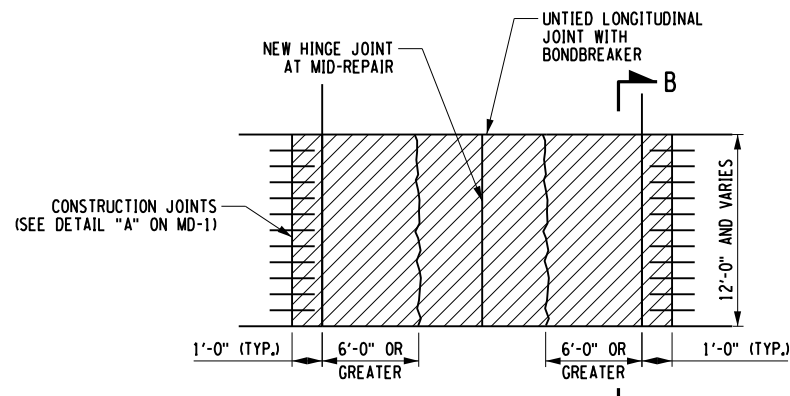
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

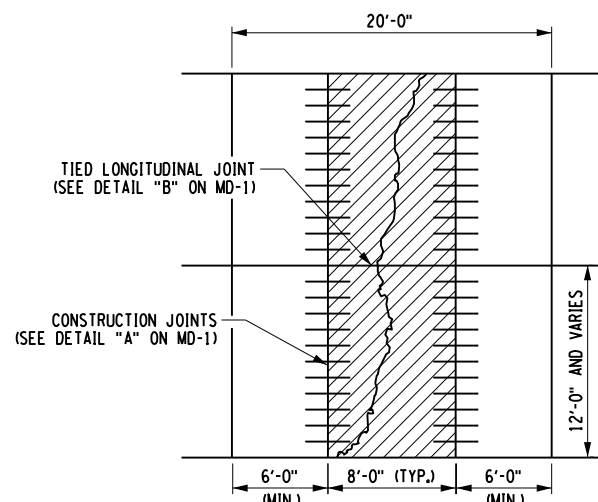
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DESIGNED BY: P. AMICONE

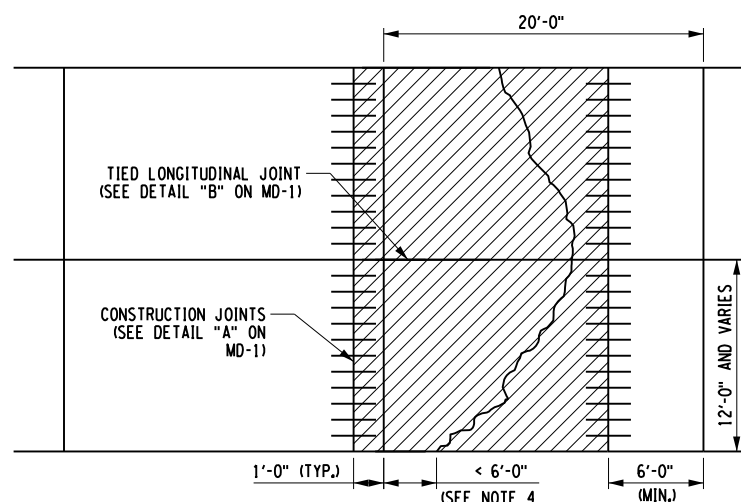
DESIGN SUPERVISOR: D. MARSH



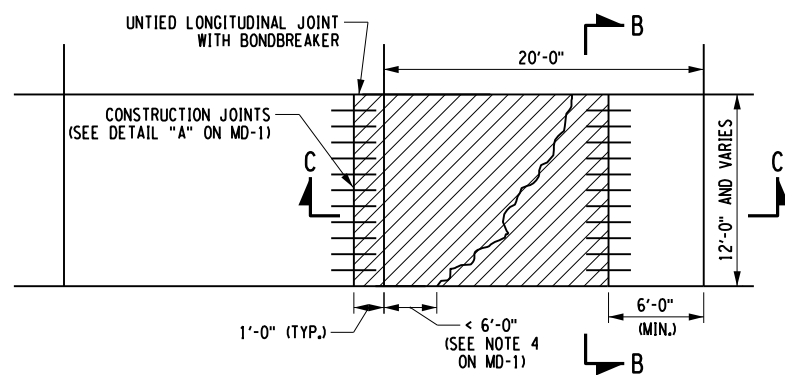
**PARTIAL WIDTH, FULL SLAB REPAIR, MULTIPLE CRACKS**  
N.T.S.



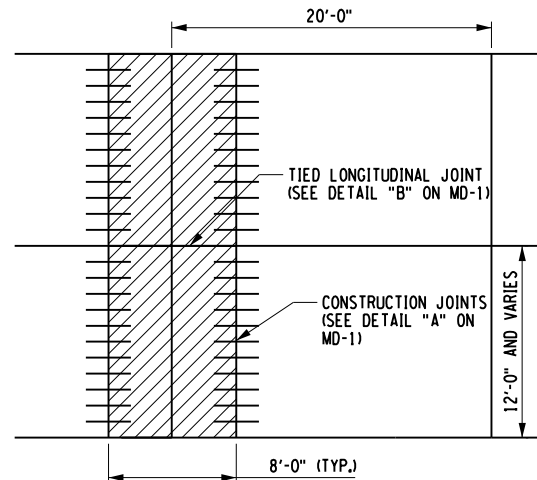
**FULL WIDTH, MID-SLAB REPAIR**  
N.T.S.



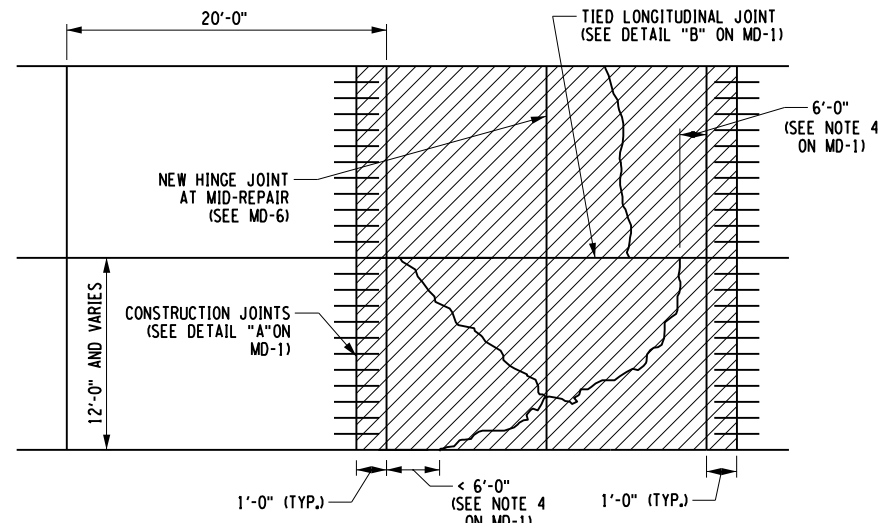
**FULL WIDTH, PARTIAL SLAB REPAIR**  
N.T.S.



**PARTIAL WIDTH, PARTIAL SLAB REPAIR**  
N.T.S.

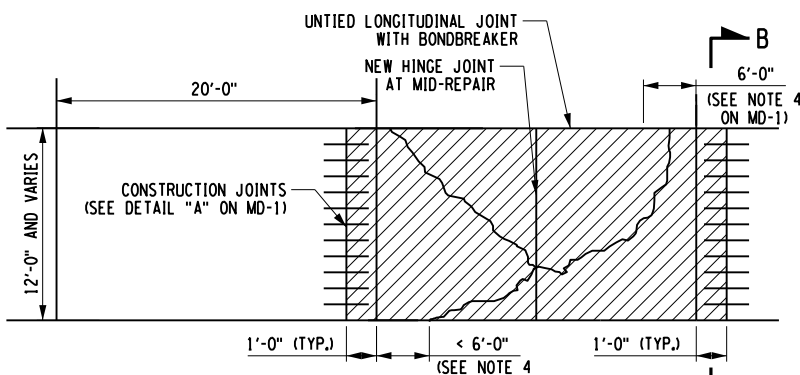


**FULL WIDTH, JOINT REPAIR**  
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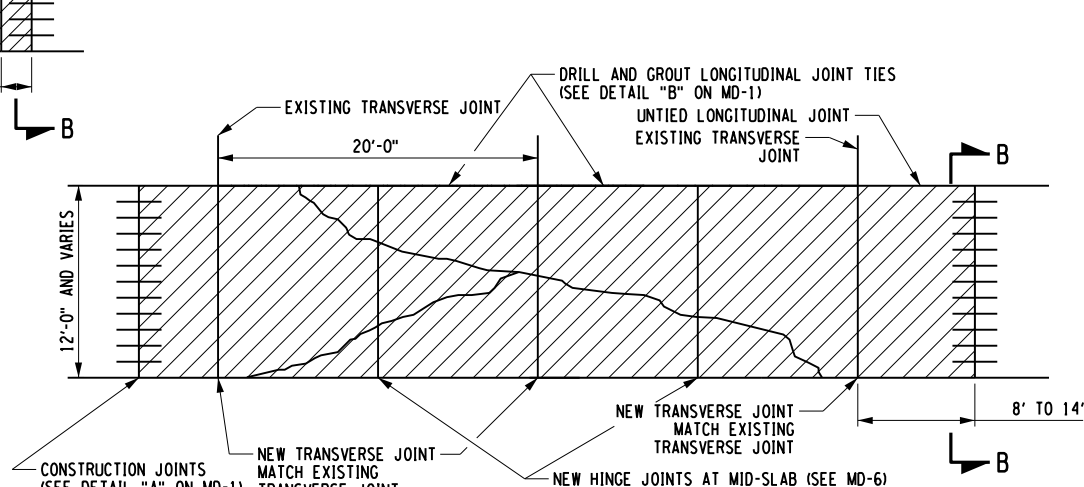


**FULL WIDTH, FULL SLAB REPAIR**  
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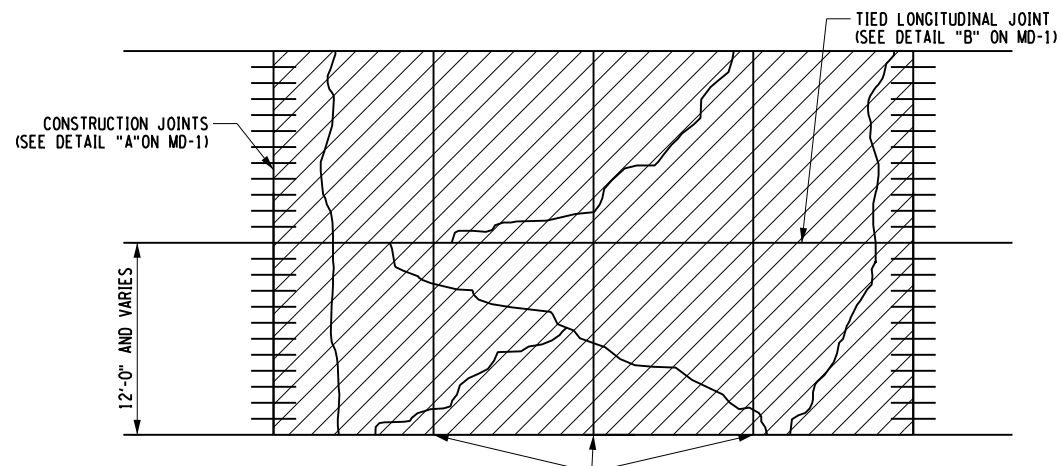
NOTE:  
THE FULL WIDTH DETAILS, SHOWN ON THIS SHEET FOR TWO (2) LANE SECTIONS, ARE TYPICAL FOR THREE (3) LANE SECTIONS.



**PARTIAL WIDTH, FULL SLAB REPAIR**  
N.T.S.



**PARTIAL WIDTH, MULTIPLE SLABS**  
N.T.S.



**FULL WIDTH, MULTIPLE SLABS**  
N.T.S.

ALTERED ON:	AFFIXED ON: 5/10/2024
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CAST-IN-PLACE FULL DEPTH REPAIR DETAILS	DRAWING NUMBER: MD-2	

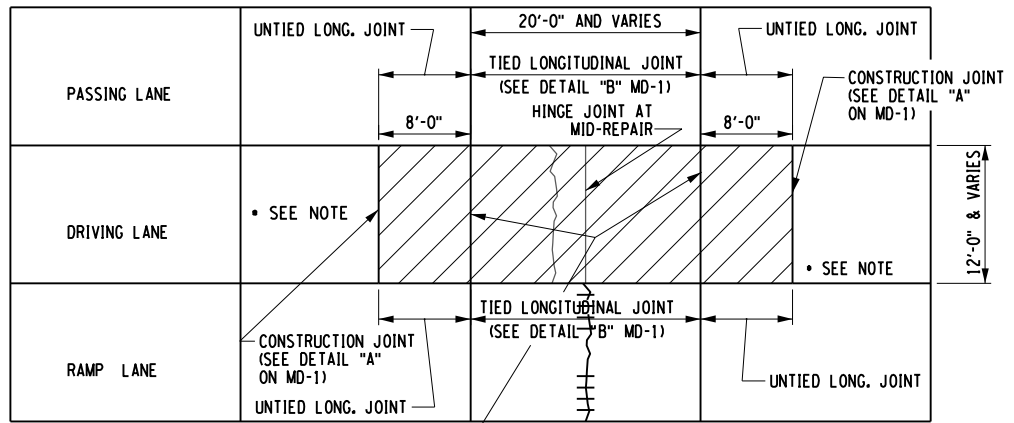
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

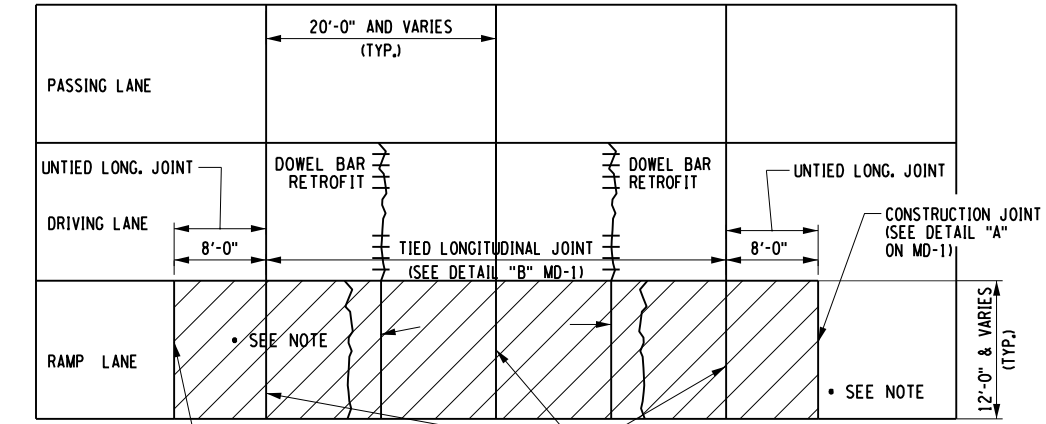
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DESIGNED BY: P. AMICONE

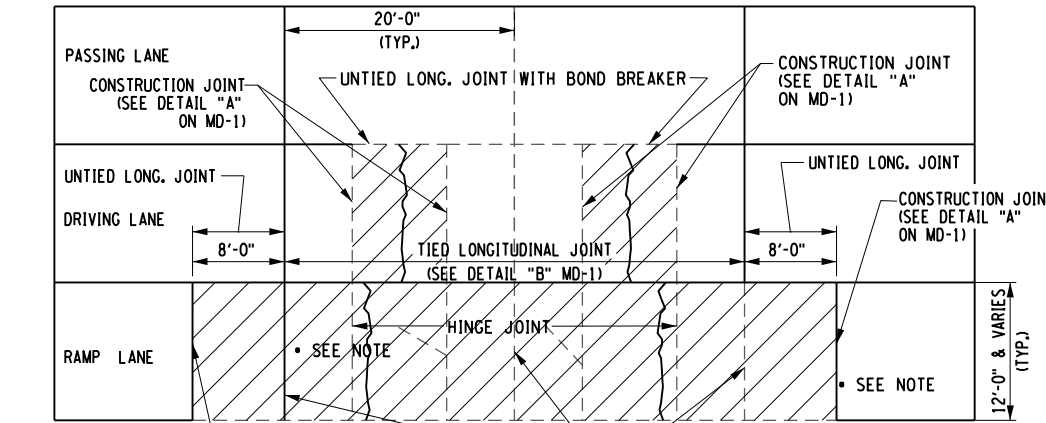
DESIGN SUPERVISOR: D. MARSH



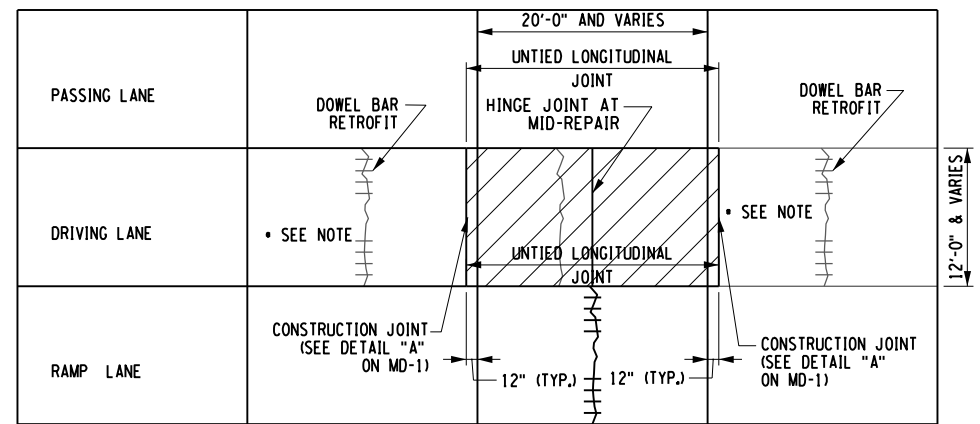
**DRIVING LANE-FULL DEPTH, FULL SLAB REPAIR (OPTION 1)**  
N.T.S.



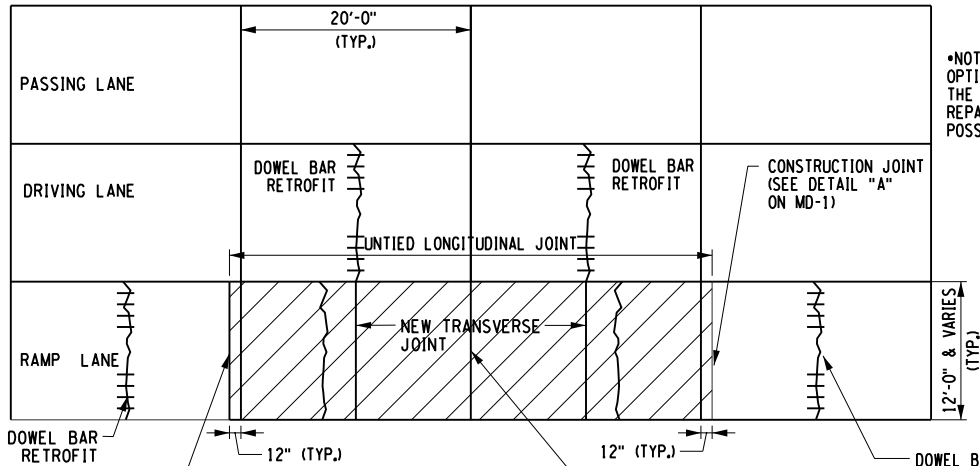
**RAMP/PASSING LANE-FULL DEPTH, FULL SLAB REPAIR (OPTION 1)**  
N.T.S.



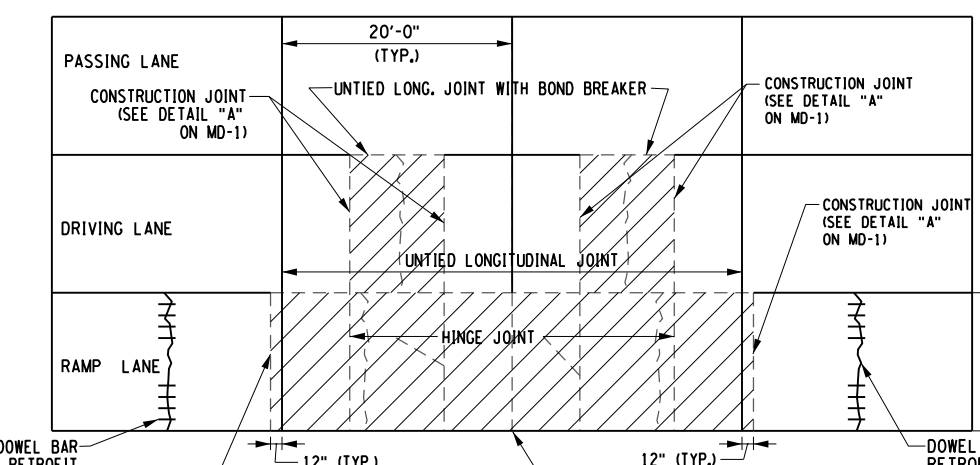
**RAMP/PASSING LANE-FULL DEPTH, FULL SLAB REPAIR W/ DRIVING LANE REPAIR (OPTION 1)**  
N.T.S.



**DRIVING LANE-FULL DEPTH, FULL SLAB REPAIR (OPTION 2)**  
N.T.S.



**RAMP/PASSING LANE-FULL DEPTH, FULL SLAB REPAIR (OPTION 2)**  
N.T.S.



**RAMP/PASSING LANE-FULL DEPTH, FULL SLAB REPAIR W/ DRIVING LANE REPAIR (OPTION 2)**  
N.T.S.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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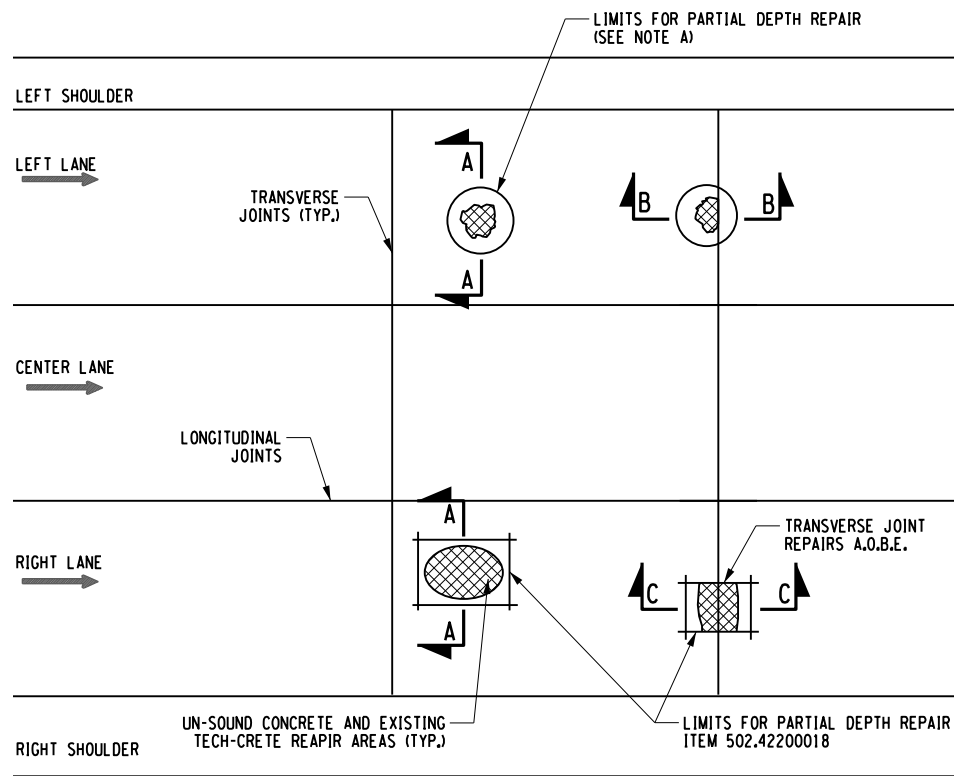
REVISIONS			
DATE	DESCRIPTION	BY	SYM.



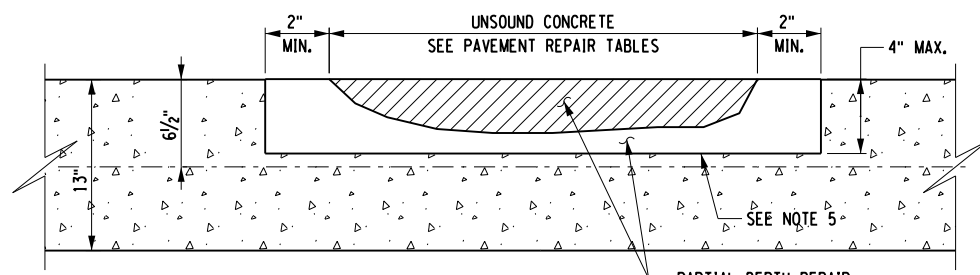
TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING THREE LANE CONCRETE FULL DEPTH SECTION REPAIR DETAILS	DRAWING NUMBER: MD-3

NOTES:

- THE CONTRACTOR SHALL NOTE THAT THE TYPICAL REMOVAL PLAN SHOWS COMMON PATTERNS AND NOT ALL FIELD CONDITIONS MAY BE SHOWN, THE ENGINEER WILL MAKE ANY CHANGES TO REPAIR SIZES AND TYPES AS NEEDED.
- OPTIONAL CONCRETE REMOVAL SAWING METHOD - LAYOUT & SAW CUTTING:  
THE CONTRACTOR SHALL MARK OUT CLEARLY THE SAW CUT BOUNDARY LINE ACCORDING TO THE PAVEMENT REPAIR TABLES AND THE TYPICAL PAVEMENT REMOVAL PLAN.  
IF, IN THE OPINION OF THE ENGINEER, THE REMOVAL MARK OUT BOUNDARY BECOMES EXCESSIVE, THEY WILL INFORM THE CONTRACTOR THAT THE REPAIR WILL BE MADE AS A FULL DEPTH REPAIR OR THE ENGINEER WILL INFORM THE CONTRACTOR THAT THE MARK OUT IS ACCEPTABLE AND THE CONCRETE REMOVAL CAN BEGIN.  
THE COST FOR ALL SAWCUTTING ALONG WITH THE REMOVAL AND DISPOSAL OF THE CONCRETE SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 502.42200018 CONCRETE REPAIR PARTIAL DEPTH.
- CONCRETE REMOVAL CORE METHOD - LAYOUT & CORING:  
THE CONTRACTOR SHALL USE A TEMPLATE TO CLEARLY MARK OUT THE CORING BOUNDARY LINE PRIOR TO STARTING THE REPAIR, THE LIMITS OF THE REPAIR BOUNDARY SHALL COMPLETELY EXTEND INTO SOUND CONCRETE. ANY JOINT OR CRACK SHALL BE POSITIONED SUCH THAT THE JOINT OR CRACK CROSSES THROUGH THE MIDDLE 1/3 OF THE REPAIR BOUNDARY. IF THE TEMPLATE CANNOT BE POSITIONED PROPERLY THE REPAIR AREA SHALL BE DETERMINED BY THE SAW CUT METHOD.
- DAMAGE AFTER SAWCUTTING:  
ONCE THE SAW CUT IS MADE THE CONTRACTOR SHALL PROTECT THE SAW CUT EDGE FROM DAMAGE. DAMAGE TO THE SAW CUT EDGE CAUSED BY THE CONTRACTOR'S OPERATIONS, AS DETERMINED BY THE ENGINEER, SHALL REQUIRE THE CONTRACTOR TO RE-CUT THE BOUNDARY LINE AT A NEW LOCATION AS DETERMINED BY THE ENGINEER.  
THE BOUNDARY LINE SHALL BE RE-CUT A MINIMUM OF 2" FROM THE DAMAGED EDGE. THE BOUNDARY LINE RE-CUT SHALL REMOVE ALL OVER SAWING MADE TO MAKE THE REPAIR. ALL ADDITIONAL WORK NEEDED TO REMOVE DAMAGED AREAS SHALL BE COMPLETED BY THE CONTRACTOR AT NO COST TO THE AUTHORITY.
- REPAIR BOTTOM:  
USING CHIPPING HAMMERS, THE CONTRACTOR SHALL LEVEL THE BOTTOM OF THE REPAIR AREA TO OBTAIN AN EVEN THICKNESS FOR THE CONCRETE REPAIR MATERIAL. ALL CONCRETE REMOVAL SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER.
- OVER SAWING OF REPAIR:  
THE CONTRACTOR SHALL OVER SAW THE PARTIAL REPAIR LIMITS TO COMPLETE THE REMOVAL OF CONCRETE. THE OVER SAWED AREAS SHALL BE CLEANED ACCORDING TO THE REQUIREMENTS FOR ITEM 502.42200018 PCC PARTIAL DEPTH REPAIRS. THE OVER SAWED AREAS SHALL BE FILLED WITH ANCHORING MATERIAL MEETING THE REQUIREMENTS OF SECTION 701-07 OF THE STANDARD SPECIFICATION. THE COST TO COMPLETE THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 502.42200018.
- THE DETAILS SHOWN ON THIS SHEET ARE TYPICAL IN THE THREE (3) LANE SECTION. THESE DETAILS ALSO APPLY IN THE MULTI-LANE SECTION IN THE AREA OF THE TOLL BOOTHS.
- AT NO TIME SHALL THE CONTRACTOR ALLOW PATCHING MATERIAL TO FLOW INTO OR CROSS A CRACK. CONTRACTOR SHALL NOT ALLOW PATCHING MATERIAL TO FLOW INTO SHOULDER AREAS.

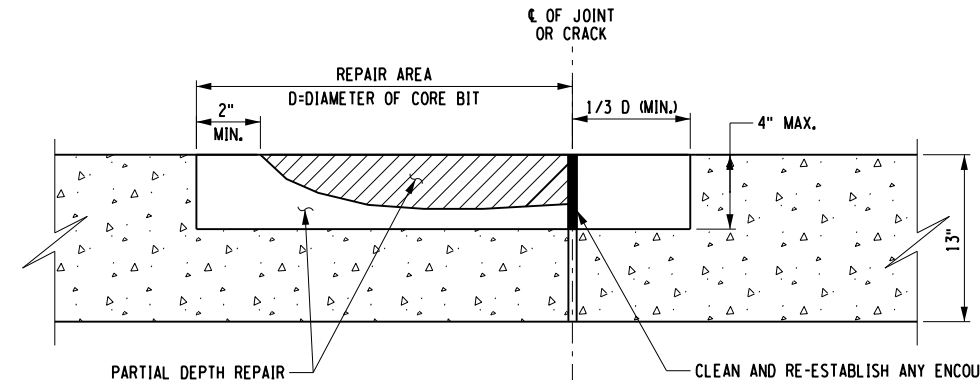


TYPICAL REMOVAL PLAN  
N.T.S.



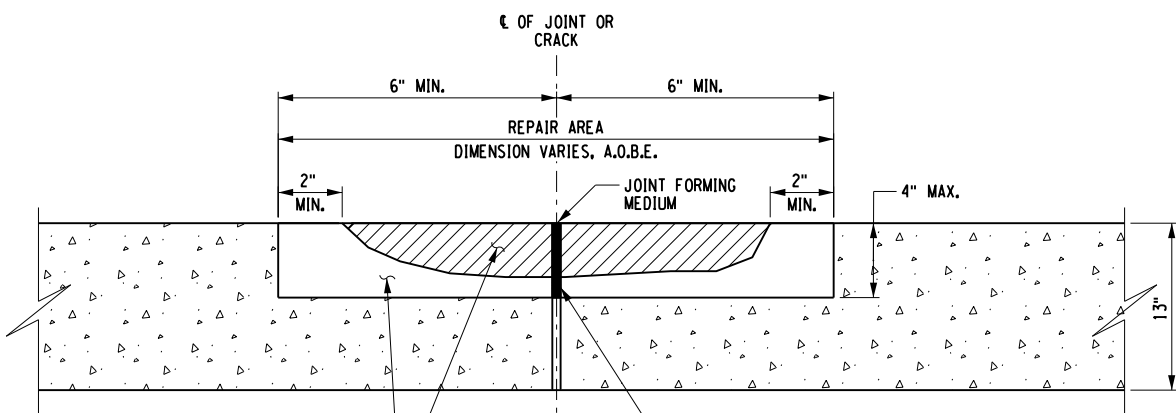
SECTION A-A  
N.T.S.

NOTE:  
JOINTS AT OR CROSSING A PARTIAL DEPTH PATCH MUST BE REFORMED WITH A PRE-MOLDED RESILIENT JOINT FILLER. THE JOINT MATERIAL SHALL MATCH THE WIDTH OF THE EXISTING JOINT AND EXTEND 1" BELOW AND BEYOND THE PATCH AREA.



SECTION B-B  
N.T.S.

CLEAN AND RE-ESTABLISH ANY ENCOUNTERED TRANSVERSE OR LONGITUDINAL JOINTS OR MIDSLAB CRACKS. PAYMENT SHALL BE MADE UNDER ITEM 502.42200018 (SEE NOTE A). JOINTS SHALL BE FILLED USING ITEM 502.90010018.



SECTION C-C  
N.T.S.

CLEAN AND RE-ESTABLISH ANY ENCOUNTERED TRANSVERSE OR LONGITUDINAL JOINTS OR MIDSLAB CRACKS. PAYMENT SHALL BE MADE UNDER ITEM 502.42200018. JOINTS SHALL BE FILLED USING ITEM 502.90010018.

CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING PARTIAL DEPTH REPAIR DETAILS	DRAWING NUMBER: MD-4

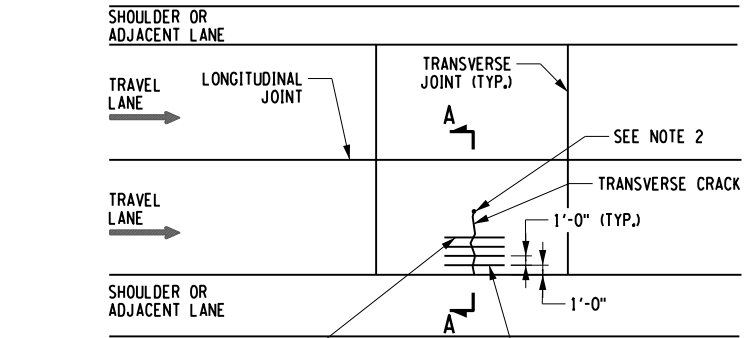
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DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

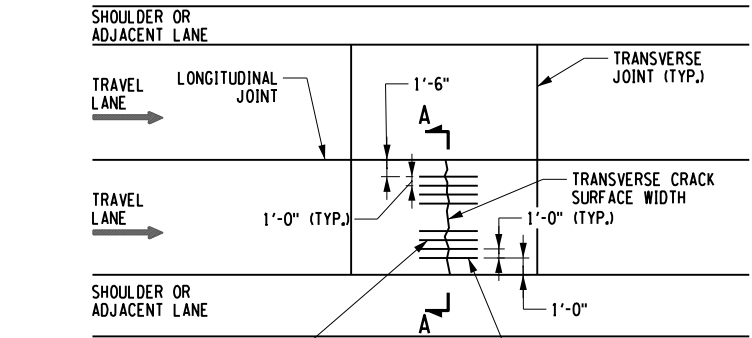
DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH



RETRO-FIT EPOXY COATED DOWEL BAR IN PCC PAVEMENT ITEM 502.70010018 (TYP.)

**PARTIAL SLAB REPAIR**  
N.T.S.

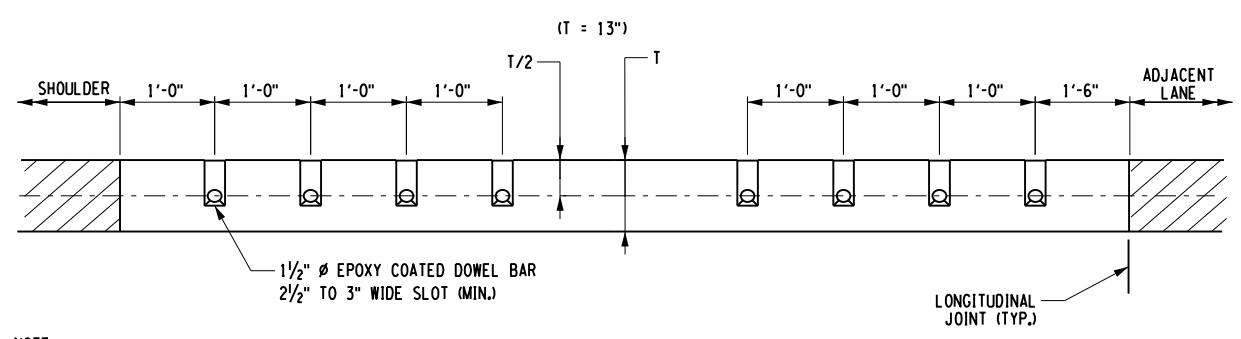


RETRO-FIT EPOXY COATED DOWEL BAR IN PCC PAVEMENT ITEM 502.70010018 (TYP.)

**FULL SLAB - CENTER REPAIR**  
N.T.S.

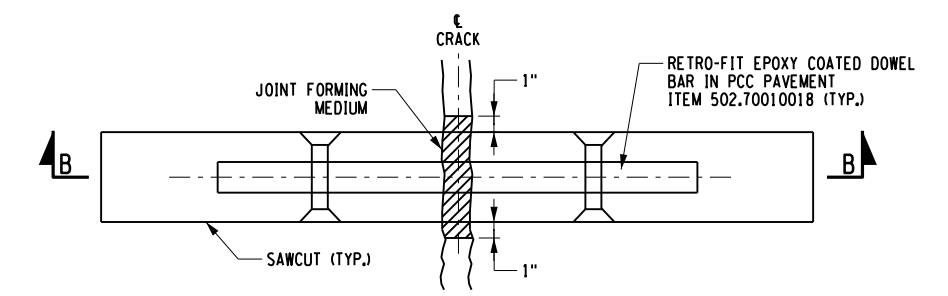
NOTES:

1. THE CONTRACTOR SHALL RETRO-FIT CRACKS ACCORDING TO THE PAVEMENT REPAIR TABLES OR AS DIRECTED BY THE ENGINEER. PRIOR TO SAWCUTTING, THE CONTRACTOR SHALL MARK OUT THE BAR LOCATIONS ACCORDING TO THE DETAILS SHOWN ON THIS SHEET. THE CONTRACTOR MAY BEGIN SAWCUTTING AFTER ACCEPTANCE OF THE MARK OUT BY THE ENGINEER.
2. WHEN RETRO-FITTING CRACKS THAT TERMINATE WITHIN A SLAB AND NOT AT AN EXISTING JOINT OR EDGE OF SLAB, THE CONTRACTOR SHALL DRILL A HOLE 25% LARGER THAN THE CRACK WIDTH, AS DETERMINED BY THE ENGINEER, FOR THE FULL DEPTH OF THE SLAB AND FILL THIS HOLE WITH THE SAME ANCHORING MATERIAL (701-07) BEING USED FOR OTHER WORK ON THE PROJECT.  
  
THE UNIT PRICE BID FOR ITEM 502.70010018 RETRO-FIT DOWELS IN PCC PAVEMENT SHALL INCLUDE THE COST TO DRILL AND FILL THIS HOLE WITH 701-07 ANCHORING MATERIAL AT ANY CRACK LOCATION AS DESCRIBED ABOVE OR WHERE ORDERED BY THE ENGINEER.
3. ANY RETRO-FIT INSTALLATION THAT DOES NOT CONFORM TO THE DETAILS OR THE SPECIFICATION SHALL BE REJECTED AND THE CONTRACTOR SHALL REPLACE THE REJECTED RETRO-FIT AS DIRECTED BY THE ENGINEER. NO PAYMENT WILL BE MADE FOR ANY LOCATION REJECTED BY THE ENGINEER.
4. CLEAN AND FILL CRACKS THAT ARE EQUAL TO OR GREATER THAN 1/4".

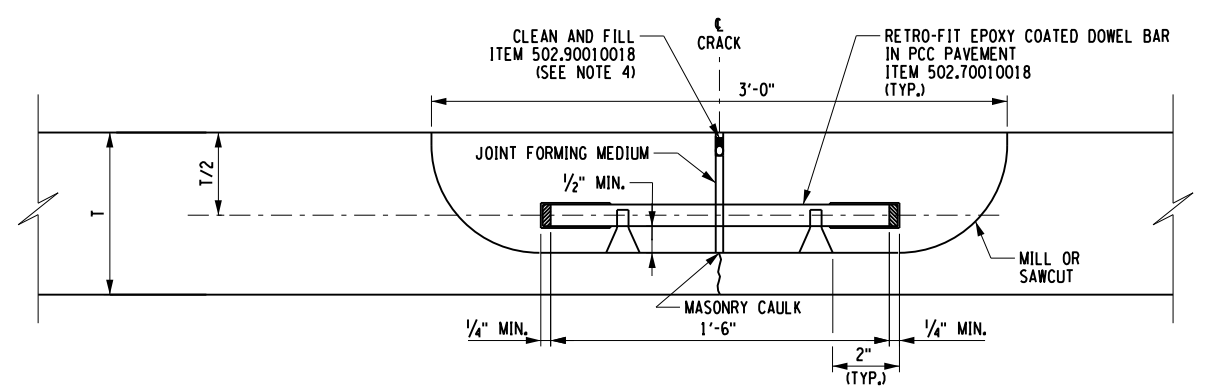


NOTE:  
FOR PARTIAL SLAB REPAIRS EPOXY COATED DOWELS SHALL BE LOCATED 1'-0" O.C. FOR THE ENTIRE LENGTH OF THE CRACK OR AS ORDERED BY THE ENGINEER.

**SECTION A-A**  
N.T.S.



**DOWEL BAR DETAIL**  
N.T.S.



**SECTION B-B**  
N.T.S.

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY

**Thruway Authority**

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TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING DOWEL BAR RETROFIT DETAILS	DRAWING NUMBER: MD-5

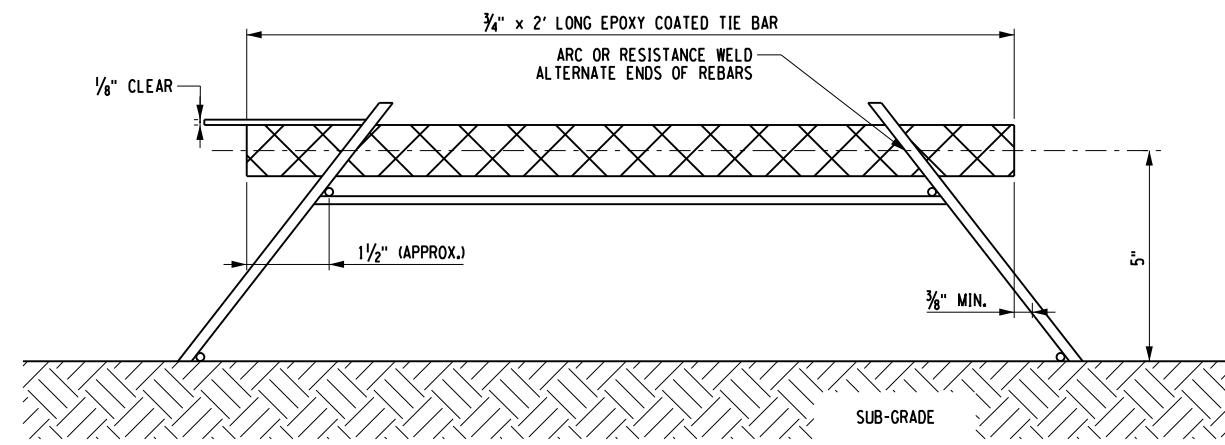
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

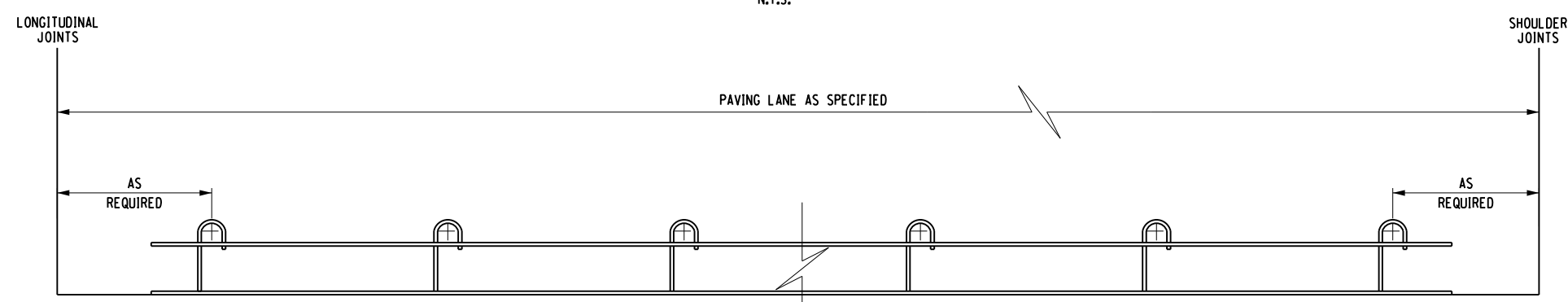
CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

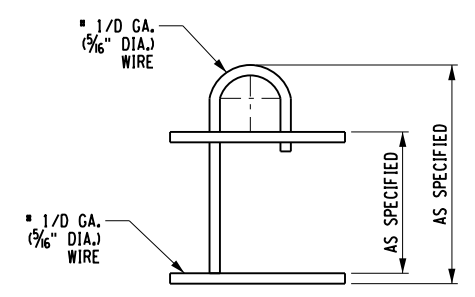


SECTION A-A  
N.T.S.

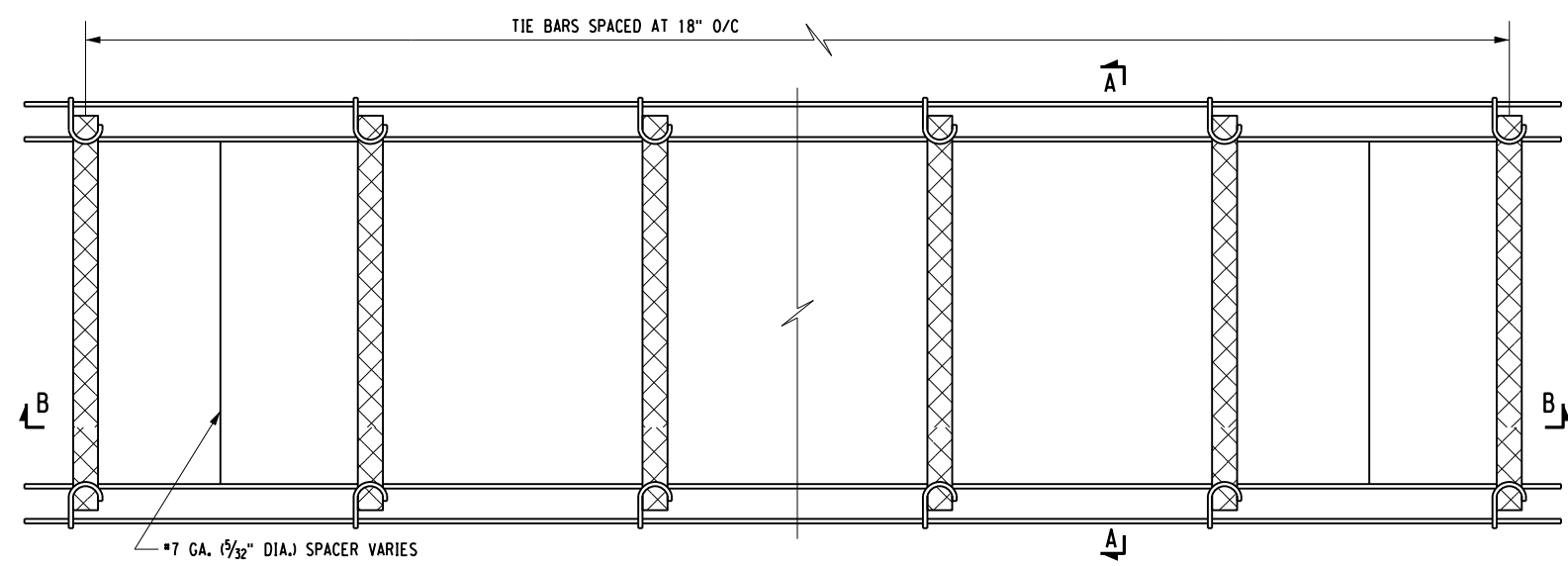


SECTION B-B  
N.T.S.

- NOTES:
1. DEFORMED BILLET STEEL SHALL CONFORM TO ASTM A-615 GRADE 60.
  2. USE EPOXY COATED DOWEL BARS.
  3. REBAR ARE TO BE SAW CUT OR SHEAR CUT.
  4. WIRE SIZES SHOWN ARE MINIMUM REQUIRED.
  5. ALL WIRED INTERSECTIONS ARE TO BE RESISTANCE WELDED.
  6. TOLERANCES:
    - A. 1/8" UNLESS OTHERWISE SPECIFIED
    - B. CENTERLINE OF INDIVIDUAL DOWELS SHALL BE PARALLEL TO SUBGRADE AND ALL OTHER DOWELS IN ASSEMBLY WITHIN 1/8".
  7. COST TO INSTALL HINGE JOINTS SHALL BE PAID UNDER ITEM 502.34010018.



DETAIL J-LEG  
N.T.S.



PLAN VIEW  
N.T.S.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

**NEW YORK**  
STATE OF  
OPPORTUNITY.

**Thruway  
Authority**

2025 High Road Tower  
Suite 1000, WI 14203-3723  
516.452.4897 - www.gma-nys.com

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING HINGE JOINT DETAILS	DRAWING NUMBER: MD-6

CONTRACT NUMBER: TAN 24-12
DATE: MAY 2024
DRAWING NUMBER: MD-6



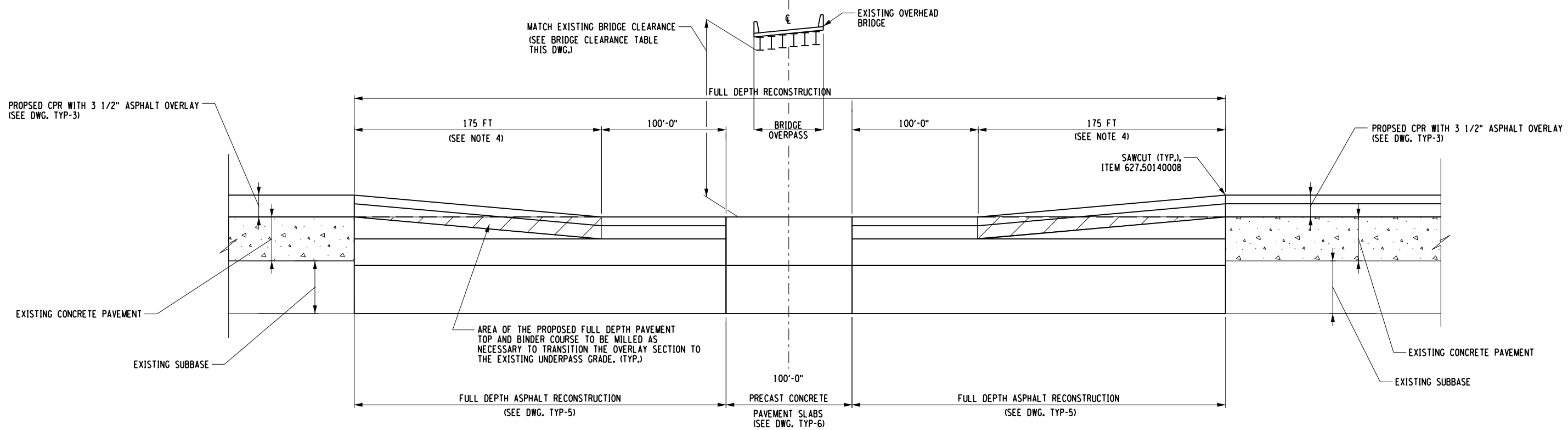
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

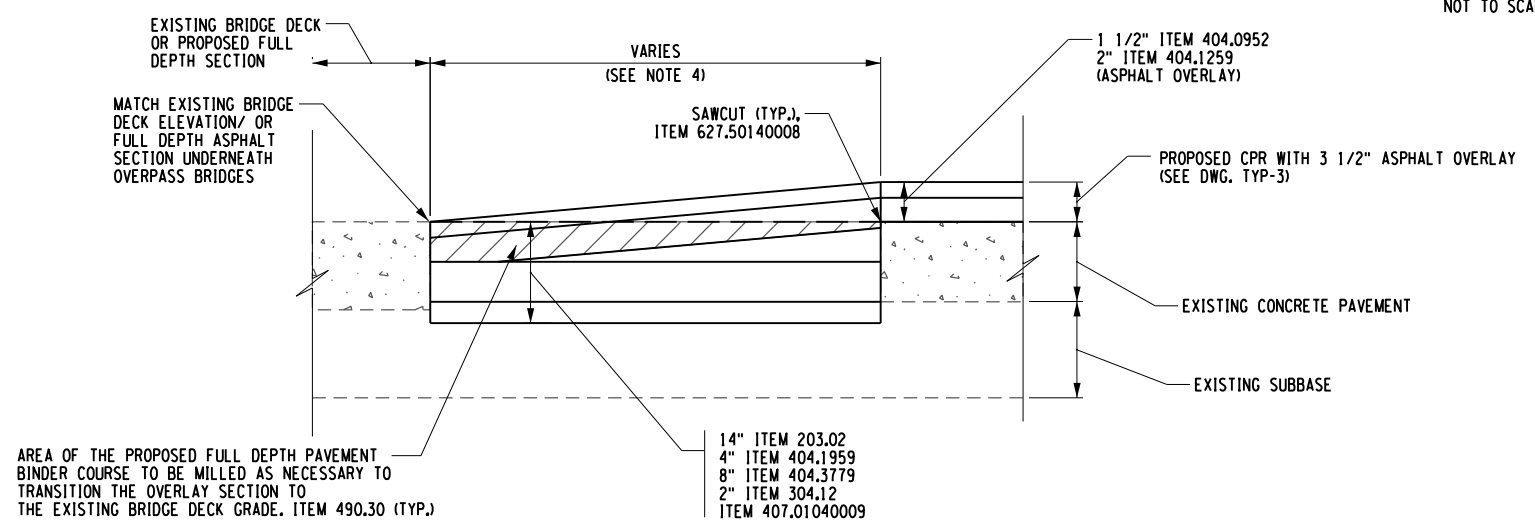
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DESIGNED BY: P. AMICONE

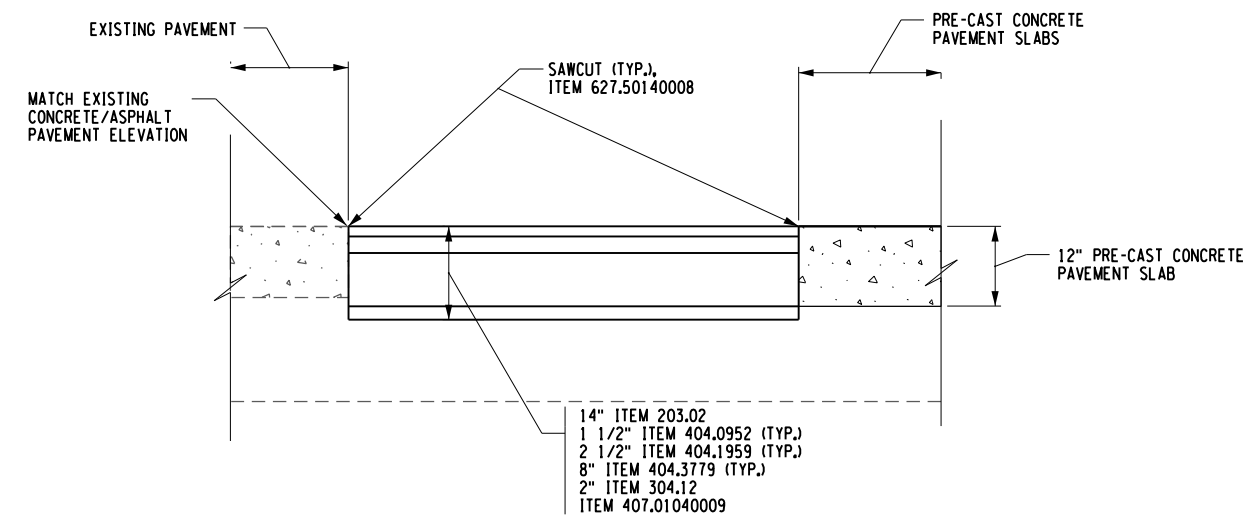
DESIGN SUPERVISOR: D. MARSH



**CPR WITH OVERLAY SECTION MATCHING UNDERNEATH BRIDGES WITH TRANSITION**  
NOT TO SCALE



**CPR WITH OVERLAY SECTION MATCHING EXISTING BRIDGE DECK PAVEMENT/ OR FULL DEPTH PAVEMENT AT OVERPASS BRIDGES**  
(SEE DWG. TYP-5 & "PL" DWGS. FOR LOCATIONS)  
NOT TO SCALE



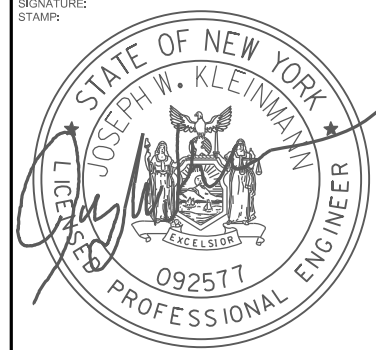
**MATCHING FULL-DEPTH RECONSTRUCTION SECTION TO EXISTING PAVEMENT AT PROJECT LIMITS**  
ML 37+82 TO ML 39+83  
ML 221+67 TO ML 226+89  
NOT TO SCALE

NOTES:

- SAWCUT, ITEM 627.50140008, SHALL BE REQUIRED AT BEGINNING AND ENDING LIMITS OF EACH FULL DEPTH RECONSTRUCTION TRANSITION, ALONG THE ASPHALT SHOULDER JOINT AND WHERE ORDERED BY THE ENGINEER IN CHARGE.
- PLACE NON-TRACKING TACK COAT ON ALL EXPOSED ASPHALT SURFACES THAT WILL BE IN CONTACT WITH NEW ASPHALT, ITEM 407.01040009.
- SAW CUTS SHALL BE MADE SO THAT SURFACE RUNOFF IS DIRECTED TO THE EDGE OF PAVEMENT.
- TRANSITION APPLIES TO TO THE FULL DEPTH RECONSTRUCTION LOCATIONS FOR THE THREE (3) BRIDGE OVERPASSES SECTIONS TRANSITIONING TO THE 3 1/2" OVERLAY SECTION AND THE TRANSITION FROM THE 3 1/2" OVERLAY TO THE EXISTING BRIDGE DECK GRADE. SEE "PL" DWGS. FOR TRANSITION LIMITS.

BRIDGE OVERPASS CLEARANCE TABLE		
MILE POST	BRIDGE OVER I-190	MIN. VERT. CLEARANCE
MP 900.70	SOUTH OGDEN	15'-1"
MP 901.06	WEISS ST	14'-6 3/4"
MP 904.13	LOUISIANA ST	14'-6 3/4"

ALTERED ON:	AFFIXED ON:	ITEM	DESCRIPTION	UNITS
	5/10/2024	203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY
		404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON
		404.1959	19 F9 BINDER COURSE ASPHALT, 50 SERIES COMPACTION	TON
		404.3779	37.5 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION	TON
		407.01040009	NON-TRACKING TACK COAT	GAL
		490.30	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	SY
		627.50140008	CUTTING PAVEMENT	LF



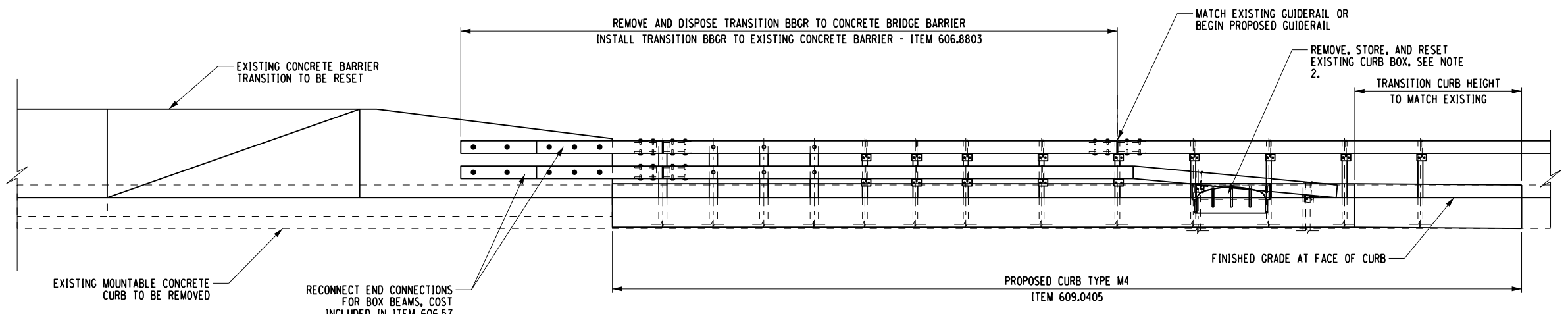
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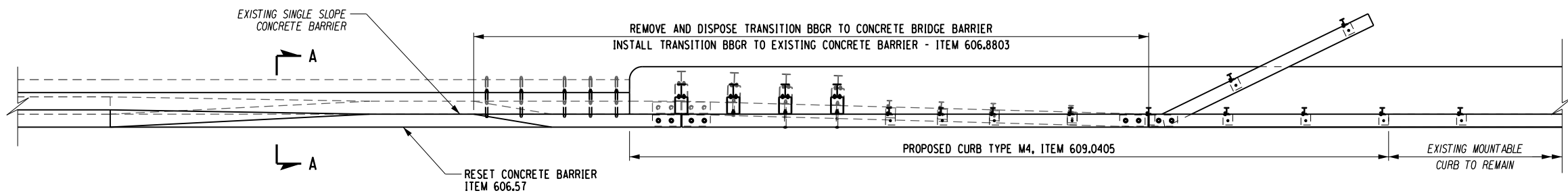


TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING MISCELLANEOUS DETAILS	DRAWING NUMBER: MD-7

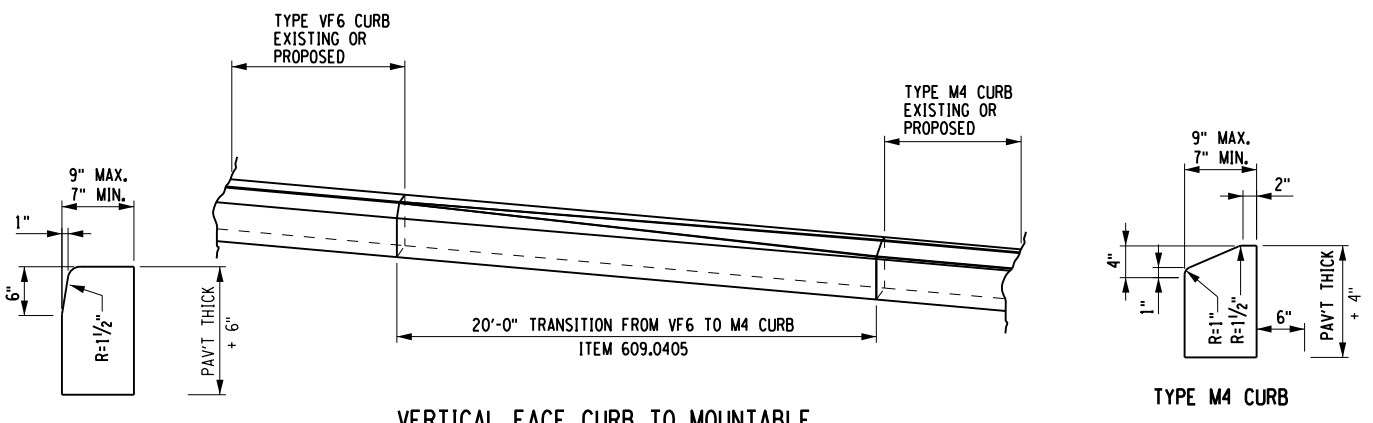
CHECKED BY: J. KLEINMANN  
 DRAFTED BY: P. AMICONE  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: P. AMICONE  
 DESIGN SUPERVISOR: D. MARSH



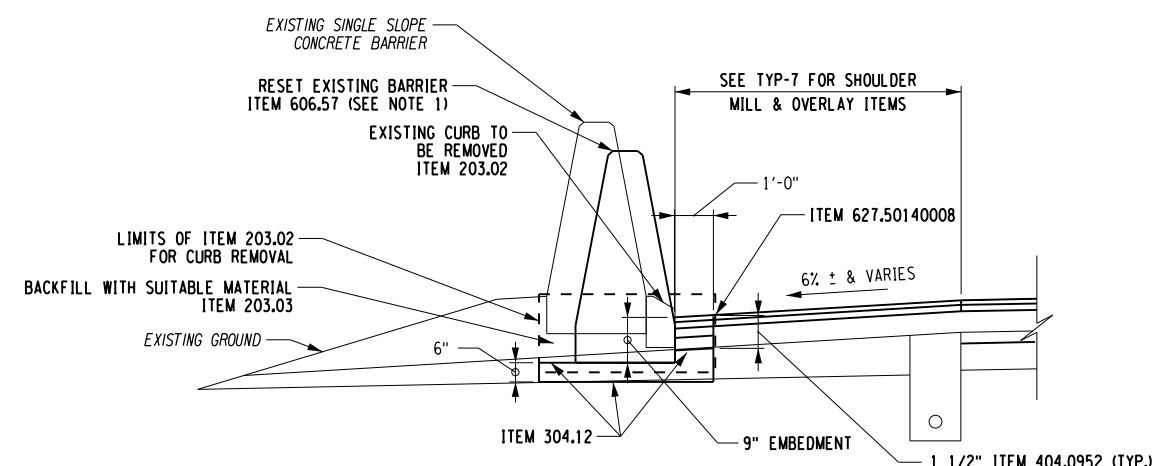
**SINGLE SLOPE CONCRETE BARRIER RESET**  
**ITEM 606.57**  
**ELEVATION**  
 N.T.S.



**SINGLE SLOPE CONCRETE BARRIER RESET**  
**ITEM 606.57**  
**PLAN**  
 N.T.S.



**VERTICAL FACE CURB TO MOUNTABLE CURB TRANSITION DETAIL**  
 N.T.S.



**SECTION A-A**  
**SINGLE SLOPE CONCRETE BARRIER RESET**  
 ML 143+97 TO ML 144+79  
 N.T.S.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

ITEM	DESCRIPTION	UNITS
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY
404.0952	9.5 F2 TOP COURSE ASPHALT, 50 SERIES COMPACTION	TON
404.1959	19 F9 BINDER COURSE ASPHALT, 50 SERIES COMPACTION	TON
404.3779	37.5 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
490.10	PRODUCTION COLD MILLING OF BITUMINOUS CONCRETE	SY
606.57	RESETTING PRECAST CONCRETE BARRIER	LF
609.0405	CAST-IN-PLACE CONCRETE CURB TYPE M4	LF
627.50140008	CUTTING PAVEMENT	LF

NOTES:

- EXISTING CONCRETE BARRIER SHALL BE REMOVED AND STORED TO ALLOW FOR EXCAVATION AND REMOVAL OF THE EXISTING CURB PRIOR TO BEING RESET. COST OF REMOVING, STORING AND RESETTING CONCRETE BARRIER TO BE INCLUDED IN ITEM 606.57.
- RESETTING OF CURB BOX PAID FOR UNDER ITEM 604.070504 FOR DS 19-10. SEE MT-9 FOR ADDITIONAL INFORMATION ON DS 19-10.
- SEE DRAWING TYP-8 FOR CURB INSTALLATION DETAILS.

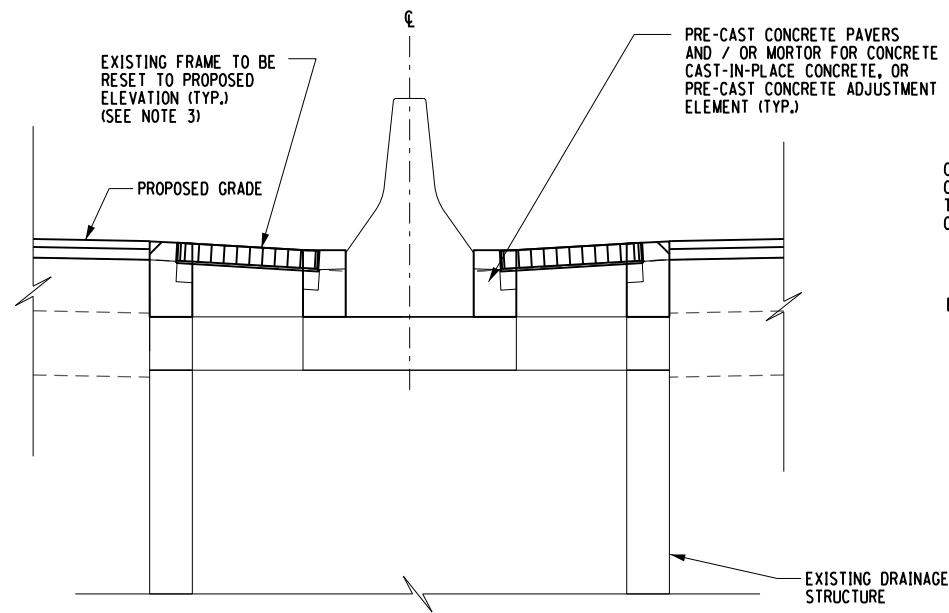
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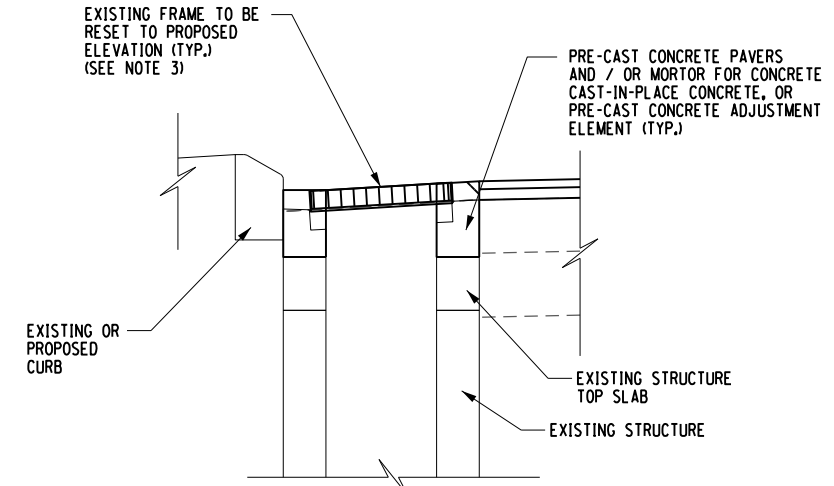
**Thruway Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING MISCELLANEOUS DETAILS	DRAWING NUMBER: MD-8

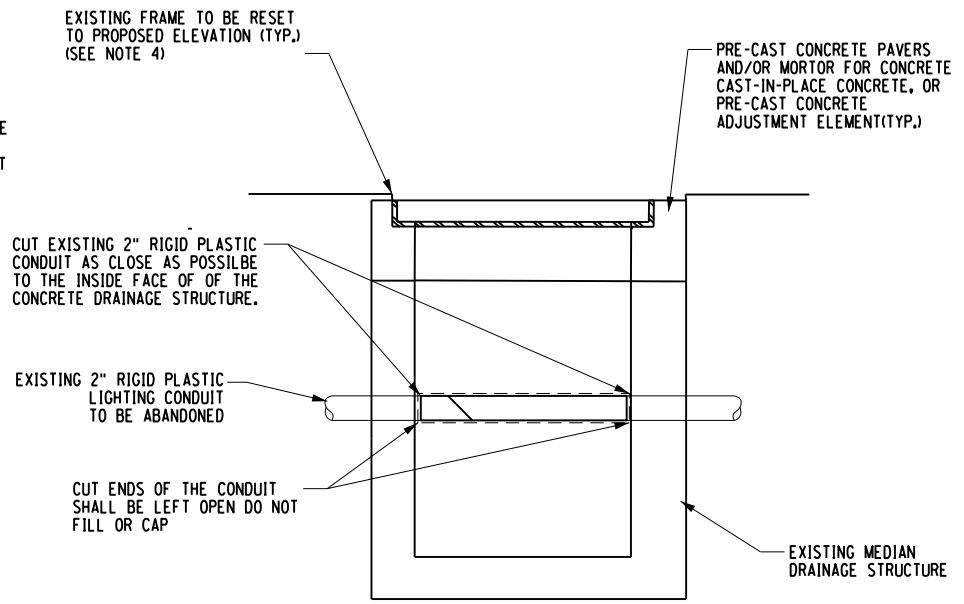
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 DRAFTED BY: P. AMICONE  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: P. AMICONE  
 DESIGN SUPERVISOR: D. MARSH



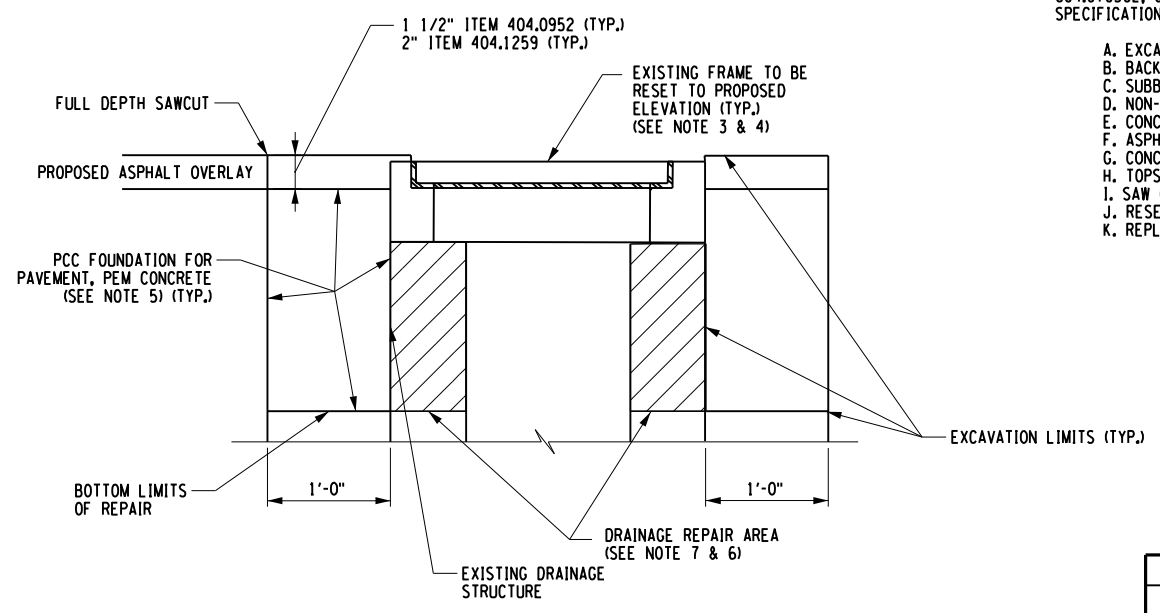
**DRAINAGE STRUCTURE ADJUSTMENT  
IN MEDIAN**  
ITEM 604.070501  
NOTE 10  
NTS



**DRAINAGE STRUCTURE ADJUSTMENT  
ALONG OUTSIDE SHOULDER**  
ITEM 604.070504  
NOTE 10  
NTS



**DRAINAGE STRUCTURE ALTER  
REMOVE EXISTING ELECTRICAL CONDUIT**  
ITEM 604.070503  
NOTE 10  
NTS



**DRAINAGE REPAIR DETAIL**  
ITEM 604.070502  
NOTE 10  
NTS

**NOTES:**

1. THE COST OF ALL DRAINAGE STRUCTURE ADJUSTMENTS TO FINAL GRADE, REINFORCEMENT BARS AND ANY NECESSARY DRILLING, GROUTING & CONCRETE SHALL BE INCLUDED IN THE PRICE BID FOR VARIOUS DRAINAGE ITEMS AND SHALL MEET THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS FOR 607-5.03 ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES. DRILLING & GROUTING TO MEET THE REQUIREMENTS OF ITEM 586.0201.
2. A MINIMUM OF 6" PRECAST CONCRETE PAVERS AND MORTAR FOR CONCRETE, OR MAXIMUM OF 2" OF MORTAR FOR CONCRETE ALONE SHALL BE USED FOR FRAME AND GREATER ADJUSTMENTS LESS THAN OR EQUAL TO 6". FOR ADJUSTMENTS GREATER THAN 6" UP TO AND INCLUDING 12", CAST-IN-PLACE CONCRETE OR A PRECAST CONCRETE ADJUSTMENT ELEMENT SHALL BE USED. A MAXIMUM OF 2" OF MORTAR FOR CONCRETE SHALL BE ALLOWED ON BOTH THE TOP AND BOTTOM OF THE PRECAST ELEMENTS.
3. ANY FRAME AND GRATE WITH HARDWARE MISSING OR DAMAGED FOR SECURING THE GRATE TO THE FRAME SHALL HAVE THAT HARDWARE REPLACED. THE COST FOR THE HARDWARE SHALL BE INCLUDED IN THE PRICE BID FOR FRAME AND GRATES.
4. UNLESS OTHERWISE SHOWN ON THE DRAINAGE STRUCTURE TABLES, IT IS ASSUMED THAT THE EXISTING FRAMES AND GRATES ARE SUITABLE FOR RESETTING AFTER REPAIR OR ALTERATION WORK IS COMPLETED. RESETTING OF EXISTING FRAMES AND GRATES SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 604.0705XX. IN CASES WHERE A NEW FRAME AND GRATE IS REQUIRED, ALTERATION OR REPAIR WORK WILL BE PAID FOR UNDER ITEM 604.0705XX AND THE NEW FRAME AND GRATE SHALL BE PAID FOR UNDER ITEM 655.0902. THE ENGINEER IN CHARGE SHALL BE THE FINAL AUTHORITY IN DETERMINING WHETHER OR NOT AN EXISTING FRAME AND GRATE IS SUITABLE FOR RESETTING.
5. EACH STRUCTURE SHALL BE EXCAVATED TO THE MINIMUM DEPTH REQUIRED TO REMOVE ALL OF THE EXISTING ASPHALT AND PORTLAND CEMENT CONCRETE PAVEMENT. THE CONTRACTOR SHALL BE ADVISED THAT EXCAVATION BELOW THIS ELEVATION MAY BE NECESSARY. REMOVE CONCRETE BLOCK, BRICK, OR PORTION OF PRECAST DRAINAGE STRUCTURE TO LIMITS OF DETERIORATION OR A.O.B.E. IF THE EXCAVATION IS LESS THAN 12" BELOW THE REQUIRED MINIMUM DEPTH, IT SHALL BE BACKFILLED WITH MATERIAL CONFORMING TO THE SPECIFICATIONS FOR ITEM 304.12 - SUBBASE COURSE TYPE II. FOR EXCAVATIONS GREATER THAN 12" BELOW THE REQUIRED MINIMUM DEPTH, THE TOP 12" SHALL BE BACKFILLED WITH MATERIAL CONFORMING TO THE SPECIFICATIONS FOR ITEM 304.12 AND THE REMAINING DEPTH SHALL BE BACKFILLED WITH MATERIAL CONFORMING TO THE SPECIFICATIONS FOR ITEM 503.1013 - PCC FOUNDATION FOR PAVEMENT, PEM CONCRETE. THE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 604.0705YY.
6. REPAIR DRAINAGE STRUCTURE WITH EITHER CAST-IN-PLACE CLASS A CONCRETE, PRECAST CONCRETE ADJUSTMENT UNITS, SPECIFICATION SECTION 706-04, AS SHOWN ON THE DETAILS, OR A.O.B.E. ANY MASONRY MORTAR USED SHALL CONFORM TO SPECIFICATION SECTION 705-21.
7. WHEN MULTIPLE COURSES OF PAVERS ARE USED FOR AN ALTERATION/REPAIR, THE STAGGERING OF THE JOINTS ON SUBSEQUENT COURSES IS REQUIRED.
8. THE CONTRACTOR SHALL PERFORM AN INITIAL INSPECTION OF EXISTING DRAINAGE STRUCTURES TO DETERMINE DEPTH OF REPAIR TO BE CONFIRMED BY THE EIC.
9. PROPOSED TOP OF GRATE ELEVATION IS LOCATED AT THE BACK, MIDDLE OF THE GRATE.
10. ALL WORK AS DESCRIBED ON THIS DETAIL AND AS SHOWN SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEMS 604.070501, 604.070502, 604.070503, AND 604.070504 FOR THE PARTICULAR WORK ITEMS INDICATED BELOW THE ASSOCIATED NYSOT SPECIFICATIONS: DESCRIPTION, MATERIALS, AND CONSTRUCTION DETAILS APPLY:

- A. EXCAVATION - PER ITEM 206.0201
- B. BACKFILL - PER ITEM 203.07
- C. SUBBASE - PER ITEM 304.12
- D. NON-TRACKING TACK COAT - PER ITEM 407.01040009
- E. CONCRETE CLASS 'C', CLASS 'A'
- F. ASPHALT BINDER - PER ITEM 404.1979
- G. CONCRETE SIDEWALK - PER ITEM 608.0101
- H. TOPSOIL, TURF ESTABLISHMENT PER ITEMS 610.1402, 610.1601
- I. SAW CUTTING - PER ITEM 627.50140008
- J. RESETTING FRAMES, GRATES, CURB BOXES AND INLET PLATES.
- K. REPLACE CURB PER ITEM 609.0401 OR ITEM 609.0405

ITEM	DESCRIPTION	UNITS
604.070501	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
604.070502	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - REPAIR EXISTING DRAINAGE STRUCTURE	EA
604.070503	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - REMOVE CONDUIT & PATCH WALL	EA
604.070504	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA

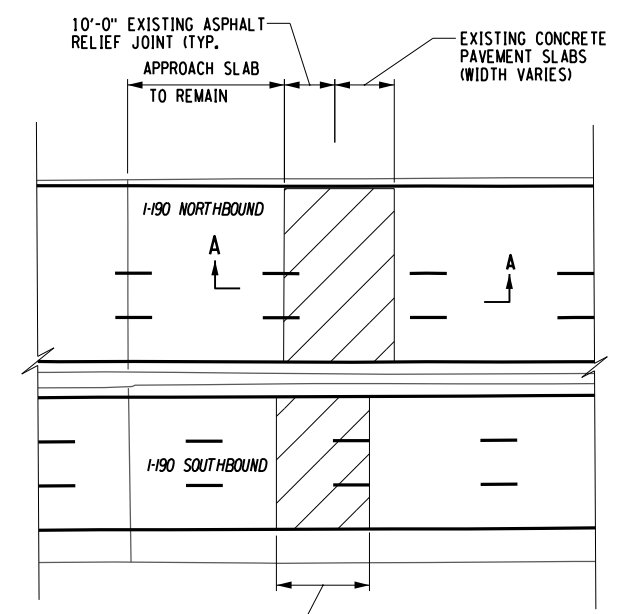
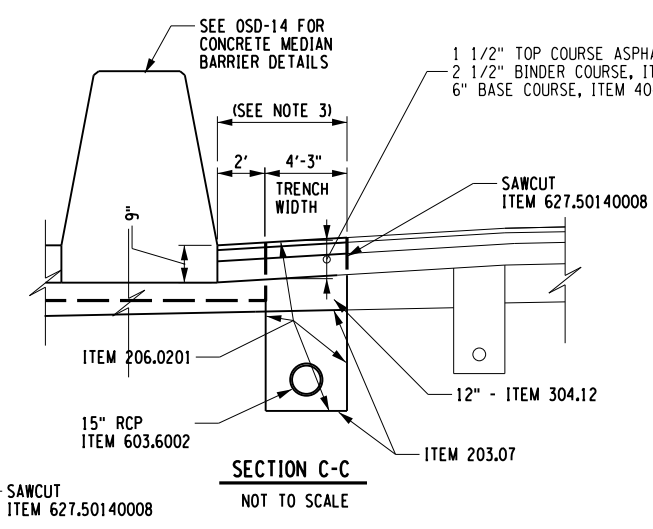
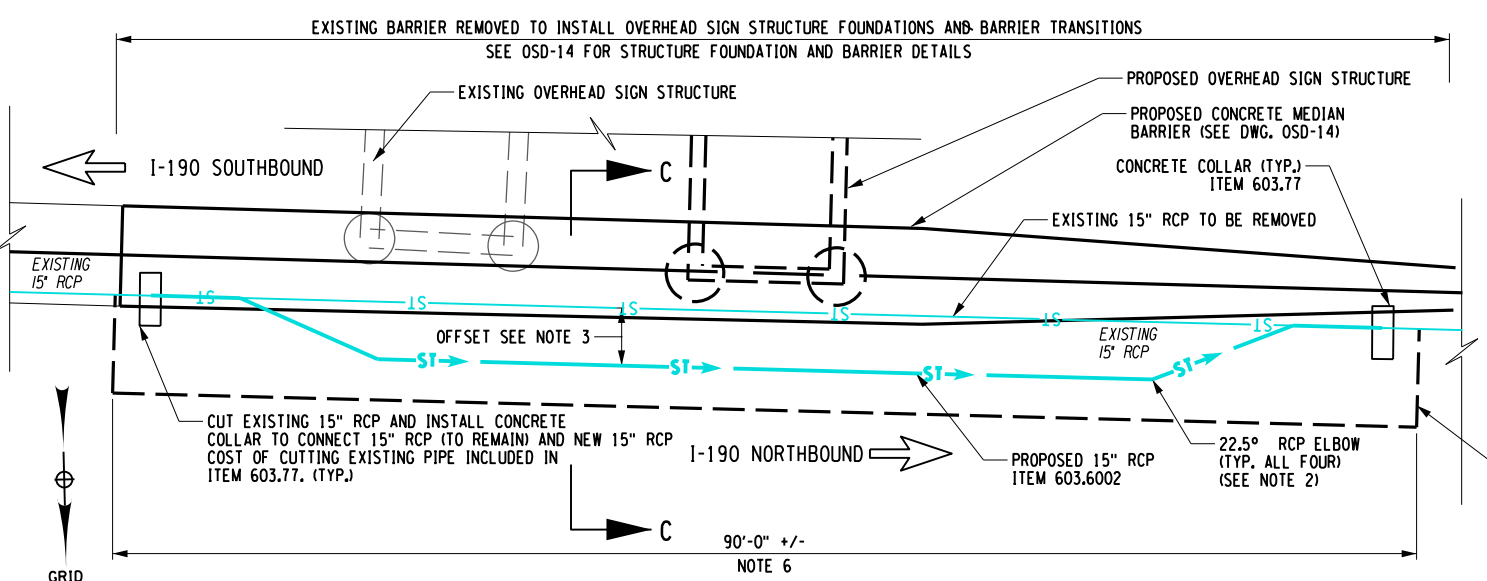
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SIGNATURE: STAMP:	SIGNATURE: STAMP: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

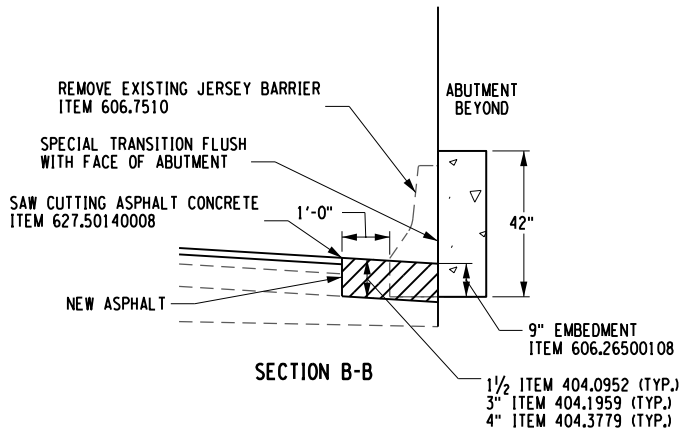
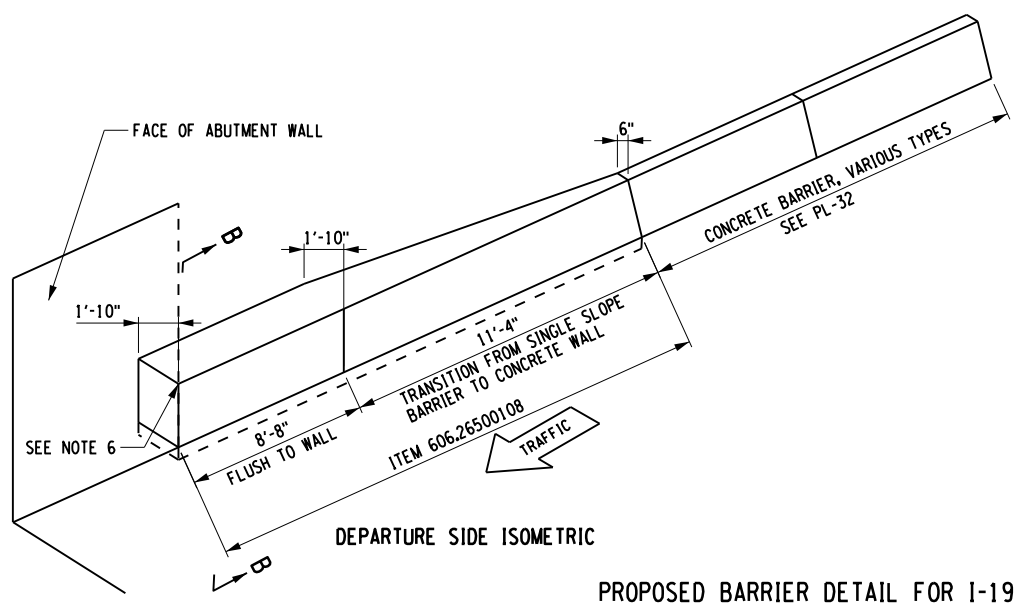
REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING MISCELLANEOUS DETAILS	DRAWING NUMBER: MD-9

CHECKED BY: J. KLEINMANN  
 P. AMICONE  
 DRAFTED BY: J. KLEINMANN  
 CHECKED BY: J. KLEINMANN  
 P. AMICONE  
 DESIGNED BY: P. AMICONE  
 DRAFTED BY: J. KLEINMANN  
 CHECKED BY: J. KLEINMANN  
 P. AMICONE  
 DESIGN SUPERVISOR: D. MARSH



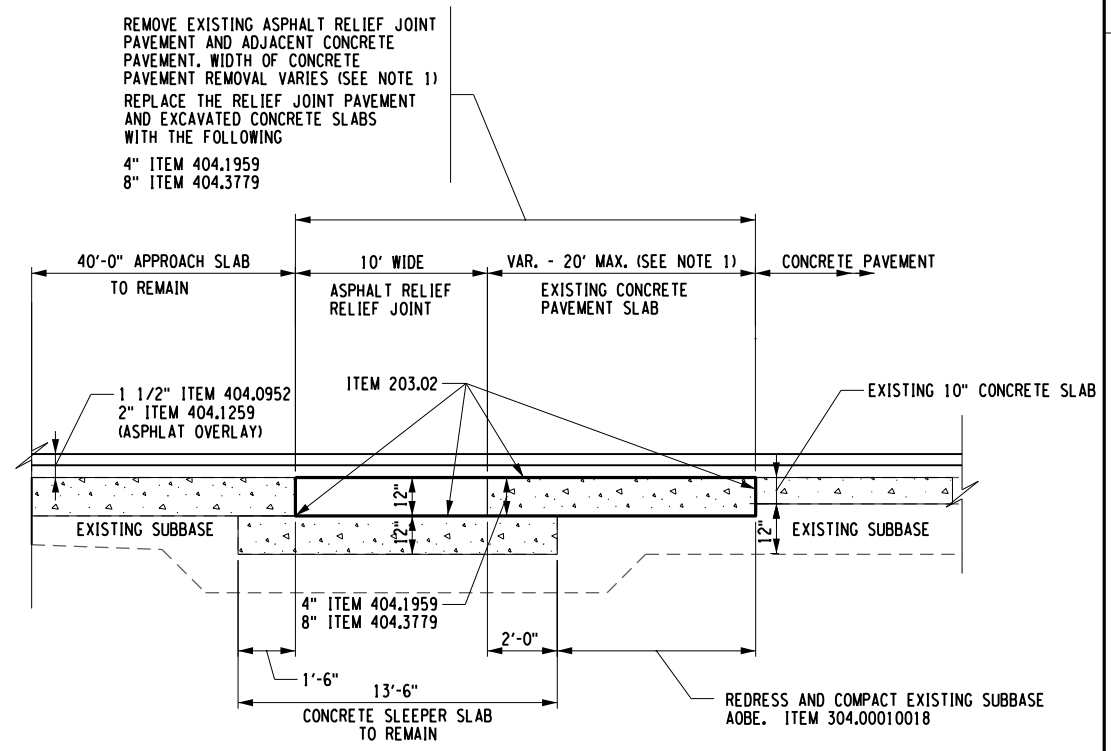
**RELOCATE EXISTING MEDIAN DRAINAGE STA. ML 23+75  
S900.38 (NEW OVERHEAD SIGN STRUCTURE)**  
NOT TO SCALE



**PROPOSED BARRIER DETAIL FOR I-190 SOUTHBOUND AT BIN 5512290**  
NOT TO SCALE

**MISCELLANEOUS DETAILS NOTES:**

- SEE CPR DWGS FOR INDIVIDUAL LIMITS OF RELIEF JOINT RECONSTRUCTION.
- THE CONTRACTOR SHALL TAKE AND RECORD INVERTS AT EACH LOCATION OF WHERE THE 15 INCH RCP IS CUT. SET THE NEW 15 INCH RCP AT A CONSISTENT GRADE BASED ON THE EXISTING INVERTS OF THE 15 INCH RCP. COST OF ANY FITTINGS AND ELBOWS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 603.6002.
- THE AMOUNT OF THE OFFSET FOR THE NEW 15 INCH RCP DEPENDS ON ACTUAL LOCATION OF THE EXISTING 15 INCH RCP MEDIAN DRAINAGE PIPE AND THE EXISTING 6 INCH UNDERDRAIN. THIS SHALL BE VERIFIED IN THE FIELD PRIOR TO INSTALLATION BUT IS PROJECTED TO BE A 2.5 TO 5 FOOT OFFSET TO CLEAR THE NEW DRILLED SHAFTS.
- IT IS INTENDED TO INSTALL THE RELOCATED MEDIAN DRAINAGE PIPE (15 INCH RCP) IN THE LEFT SHOULDER FOR NORTHBOUND I-190. EXACT LOCATION OF THE EXISTING 15 INCH RCP AND FIELD CONDITIONS SHALL DETERMINE IF THE LEFT SHOULDER FOR SOUTHBOUND I-190 IS A MORE APPROPRIATE LOCATION.
- CONTRACTOR SHALL DETERMINE IN THE FIELD THE REQUIRED LENGTH OF RELOCATED STORM PIPE TO AVOID CONFLICTS WITH THE EXISTING AND PROPOSED OVERHEAD SIGN STRUCTURE AS WELL AS THE PROPOSED CONCRETE MEDIAN BARRIER. IF THE EXISTING STORM DRAIN DOES NOT CONFLICT WITH THE REMOVAL OF THE EXISTING OVERHEAD SIGN STRUCTURE, THE INSTALLATION OF THE NEW FOUNDATIONS FOR THE PROPOSED SIGN STRUCTURE, OR THE INSTALLATION OF THE PROPOSED MEDIAN BARRIER, IT SHALL REMAIN AS IS AND NOT BE RELOCATED.
- NON-SHRINK GROUT SHALL BE PLACED BETWEEN THE EXISTING BRIDGE ABUTMENT AND THE TRANSITION CONCRETE BARRIER. COST FOR THE GROUT SHALL BE INCLUDED IN ITEM 606.26500108.



**TYPICAL ASPHALT RELIEF JOINT REHABILITATION DETAIL  
SECTION A-A  
NOT TO SCALE**

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

JOSEPH W. KLEINMANN  
LICENSED PROFESSIONAL ENGINEER  
092577

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING MISCELLANEOUS DETAILS	DRAWING NUMBER: MD-10

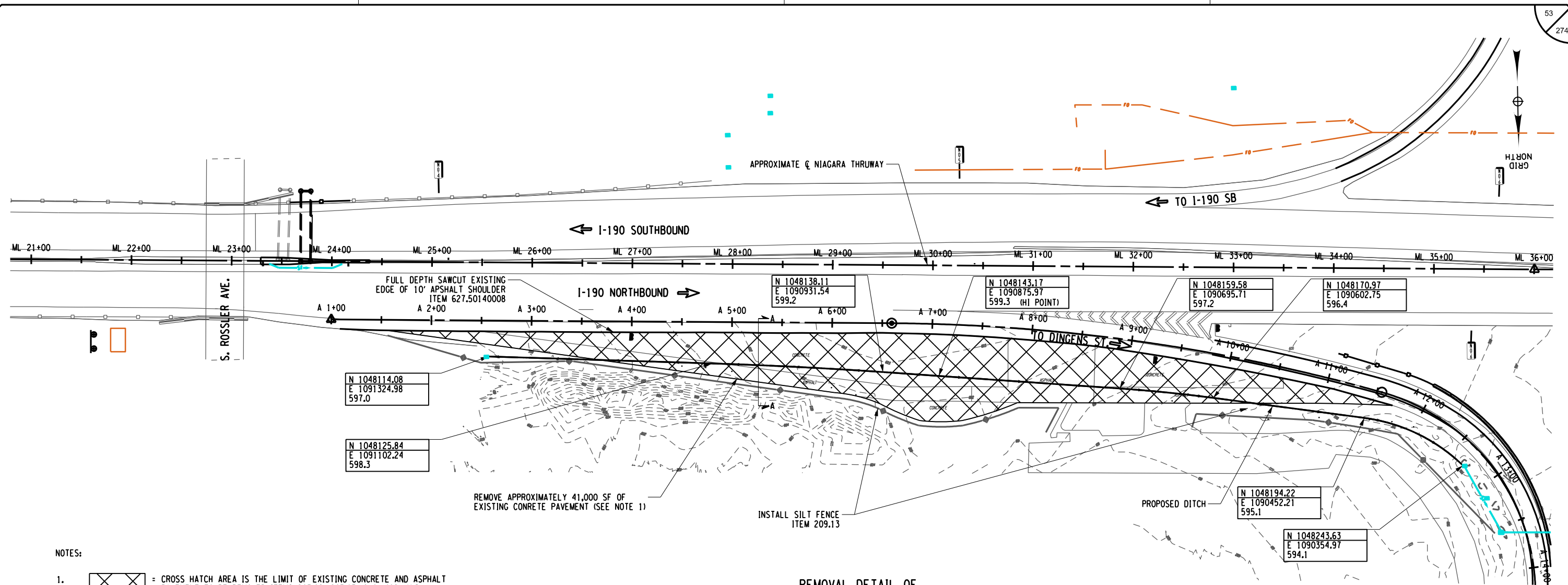
CHECKED BY: J. KLEINMANN

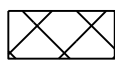
DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

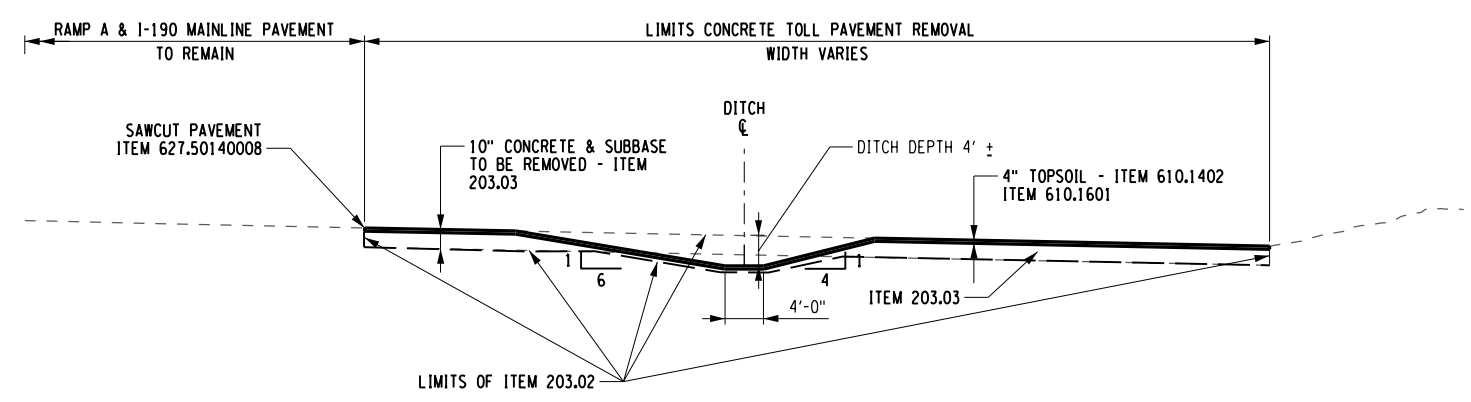
DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

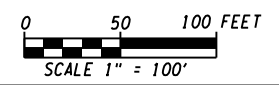



NOTES:  
1.  = CROSS HATCH AREA IS THE LIMIT OF EXISTING CONCRETE AND ASPHALT PAVEMENT TO BE REMOVED ITEMS 203.02, 203.03, 610.1402, AND 610.1601.

**REMOVAL DETAIL OF REMAINING TOLL PAVEMENT**  
N.T.S.



**SECTION A-A**  
**PROPOSED DITCH AND CONCRETE REMOVAL**  
N.T.S.



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.




TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2  
TITLE OF DRAWING  
MISCELLANEOUS DETAILS

CONTRACT NUMBER:  
TAN 24-12  
DATE:  
MAY 2024  
DRAWING NUMBER:  
MD-11

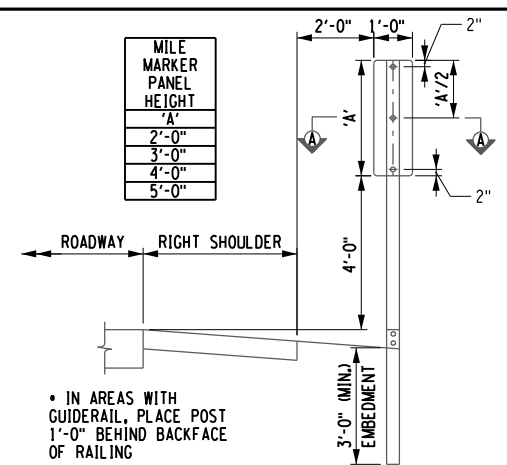
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. THOMPSON

CHECKED BY: J. KLEINMANN

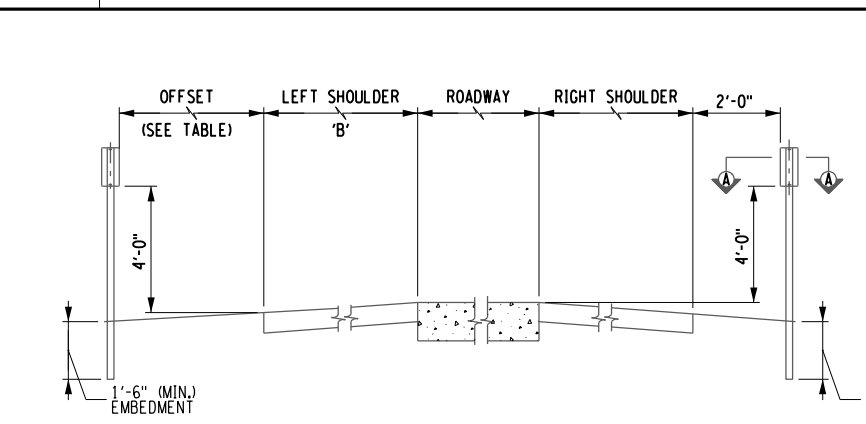
DRAFTED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



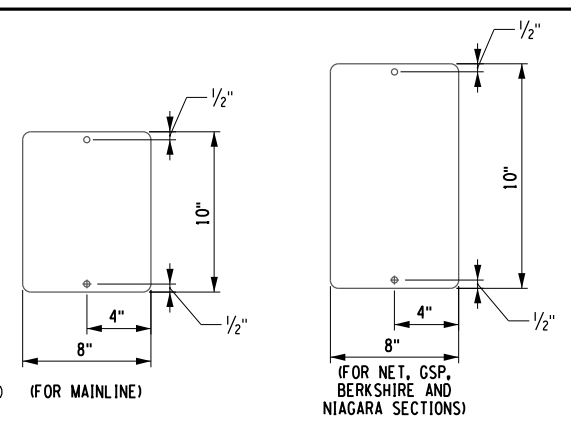
**MILE MARKER POST MOUNT INSTALLATION DETAIL**  
N.T.S.

• IN AREAS WITH GUIDERAIL, PLACE POST 1'-0" BEHIND BACKFACE OF RAILING

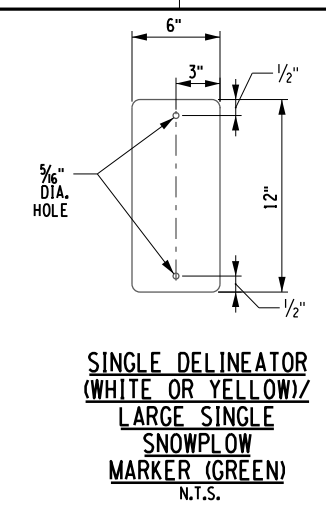


'B'	OFFSET
4'-0"	8'-0"
6'-0"	6'-0"
10'-0"	4'-0"
12'-0" - 14'-0"	2'-0"

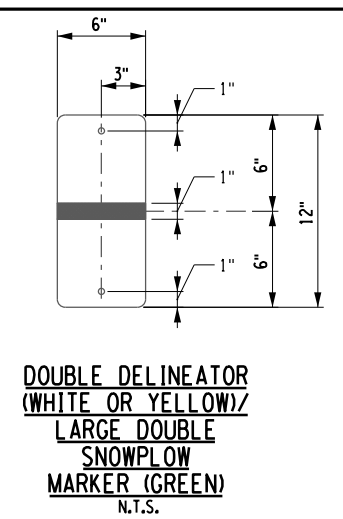
**TENTH MILE MARKER AND DELINEATOR POST MOUNT INSTALLATION DETAIL**  
N.T.S.



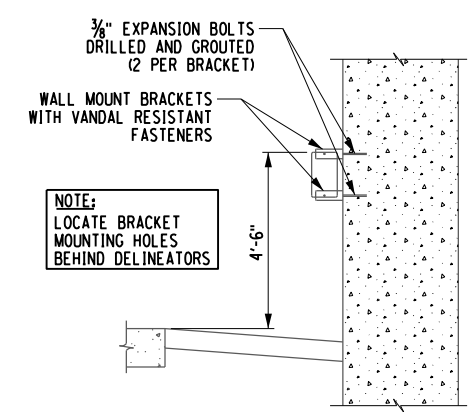
**TENTH MILE MARKER**  
N.T.S.



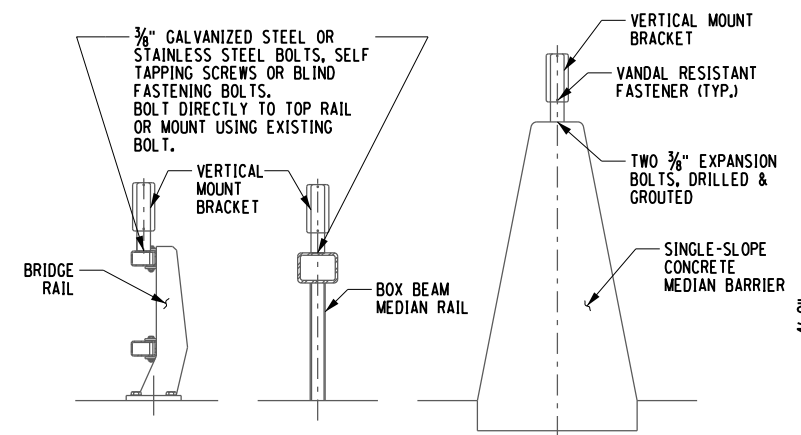
**SINGLE DELINEATOR (WHITE OR YELLOW)/LARGE SINGLE SNOWPLOW MARKER (GREEN)**  
N.T.S.



**DOUBLE DELINEATOR (WHITE OR YELLOW)/LARGE DOUBLE SNOWPLOW MARKER (GREEN)**  
N.T.S.

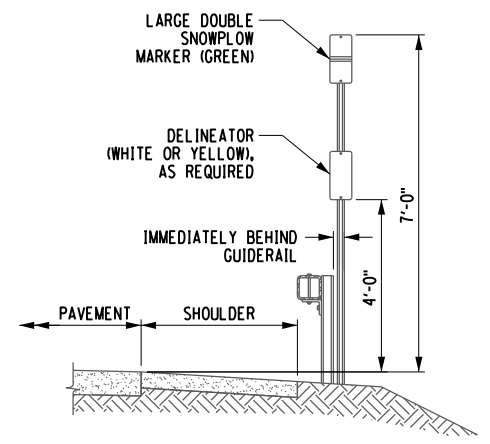


**TENTH MILE MARKER AND DELINEATOR WALL MOUNT INSTALLATION DETAIL**  
N.T.S.

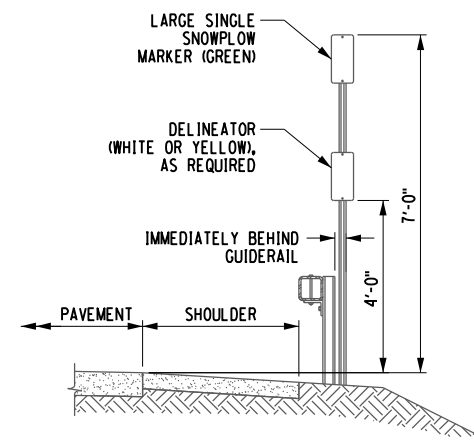


**DELINEATOR VERTICAL MOUNT INSTALLATION DETAILS**  
N.T.S.

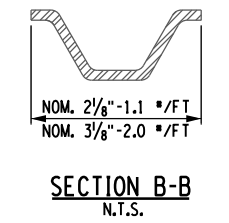
**CURB SECTION**  
N.T.S.



**SNOWPLOW MARKER/DELINEATOR AT BEGINNING OF GUIDERAIL SECTION**  
N.T.S.



**SNOWPLOW MARKER/DELINEATOR AT END OF GUIDERAIL SECTION**  
N.T.S.



**SECTION B-B**  
N.T.S.

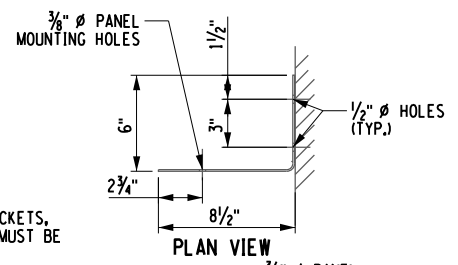
**NOTES:**

- MATERIAL SHALL CONFORM TO EITHER A.S.T.M. A-499 OR A.S.T.M. A-36.
- GALVANIZING SHALL CONFORM TO A.S.T.M. A-123.
- REMOVE ALL BURRS AND SHARP EDGES.
- TENTH MILE MARKERS AND DELINEATORS: FOR INSTALLATION WHERE THE DISTANCE FROM FINISHED GRADE TO TOP OF POST IS LESS THAN OR EQUAL TO 5'-0", USE 1.1 #/FT POST SIZE. WHERE THE DISTANCE IS GREATER THAN 5'-0", USE 2.0 #/FT POST SIZE, UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.
- MILE MARKERS (PANEL HEIGHTS OF 2'-0" TO 5'-0") SHALL BE MOUNTED ON 2.0 #/FT POSTS REGARDLESS OF POST HEIGHT UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.
- LARGE SINGLE AND DOUBLE SNOWPLOW MARKERS SHALL BE USED ON ALL FREEWAYS AND EXPRESSWAYS.
- LARGE DELINEATORS AND LARGE SNOWPLOW MARKERS SHALL BE MOUNTED ON 2.0 #/FT. POSTS REGARDLESS OF POST HEIGHT UNLESS OTHERWISE SHOWN IN THE CONTRACT DOCUMENTS.

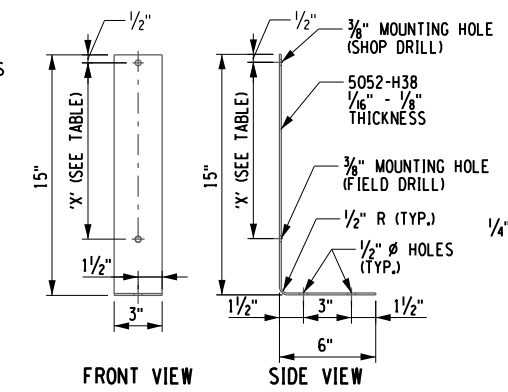
**VERTICAL MOUNT BRACKET NOTES:**

- WHEN GALVANIZED STEEL IS USED FOR BRACKETS, SPACERS THAT DO NOT INDUCE CORROSION MUST BE USED.
- BRACKETS MAY BE USED FOR MOUNTING DELINEATORS OR REFERENCE MARKERS ON BRIDGE RAIL, GUIDE RAIL, OR AS OTHERWISE NEEDED.

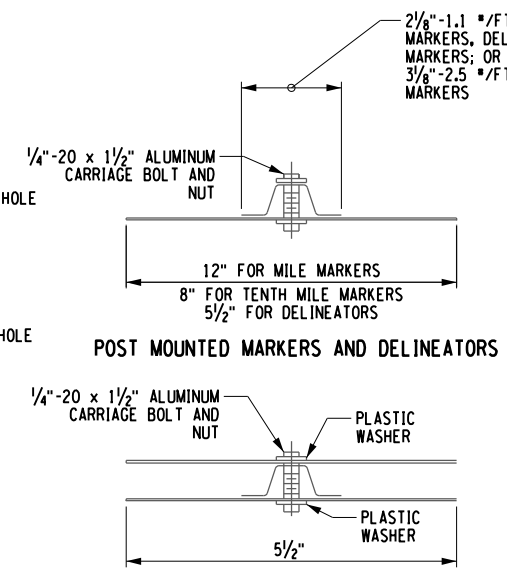
'X'	DESCRIPTION
11"	FOR 5 1/2" X 12" DELINEATOR
9"	FOR 8" X 10" TENTH MILE MARKER
13"	FOR 8" X 14" TENTH MILE MARKER



**WALL MOUNT BRACKET**  
N.T.S.

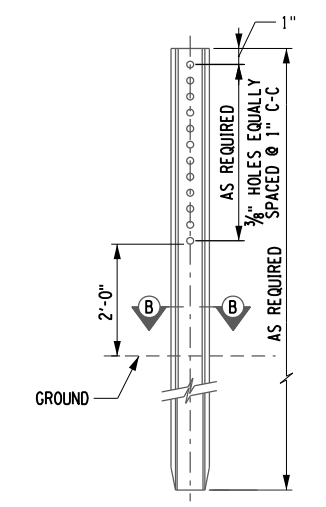


**VERTICAL MOUNT BRACKET**  
N.T.S.



**POST MOUNTED MARKERS AND DELINEATORS**

**BACK TO BACK DELINEATORS SECTION A-A**  
N.T.S.



**U-SECTION POST DETAIL**  
N.T.S.

ALTERED ON: AFFIXED ON: 5/10/2024

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY

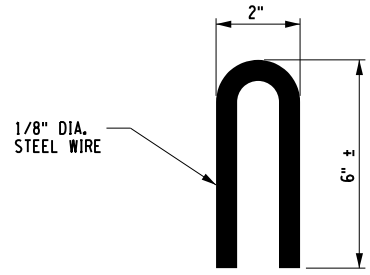
Thruway Authority

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING DELINEATOR DETAILS	DRAWING NUMBER: MD-12

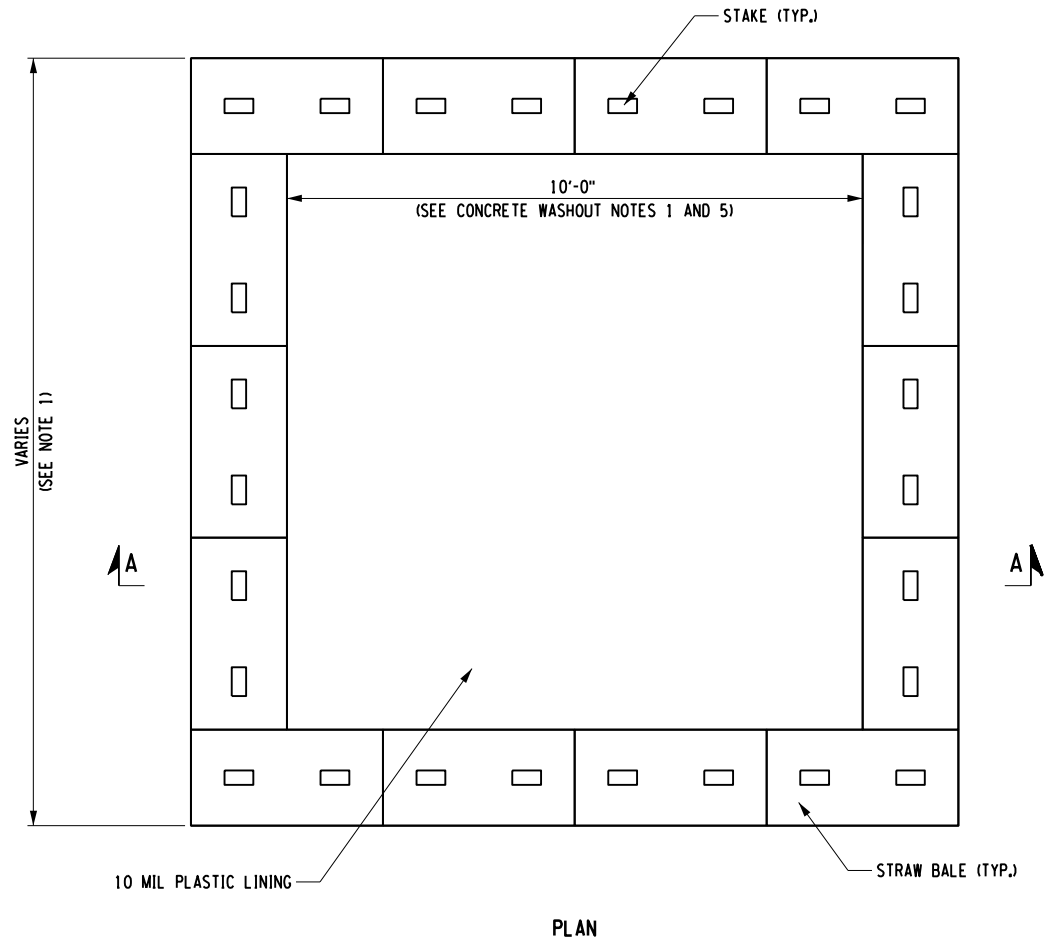


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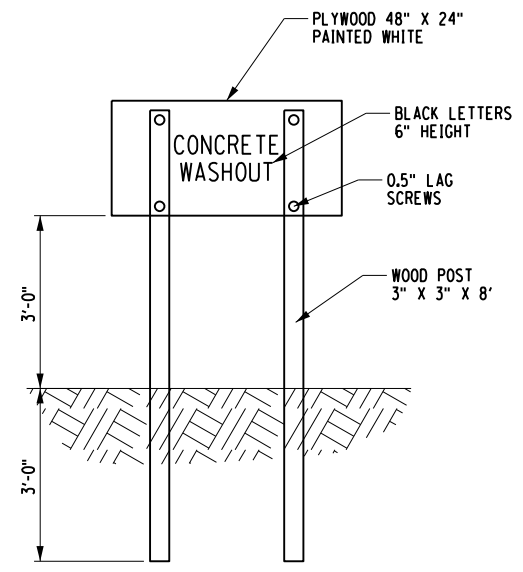
1. ACTUAL LAYOUT AND LOCATION TO BE DETERMINED IN FIELD.
2. THE CONCRETE WASHOUT SIGN SHALL BE INSTALLED WITHIN 5 FEET OF THE TEMPORARY CONCRETE WASHOUT FACILITY.
3. LOCATE WASHOUT AREA AT LEAST 50 FEET FROM STORM DRAINS OR WATER BODIES, DO NOT ALLOW RUNOFF FROM THIS AREA BY CONSTRUCTING A TEMPORARY PIT OR BERMED AREA LARGE ENOUGH FOR LIQUID OR SOLID WASTE.
4. WASH OUT WASTES INTO THE TEMPORARY PIT WHERE THE CONCRETE CAN SET, BE BROKEN UP, AND THEN DISPOSED OF PROPERLY.
5. TEMPORARY CONCRETE WASHOUT FACILITY (TYPE ABOVE GRADE) SHALL BE CONSTRUCTED AS SHOWN ON THE DETAILS WITH A RECOMMENDED MINIMUM LENGTH AND MINIMUM WIDTH OF 10 FEET, BUT WITH SUFFICIENT QUANTITY AND VOLUME TO CONTAIN ALL LIQUID AND CONCRETE WASTE GENERATED BY WASHOUT OPERATIONS.
6. STRAW BALES, WOOD STAKES, AND SANDBAG MATERIALS SHALL BE IN ACCORDANCE WITH SECTION 209 OF THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION SPECIFICATIONS AND STANDARD SHEETS.
7. PLASTIC LINING MATERIAL SHALL BE A MINIMUM OF 10 MIL IN POLYETHYLENE SHEETING AND SHOULD BE FREE OF HOLES, TEARS, OR OTHER DEFECTS THAT COMPROMISE THE IMPERMEABILITY OF THE MATERIAL.
8. WHEN TEMPORARY CONCRETE WASHOUT FACILITIES ARE NO LONGER REQUIRED FOR THE WORK, THE HARDENED CONCRETE SHALL BE REMOVED AND DISPOSED FROM THE SITE OF THE WORK.
9. HOLES, DEPRESSIONS, OR OTHER GROUND DISTURBANCE CAUSED BY THE REMOVAL OF THE TEMPORARY CONCRETE WASHOUT FACILITY SHALL BE BACKFILLED AND REPAIRED.
10. TEMPORARY CONCRETE WASHOUT FACILITIES SHALL BE MAINTAINED TO PROVIDE ADEQUATE HOLDING CAPACITY WITH A MINIMUM FREEBOARD OF 4 INCHES. MAINTAINING TEMPORARY CONCRETE WASHOUT FACILITIES SHALL INCLUDE REMOVING AND DISPOSING OF HARDENED CONCRETE AND RETURNING THE FACILITIES TO A FUNCTIONAL CONDITION. HARDENED CONCRETE MATERIALS SHALL BE REMOVED AND DISPOSED.
11. WASHOUT FACILITIES SHALL BE CLEANED, OR NEW FACILITIES SHALL BE CONSTRUCTED AND READY FOR USE ONCE THE WASHOUT IS 75% FULL.
12. THE COST FOR THE CONCRETE WASHOUTS SHALL BE INCLUDED IN THE PRICE BID FOR ALL CONCRETE ITEMS.
13. CONTRACTOR IS RESPONSIBLE FOR OBTAINING PERMITS FOR DISPOSAL OUTSIDE OF NYSTA ROW AT NO COST TO THE OWNER.



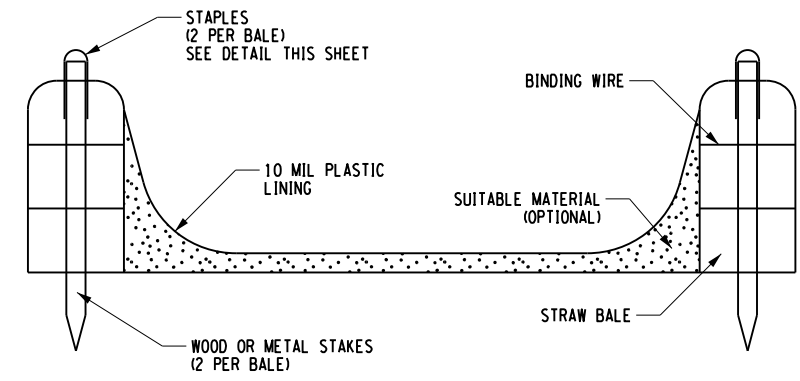
STAPLE DETAIL  
N.T.S.



PLAN



CONCRETE WASHOUT SIGN DETAIL  
OR EQUIVALENT  
N.T.S.



SECTION A-A

CONCRETE WASHOUT SIGN DETAIL  
OR EQUIVALENT  
N.T.S.

CHECKED BY: J. KLEINMANN

DRAFTED BY: P. THOMPSON

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

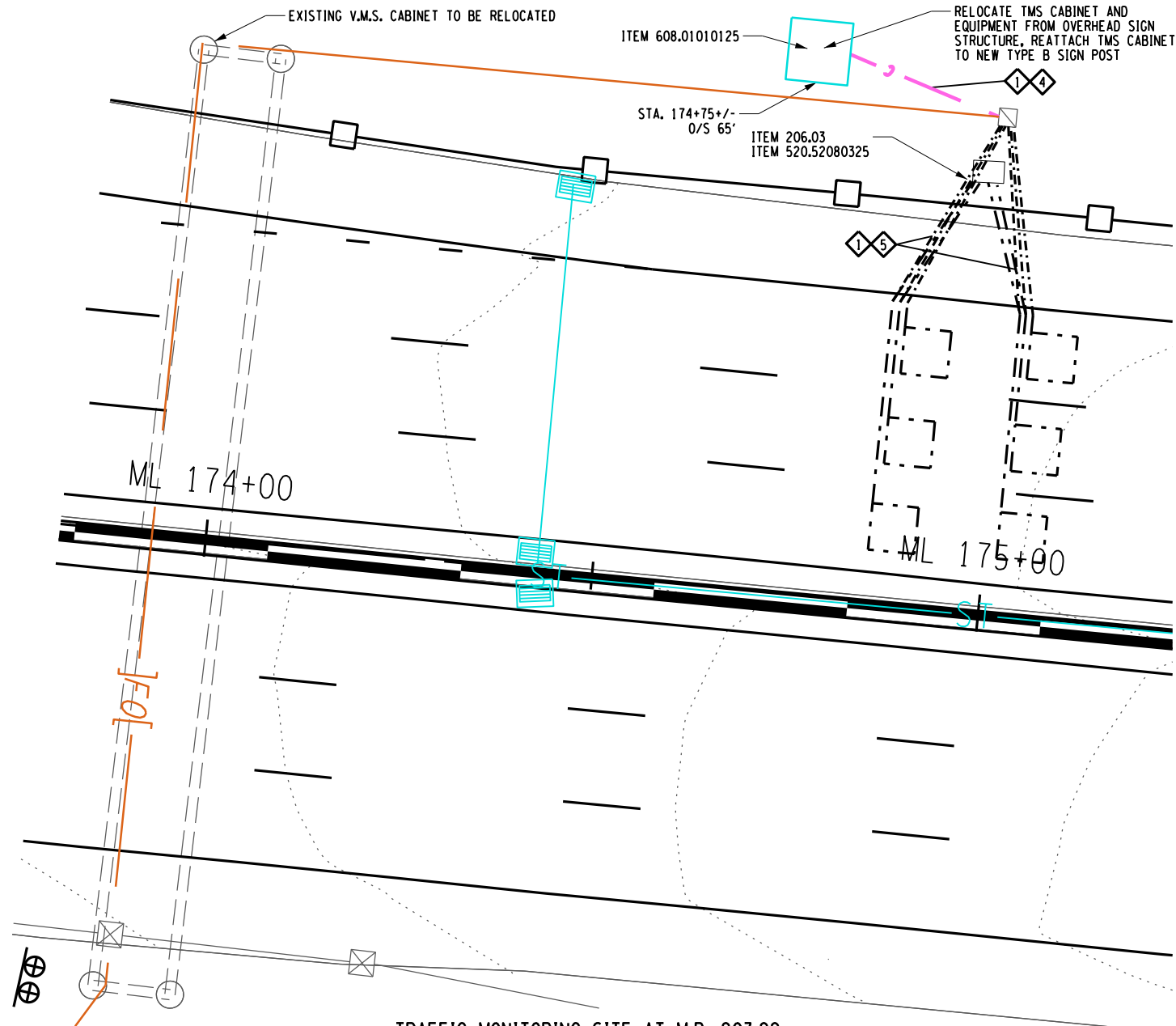
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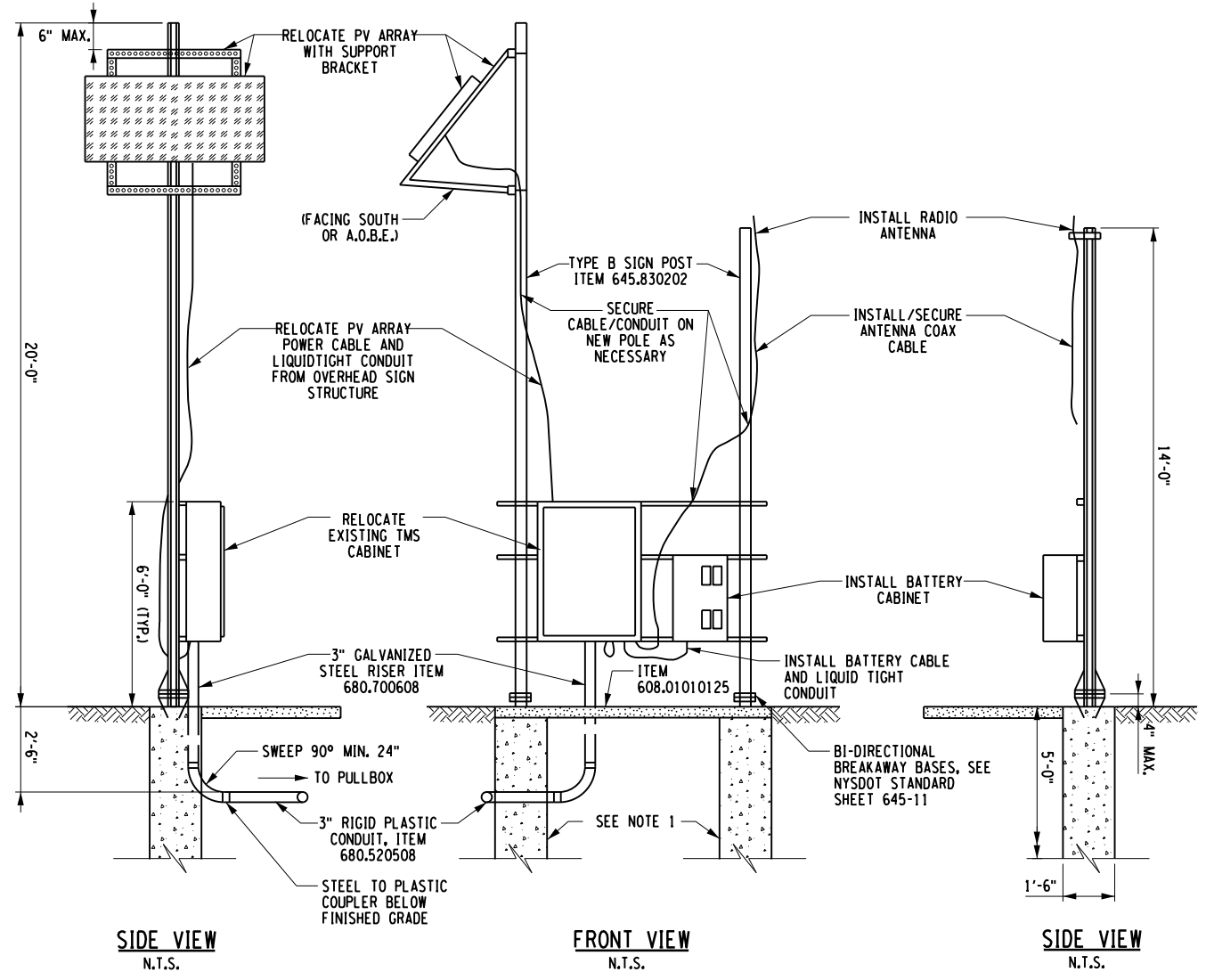
REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING MISCELLANEOUS DETAILS CONCRETE WASHOUT	DRAWING NUMBER: MD-13

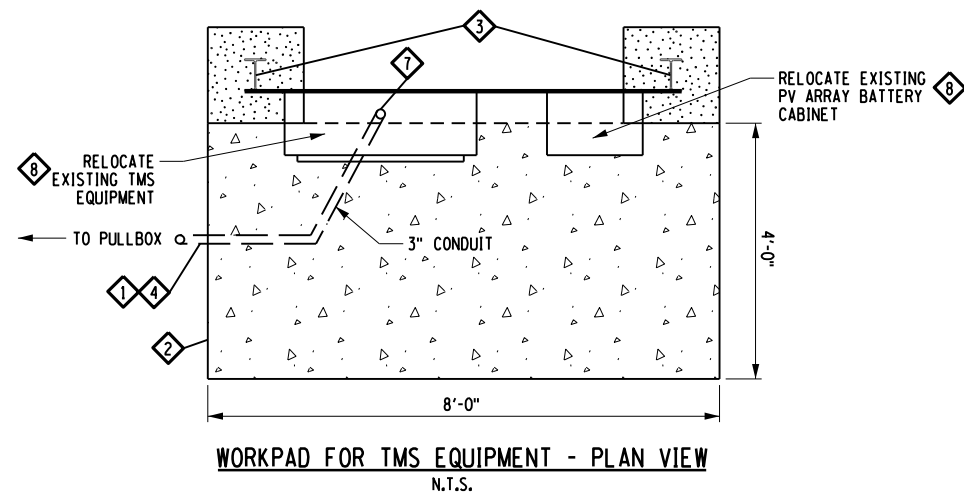
CHECKED BY: J. KLEINMANN  
 DRAFTED BY: P. THOMPSON  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: P. THOMPSON  
 DESIGN SUPERVISOR: D. MARSH



**TRAFFIC MONITORING SITE AT M.P. 903.22**  
N.T.S.



**SOLAR-POWERED TMS EQUIPMENT**  
N.T.S.



**WORKPAD FOR TMS EQUIPMENT - PLAN VIEW**  
N.T.S.

- NOTES:**
- FOR FOUNDATION DETAIL SEE NYSOT STANDARD SHEET 645-10.
  - UNLESS OTHERWISE NOTED BY OTHER PAY ITEMS ON THIS DRAWING, ALL WORK TO RELOCATE THE SOLAR POWERED T.M.S. EQUIPMENT AND WIRING, IS INCLUDED IN THE PRICE BID FOR ITEM 683.1050 25.
  - PREFORMED LOOPS SHALL BE INSTALLED PER NYSTA STANDARD SHEET TA 680-01.
  - ITEM SUPERVISION SHALL BE CONTACTED AT 716-824-7483 PRIOR TO LOOP WORK TO ALLOW THE EQUIPMENT TO BE POWERED DOWN AND DISCONNECTED.
  - PROPOSED 1" LOOP CONDUIT SHALL BE RUN TO THE EXISTING PULLBOX, CONTRACTOR TO FIELD VERIFY THE LOCATION OF THE PULLBOX.

REF. #	ITEM	DESCRIPTION	UNIT
1	206.03	CONDUIT EXCAVATION AND RESTORATION	LF
2	608.01010125	WORK PAD, CONCRETE 4' X 8'	EA
3	645.830202	TYPE B SIGN POSTS, GLAVANIZED, W6X9 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA
4	680.520508	TRAFFIC SIGNAL CONDUIT, RIGID PLASTIC, CLASS 1, 3" DIA.	LF
5	680.52080325	1 NPS CONDUIT, FLEXIBLE, LIQUID TIGHT PVC	LF
6	680.5840 25	PREFORMED INDUCTANCE LOOP DETECTOR (ASPHALT OVERLAY) INSTALLATION	EA
7	680.700608	RISER ASSEMBLY, 3" DIAMETER	EA
8	683.1050 25	RELOCATE TRAFFIC MONITORING SITE EQUIPMENT	EA

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY** | **Thruway Authority**

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 CONTRACT NUMBER: TAN 24-12

LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 DATE: MAY 2024

TITLE OF DRAWING: TMS CABINET RELOCATION  
 DRAWING NUMBER: MD-14



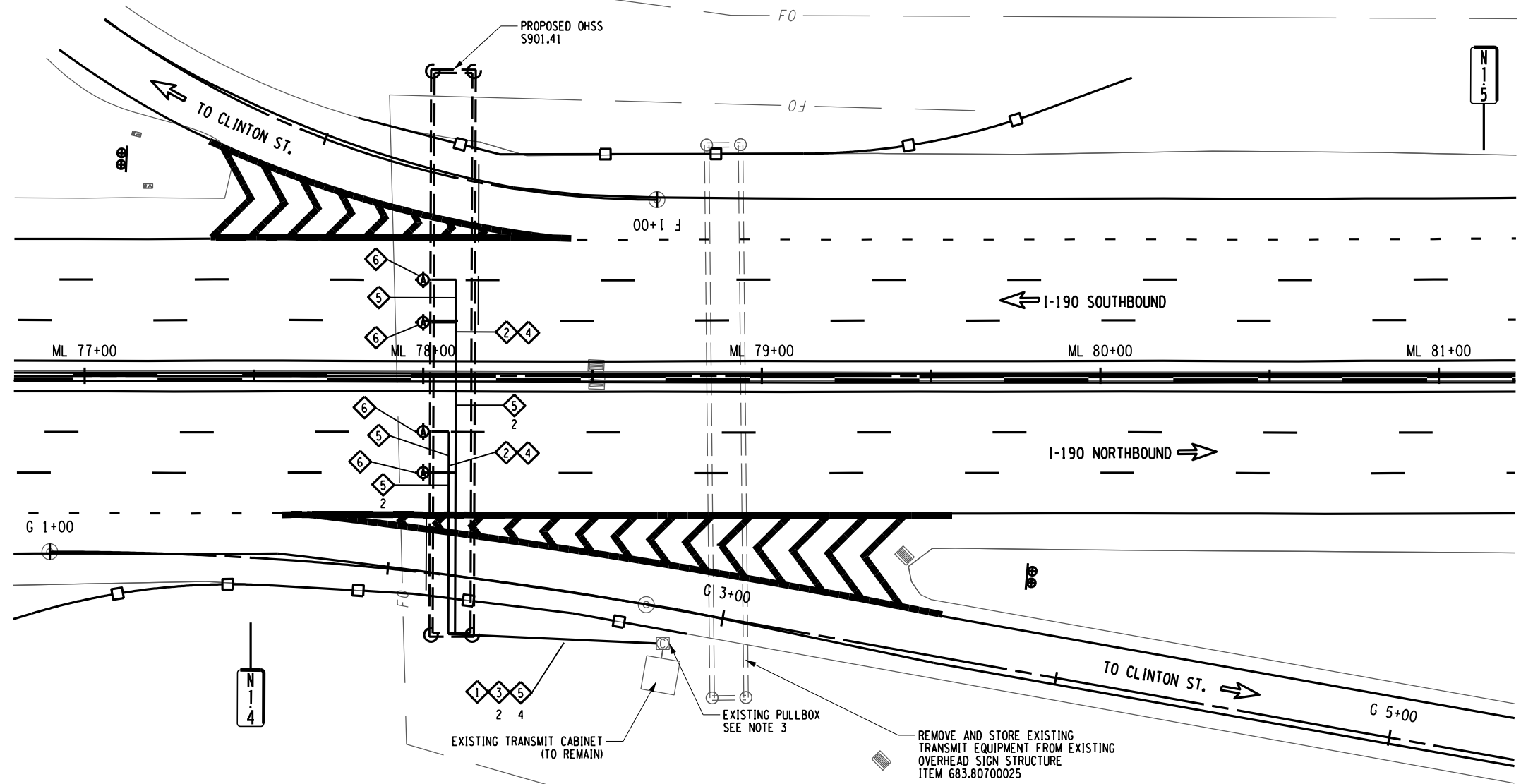
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. THOMPSON

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



NOTES:


- EXISTING TRANSMIT ANTENNAS SHALL BE INSTALLED DIRECTLY OVER PROPOSED LANE LINES, A.O.B.E.. TRANSMIT ANTENNAS SHALL BE MOUNTED PER MANUFACTURERS INSTRUCTIONS. ALL COSTS TO BE INCLUDED UNDER ITEM 683.80700025. NO ADDITIONAL PAYMENT SHALL BE MADE FOR THE TRANSMIT MOUNTING SYSTEMS.
- WHEN REMOVING THE EXISTING TRANSMIT ANTENNAS FROM THE EXISTING OVERHEAD SIGN STRUCTURE, THE CONTRACTOR SHALL PULL BACK AND COMPLETELY REMOVE THE EXISTING COAXIAL CABLE FROM THE EXISTING CONDUIT AND CABINET IN ORDER TO REINSTALL THE NEW CABLES FROM THE EXISTING CABINET AND PULLBOX TO THE NEW OVERHEAD STRUCTURE. CONTRACTOR SHALL COORDINATE WITH NYSTA TO MINIMIZE DOWNTIME OF THIS TRANSMIT SITE.
- MODIFY EXISTING PULLBOX TO ACCEPT NEW CONDUIT, ITEM 680.77000001.
- ANY MOUNTING HARDWARE SHALL BE INCLUDED IN PRICE BID FOR ITEM 683.80700025

**ITS TRANSMIT EQUIPMENT RELOCATION TO S901.41**  
N.T.S.

REF. #	ITEM	DESCRIPTION	UNIT
1	206.03	CONDUIT EXCAVATION AND RESTORATION	LF
2	206.03120025	CONDUIT INSTALLATION ON ABOVE GRADE STRUCTURES	LF
3	670.2602	RIGID PLASTIC CONDUIT, 2"	LF
4	680.520105	CONDUIT, METAL STEEL, ZINC COATED, 1 1/2"	LF
5	680.77510025	TRANSMIT COAXIAL CABLE - TYPE A	LF
6	683.80700025	RELOCATE TRANSMIT SITE EQUIPMENT	EA

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ITS TRANSMIT RELOCATION STRUCTURE S901.41	DRAWING NUMBER: MD-15

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

58  
274

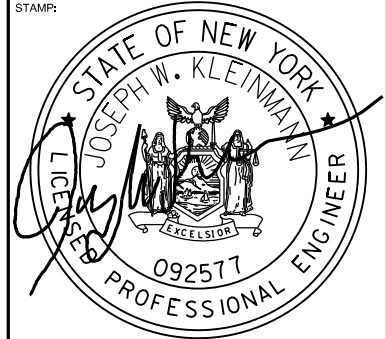
TABLE OF PROPOSED DELINEATORS										
STA. TO STA.		SIDE	SPACING (FT)	ITEM 646.0603--25 (EA)		ITEM 646.0610--25 (EA)		ITEM 646.0612--25 (EA)		
				WHITE	YELLOW	WHITE	YELLOW	YELLOW		
INTERCHANGE 1										
STA. A 9+81	STA. A 16+19	RT	50	13						
STA. A 9+81	STA. A 12+00	LT	50		5					
STA. A 12+00	STA. A 14+04	LT	50						5	
STA. B 1+00	STA. B 6+11	RT	50	10						
STA. B 3+62	STA. B 6+11	RT	50		6					
STA. C 1+50	STA. C 10+76	RT	50	18						
STA. C 2+00	STA. C 9+50	LT	50		15					
STA. C1 1+00	STA. C1 3+22	LT	50						5	
STA. D 1+23	STA. D 9+10	RT	50	16						
STA. D 2+45	STA. D 9+00	LT	50		14					
STA. D1 1+75	STA. D1 3+20	LT	50						4	
INTERCHANGE 2										
STA. E 1+00	STA. E 7+60	RT	50	14						
STA. E 1+00	STA. E 6+30	LT	50		12					
STA. F 1+20	STA. F 6+00	RT	50	9						
STA. F 2+20	STA. F 5+00	LT	50		7					
STA. G 1+55	STA. G 10+50	RT	100	10						
STA. G 3+50	STA. G 10+50	LT	100		8					
STA. H 2+20	STA. H 11+15	RT	100	10						
STA. H 2+20	STA. H 10+25	LT	100		9					
INTERCHANGE 3										
STA. J 1+90	STA. J 11+15	RT	50	18						
STA. J 3+40	STA. J 11+15	LT	50		17					
STA. K 1+00	STA. K 7+50	RT	100	6						
STA. K 1+00	STA. K 5+70	LT	100		6					
INTERCHANGE 4										
STA. L 1+00	STA. L 3+30	RT	50	5						
STA. L 1+50	STA. L 2+30	LT	50		3					
STA. L1 2+00	STA. L1 6+95	RT	50	11						
STA. L1 2+00	STA. L1 6+95	MED	50						11	
STA. L1 2+00	STA. L1 6+95	LT	50	9						
STA. M 1+50	STA. M 3+74	RT	50	5						
STA. M 2+40	STA. M 3+74	LT	50		3					
STA. N 1+50	STA. N 6+30	RT	50	9						
STA. N 2+90	STA. N 6+30	LT	50		7					
STA. O1 1+90	STA. O1 5+11	RT	50	7						
STA. O1 1+90	STA. O1 5+11	MED	50						8	
STA. O1 1+90	STA. O1 5+11	LT	50	6						
STA. O 1+00	STA. O 5+60	RT	50	10						
STA. O 1+00	STA. O 4+60	LT	50		7					

TABLE OF PROPOSED DELINEATORS										
STA. TO STA.		SIDE	SPACING (FT)	ITEM 646.0603--25 (EA)		ITEM 646.0610--25 (EA)		ITEM 646.0612--25 (EA)		
				WHITE	YELLOW	WHITE	YELLOW	YELLOW		
INTERCHANGE 5										
STA. P 1+00	STA. P 6+98	RT	50	11						
STA. P 2+70	STA. P 5+35	LT	50		7					
STA. P 5+80	STA. P 6+98	MED	50						3	
STA. Q1 1+90	STA. Q1 3+11	RT	50	3						
STA. Q1 1+90	STA. Q1 3+11	MED	50						3	
STA. Q1 1+90	STA. Q1 3+11	LT	50	3						
STA. Q 1+00	STA. Q 7+80	RT	50	14						
STA. Q 3+00	STA. Q 6+85	LT	50		9					
STA. R 1+30	STA. R 7+85	RT	50	14						
STA. R 3+00	STA. R 7+00	LT	50		9					
STA. R 1+40	STA. R 2+66	MED	50						3	
STA. R1 1+50	STA. R1 2+80	RT	50	4						
STA. R1 1+80	STA. R1 2+80	MED	50						3	
STA. R1 1+25	STA. R1 2+50	LT	50	4						
STA. S 1+50	STA. S 7+87	RT	50	12						
STA. S 3+00	STA. S 6+70	LT	50		8					
MAINLINE										
STA. ML 24+00	STA. ML 32+80	RT	100	9						
STA. ML 37+82	STA. ML 83+89	MED	264						18	
STA. ML 40+30	STA. ML 51+29	NB	100	10						
STA. ML 39+75	STA. ML 46+75	SB	100	6						
STA. ML 46+75	STA. ML 60+00	SB	132	8						
STA. ML 51+29	STA. ML 70+00	NB	132	11		1				
STA. ML 60+00	STA. ML 70+36	SB	100	8						
STA. ML 70+36	STA. ML 78+15	SB	132	4						
STA. ML 70+00	STA. ML 77+50	NB	100	6						
STA. ML 77+50	STA. ML 98+50	NB	132	8		2				
STA. ML 78+15	STA. ML 83+10	SB	100	4						
STA. ML 83+10	STA. ML 126+00	SB	132	16		10				
STA. ML 83+89	STA. ML 87+91	MED	264				2			
STA. ML 87+91	STA. ML 97+61	MED	264					8		4
STA. ML 97+61	STA. ML 108+87	MED	264						8	
STA. ML 98+50	STA. ML 107+15	NB	100			8				
STA. ML 107+15	STA. ML 130+50	NB	132	10		3				
STA. ML 108+87	STA. ML 118+26	MED	264						4	
STA. ML 118+26	STA. ML 123+33	MED	264				4			
STA. ML 123+33	STA. ML 131+08	MED	264						3	
STA. ML 130+50	STA. ML 139+00	NB	100	6		2				
STA. ML 126+00	STA. ML 131+00	SB	100	4						
STA. ML 131+08	STA. ML 133+50	MED	264					2		
STA. ML 131+00	STA. ML 156+00	SB	132	10		5				
STA. ML 133+50	STA. ML 140+69	MED	264						2	
STA. ML 139+00	STA. ML 165+00	NB	132	8		7				
STA. ML 140+69	STA. ML 143+36	MED	264					2		
STA. ML 143+36	STA. ML 154+75	MED	264						4	
STA. ML 152+35	STA. ML 154+75	MED	264					2		
STA. ML 154+75	STA. ML 157+74	MED	264						1	
STA. ML 156+00	STA. ML 164+45	SB	100	1		6				
STA. ML 157+74	STA. ML 164+50	MED	264					4		
STA. ML 165+00	STA. ML 170+85	NB	100	3		2				
STA. ML 164+45	STA. ML 167+65	SB	132	2						
STA. ML 164+50	STA. ML 167+94	MED	264						2	

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_  
STAMP: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_  
STAMP: \_\_\_\_\_



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING MISCELLANEOUS TABLES	DRAWING NUMBER: MT-1

YD.

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

**TABLE OF PROPOSED DELINEATORS**

STA. TO STA.	SIDE	SPACING (FT)	ITEM 646.0603--25 (EA)		ITEM 646.0610--25 (EA)		ITEM 646.0612--25 (EA)	
			WHITE	YELLOW	WHITE	YELLOW	YELLOW	
MAINLINE (CONTINUED)								
STA. ML 167+65	STA. ML 174+50	SB	100	4	2			
STA. ML 167+94	STA. ML 170+60	MED	264			2		
STA. ML 170+60	STA. ML 176+87	MED	264					2
STA. ML 170+85	STA. ML 176+70	NB	132	2				
STA. ML 174+50	STA. ML 205+00	SB	132	14	3			
STA. ML 176+87	STA. ML 180+08	MED	264			2		
STA. ML 176+70	STA. ML 186+00	NB	100	6	1			
STA. ML 180+08	STA. ML 186+16	MED	264					3
STA. ML 186+16	STA. ML 188+54	MED	264			2		
STA. ML 186+00	STA. ML 199+00	NB	132	4	3			
STA. ML 188+54	STA. ML 194+55	MED	264					2
STA. ML 194+55	STA. ML 198+53	MED	264			2		
STA. ML 198+53	STA. ML 202+09	MED	264					2
STA. ML 199+00	STA. ML 205+00	NB	100		5			
STA. ML 202+09	STA. ML 204+73	MED	264			2		
STA. ML 204+73	STA. ML 226+80	MED	264					9
STA. ML 205+00	STA. ML 211+60	SB	100	5				
STA. ML 205+00	STA. ML 212+60	NB	132	3				
STA. ML 211+60	STA. ML 220+50	SB	132	5				
STA. ML 212+60	STA. ML 220+60	NB	100	7				
STA. ML 220+60	STA. ML 227+25	NB	132	3	2			
STA. ML 220+50	STA. ML 225+00	SB	100	3	1			
STA. ML 225+00	STA. ML 226+25	SB	132	2				
<b>TOTAL</b>				443	152	63	34	101

**SNOWPLOW MARKER TABLE**

LOCATION (STATION)	LOCATION (M.P.)	SIDE	ITEM 646.0801--25 INSTALL SNOWPLOW MARKER, SINGLE UNIT (EA)	ITEM 646.0802--25 INSTALL SNOWPLOW MARKER, DOUBLE UNIT (EA)
INTERCHANGE 2				
H 9+25	-	RT		2
INTERCHANGE 3				
J 3+26	-	LT		2
J 4+90	-	RT	1	
J 9+87	-	LT	1	
K 3+10	-	RT		2
INTERCHANGE 4				
L1 5+00	-	LT	1	
L1 3+75	-	RT		2
N 2+00	-	RT	1	
O1 2+40	-	RT		2
INTERCHANGE 5				
P 2+83	-	RT	1	
P 5+21	-	RT		2
P 6+73	-	RT	1	
Q 1+26	-	RT		2
Q 4+59	-	RT	1	
MAINLINE				
ML 53+20	900.94	RT		2
ML 59+65	901.10	RT	1	
ML 76+40	901.38	RT		2

**SNOWPLOW MARKER TABLE (CONTINUED)**

LOCATION (STATION)	LOCATION (M.P.)	SIDE	ITEM 646.0801--25 INSTALL SNOWPLOW MARKER, SINGLE UNIT (EA)	ITEM 646.0802--25 INSTALL SNOWPLOW MARKER, DOUBLE UNIT (EA)
MAINLINE				
ML 77+50	901.40	RT	1	
ML 81+60	901.49	RT		2
ML 91+65	901.67	RT	1	
ML 128+34	902.37	RT	1	
ML 172+65	903.21	RT		2
ML 174+38	903.24	RT	1	
ML 213+65	903.98	RT		2
ML 53+87	900.96	LT	1	
ML 55+20	900.99	LT		2
ML 59+43	901.07	LT	1	
ML 63+59	901.16	LT		2
ML 74+92	901.38	LT	1	
ML 76+63	901.42	LT		2
ML 78+80	901.46	LT	1	
ML 80+00	901.48	LT		2
ML 83+22	901.54	LT	1	
ML 124+54	902.29	LT		2
ML 136+77	902.53	LT		2
ML 138+78	902.57	LT	1	
ML 211+00	903.94	LT		2
<b>TOTAL</b>			18	38

**TABLE OF PROPOSED MILE MARKERS**

STA. TO STA.	SIDE	SPACING (FT)	MILEPOST	ITEM 646.0626--25 (EA)		ITEM 646.0603--25 (EA)		ITEM 646.0610--25 (EA)	
				MILE MARKERS		TENTH MILE MARKERS			
MAINLINE SOUTHBOUND									
STA. SB 40+86	STA. SB 50+52	LT	528	900.7 TO 900.9		3			
STA. SB 56+05		LT		901.0	1				
STA. SB 60+22	STA. SB 102+12	LT	528	901.1 TO 901.9		6		3	
STA. SB 107+58		LT		902.0	1				
STA. SB 113+01	STA. SB 156+33	LT	528	902.1 TO 902.9		8		1	
STA. SB 161+73		LT		903.0	1				
STA. SB 167+81	STA. SB 209+22	LT	528	903.1 TO 903.9		7		2	
STA. SB 214+26		LT		904.0	1				
STA. SB 219+69		LT		904.1		1			
STA. SB 225+05		LT		904.2		1			
MAINLINE NORTHBOUND									
STA. NB 40+55	STA. NB 51+26	RT	528	900.7 TO 900.9		3			
STA. NB 56+10		RT		901.0	1				
STA. NB 61+28	STA. NB 104+05	RT	528	901.1 TO 901.9		7		2	
STA. NB 109+04		RT		902.0	1				
STA. NB 113+69	STA. NB 156+45	RT	528	902.1 TO 902.9		9			
STA. NB 161+69		RT		903.0	1				
STA. NB 167+00	STA. NB 210+16	RT	528	903.1 TO 903.9		6		3	
STA. NB 214+64		RT		904.0	1				
STA. NB 219+96		RT		904.1		1			
STA. NB 224+29		RT		904.2		1			
<b>TOTAL</b>					8	53		11	

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS

DATE	DESCRIPTION	BY	SYM.

  	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING MISCELLANEOUS TABLES	DRAWING NUMBER: MT-2

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

REMOVAL & DISPOSAL OF DELINEATORS AND MARKERS  
ITEM 646.50----25

Table with columns: STA. TO STA., SIDE, DESCRIPTION, QUANTITY (EA). Includes Interchange 1, 2, 3, 4 and Mainline sections.

REMOVAL & DISPOSAL OF DELINEATORS AND MARKERS  
ITEM 646.50----25

Table with columns: STA. TO STA., SIDE, DESCRIPTION, QUANTITY (EA). Includes Interchange 5 and Mainline sections.

REMOVAL & DISPOSAL OF DELINEATORS AND MARKERS  
ITEM 646.50----25

Table with columns: STA. TO STA., SIDE, DESCRIPTION, QUANTITY (EA). Includes Mainline (Continued) section.

PROPOSED CURB TABLE

Table with columns: LOCATION (BEGIN, END), SIDE, ITEM 609.0401 (LF), ITEM 609.0405 (LF). Includes a TOTAL row.

ALTERED ON: 5/10/2024  
AFFIXED ON:  
SIGNATURE: STAMP:  
SIGNATURE: STAMP:  
Professional Engineer Seal for Joseph W. Kleinmann, State of New York, License No. 092577.

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REVISIONS table with columns: DATE, DESCRIPTION, BY, SYM.

NEW YORK STATE OF OPPORTUNITY  
Thruway Authority  
GMA logo

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
TITLE OF DRAWING: MISCELLANEOUS TABLES

CONTRACT NUMBER: TAN 24-12  
DATE: MAY 2024  
DRAWING NUMBER: MT-3

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

PAVEMENT ELEVATION TRANSITION TABLE with columns: START STA., END STA., LENGTH (FT), NOTES. Includes rows for ML 41+75 to R 5+50 and RAMP E to RAMP R.

VERTICAL CURB TO MOUNTABLE CURB TRANSITION TABLE with columns: BEGIN, END, SIDE. Includes rows for ML 84+15 to ML 202+20.

NOTE: 1. VERTICAL FACE CURB TO MOUNTABLE CURB TRANSITIONS SHALL BE PAID FOR UNDER ITEM 609.0405 AS SHOWN ON THE TRANSITION DETAIL ON MD-8.

SHOULDER PAVEMENT CROSS SLOPE TRANSITION TABLE with columns: FEATURE, START STATION, END STATION, TRANSITION (FT). Lists various bridges like CLINTON STREET BRIDGE, SENECA STREET BRIDGE, etc.

ITEM 621.51050003 - GRADING MAINTENANCE, CLEANING, TRIMMING & RESHAPING EXISTING ROADSIDE SECTION. Table with columns: RAMP STATION (FROM, TO), LENGTH (LF), SIDE, TOTAL LENGTH (LF).

ITEM 621.05 - CLEAN, GRADE, AND SHAPE EXISTING ROADSIDE SECTION. Table with columns: BEGIN STA., END STA., LENGTH (LF).

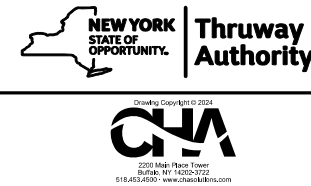
PREFORMED INDUCTANCE LOOP DETECTOR ITEM 680.5840--25. Table with columns: LOOP NAME, LOCATION (STA.), SIDE, QUANTITY (EA).

1 NPS CONDUIT, FLEXIBLE, LIQUID TIGHT PVC ITEM 680.52080325. Table with columns: LOOP NAME, LOCATION (STA.), SIDE, LENGTH (LF).

Professional Engineer stamp for Joseph W. Kleinmann, State of New York, License No. 092577. Includes signature lines and date 5/10/2024.

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REVISIONS table with columns: DATE, DESCRIPTION, BY, SYM.



Project information table with columns: TITLE OF PROJECT, LOCATION OF PROJECT, TITLE OF DRAWING, CONTRACT NUMBER, DATE, DRAWING NUMBER.

DRAINAGE TABLE PAGE 1 OF 10

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 3-1	ML 36+41.51 0.8' RT	REFER TO DS 3-2	-							
DS 3-2	ML 36+41.82 5.1' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 85 LF OF OUTLET PIPE FROM DS 3-2 TO DS 3-3.	-					85.0	1.0	
DS 3-3	ML 36+44.79 86.1' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 128 LF OF OUTLET PIPE FROM DS 3-3 TO DS 3-4.	-					128.0	1.0	
DS 3-4	ML 37+72.41 72.4' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 72 LF OF OUTLET PIPE FROM DS 3-4 TO DS 3-7.	-					72.0	1.0	
DS 3-5	ML 37+84.43 65.1' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 60 LF OF OUTLET PIPE FROM DS 3-5 TO DS 3-6.	593.10				1.0	60.0		
DS 3-6	C 9+39.60 33.6' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 134 LF OF OUTLET PIPE FROM DS 3-6 TO DS 5-3.	-					134.0	1.0	
DS 3-7	D 2+69.00 37.0' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 158 LF OF OUTLET PIPE FROM DS 3-7 TO DS 5-4.	-					158.0	1.0	
DS 4-1	A 12+70.85 20.3' RT	CLEAN EXISTING DRAINAGE STRUCTURE. SEE PROPOSED DRAINAGE INSTALLATION TABLE ON MT-14 FOR PROPOSED WORK.	593.95						1.0	
DS 4-2	A 13+14.91 22.1' RT	SEE PROPOSED DRAINAGE INSTALLATION TABLE ON MT-14 FOR PROPOSED WORK.	594.10							
DS 4-3	C1 10+15.91 14.2' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 80 LF OF OUTLET PIPE FROM DS 4-3 TO DS 4-4.	-					80.0	1.0	
DS 4-4	C1 10+88.98 15.1' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 167 LF OF OUTLET PIPE FROM DS 4-4 TO DS 4-6.	-					167.0	1.0	
DS 4-5	C1 10+11.91 39.9' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 56 LF OF OUTLET PIPE FROM DS 4-5 TO DS 4-3.	-					56.0	1.0	
DS 4-6	C 6+96.40 39.4' RT	CLEAN AND PERFORM VIDEO INSPECTION OF 200 LF OF PIPELINE FROM DS 4-6 TO DS 3-6. SEE PROPOSED DRAINAGE INSTALLATION TABLE ON MT-14 FOR PROPOSED WORK.	593.21					200.0		200.0
DS 4-7	C 4+09.80 24.9' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 221 LF OF OUTLET PIPE FROM DS 4-7 TO DS 4-6.	-					221.0	1.0	
DS 4-8	ML 39+55.80 317.8' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 231 LF OF OUTLET PIPE FROM DS 4-8 TO DS 5-3.	-					231.0	1.0	
DS 4-9	C 2+36.38 44.3' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 162 LF OF OUTLET PIPE FROM DS 4-9 TO DS 4-8.	-					162.0	1.0	
DS 4-10	B 3+50.55 19.1' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 49 LF OF OUTLET PIPE FROM DS 4-10 TO DS 4-11.	-					49.0	1.0	
DS 4-11	B 2+97.96 19.5' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 126 LF OF OUTLET PIPE FROM DS 4-11 TO DS 4-12.	-					126.0	1.0	
DS 4-12	D 5+88.92 48.1' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 241 LF OF OUTLET PIPE FROM DS 4-12 TO DS 3-7.	-					241.0	1.0	
DS 5-1	ML 38+52.12 53.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 83 LF OF OUTLET PIPE FROM DS 5-1 TO DS 3-4.	592.49				1.0	83.0		

DRAINAGE TABLE PAGE 1 OF 10 CONTINUED

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 5-2	ML 38+95.80 48.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 45 LF OF OUTLET PIPE FROM DS 5-2 TO DS 5-1.	591.55				1.0	45.0		
DS 5-3	ML 39+42.31 87.2' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 96 LF OF OUTLET PIPE FROM DS 5-3 TO DS 5-6.	-					96.0	1.0	
DS 5-4	ML 39+78.13 90.9' IT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 91 LF OF OUTLET PIPE FROM DS 5-4 TO DS 5-5.	-					91.0	1.0	
DS 5-5	ML 39+82.35 2.7' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	589.99	1.0		1.0				
DS 5-6	ML 39+82.80 2.7' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 95 LF OF OUTLET PIPE FROM DS 5-6 TO DS 5-8.	590.06	1.0				95.0		
DS 5-7	ML 40+47.59 58.0' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 51 LF OF OUTLET PIPE FROM DS 5-7 TO DS 5-8.	589.67				1.0	51.0		
DS 5-8	ML 40+48.29 6.7' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 51 LF OF OUTLET PIPE FROM DS 5-8 TO DS 5-12.	590.12	1.0				51.0		
DS 5-9	ML 40+59.27 3.6' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 44 LF OF OUTLET PIPE FROM DS 5-9 TO DS 5-13. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	589.89	1.0		1.0		44.0		
DS 5-10	ML 40+63.77 61.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 57 LF OF OUTLET PIPE FROM DS 5-10 TO DS 5-9.	589.52				1.0	57.0		
DS 5-11	ML 41+05.21 82.5' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 83 LF OF OUTLET PIPE FROM DS 5-11 TO DS 5-12.	-					83.0	1.0	
DS 5-12	ML 41+03.91 2.2' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 3 LF OF INLET PIPE FROM DS 5-12 TO DS 5-13.	589.91	1.0				3.0		
DS 5-13	ML 41+04.17 3.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 82 LF OF OUTLET PIPE FROM DS 5-13 TO DS 5-14. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	589.76	1.0		1.0		82.0		
DS 5-14	ML 41+27.54 79.1' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 5-15	ML 41+86.19 85.0' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 5-16	ML 42+20.17 82.8' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 5-17	ML 44+13.87 2.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 81 LF OF OUTLET PIPE FROM DS 5-17 TO DS 5-19.	591.76	1.0				81.0		
DS 5-18	ML 44+14.25 3.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	591.55	1.0		1.0				
DS 5-19	ML 44+12.11 80.7' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 191 LF OF OUTLET PIPE FROM DS 5-19 TO DS 5-16.	-					191.0	1.0	
DS 5-20	ML 44+29.31 90.9' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 20 LF OF OUTLET PIPE FROM DS 5-20 TO DS 5-19.	-					20.0	1.0	
DS 5-21	ML 44+39.62 80.6' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 252 LF OF OUTLET PIPE FROM DS 5-21 TO DS 5-15.	-					252.0	1.0	
DS 6-1	ML 46+54.50 4.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	592.84	1.0		1.0				
DS 6-2	ML 46+54.89 2.4' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 84 LF OF OUTLET PIPE FROM DS 6-2 TO DS STA. ML 46+63.31.	592.82	1.0				84.0		
DS 6-3	ML 46+63.35 82.1' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 252 LF OF OUTLET PIPE FROM DS 6-3 TO 5-21.	-					252.0	1.0	
DS 6-4	ML 44+39.62 80.6' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 252 LF OF OUTLET PIPE FROM DS 6-4 TO DS 5-23.	-					252.0	1.0	

CHECKED BY: J.KLEINMANN

DRAFTED BY: N.HOY

CHECKED BY: J.KLEINMANN

DESIGNED BY: P.THOMPSON

DESIGN SUPERVISOR: D.MARSH


ITEM	DESCRIPTION	UNITS
604.070501	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
604.070502	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - RECONSTRUCT ROOF SLAB	EA
604.070503	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - REMOVE CONDUIT & PATCH WALL	EA
604.070504	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF
621.04	CLEANING DRAINAGE STRUCTURES	EA
634.07060001	VIDEO INSPECTION OF PIPELINES	LF

ALTERED ON: \_\_\_\_\_

AFFIXED ON: **5/10/2024**



SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING MISCELLANEOUS TABLES	DRAWING NUMBER: MT-5

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DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 6-5	ML 49+23.96 84.4'	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 261 LF OF OUTLET PIPE FROM DS 6-5 TO DS 6-3.	-					261.0	1.0	
DS 6-6	ML 49+30.42 2.7' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	593.35	1.0						
DS 6-7	ML 49+31.01 3.3' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 66 LF OF OUTLET PIPE FROM DS 6-4 TO DS STA. ML 49+34.70. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	593.68	1.0		1.0		66.0		
DS 6-8	ML 49+34.71 69.3' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 275 LF OF OUTLET PIPE FROM DS 6-8 TO DS 6-1.	-					275.0	1.0	
DS 6-9	ML 51+97.65 2.9' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 75 LF OF OUTLET PIPE FROM DS 6-9 TO DS 6-12.	592.89	1.0				75.0		
DS 6-10	ML 51+99.74 3.2' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	594.21	1.0		1.0				
DS 6-11	ML 52+01.12 69.3' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 263 LF OF OUTLET PIPE FROM DS 6-11 TO DS 6-8.	-					263.0	1.0	
DS 6-12	ML 52+02.43 75.5' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 283 LF OF OUTLET PIPE FROM DS 6-12 TO DS 6-5.	-					283.0	1.0	
DS 7-1	ML 54+37.24 3.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	593.32	1.0		1.0				
DS 7-2	ML 54+37.85 3.0' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 75 LF OF OUTLET PIPE FROM DS 7-2 TO DS STA. ML 54+35.37.	591.60	1.0				75.0		
DS 7-3	ML 54+34.49 66.1' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 231 LF OF OUTLET PIPE FROM DS 7-3 TO DS 6-11.	-					231.0	1.0	
DS 7-4	ML 54+35.34 77.9' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 236 LF OF OUTLET PIPE FROM DS 7-4 TO DS 6-12.	-					236.0	1.0	
DS 7-5	ML 56+50.49 74.5' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 219 LF OF OUTLET PIPE FROM DS 7-5 TO DS 7-4.	-					219.0	1.0	
DS 7-6	ML 56+43.30 67.6' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 206 LF OF OUTLET PIPE FROM DS 7-6 TO DS 7-3.	-					206.0	1.0	
DS 7-7	ML 56+50.59 3.6' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	592.48	1.0		1.0				
DS 7-8	ML 56+51.17 2.8' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 75 LF OF OUTLET PIPE FROM DS 7-8 TO DS 7-5.	591.60	1.0				75.0		
DS 7-9	ML 58+66.98 74.3' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 220 LF OF OUTLET PIPE FROM DS 7-9 TO DS 7-5.	-					220.0	1.0	
DS 7-10	ML 58+77.60 74.0' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 11 LF OF OUTLET PIPE FROM DS 7-10 TO DS 7-9.	-					11.0	1.0	
DS 7-11	ML 58+87.70 2.7' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 75 LF OF OUTLET PIPE FROM DS 7-11 TO DS 7-10.	589.06	1.0				75.0		

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 7-12	ML 58+87.71 3.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	591.00	1.0				1.0		
DS 7-13	ML 59+04.98 70.4' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 258 LF OF OUTLET PIPE FROM DS 7-13 TO DS 7-6.	-					258.0	1.0	
DS 7-14	ML 59+21.32 3.6' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 29 LF OF OUTLET PIPE FROM DS 7-14 TO 7-11.	589.19	1.0				29.0		
DS 7-15	ML 59+36.39 54.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 35 LF OF OUTLET PIPE FROM DS 7-8 TO DS 7-13.	589.91				1.0	35.0		
DS 7-16	ML 59+70 4.9' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	589.91	1.0				1.0		
DS 7-17	ML 60+17.30 50.5' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 83 LF OF INLET PIPE FROM DS 8-1 TO DS 7-17 AND 80 LF OF OUTLET PIPE FROM DS 7-17 TO DS 7-15.	589.97				1.0	163.0		
DS 7-18	ML 60+15.42 67.9' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 77 LF OF INLET PIPE FROM DS 8-2 TO DS 7-18 AND 140 LF OF OUTLET PIPE FROM DS 7-18 TO DS 7-10.	-					217.0	1.0	
DS 7-19	ML 60+36.20 3.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	590.95	1.0				1.0		
DS 7-20	ML 60+36.45 2.5' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 72 LF OF OUTLET PIPE FROM DS 7-20 TO DS 7-18.	589.37	1.0				72.0		
DS 8-1	ML 60+98.22 75.2' LT	REFER TO DS 7-17.	-							
DS 8-2	ML 60+90.34 79.7' RT	REFER TO DS 7-18	-				1.0			
DS 8-3	ML 62+86.47 3.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 83 LF OF OUTLET PIPE FROM DS 8-3 TO DS 8-5. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	592.55	1.0				83.0		
DS 8-4	ML 62+88.52 2.8' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	591.41	1.0						
DS 8-5	ML 62+92.08 83.1' LT	REFER TO DS 8-3.	-							
DS 8-6	ML 65+84.13 2.4' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	593.47	1.0						
DS 8-7	ML 65+84.42 3.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 74 LF OF OUTLET PIPE FROM DS 8-7 TO DS 8-8. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	593.97	1.0				1.0	74.0	
DS 8-8	ML 65+89.97 74.2' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 12 LF OF OUTLET PIPE FROM DS 8-8 TO DS 8-9.	-					12.0	1.0	
DS 8-9	ML 65+88.09 85.9' LT	REFER TO DS 8-8.	-							
DS 9-1	ML 68+89.62 1.5' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	595.47	1.0						
DS 9-2	ML 68+89.94 3.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 73 LF OF OUTLET PIPE FROM DS 9-2 TO DS 9-3. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	595.46	1.0				1.0	73.0	
DS 9-3	ML 68+98.78 73.3' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 9 LF OF OUTLET PIPE FROM DS 9-3 TO DS 9-4.	-					9.0	1.0	
DS 9-4	ML 68+98.95 82.0' LT	REFER TO DS 9-3.	-							
DS 9-5	ML 70+70.00 3.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 77 LF OF OUTLET PIPE FROM DS 9-5 TO DS 9-7. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	596.41	1.0				1.0	77.0	
DS 9-6	ML 70+70.34 1.6' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	596.37	1.0						

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ITEM	DESCRIPTION	UNITS
604.070501	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
604.070502	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - RECONSTRUCT ROOF SLAB	EA
604.070503	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - REMOVE CONDUIT & PATCH WALL	EA
604.070504	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF
621.04	CLEANING DRAINAGE STRUCTURES	EA
634.07060001	VIDEO INSPECTION OF PIPELINES	LF

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 90.7 TO MP 90.4.2	DATE: MAY 2024
TITLE OF DRAWING MISCELLANEOUS TABLES		DRAWING NUMBER: MT-6

CHECKED BY: J. KLEINMANN  
 DRAFTED BY: N. HOY  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: P. THOMPSON  
 DESIGN SUPERVISOR: D. MARSH

DRAINAGE TABLE PAGE 3 OF 10

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 9-7	ML 70+72.15 77.8' LT	REFER TO DS 9-5.	-							
DS 9-8	E 4+97.53 24.3' RT	REFER TO DS 9-9.	-							
DS 9-9	ML 73+40.26 86.42 LT	CLEAN 75 LF OF PIPE FROM DS 9-9 TO DS 9-8.	-					75.0		
DS 9-10	ML 73+47.51 73.8' LT	REFER TO DS 9-10.	-							
DS 9-11	ML 73+47.77 2.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	597.75	1.0						
DS 9-12	ML 73+47.98 3.3' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 70 LF OF OUTLET PIPE FROM DS 9-12 TO DS 9-10. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	597.80	1.0	1.0			70.0		
DS 10-1	ML 75+87.82 2.9' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 75 LF OF OUTLET PIPE FROM DS 10-1 TO DS 10-3. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	599.00	1.0		1.0		75.0		
DS 10-2	ML 75+89.46 2.9' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	598.96	1.0						
DS 10-3	ML 75+88.69 74.3' LT	REFER TO DS 10-1.	-							
DS 10-4	F 4+62.92 39.1' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 13 LF OF OUTLET PIPE FROM DS 10-4 TO DS 10-8.	-					13.0	1.0	
DS 10-5	ML 78+51.10 2.9' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 86 LF OF OUTLET PIPE FROM DS 10-5 TO DS 10-7. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	600.82	1.0		1.0		86.0		
DS 10-6	ML 78+51.27 2.2' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	601.62	1.0						
DS 10-7	ML 78+51.08 86.7' LT	REFER TO DS 10-5.	-							
DS 10-8	F 4+73.50 39.6' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 67 LF OF INLET PIPE FROM DS 10-13 TO DS 10-8 AND 72 LF OF OUTLET PIPE FROM DS 10-8 TO 10-10.	-					139.0	1.0	
DS 10-9	F 5+64.97 15.2' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 5 LF OF OUTLET PIPE.	-				1.0	5.0		
DS 10-10	F 4+67.82 33.2' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 100 LF OF OUTLET PIPE FROM DS 10-10 TO DS 10-9.	-					100.0	1.0	
DS 10-11	ML 79+42.17 53.1' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 65 LF OF OUTLET PIPE FROM DS 10-11 TO DS 10-12.	601.76				1.0	65.0		
DS 10-12	G 3+53.57 33.1' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 292 LF OF OUTLET PIPE FROM DS 10-12 TO DS 11-4	-					292.0	1.0	
DS 10-13	E 1+99.81 22.7' RT	REFER TO DS 10-8.	-							
DS 11-1	ML 81+40.45 2.8' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	605.36	1.0						

DRAINAGE TABLE PAGE 3 OF 10 CONTINUED

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 11-2	ML 81+41.07 2.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 100 LF OF OUTLET PIPE FROM DS 11-2 TO DS 11-3. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	604.91	1.0		1.0		100.0		
DS 11-3	ML 81+43.91 98.5' LT	REFER TO DS 11-2.	-							
DS 11-4	G 6+44.91 16.3' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 176 LF OF OUTLET PIPE FROM DS 11-4 TO DS 11-10.	-						176.0	1.0
DS 11-5	G 7+02.38 36.2' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 147 LF OF OUTLET PIPE FROM DS 11-5 TO DS 11-11.	-						147.0	1.0
DS 11-6	ML 83+79.19 48.8' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 80 LF OF OUTLET PIPE FROM DS 11-3 TO DS 11-11.	607.55				1.0	80.0		
DS 11-7	ML 83+80.51 2.4' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 48 LF OF OUTLET PIPE FROM DS 11-7 TO DS 11-6.	608.37	1.0				48.0		
DS 11-8	ML 83+81.04 2.8' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	608.31	1.0		1.0				
DS 11-9	ML 83+82.70 49.5' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 47 LF OF OUTLET PIPE FROM DS 11-9 TO DS 11-8.	607.65				1.0	47.0		
DS 11-10	G 8+18.66 25.3' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 81 LF OF OUTLET PIPE FROM DS 11-10 TO DS 11-11.	-					81.0	1.0	
DS 11-11	G 8+51.60 48.7' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 176 LF OF OUTLET PIPE FROM DS 11-11 TO DS 11-12.	-					176.0	1.0	
DS 11-12	G 10+27.37 44.9' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF SOUTH INLET PIPE AND CLEAN 5 LF OF SOUTHWEST OUTLET PIPE.	-					5.0	1.0	
DS 12-1	ML 88+12.49 3.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	613.28	1.0		1.0				
DS 12-2	ML 88+12.55 2.5' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 278 LF OF INLET PIPE FROM DS 12-6 TO DS 12-2 AND CLEAN 115 LF OF OUTLET PIPE FROM DS 12-2 TO 12-3.	612.94	1.0				393.0		
DS 12-3	H 1+52.55 18.7' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 51 LF OF OUTLET PIPE FROM DS 12-3 TO DS 12-4.	-					51.0	1.0	
DS 12-4	H 1+74.51 26.7' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 256 LF OF OUTLET PIPE FROM DS 12-4 TO DS 12-8.	-					256.0	1.0	
DS 12-5	ML 90+90.16 2.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	615.76	1.0		1.0				
DS 12-6	ML 90+90.24 3.2' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 248 LF OF INLET PIPE FROM DS 12-6 TO DS 12-10.	614.53	1.0				248.0		
DS 12-7	H 4+29.81 37.2' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 68 LF OF OUTLET PIPE FROM DS 12-7 TO DS 12-8.	-					68.0	1.0	
DS 12-8	H 4+31.344 30.3' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 249 LF OF OUTLET PIPE FROM DS 12-8 TO DS 12-12.	-					249.0	1.0	
DS 12-9	ML 93+39.01 2.7' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	618.01	1.0		1.0				
DS 12-10	ML 93+39.04 2.7' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 202 LF OF INLET PIPE FROM DS 12-10 TO DS 12-14.	616.06	1.0				202.0		
DS 12-11	H 6+81.95 42.2' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 77 LF OF OUTLET PIPE FROM DS 12-11 TO DS 12-12.	-					77.0	1.0	
DS 12-12	H 6+79.23 35.1' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 193 LF OF OUTLET PIPE FROM DS 12-12 TO DS 12-16.	-					193.0	1.0	
DS 12-13	ML 95+39.71 2.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 80 LF OF OUTLET PIPE FROM DS 12-13 TO DS 12-15.	618.49	1.0				80.0		

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ITEM	DESCRIPTION	UNITS
604.070501	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
604.070502	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - RECONSTRUCT ROOF SLAB	EA
604.070503	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - REMOVE CONDUIT & PATCH WALL	EA
604.070504	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF
621.04	CLEANING DRAINAGE STRUCTURES	EA
634.07060001	VIDEO INSPECTION OF PIPELINES	LF

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.






TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING MISCELLANEOUS TABLES	DRAWING NUMBER: MT-7

ALTERED ON: 5/10/2024

AFFIXED ON:

SIGNATURE STAMP:






DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 12-14	ML 95+39.79 3.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	619.71	1.0		1.0				
DS 12-15	H 8+86.75 32.6' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 82 LF OF OUTLET PIPE FROM DS 12-15 TO DS 12-16.	-					82.0	1.0	
DS 12-16	H 8+67.61 46.9' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 221 LF OF OUTLET PIPE FROM DS 12-16 TO DS 13-5.	-					221.0	1.0	
DS 13-1	H 10+40.65 4.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 85 LF OF OUTLET PIPE FROM DS 13-1 TO DS 13-5.	617.19				1.0	85.0		
DS 13-2	ML 97+37.62 3.3' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 5 LF OF INLET PIPE FROM DS 13-2 TO DS 13-3. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	620.98	1.0		1.0		5.0		
DS 13-3	ML 97+38.00 2.1' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 88 LF OF OUTLET PIPE FROM DS 13-3 TO DS 13-1.	621.06	1.0				88.0		
DS 13-4	ML 97+38.82 49.7' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 45 LF OF OUTLET PIPE FROM DS 13-4 TO DS 13-2.	622.26				1.0	45.0		
DS 13-5	H 10+91.22 75.6' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 255 LF OF OUTLET PIPE FROM DS 13-5 TO DS 13-7.	-					255.0	1.0	
DS 13-6	ML 99+89.27 50.4' RT	CLEAN EXISTING DRAINAGE STRUCTURE.	-						1.0	
DS 13-7	ML 100+35.02 98.7' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 14-1	ML 108+86.59 47.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 70 LF OF OUTLET PIPE FROM DS 14-1 TO DS 14-4.	620.49				1.0	70.0		
DS 14-2	ML 108+86.66 4.2' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	620.91	1.0		1.0				
DS 14-3	ML 108+87.22 1.7' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 48 LF OF OUTLET PIPE FROM DS 14-3 TO DS 14-1.	621.37	1.0				48.0		
DS 14-4	ML 108+88.54 117.5' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 198 LF OF OUTLET PIPE FROM DS 14-4 TO DS 14-5.	-					198.0	1.0	
DS 14-5	ML 106+87.20 105.3' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 15-1	ML 110+46.38 112.8' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF INLET PIPE TO SOUTHEAST AND CLEAN 156 LF OF OUTLET PIPE FROM DS 15-1 TO DS 14-5.	-					161.0	1.0	
DS 15-2	ML 110+60.77 48.4' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 64 LF OF OUTLET PIPE FROM DS 15-1 TO STA. ML 110+46.30.	616.36				1.0	64.0		
DS 15-3	ML 110+94.01 2.7' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 57 LF OF OUTLET PIPE FROM DS 15-3 TO DS 15-2.	616.58	1.0				57.0		
DS 15-4	ML 110+94.39 3.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	616.13	1.0		1.0				
DS 15-5	ML 114+35.85 102.5' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 234 LF OF OUTLET PIPE FROM DS 15-5 TO DS 15-19.	-					234.0	1.0	

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 15-6	ML 114+36.28 2.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	608.57	1.0		1.0				
DS 15-7	ML 114+36.31 3.2' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 50 LF OF OUTLET PIPE FROM DS 15-7 TO DS 15-8.	608.63	1.0					50.0	
DS 15-8	ML 114+36.66 49.5' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 52 LF OF OUTLET PIPE FROM DS 15-8 TO DS 15-5.	607.21						1.0	52.0
DS 15-9	ML 115+81.83 50.1' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 50 LF OF OUTLET PIPE FROM DS 15-9 TO DS 15-11.	605.43						1.0	50.0
DS 15-10	ML 115+82.36 3.2' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 50 LF OF OUTLET PIPE FROM DS 15-10 TO DS 15-14.	606.90	1.0						50.0
DS 15-11	ML 116+31.18 49.4' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 50 LF OF OUTLET PIPE FROM DS 15-11 TO DS 15-15.	605.32						1.0	50.0
DS 15-12	ML 116+31.28 2.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	606.80	1.0		1.0				
DS 15-13	ML 116+31.42 48.7' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 50 LF OF OUTLET PIPE FROM DS 15-13 TO DS 15-12.	606.62						1.0	50.0
DS 15-14	ML 116+31.85 3.4' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 52 LF OF OUTLET PIPE FROM DS 15-14 TO DS 15-17.	606.81	1.0						52.0
DS 15-15	ML 116+80.34 49.6' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 52 LF OF OUTLET PIPE FROM DS 15-15 TO DS 15-19.	605.45						1.0	52.0
DS 15-16	ML 116+82.90 2.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	606.70	1.0		1.0				
DS 15-17	ML 116+83.16 3.0' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 50 LF OF OUTLET PIPE FROM DS 15-17 TO DS 15-15.	606.88	1.0						50.0
DS 15-18	ML 116+83.64 48.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 52 LF OF OUTLET PIPE FROM DS 15-18 TO DS 15-13.	606.55						1.0	52.0
DS 15-19	ML 116+69.78 100.7' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 232 LF OF OUTLET PIPE FROM DS 15-19 TO DS 16-7.	-							232.0
DS 16-1	ML 117+44.02 48.5' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 60 LF OF OUTLET PIPE FROM DS 16-1 TO DS 15-18.	606.75						1.0	60.0
DS 16-2	ML 117+45.97 2.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 65 LF OF OUTLET PIPE FROM DS 16-2 TO DS 15-15. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	606.91	1.0		1.0				65.0
DS 16-3	ML 118+22.04 49.5' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 66 LF OF OUTLET PIPE FROM DS 16-3 TO DS 16-6.	607.61						1.0	66.0
DS 16-4	ML 118+60.42 48.9' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 56 LF OF OUTLET PIPE FROM DS 16-4 TO DS 16-7.	606.96						1.0	56.0
DS 16-5	ML 118+67.26 2.5' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 48 LF OF OUTLET PIPE FROM DS 16-5 TO DS 16-4.	608.29	1.0						48.0
DS 16-6	ML 118+67.48 2.9' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	607.72	1.0		1.0				
DS 16-7	ML 119+05.18 82.6' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 85 LF OF OUTLET PIPE FROM DS 16-7 TO DS 16-8.	-							85.0
DS 16-8	ML 119+92.03 83.4' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-							5.0
DS 16-9	J 10+77.57 37.3' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 57 LF OF OUTLET PIPE FROM DS 16-9 TO DS 16-10.	-							57.0

ITEM	DESCRIPTION	UNITS
604.070501	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
604.070502	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - RECONSTRUCT ROOF SLAB	EA
604.070503	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - REMOVE CONDUIT & PATCH WALL	EA
604.070504	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF
621.04	CLEANING DRAINAGE STRUCTURES	EA
634.07060001	VIDEO INSPECTION OF PIPELINES	LF

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_




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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY** | Thruway Authority

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 TITLE OF DRAWING: MISCELLANEOUS TABLES

CONTRACT NUMBER: TAN 24-12  
 DATE: MAY 2024  
 DRAWING NUMBER: MT-8



CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

DRAINAGE TABLE PAGE 5 OF 10

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 16-10	J 10+88.36 18.7' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 79 LF OF OUTLET PIPE FROM DS 16-10 TO DS 16-11.	-					79.0	1.0	
DS 16-11	J 10+00.64 28.2' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 240 LF OF OUTLET PIPE FROM DS 16-11 TO DS 16-12.	-					240.0	1.0	
DS 16-12	J 7+56.46 53.2' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 111 LF OF OUTLET PIPE FROM DS 16-12 TO DS 16-13.	-					111.0	1.0	
DS 16-13	J 7+20.80 52.5' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 16-14	K 1+99.19 27.3' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 16-15	J 4+94.04 63.8' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 250 LF OF OUTLET PIPE FROM DS 16-15 TO DS 16-12.	-					250.0	1.0	
DS 17-1	J 3+72.666 68.9' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 112 LF OF OUTLET PIPE FROM DS 17-1 TO DS 16-15.	-					112.0	1.0	
DS 17-2	K 2+64.55 26.5' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 67 LF OF OUTLET PIPE FROM DS 17-2 TO DS 16-14.	-					67.0	1.0	
DS 17-3	K 3+83.64 21.6' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 119 LF OF OUTLET PIPE FROM DS 17-3 TO DS 17-2.	-					119.0	1.0	
DS 17-4	ML 126+39.10 4.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	609.54	1.0		1.0				
DS 17-5	ML 126+39.67 2.2' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 233 LF OF OUTLET PIPE FROM DS 17-5 TO DS 17-8.	610.18	1.0				233.0		
DS 17-6	K 6+38.12 4.0' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 21 LF OUTLET PIPE FROM DS 17-6 TO DS 17-10.	-				1.0	21.0		
DS 17-7	ML 128+72.97 3.8' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	608.17	1.0		1.0				
DS 17-8	ML 128+73.18 2.2' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 49 LF OF OUTLET PIPE FROM DS 17-8 TO DS 17-9.	608.65	1.0				49.0		
DS 17-9	ML 128+73.68 48.2' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 31 LF OF OUTLET PIPE FROM DS 17-9 TO DS 17-6.	606.94				1.0	31.0		
DS 17-10	K 6+40.89 24.4' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 257 LF OF OUTLET PIPE FROM DS 17-10 TO DS 17-3.	-					257.0	1.0	
DS 17-11	ML 131+09.44 1.8' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 60 LF OF OUTLET PIPE FROM DS 17-11 TO DS 17-13.	606.78	1.0				60.0		
DS 17-8	ML 131+09.49 2.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	606.79	1.0		1.0				
DS 17-13	131+09.01 60.6' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 42 LF OF OUTLET PIPE FROM DS 17-13 TO DS 17-14.	-					42.0	1.0	
DS 17-14	ML 131+04.63 101.8' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF INLET PIPE FROM WEST AND CLEAN 222 LF OF OUTLET PIPE FROM DS 17-14 TO DS 17-10.	-					227.0	1.0	
DS 18-1	ML 133+44.87 58.7' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 22 LF OF OUTLET PIPE FROM DS 18-1 TO DS 18-5.	604.24				1.0	22.0		

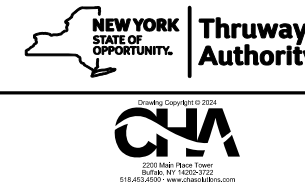
ITEM	DESCRIPTION	UNITS
604.070501	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
604.070502	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - RECONSTRUCT ROOF SLAB	EA
604.070503	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - REMOVE CONDUIT & PATCH WALL	EA
604.070504	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF
621.04	CLEANING DRAINAGE STRUCTURES	EA
634.07060001	VIDEO INSPECTION OF PIPELINES	LF

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

DRAINAGE TABLE PAGE 5 OF 10 CONTINUED

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 18-2	ML 133+53.63 1.9' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 59 LF OF OUTLET PIPE FROM DS 18-2 TO DS 18-1.	605.30	1.0				59.0		
DS 18-3	ML 133+53.73 2.8' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	605.32	1.0		1.0				
DS 18-4	ML 133+63.93 48.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 49 LF OF OUTLET PIPE FROM DS 18-4 TO DS 18-3.	604.09				1.0	49.0		
DS 18-5	ML 133+32.49 76.1' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 255 LF OF OUTLET PIPE FROM DS 18-5 TO DS 18-9.	-					255.0	1.0	
DS 18-6	ML 135+85.21 59.5' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 28 LF OF OUTLET PIPE FROM DS 18-6 TO DS 18-9.	602.99				1.0	28.0		
DS 18-7	ML 135+86.85 2.4' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 58 LF OF OUTLET PIPE FROM DS 18-7 TO DS 18-6.	604.27	1.0				58.0		
DS 18-8	ML 135+86.96 2.9' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	604.30	1.0		1.0				
DS 18-9	ML 135+87.80 86.8' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 271 LF OF OUTLET PIPE FROM DS 18-9 TO DS 19-4.	-					271.0	1.0	
DS 19-1	ML 138+50.87 54.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 34 LF OF OUTLET PIPE FROM DS 19-1 TO DS 19-4.	603.89				1.0	34.0		
DS 19-2	ML 138+51.21 2.0' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 52 LF OF OUTLET PIPE FROM DS 19-2 TO DS 19-1.	605.34	1.0				52.0		
DS 19-3	ML 138+51.26 2.9' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	605.37	1.0		1.0				
DS 19-4	ML 138+57.96 87.6' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 321 LF OF OUTLET PIPE FROM DS 19-4 TO DS 19-7.	-					321.0	1.0	
DS 19-5	ML 140+31.66 49.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 40 LF OF OUTLET PIPE FROM DS 19-5 TO DS 19-6.	605.42				1.0	40.0		
DS 19-6	ML 140+35.13 90.0' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 180 LF OF OUTLET PIPE FROM DS 19-6 TO DS 19-13.	-					180.0	1.0	
DS 19-7	140+78.55 78.2' RT	REFER TO DS 19-4.	-							
DS 19-8	ML 145+03.49 2.7' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 44 LF OF OUTLET PIPE FROM DS 19-8 TO DS 19-11.	605.39	1.0				44.0		
DS 19-9	ML 145+03.49 2.6' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	605.52	1.0		1.0				
DS 19-10	ML 145+03.98 49.2' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 51 LF OF OUTLET PIPE FROM DS 19-10 TO DS 19-9.	604.73				1.0	51.0		
DS 19-11	ML 145+04.70 49.5' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 32 LF OF OUTLET PIPE FROM DS 19-11 TO DS 19-12.	604.36				1.0	32.0		
DS 19-12	ML 144+97.15 80.0' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 19-13	ML 142+13.89 79.9' LT	REFER TO DS 19-6.	-							
DS 20-1	ML 147+86.82 49.0' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN AND PERFORM VIDEO INSPECTION OF 34 LF OF OUTLET PIPE FROM DS 20-1 TO DS 20-13.	601.96				1.0	34.0		34.0
DS 20-2	ML 147+87.43 49.2' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN AND PERFORM VIDEO INSPECTION OF 49 LF OF OUTLET PIPE FROM DS 20-2 TO DS 20-4.	602.32				1.0	49.0		49.0
DS 20-3	ML 147+90.25 3.5' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN AND PERFORM VIDEO INSPECTION OF 50 LF OF OUTLET PIPE FROM DS 20-3 TO DS 20-1.	603.04	1.0				50.0		50.0
DS 20-4	ML 147+90.51 2.3' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	603.03	1.0		1.0				

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING MISCELLANEOUS TABLES	DRAWING NUMBER: MT-9

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ALTERED ON: \_\_\_\_\_

AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_

DRAINAGE TABLE PAGE 6 OF 10

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 20-5	ML 150+78.67 48.8' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN AND PERFORM VIDEO INSPECTION OF 26 LF OF OUTLET PIPE FROM DS 20-5 TO DS 20-7.	599.78				1.0	26.0		26.0
DS 20-6	ML 150+81.48 49.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 23 LF OF OUTLET PIPE DS 20-6 TO DS 20-8.	600.06				1.0	23.0		
DS 20-7	ML 151+04.29 49.2' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN AND PERFORM VIDEO INSPECTION OF 63 LF OF OUTLET PIPES FROM DS 20-7 TO DS 20-15.	599.71				1.0	63.0		63.0
DS 20-8	ML 151+04.58 49.5' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN AND PERFORM VIDEO INSPECTION OF 52 LF OF OUTLET PIPES FROM DS 20-8 TO DS 20-9.	600.12				1.0	52.0		52.0
DS 20-9	ML 151+04.59 2.8' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	600.81	1.0		1.0				
DS 20-10	ML 151+04.61 3.1' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN AND PERFORM VIDEO INSPECTION OF 46 LF OF OUTLET PIPE FROM DS 20-10 TO DS 20-7.	600.65	1.0				46.0		46.0
DS 20-11	ML 151+16.66 49.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 13 LF OF OUTLET PIPE FROM DS 20-11 TO DS 20-8.	600.14				1.0	13.0		
DS 20-12	ML 151+17.34 49.1' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN AND PERFORM VIDEO INSPECTION OF 14 LF OF OUTLET PIPE FROM DS 20-12 TO DS 20-7.	599.75				1.0	14.0		14.0
DS 20-13	ML 147+80.47 81.9' RT	CLEAN AND PERFORM VIDEO INSPECTION OF 30 LF OF OUTLET PIPE FROM DS 20-13 TO DS 20-14.	-					30.0		30.0
DS 20-14	ML 147+83.62 112.7' RT	REFER TO DS 20-13.	-							
DS 20-15	ML 150+55.47 89.5' RT	REFER TO DS 20-7.	-							
DS 21-1	ML 154+61.18 48.2' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 52 LF OF OUTLET PIPE FROM DS 21-1 TO DS 21-11.	605.86				1.0	52.0		
DS 21-2	ML 154+61.14 48.5' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 51 LF OF OUTLET PIPE FROM DS 21-2 TO DS 21-4.	606.05				1.0	51.0		
DS 21-3	ML 154+73.02 4.0' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 50 LF OF OUTLET PIPE FROM DS 21-3 TO DS 21-1.	606.64	1.0				50.0		
DS 21-4	ML 154+73.75 2.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	606.62	1.0		1.0				
DS 21-5	ML 156+97.92 51.9' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 53 LF OF OUTLET PIPE FROM DS 21-5 TO DS 21-7.	611.22				1.0	53.0		
DS 21-6	ML 156+99.53 49.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 66 LF OF OUTLET PIPE DS 21-12.	611.01				1.0	66.0		
DS 21-7	ML 157+06.20 2.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	611.75	1.0		1.0				
DS 21-8	ML 157+06.54 3.5' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 49 LF OF OUTLET PIPE FROM DS 21-8 TO DS 21-6.	611.77	1.0				49.0		

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

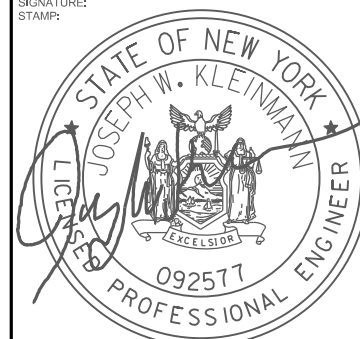
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ITEM	DESCRIPTION	UNITS
604.070501	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
604.070502	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - RECONSTRUCT ROOF SLAB	EA
604.070503	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - REMOVE CONDUIT & PATCH WALL	EA
604.070504	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF
621.04	CLEANING DRAINAGE STRUCTURES	EA
634.07060001	VIDEO INSPECTION OF PIPELINES	LF

ALTERED ON: 5/10/2024

AFFIXED ON: 5/10/2024

SIGNATURE: STAMP: 

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DRAINAGE TABLE PAGE 6 OF 10 CONTINUED

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 21-9	ML 158+80.54 65.5' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN AND PERFORM VIDEO INSPECTION OF 200 LF OF PIPE FROM DS 21-9 TO DS 21-10.	-					200.0	1.0	200.0
DS 21-10	ML 158+60.87 127.7' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 161 LF OF OUTLET PIPE FROM DS 21-10 TO DS 21-12.	-					161.0	1.0	
DS 21-11	ML 154+41.36 95.9' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 21-12	ML 157+00.30 114.5' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 259 LF OF OUTLET PIPE FROM DS 21-12 TO DS 21-11.	-					259.0	1.0	
DS 22-1	ML 164+25.87 51.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 68 LF OF OUTLET PIPE FROM DS 22-1 TO DS 22-4.	611.53				1.0	68.0		
DS 22-2	ML 164+26.69 2.9' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 54 LF OF OUTLET PIPE FROM DS 22-2 TO DS 22-1. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	612.58	1.0		1.0		54.0		
DS 22-3	ML 164+50.36 57.9' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 59 LF OF OUTLET PIPE FROM DS 22-3 TO DS 22-2.	612.35				1.0	59.0		
DS 22-4	ML 164+28.39 119.4' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 202 LF OF OUTLET PIPE FROM DS 22-4 TO DS 22-8.	-					202.0	1.0	
DS 22-5	M 3+11.58 35.8' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 106 LF OF OUTLET PIPE FROM DS 22-5 TO DS 22-9.	-					106.0	1.0	
DS 22-6	ML 166+22.66 2.7' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 58 LF OF OUTLET PIPE FROM DS 22-6 TO DS 22-7. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	609.83	1.0		1.0		58.0		
DS 22-7	ML 166+23.65 56.4' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 68 LF OF OUTLET PIPE FROM DS 22-5 TO DS 22-8.	607.90				1.0	68.0		
DS 22-8	ML 166+33.64 122.4' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 205 LF OF OUTLET PIPE FROM DS 22-8 TO DS 22-13.	-					205.0	1.0	
DS 22-9	M 3+23.69 70.3' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 217 LF OF OUTLET PIPE FROM DS 22-9 TO DS 23-1.	-					217.0	1.0	
DS 22-10	ML 168+13.78 2.6' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	607.01	1.0		1.0				
DS 22-11	ML 168+14.38 4.0' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 57 LF OF OUTLET PIPE FROM DS 22-11 TO DS 22-12.	606.92	1.0				57.0		
DS 22-12	ML 168+36.79 53.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 60 LF OF OUTLET PIPE FROM DS 22-12 TO DS 22-13.	605.35				1.0	60.0		
DS 22-13	ML 168+44.65 112.9' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 23-1	L 1 5+06.53 69.1' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 116 LF OF OUTLET PIPE FROM DS 23-1 TO DS 23-4.	-					116.0	1.0	
DS 23-2	L 3+48.55 39.1' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 40 LF OF OUTLET PIPE FROM DS 23-2 TO DS 23-3.	-					40.0	1.0	
DS 23-3	L 1 3+38.72 2.8' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 43 LF OF OUTLET PIPE FROM DS 23-3 TO DS 23-4.	-				1.0	43.0		
DS 23-4	L 1 3+25.82 44.5' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 165 LF OF OUTLET PIPE FROM DS 23-4 TO DS 23-5.	-					165.0	1.0	
DS 23-5	L 1 1+37.71 62.9' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 23-6	L 1 1+30.59 52.7' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 12 LF OF OUTLET PIPE FROM DS 23-6 TO DS 23-5.	-				1.0	12.0		
DS 23-7	L 1 1+30.34 56.8' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 111 LF OF OUTLET PIPE FROM DS 23-7 TO DS 23-6.	-					111.0		

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DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY  
Thruway Authority

2025 Mid-Range Tower  
Buffalo, NY 14203-3723  
518.652.4800 - www.thruwayauthority.com

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
TITLE OF DRAWING: MISCELLANEOUS TABLES

CONTRACT NUMBER: TAN 24-12  
DATE: MAY 2024  
DRAWING NUMBER: MT-10

DRAINAGE TABLE PAGE 7 OF 10

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 23-8	L1 1+44.79 77.9' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 28 LF OF OUTLET PIPE FROM DS 23-8 TO DS 23-9.	-					28.0	1.0	
DS 23-9	L1 1+17.35 77.8' RT	REFER TO DS 23-8.	-							
DS 23-10	01 1+67.80 18.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 5 LF OF BOTH INLET AND OUTLET PIPE.	-				1.0	5.0		
DS 23-11	01 1+66.84 41.4' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 12-12	01 2+06.50 4.5' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 41 LF OF OUTLET PIPE FROM DS 23-12 TO DS 23-10.	-				1.0	41.0		
DS 23-13	01 3+04.62 42.3' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 131 LF OF OUTLET PIPE FROM DS 23-13 TO DS 12-11.	-					131.0	1.0	
DS 23-14	0 2+99.02 3.0' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 44 LF OF OUTLET PIPE FROM DS 23-14 TO DS 23-13.	-				1.0	44.0		
DS 23-15	N 4+95.40 34.3' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 131 LF OF OUTLET PIPE FROM DS 23-15 TO DS 24-18.	-					131.0	1.0	
DS 24-1	ML 170+43.95 3.6' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 34 LF OF OUTLET PIPE FROM DS 24-1 TO DS 24-6.	604.19	1.0				34.0		
DS 24-2	ML 170+44.82 3.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	604.24	1.0	1.0					
DS 24-3	ML 170+45.36 54.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 35 LF OF OUTLET PIPE FROM DS 24-3 TO DS 24-7.	604.88				1.0	35.0		
DS 24-4	ML 170+78.71 59.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 60 LF OF OUTLET PIPE FROM DS 24-4 TO DS 24-6.	602.46				1.0	60.0		
DS 24-5	ML 170+79.07 3.9' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	604.12	1.0						
DS 24-6	ML 170+79.51 2.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 55 LF OF OUTLET PIPE FROM DS 24-6 TO DS 24-7. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	604.15	1.0	1.0			55.0		
DS 24-7	ML 170+79.59 55.6' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 50 LF OF OUTLET PIPES FROM DS 24-7 FROM DS 24-16.	604.80				1.0	50.0		
DS 24-8	ML 171+41.50 59.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 62 LF OF OUTLET PIPE FROM DS 24-8 TO DS 24-7.	604.71				1.0	62.0		
DS 24-9	ML 171+43.49 3.2' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 66 LF OF OUTLET PIPE FROM DS 24-9 TO DS 24-6.	604.09	1.0				66.0		
DS 24-10	ML 171+45.17 2.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	604.29	1.0		1.0				
DS 24-11	ML 172+86.77 2.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 140 LF OF INLET PIPE TO DS 24-11 TO DS 25-10. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	606.08	1.0		1.0		140.0		

DRAINAGE TABLE PAGE 7 OF 10 CONTINUED

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 24-12	ML 174+42.49 2.3' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 168 LF OF INLET PIPE TO DS 24-12 TO DS 25-4. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	608.34	1.0				1.0		168.0
DS 24-13	ML 174+42.91 3.0' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	607.82	1.0						
DS 24-14	ML 174+43.09 49.7' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 50 LF OF OUTLET PIPE FROM DS 24-14 TO DS 24-12.	607.95				1.0	50.0		
DS 24-15	ML 169+94.43 101.7' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 24-16	ML 170+93.08 104.1' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 24-17	171+81.84 133.9' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 185 LF OF OUTLET PIPE FROM DS 24-17 TO DS 24-15.	-					185.0	1.0	
DS 24-18	174+22.47 112.2' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 239 LF OF OUTLET PIPE FROM DS 24-18 TO DS 24-17.	-					239.0	1.0	
DS 25-1	0 5+15.45 4.8' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 69 LF OF OUTLET PIPE FROM DS 25-1 TO DS 25-5.	-					1.0	69.0	
DS 25-2	ML 176+11.09 4.4' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 73 LF OF OUTLET PIPE FROM DS 25-2 TO DS 25-1.	610.49	1.0				73.0		
DS 25-3	ML 176+11.56 47.9' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 48 LF OF OUTLET PIPE FROM DS 25-3 TO DS 25-4.	609.45				1.0	48.0		
DS 25-4	ML 176+11.98 3.3' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	610.51	1.0			1.0			
DS 25-5	ML 176+19.75 140.3' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 201 LF OF OUTLET PIPE FROM DS 25-5 TO DS 24-18.	-					201.0	1.0	
DS 25-6	ML 177+52.51 111.2' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 139 LF OF OUTLET PIPE FROM DS 25-6 TO DS 25-5.	-					139.0	1.0	
DS 25-7	ML 178+64.53 102.8' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 115 LF OF OUTLET PIPE FROM DS 25-7 TO DS 25-6.	-					115.0	1.0	
DS 25-8	ML 179+65.14 112.7' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 238 LF OF OUTLET PIPE FROM DS 25-8 TO DS 25-7.	-					238.0	1.0	
DS 25-9	ML 180+40.14 106.8' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 73 LF OF OUTLET PIPE FROM DS 25-9 TO DS 25-8.	-					73.0	1.0	
DS 25-10	ML 182+22.57 2.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 46 LF OF OUTLET PIPE FROM DS 25-10 TO DS 25-11. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	606.80	1.0				1.0	46.0	
DS 25-11	ML 182+23.04 47.8' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 58 LF OF OUTLET PIPE FROM DS 25-11 TO DS 25-12.	605.31				1.0	58.0		
DS 25-12	ML 182+27.98 105.3' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 183 LF OF OUTLET PIPE FROM DS 25-12 TO DS 25-9.	-					183.0	1.0	
DS 26-1	ML 184+05.77 6.7' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	603.42	1.0						
DS 26-2	ML 184+07.25 1.3' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 50 LF OF OUTLET PIPE FROM DS 26-2 TO DS 26-3. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	603.32	1.0			1.0	50.0		
DS 26-3	ML 184+07.98 46.9' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 53 LF OF OUTLET PIPE FROM DS 26-3 TO DS 26-18.	601.86				1.0	53.0		
DS 26-4	ML 185+46.38 0.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 61 LF OF OUTLET PIPE FROM DS 26-4 TO DS 26-9.	601.45	1.0			1.0	61.0		
DS 26-5	ML 185+47.11 7.1' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	601.54	1.0						

ITEM	DESCRIPTION	UNITS
604.070501	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
604.070502	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - RECONSTRUCT ROOF SLAB	EA
604.070503	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - REMOVE CONDUIT & PATCH WALL	EA
604.070504	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF
621.04	CLEANING DRAINAGE STRUCTURES	EA
634.07060001	VIDEO INSPECTION OF PIPELINES	LF

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING: MISCELLANEOUS TABLES	DRAWING NUMBER: MT-11

CHECKED BY: J. KLEINMANN  
 DRAFTED BY: N. HOY  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: P. THOMPSON  
 DESIGN SUPERVISOR: D. MARSH

DRAINAGE TABLE PAGE 8 OF 10

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 26-6	ML 185+48.38 46.5' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 60 LF OF OUTLET PIPE FROM DS 26-6 TO DS 26-8.	600.20				1.0	60.0		
DS 26-7	ML 186+08.71 5.4' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	601.24	1.0						
DS 26-8	ML 186+08.72 46.6' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 45 LF OF OUTLET PIPE FROM DS 26-8 TO DS 26-19.	600.25				1.0	45.0		
DS 26-9	ML 186+08.82 0.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 50 LF OF OUTLET PIPES FROM DS 26-9 TO DS 26-8. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	601.27	1.0		1.0		50.0		
DS 26-10	ML 186+53.98 8.8' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	601.49	1.0						
DS 26-11	ML 186+54.07 1.7' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 45 LF OF OUTLET PIPE FROM DS 26-11 TO DS 26-9. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	601.45	1.0		1.0		45.0		
DS 26-12	ML 186+54.09 43.6' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 45 LF OF OUTLET PIPE FROM DS 26-12 TO DS 26-8.	600.81				1.0	45.0		
DS 26-13	ML 188+36.26 1.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 46 LF OF OUTLET PIPE FROM DS 26-13 TO DS 26-14. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	603.18	1.0		1.0		46.0		
DS 26-14	ML 188+36.32 46.7' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 51 LF OF OUTLET PIPE FROM DS 26-14 TO DS 26-24.	602.70				1.0	51.0		
DS 26-15	ML 188+37.02 4.6' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	603.08	1.0						
DS 26-16	ML 188+56 50.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 54 LF OF OUTLET PIPE FROM DS 26-16 TO DS 26-15.	602.44				1.0	54.0		
DS 26-17	ML 183+85.80 112.1' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF INLET PIPE AND CLEAN 305 LF OF OUTLET PIPE FROM DS 26-17 TO DS 26-20.	-					310.0	1.0	
DS 26-18	ML 184+10.77 99.4' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 179 LF OF OUTLET PIPE FROM DS 26-18 TO DS 25-12.	-					179.0	1.0	
DS 26-19	ML 186+16.31 92.4' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 201 LF OF OUTLET PIPE FROM DS 26-19 TO DS 26-18.	-					201.0	1.0	
DS 26-20	ML 186+84.41 112.6' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 26-21	ML 186+85.37 93.9' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 70 LF OF OUTLET PIPE FROM DS 26-21 TO DS 26-19.	-					70.0	1.0	
DS 26-22	ML 187+20.67 91.5' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 36 LF OF OUTLET PIPE FROM DS 26-22 TO DS 26-21.	-					36.0	1.0	
DS 26-23	ML 187+88.94 105.5' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
DS 26-24	ML 188+39.78 99.0' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 119 LF OF OUTLET PIPE FROM DS 26-24 TO DS 26-22.	-					119.0	1.0	
DS 27-1	ML 190+11.28 47.6' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 57 LF OF OUTLET PIPE FROM DS 27-1 TO DS 27-9.	604.27				1.0	57.0		

ITEM	DESCRIPTION	UNITS
604.070501	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
604.070502	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - RECONSTRUCT ROOF SLAB	EA
604.070503	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - REMOVE CONDUIT & PATCH WALL	EA
604.070504	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF
621.04	CLEANING DRAINAGE STRUCTURES	EA
634.07060001	VIDEO INSPECTION OF PIPELINES	LF

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

DRAINAGE TABLE PAGE 8 OF 10 CONTINUED

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 27-2	ML 190+11.43 2.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 48 LF OF OUTLET PIPE FROM DS 27-2 TO DS 27-1. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	605.06	1.0		1.0		48.0		
DS 27-3	ML 190+11.78 3.6' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	604.95	1.0						
DS 27-4	ML 190+12.31 49.8' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 50 LF OF OUTLET PIPE FROM DS 27-4 TO DS 27-3.	604.03				1.0	50.0		
DS 27-5	ML 193+61.45 3.5' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	608.59	1.0						
DS 27-6	ML 193+61.47 49.4' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 49 LF OF INLET PIPE FROM DS 27-6 TO DS 27-5.	607.39				1.0	49.0		
DS 27-7	ML 193+61.67 2.5' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 46 LF OF OUTLET PIPE FROM DS 27-7 TO DS 27-8. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	608.52	1.0		1.0		46.0		
DS 27-8	ML 193+63.04 48.2' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 67 LF OF OUTLET PIPE FROM DS 27-8 TO DS 27-12.	609.11				1.0	67.0		
DS 27-9	ML 190+13.21 104.5' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 173 LF OF OUTLET PIPE FROM DS 27-9 TO DS 26-24.	-					173.0	1.0	
DS 27-10	ML 191+87.77 111.2' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 174 LF OF OUTLET PIPE FROM DS 27-10 TO DS 27-9.	-					174.0	1.0	
DS 27-11	ML 193+61.47 49.2' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 449 LF OF OUTLET PIPE FROM DS 27-11 TO DS 26-23.	-					449.0	1.0	
DS 27-12	ML 193+66.83 115.8' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 181 LF OF OUTLET PIPE FROM DS 27-12 TO DS 27-10.	-					181.0	1.0	
DS 27-13	ML 195+31.12 113.9' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 167 LF OF OUTLET PIPE FROM DS 27-13 TO DS 27-12.	-					167.0	1.0	
DS 27-14	ML 195+34.83 122.2' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 292 LF OF OUTLET PIPE FROM DS 27-14 TO DS 27-11.	-					292.0	1.0	
DS 28-1	ML 198+25.35 4.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	608.02	1.0						
DS 28-2	ML 198+25.58 2.5' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 48 LF OF OUTLET PIPE FROM DS 28-2 TO DS 28-3. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	608.05	1.0		1.0		48.0		
DS 28-3	ML 198+26.05 47.7' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 64 LF OF OUTLET PIPE FROM DS 28-3 TO DS 28-14.	607.35				1.0	64.0		
DS 28-4	ML 198+53.89 49.3' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 57 LF OF OUTLET PIPE FROM DS 28-4 TO DS 28-1.	607.15				1.0	57.0		
DS 28-5	ML 200+45.26 49.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 83 LF OF OUTLET PIPE FROM DS 28-5 TO DS 28-15.	605.64				1.0	83.0		
DS 28-6	ML 200+45.85 2.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 49 LF OF OUTLET PIPE FROM DS 28-6 TO DS 28-5. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	606.24	1.0		1.0		49.0		
DS 28-7	ML 200+46.11 53.1' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 52 LF OF OUTLET PIPE FROM DS 28-7 TO DS 28-8.	605.92				1.0	52.0		
DS 28-8	ML 200+46.23 2.8' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	606.35	1.0						
DS 28-9	ML 202+14.63 51.9' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 58 LF OF OUTLET PIPE FROM DS 28-9 TO DS 28-11.	604.61				1.0	58.0		

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

DESIGNED BY: P. THOMPSON  
CHECKED BY: J. KLEINMANN  
DRAFTED BY: N. HOY  
DESIGN SUPERVISOR: D. MARSH

NEW YORK STATE OF OPPORTUNITY  
Thruway Authority

GMA  
2018 Mid-Range Tower  
Suite 1100, WI 10020-3723  
616.453.4897 www.gma.com

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING MISCELLANEOUS TABLES	DRAWING NUMBER: MT-12

DRAINAGE TABLE PAGE 9 OF 10

Table with 11 columns: DS #, STATION / OFFSET (FT), DESCRIPTION OF WORK, PROP T.G. ELEV., ITEM 604.070501 (EA), ITEM 604.070502 (EA), ITEM 604.070503 (EA), ITEM 604.070504 (EA), ITEM 621.03 (LF), ITEM 621.04 (EA), ITEM 634.07060001 (LF). Rows include station data for DS 28-10 through DS 29-11.

DRAINAGE TABLE PAGE 9 OF 10 CONTINUED

Table with 11 columns: DS #, STATION / OFFSET (FT), DESCRIPTION OF WORK, PROP T.G. ELEV., ITEM 604.070501 (EA), ITEM 604.070502 (EA), ITEM 604.070503 (EA), ITEM 604.070504 (EA), ITEM 621.03 (LF), ITEM 621.04 (EA), ITEM 634.07060001 (LF). Rows include station data for DS 29-12 through DS 31-7.

Table with 3 columns: ITEM, DESCRIPTION, UNITS. Lists items 604.070501 through 634.07060001 with their respective descriptions and units (EA, LF).

Professional Engineer stamp for Joseph W. Kleinmann, State of New York, License No. 092577. Includes fields for ALTERED ON, AFFIXED ON (5/10/2024), SIGNATURE, and STAMP.

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REVISIONS table with columns: DATE, DESCRIPTION, BY, SYM.

Logos for New York State Thruway Authority and QMA (Quality Management Associates).

Project information table including TITLE OF PROJECT (I-190 HIGHWAY REHABILITATION), LOCATION OF PROJECT (I-190 NIAGARA THRUWAY), TITLE OF DRAWING (MISCELLANEOUS TABLES), CONTRACT NUMBER (TAN 24-12), DATE (MAY 2024), and DRAWING NUMBER (MT-13).

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

DRAINAGE TABLE PAGE 10 OF 10

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 31-8	R1 1+27.00 115.0' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN AND PERFORM VIDEO INSPECTION OF 110 LF OF PIPE FROM DS 31-8 TO DS 31-11.	-					110.0	1.0	110.0
DS 31-9	R1 1+45.96 118.7' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 63 LF OF OUTLET PIPE FROM DS 31-9 TO DS 31-6.	-					63.0	1.0	
DS 31-10	R1 1+32.21 74.6' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 47 LF OF OUTLET PIPE FROM DS 31-10 TO DS 31-9.	-					47.0	1.0	
DS 31-11	-	REFER TO DS 31-8.	-							
DS 32-1	ML 219+99.03 50.0' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	581.89				1.0			
DS 32-2	ML 220+71.68 3.1' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 29 LF OF OUTLET PIPE FROM DS 32-2 TO DS 32-4. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	580.38	1.0		1.0		29.0		
DS 32-3	ML 220+72.05 3.2' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	580.33	1.0						
DS 32-4	ML 221+00.70 2.8' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 72 LF OF OUTLET PIPES FROM DS 32-4 TO DS 32-10. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	580.35	1.0		1.0		72.0		
DS 32-5	ML 221+01.03 3.5' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	580.33	1.0						
DS 32-6	ML 221+69.01 3.2' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 68 LF OF OUTLET PIPE FROM DS 32-6 TO DS 32-4. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	580.45	1.0		1.0		68.0		
DS 32-7	ML 221+69.26 3.3' RT	RECONSTRUCT ROOF SLAB. ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	580.48	1.0	1.0					
DS 32-8	ML 223+71.78 54.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	581.98				1.0			
DS 32-9	ML 224+10.99 2.7' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 68 LF OF OUTLET PIPE FROM DS 32-9 TO DS 32-13.	584.07	1.0				68.0		
DS 32-10	ML 220+66.76 61.3' LT	CLEAN AND PERFORM VIDEO INSPECTION OF 260 LF OF PIPE FROM DS 32-10 TO DS 31-8.	-					260.0		260.0
DS 32-11	ML 220+40.51 66.8' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 377 LF OF OUTLET PIPE FROM DS 32-11 TO DS 32-13.						377.0	1.0	
DS 32-12	ML 223+63.47 68.4' LT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 17 LF OF OUTLET PIPE FROM DS 32-12 TO DS 32-8.						17.0	1.0	
DS 32-13	ML 224+12.45 67.1' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 283 LF OF OUTLET PIPE FROM DS 32-13 TO DS 33-6.						283.0	1.0	
DS 33-1	ML 225+09.59 50.0' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE.	586.02				1.0			
DS 33-2	ML 226+83.77 48.4' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 49 LF OF OUTLET PIPE FROM DS 33-2 TO DS 33-4.	591.51				1.0	49.0		

DRAINAGE TABLE PAGE 10 OF 10 CONTINUED

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	ITEM 604.070501 (EA)	ITEM 604.070502 (EA)	ITEM 604.070503 (EA)	ITEM 604.070504 (EA)	ITEM 621.03 (LF)	ITEM 621.04 (EA)	ITEM 634.07060001 (LF)
DS 33-3	ML 226+84.30 3.5' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 48 LF OF OUTLET PIPE FROM DS 33-3 TO DS 33-5.	592.08	1.0					48.0	
DS 33-4	ML 226+84.42 2.3' LT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE. REMOVE CONDUIT & PATCH WALL (SEE DETAIL ON DWG. MD-8)	591.92	1.0		1.0				
DS 33-5	ML 226+93.65 47.1' RT	ADJUST EXISTING FRAME AND GRATE TO PROPOSED FINISHED GRADE AND CLEAN 35 LF OF OUTLET PIPE FROM DS 33-5 TO DS 33-6.	591.72				1.0	35.0		
DS 33-6	ML 226+93.51 85.9' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 99 LF OF OUTLET PIPE FROM DS 33-6 TO DS 33-7.	-					99.0	1.0	
DS 33-7	ML 227+92.50 69.5' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND CLEAN 5 LF OF OUTLET PIPE.	-					5.0	1.0	
TOTAL				143.0	1.0	74.0	90.0	32666.0	153.0	1379.0

ITEM	DESCRIPTION	UNITS
604.070501	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
604.070502	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - RECONSTRUCT ROOF SLAB	EA
604.070503	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - REMOVE CONDUIT & PATCH WALL	EA
604.070504	ALTERING DRAINAGE STRUCTURES, LEACHING BASINS AND MANHOLES - ADJUST RIM	EA
621.03	CLEANING CLOSED DRAINAGE SYSTEMS	LF
621.04	CLEANING DRAINAGE STRUCTURES	EA
634.07060001	VIDEO INSPECTION OF PIPELINES	LF

DRAINAGE REMOVAL TABLE

DRAINAGE REMOVAL NO.	STATION / OFFSET (FT)	DESCRIPTION OF WORK	ITEM 206.0201 (CY)
DR 4-1	A 13+14.91 22.1' RT	REMOVE EXISTING DRAINAGE STRUCTURE AND PIPE CONNECTING TO DS 4-1.	8.3
TOTAL			8.3

ITEM	DESCRIPTION	UNITS
203.07	SELECT GRANULAR FILL	CY
206.0201	TRENCH AND CULVERT EXCAVATION	CY
552.17	SHIELDS AND SHORING	SF
603.6002	REINFORCED CONCRETE PIPE CLASS III, 15 INCH DIAMETER	LF
603.77	CONCRETE COLLARS	EA
603.9818	SMOOTH INTERIOR CORRUGATED POLYETHYLENE CULVERT AND STORMDRAIN 18 INCH DIAMETER	LF
604.300691	RECTANGULAR DRAINAGE STRUCTURE TYPE F FOR PARALLEL BAR #11 PCB FRAME	LF
655.0902	PARALLEL BAR FRAME 11 PCB & PARALLEL BAR GRATE 11 PCB	EA

NOTES:  
1. CONTRACTOR SHALL FIELD VERIFY THE EXISTING PIPE INVERTS OF DS 4-6.


PROPOSED DRAINAGE INSTALLATION TABLE

DS #	STATION / OFFSET (FT)	DESCRIPTION OF WORK	PROP T.G. ELEV.	INVERTS				ITEM 203.07 (CY)	ITEM 206.0201 (CY)	ITEM 552.17 (SF)	ITEM 603.6002 (LF)	ITEM 603.77 (EA)	ITEM 603.9818 (LF)	ITEM 604.300691 (LF)	ITEM 655.0902 (EA)
				N	S	E	W								
DS 1-1	ML 23+31 2.9' RT	INSTALL PROPOSED 15" RCP OUTLET PIPE AND CONNECT EXISTING RCP WITH CONCRETE COLLARS. SEE MD-10 FOR DETAILS ON PIPE INSTALLATION WORK.	-					67.00	71.00			90.0	2.0		
DS 4-1	A 12+70.85 20.3' RT	CLEAN EXISTING DRAINAGE STRUCTURE AND INSTALL 75 LF OF 18" SICPP OUTLET PIPE FROM DS 4-1 TO DS 4-2.	593.95	590.72				46.00	53.00			75.0			
DS 4-2	A 13+55.68 23.5' RT	INSTALL NEW TYPE F DRAINAGE STRUCTURE AND INSTALL 108 LF OF 18" SICPP OUTLET PIPE FROM DS 4-2 TO DS 4-6.	594.66		589.97		589.87	90.00	106.00	204.00		108.0	6.0	1.0	
DS 4-6	C 6+96.40 39.4' RT	INSTALL NEW TYPE F DRAINAGE STRUCTURE. CONNECT EXISTING NE, NW, AND SW PIPES TO NEW STRUCTURE. CLEAN AND PERFORM VIDEO INSPECTION OF 200 LF OF PIPELINE FROM DS 4-6 TO DS 3-6.	593.21	NOTE 1	587.00		588.79	15.00	23.00	257.00		7.0		1.0	
TOTAL								218.0	253.0	461.0	90.0	2.0	183.0	13.0	2.0


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REVISIONS

DATE	DESCRIPTION	BY	SYM.



**Thruway Authority**



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
MISCELLANEOUS TABLES

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
MT-14

CHECKED BY: J. KLEINMANN

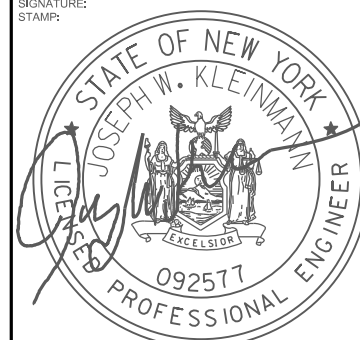
DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

71  
274

<p>ALTERED ON:</p> <p>SIGNATURE STAMP:</p>	<p>AFFIXED ON: 5/10/2024</p> <p>SIGNATURE STAMP:</p> 
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GUIDERAIL REMOVAL TABLE

Table with 13 columns: LOCATION (STATION TO STATION), LOCATION (M.P. TO M.P.), SIDE, and 10 quantity columns (606.63 LF, 606.71 LF, 606.73 LF, 606.75 LF, 606.7510 LF, 606.7910 EA, 606.7920 EA, 606.7921 EA, 606.8103--25 EA, 654.0701 EA). Rows include ML 23+58 through ML 220+85.

GUIDERAIL REMOVAL TABLE - CONTINUED

Continuation of the table, including RAMPs, INTERCHANGE 1 through 5, and a TOTAL row. Locations include C1 11+26, G 1+62, H 9+40, J 3+36, J 4+90, K 2+85, ML 163+33, ML 168+26, N 1+01, O 2+13, ML 204+23.

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

Table with 3 columns: ITEM, DESCRIPTION, UNITS. Lists items like 606.63 (REMOVING AND STORING BOX BEAM GUIDE RAILING), 606.71 (REMOVING AND DISPOSING CORRUGATED BEAM GUIDE RAILING), etc.

ALTERED ON: 5/10/2024. SIGNATURE: [Signature], STAMP: [Professional Engineer Seal for Joseph W. Kleinmann, License No. 092577].

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REVISIONS table with columns: DATE, DESCRIPTION, BY, SYM.

NEW YORK STATE OF OPPORTUNITY Thruway Authority logo. TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION. LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2. CONTRACT NUMBER: TAN 24-12. DATE: MAY 2024. DRAWING NUMBER: MT-15.



CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ITEM	DESCRIPTION	UNITS
606.10	BOX BEAM GUIDE RAILING	LF
606.100002	BOX BEAM GUIDE RAILING (SHOP BENT OR SHOP MITERED)	LF
606.1001--25	BOX BEAM GUIDE RAILING (ATTACHED TO CONCRETE BARRIER)	LF
606.120101	BOX BEAM END PIECE	EA
606.120102	BOX BEAM GUIDE RAILING END ASSEMBLY, TYPE I	EA
606.1203	BOX BEAM END ASSEMBLY, TYPE III	EA
606.26500108	SPECIAL TRANSITION - 42" SINGLE-SLOPE CONCRETE MEDIAN BARRIER TO VERTICAL FACE BLOCK	EA
606.2701	HPBO (MOD.) CORRUGATED BEAM GUIDE RAILING	LF
606.2703	ANCHORAGE UNIT FOR HPBO (MOD.) CORRUGATED BEAM GUIDE RAILING	EA
606.3041	SINGLE-SLOPE CONCRETE MEDIAN BARRIER (OPTIONAL)	LF
606.3042	SINGLE-SLOPE CONCRETE MEDIAN BARRIER (PRECAST)	LF
606.3061	SINGLE-SLOPE CONCRETE HALF SECTION BARRIER (OPTIONAL)	LF
606.3062	SINGLE-SLOPE CONCRETE HALF SECTION BARRIER (PRECAST)	LF
606.53	RESETTING BOX BEAM GUIDE RAILING	LF
606.57	RESETTING PRECAST CONCRETE BARRIER	LF
606.59200125	RESETTING END TERMINALS FOR BOX BEAM GUIDE RAIL AND MEDIAN BARRIER	EA
606.8803	TRANSITION BETWEEN BOX BEAM GUIDE RAIL AND SINGLE SLOPE HALF SECTION CONCRETE BARRIER (ONE OR TWO WAY OPERATION)	EA
606.8806	TRANSITION BETWEEN BOX BEAM GUIDE RAIL TO FULL-SECTION SINGLE SLOPE CONCRETE BARRIER (ONE OR TWO WAY OPERATION)	EA
606.8901	TRANSITION - HPBO (MOD.) CORRUGATED BEAM GUIDE RAIL TO BOX BEAM GUIDE RAIL	EA
606.9001	TRANSITION BETWEEN STANDARD (NJ) CONCRETE BARRIER AND SINGLE-SLOPE CONCRETE BARRIER	EA
606.9003	TRANSITION BETWEEN HALF-SECTION AND FULL-SECTION SINGLE-SLOPE CONCRETE BARRIER	EA
654.51350015	MASH-COMPLIANT REUSABLE IMPACT ATTENUATOR, TL 3, 85FT OBSTRUCTION WIDTH	EA

GUIDERAIL INSTALLATION TABLE																										
LOCATION (STATION TO STATION)	LOCATION (M.P. TO M.P.)	SIDE	606.10	606.100002	606.1001--25	606.120101	606.120102	606.1203	606.26500108	606.2701	606.2703	606.3041	606.3042	606.3061	606.3062	606.53	606.57	606.59200125	606.8803	606.8806	606.8901	606.9001	606.9003	654.51350015		
			LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA
<b>MAINLINE</b>																										
ML 23+58	ML 24+18	900.372	900.383	LT																					60.0	
ML 39+65	ML 40+63	900.683	900.702	NB MEDIAN				98.0																		
ML 42+42	ML 44+62	900.735	900.767	RT	144.0				1	1																
ML 52+00	ML 59+66	900.914	901.070	RT	595.0															1						
ML 53+87	ML 56+17	900.963	901.002	LT	154.0				2	1																
ML 58+88	ML 59+78	901.056	901.073	NB MEDIAN				90.0																		
ML 59+29	ML 60+24	901.081	901.099	SB MEDIAN				95.0																		
ML 59+43	ML 63+60	901.084	901.170	LT	278.0							1														68.0
ML 76+60	ML 78+71	901.383	901.423	RT	144.0						1	1														
ML 77+84	ML 80+03	901.477	901.479	LT	147.0							2	1													
ML 81+44	ML 85+04	901.478	901.546	RT	278.0							1														
ML 83+28	ML 84+05	901.541	901.555	LT																						68.0
ML 87+23	ML 97+78	901.605	901.826	LT																						70.0
ML 107+78	ML 119+41	901.967	902.216	RT																						1.0
ML 109+26	ML 117+71	902.020	902.199	LT																						2
ML 121+99	ML 124+54	902.259	902.294	LT																						2
ML 123+74	ML 128+34	902.272	902.367	RT																						2
ML 133+52	ML 140+58	902.455	902.611	RT																						2
ML 133+17	ML 136+77	902.459	902.530	LT	278.0							1														2
ML 138+78	ML 141+28	902.566	902.613	LT	216.0																					1
ML 143+31	ML 152+19	902.566	902.830	RT																						2
ML 143+65	ML 152+18	902.658	902.830	LT																						2
ML 154+29	ML 154+95	902.859	902.872	RT																						2
ML 165+68	ML 168+48	903.076	903.128	RT	245.0																					66.0
ML 169+74	ML 178+39	903.138	903.302	LT	802.0																					2
ML 179+14	ML 186+56	903.336	903.470	RT	688.0																					2
ML 179+76	ML 180+35	903.328	903.340	LT																						59.0
ML 186+06	ML 186+57	903.461	903.470	LT																						51.0
ML 188+09	ML 188+67	903.500	903.511	RT																						58.0
ML 188+12	ML 195+47	903.500	903.624	LT	673.0																					2
ML 194+83	ML 195+30	903.630	903.638	RT																						47.0
ML 197+08	ML 202+67	903.655	903.761	LT	495.0																					2
ML 204+23	P 2+38	903.793	-	RT	79.0	131.0			1																	1
ML 204+38	R 8+14	903.788	-	LT	669.0																					1
ML 213+65	ML 222+25	903.980	904.137	RT	629.0																					2
ML 221+38	ML 222+32	904.138	904.144	LT																						1
SUBTOTAL - MAINLINE			6514.0	131.0	298.0	8.0	6.0	4.0	1.0	6028.0	1.0	0.0	20.0	100.0	136.0	411.0	82.0	1.0	34.0	0.0	14.0	0.0	1.0	1.0	1.0	

ALTERED ON:

AFFIXED ON: 5/10/2024

SIGNATURE: [Signature]

STAMP: [Professional Engineer Seal for Joseph W. Kleinmann, State of New York, License No. 092577]

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REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING MISCELLANEOUS TABLES	DRAWING NUMBER: MT-16

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN


DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

GUIDERAIL INSTALLATION TABLE - CONTINUED



LOCATION (STATION TO STATION)	LOCATION (M.P. TO M.P.)	SIDE	606.10	606.100002	606.1001--25	606.120101	606.120102	606.1203	606.26500108	606.2701	606.2703	606.3041	606.3042	606.3061	606.3062	606.53	606.57	606.59200125	606.8803	606.8806	606.8901	606.9001	606.9003	654.51350015		
			LF	LF	LF	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA	EA
INTERCHANGE 1																										
A 11+03	A 14+93	-	-	LT	18.0				1			280								1		1				
INTERCHANGE 2																										
H 8+51	H 11+12	-	-	RT	230.0			1	1																	
INTERCHANGE 3																										
J 3+26	J 9+87	-	-	LT						643.0	1															
J 4+90	ML 131+74	-	-	RT						802.0	1								1		1					
K 2+43	ML 131+47	-	-	RT	564.0				1										1							
INTERCHANGE 4																										
ML 163+33	L1 3+16	903.03	-	LT	266.0	380.0			1										1							
M 1+05	L1 4+79	-	-	RT	206.0	224.0		1											1							
N 1+01	N 2+23	-	-	RT	6.0					13.0	1								1		1					
O1 1+43	ML 177+27	-	-	RT	282.0	523.0			1										1							
SUBTOTAL - INTERCHANGES					1572.0	1127.0	0.0	2.0	4.0	1.0	0.0	1458.0	3.0	280.0	0.0	0.0	0.0	0.0	0.0	6.0	1.0	2.0	1.0	0.0	0.0	
TOTAL - ALL GUIDE RAIL					8086.0	1258.0	298.0	10.0	10.0	5.0	1.0	7486.0	4.0	280.0	20.0	100.0	136.0	411.0	82.0	1.0	40.0	1.0	16.0	1.0	1.0	

ITEM	DESCRIPTION	UNITS
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606.1203	BOX BEAM END ASSEMBLY, TYPE III	EA
606.26500108	SPECIAL TRANSITION - 42" SINGLE-SLOPE CONCRETE MEDIAN BARRIER TO VERTICAL FACE BLOCK	EA
606.2701	HPBO (MOD.) CORRUGATED BEAM GUIDE RAILING	LF
606.2703	ANCHORAGE UNIT FOR HPBO (MOD.) CORRUGATED BEAM GUIDE RAILING	EA
606.3041	SINGLE-SLOPE CONCRETE MEDIAN BARRIER (OPTIONAL)	LF
606.3042	SINGLE-SLOPE CONCRETE MEDIAN BARRIER (PRECAST)	LF
606.3061	SINGLE-SLOPE CONCRETE HALF SECTION BARRIER (OPTIONAL)	LF
606.3062	SINGLE-SLOPE CONCRETE HALF SECTION BARRIER (PRECAST)	LF
606.53	RESETTING BOX BEAM GUIDE RAILING	LF
606.57	RESETTING PRECAST CONCRETE BARRIER	LF
606.59200125	RESETTING END TERMINALS FOR BOX BEAM GUIDE RAIL AND MEDIAN BARRIER	EA
606.8803	TRANSITION BETWEEN BOX BEAM GUIDE RAIL AND SINGLE SLOPE HALF SECTION CONCRETE BARRIER (ONE OR TWO WAY OPERATION)	EA
606.8806	TRANSITION BETWEEN BOX BEAM GUIDE RAIL TO FULL-SECTION SINGLE SLOPE CONCRETE BARRIER (ONE OR TWO WAY OPERATION)	EA
606.8901	TRANSITION - HPBO (MOD.) CORRUGATED BEAM GUIDE RAIL TO BOX BEAM GUIDE RAIL	EA
606.9001	TRANSITION BETWEEN STANDARD (NJ) CONCRETE BARRIER AND SINGLE-SLOPE CONCRETE BARRIER	EA
606.9003	TRANSITION BETWEEN HALF-SECTION AND FULL-SECTION SINGLE-SLOPE CONCRETE BARRIER	EA
654.51350015	MASH-COMPLIANT REUSABLE IMPACT ATTENUATOR, TL 3, 85FT OBSTRUCTION WIDTH	EA

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING MISCELLANEOUS TABLES	DRAWING NUMBER: MT-17

SIGN REMOVAL TABLE

LOCATION	STATION	SIDE	DESCRIPTION	ITEM 647.20	ITEM 647.21	ITEM 647.51	ITEM 647.52	ITEM 647.53	ITEM 647.61	ITEM 647.62	ITEM 647.65
1-1	ML 22+42	RT	EXIT 1/S OGDEN ST/DINGENS ST					1			2
1-2	ML 23+53	LT	I-90 INTERCHANGE SPLIT		1						
1-3	ML 32+83	RT	EXIT 1			1					
1-4	ML 32+21	RT	RAMP 20 MPH			1					
2-1	ML 37+42	CENTER	NO U-TURN						1		
2-2	ML 37+61	LT	EXIT 1							1	
2-3	ML 37+61	RT	MERGE ARROW						1		
3-1	ML 43+18	LT	RT CURVE / EXIT 30 MPH						1		
3-2	ML 44+64	-	I-90 INTERCHANGE SPLIT		1						
3-3	ML 50+85	LT	EMERGENCY STOPPING ONLY						1		
3-4	ML 51+87	RT	I-190 NORTH						1		
3-5	ML 55+15	RT	DESTINATION SIGN				1				2
4-1	ML 57+83	RT	STATE SPEED LIMIT 55						1		
4-2	ML 63+39	RT	EMERGENCY STOPPING ONLY						1		
4-3	ML 64+24	LT	STATE SPEED LIMIT 55						1		
4-4	ML 66+50	RT	BUCKLE UP						1		
5-1	ML 72+29	LT	MERGE ARROW						1		
5-2	ML 72+17	RT	EXIT 2	1							
5-3	ML 74+46	LT	LICENSE SUSPENDED						1		
5-4	ML 75+84	RT	EXIT 40 MPH						1		
5-5	ML 77+12	LT	EXIT 2							1	
5-6	ML 78+88	LT	EXIT 2		1						
5-7	ML 78+88	LT	EXIT 1 ADVANCE					1			
5-8	ML 79+78	RT	EXIT 2							1	
5-9	ML 81+41	LT	RT CURVE / EXIT 20 MPH						1		
5-10	ML 83+77	LT	EXIT 2	1							
5-11	ML 83+77	LT	FINES DOUBLED			1					
6-1	ML 91+89	RT	RT CURVE / 50 MPH ADVISORY						1		
6-2	ML 95+92	RT	MERGE ARROW						1		
7-1	ML 108+88	LT	LT CURVE / 40 MPH ADVISORY						1		
7-2	ML 110+79	LT	EMERGENCY STOPPING ONLY						1		
7-3	ML 110+59	RT	I-190 NORTH						1		
8-1	ML 118+38	RT	RT CURVE			1					
8-2	ML 118+38	RT	EXIT 4 ADVANCE	1							
8-3	ML 124+43	LT	EXIT 3							1	
8-4	ML 126+23	LT	EXIT 3		1						
8-5	ML 126+23	LT	EXIT 2 ADVANCE					1			
8-6	ML 126+23	LT	EXIT 30 MPH			1					
9-1	ML 127+25	RT	MERGE ARROW						1		
9-2	ML 131+57	LT	EXIT 2	1							
9-3	ML 131+57	LT	LT CURVE / 50 MPH ADVISORY			1					
9-4	ML 140+04	RT	STATE SPEED LIMIT 55						1		
10-1	ML 144+78	RT	I-190 NORTH						1		
10-2	ML 147+86	RT	EMERGENCY STOPPING ONLY						1		
10-3	ML 152+11	LT	I-190 SOUTH						1		
12-1	ML 165+70	LT	MERGE ARROW						1		
12-2	ML 166+60	LT	EXIT 4							1	
12-3	ML 168+15	LT	EXIT 4		1						
12-4	ML 168+15	LT	EXIT 3 ADVANCE					1			
12-5	ML 168+15	LT	RT CURVE / 20 MPH ADVISORY			1					
12-6	ML 168+16	RT	LICENSE SUSPENDED			1					
12-7	ML 170+87	RT	RT CURVE / 25 MPH ADVISORY			1					
12-8	ML 170+87	RT	EXIT 5 ADVANCE		1						
12-9	ML 170+87	RT	EXIT 4					1			
12-10	ML 172+26	RT	EXIT 4							1	

SIGN REMOVAL TABLE CONTINUED

LOCATION	STATION	SIDE	DESCRIPTION	ITEM 647.20	ITEM 647.21	ITEM 647.51	ITEM 647.52	ITEM 647.53	ITEM 647.61	ITEM 647.62	ITEM 647.65
13-1	ML 174+04	RT	MERGE ARROW			1					
13-2	ML 174+04	LT	EXIT 4		1						
13-3	ML 184+72	RT	I-190 NORTH						1		
14-1	ML 188+63	LT	EMERGENCY STOPPING ONLY						1		
14-2	ML 193+22	RT	EMERGENCY STOPPING ONLY						1		
14-3	ML 194+79	LT	BUCKLE UP						1		
14-4	ML 198+98	RT	EXIT 5	1							
15-1	ML 201+98	LT	I-190 SOUTH						1		
15-2	ML 204+90	RT	EXIT 5		1						
15-3	ML 204+90	RT	EXIT 20 MPH					1			
15-4	ML 204+90	LT	STATE SPEED LIMIT 55			1					
15-5	ML 206+84	RT	EXIT 5							1	
15-6	ML 209+59	RT	MERGE ARROW						1		
15-7	ML 213+97	LT	MERGE ARROW						1		
16-1	ML 216+51	RT	DESTINATION SIGN					1			2
16-2	ML 218+28	LT	EXIT 5							1	
16-3	ML 220+62	RT	DESTINATION SIGN					1			2
16-4	ML 221+41	LT	EXIT 5					1			
16-5	ML 221+41	LT	EXIT 4 ADVANCE					1			
16-6	ML 222+02	LT	EXIT 25 MPH						1		
16-7	ML 223+46	LT	BUCKLE UP						1		
16-8	ML 224+93	RT	STATE SPEED LIMIT 55						1		

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN


DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ITEM	DESCRIPTION	UNITS
647.20	REMOVAL OF CANTILEVER OVERHEAD SIGN PANEL(S), STRUCTURE, AND FOUNDATIONS	EA
647.21	REMOVAL OF SINGLE SPAN OVERHEAD SIGN PANEL(S), STRUCTURE, AND FOUNDATIONS	EA
647.51	REMOVE AND DISPOSE SIGN PANEL, SIGN PANEL ASSEMBLY SIZE I (UNDER 30 SQUARE FEET)	EA
647.52	REMOVE AND DISPOSE SIGN PANEL, SIGN PANEL ASSEMBLY SIZE II (30-100 SQUARE FEET)	EA
647.53	REMOVE AND DISPOSE SIGN PANEL, SIGN PANEL ASSEMBLY SIZE III (OVER 100 SQUARE FEET)	EA
647.61	REMOVE AND DISPOSE SIGNS, GROUND MOUNTED TYPE A SIGN SUPPORTS AND FOUNDATIONS - SIZE I (UNDER 30 SQUARE FEET)	EA
647.62	REMOVE AND DISPOSE SIGNS, GROUND MOUNTED TYPE A SIGN SUPPORTS AND FOUNDATIONS - SIZE II (30-100 SQUARE FEET)	EA
647.65	REMOVE AND DISPOSE TYPE B GROUND MOUNTED SIGN SUPPORT AND FOUNDATION	EA


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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



**Thruway Authority**



2023 Mid-Range Tower  
Suamico, WI 54233-3723  
618.465.4897 - www.thruwayauthority.com

TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
**MISCELLANEOUS TABLES**

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
MT-18

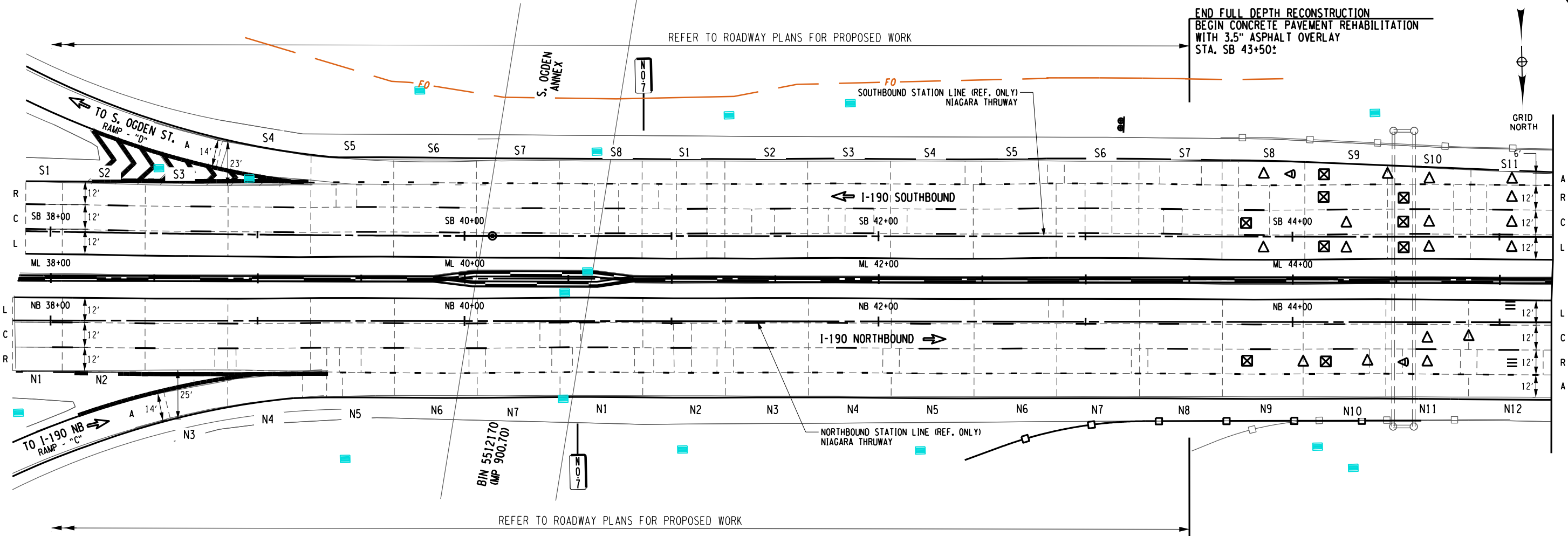
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH



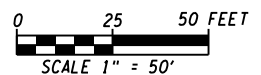
- NOTES:
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPR-1.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

DOWEL BAR RETROFIT REPAIRS						
LOCATION				REPAIR ITEMS		REMARKS
MILE MARKER	STATION	SLAB NUMBER	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
900.79	NB 45+06	N12L	12	8	12	
900.79	NB 45+06	N12R	12	8	12	

PARTIAL DEPTH REPAIR						
LOCATION			REPAIR DIMENSIONS			REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	
900.76	NB 43+85	N9R	1	1	1.0	
900.77	NB 44+25	N10R	1	1	1.0	
900.76	SB 43+85	S8C	2	2	4.0	
900.76	SB 44+25	S9A	3	2	6.0	
900.76	SB 44+25	S9R	2	1	2.0	
900.76	SB 44+25	S9L	3	2	6.0	
900.77	SB 44+63	S10R	2	2	4.0	
900.77	SB 44+63	S10C	1	1	1.0	
900.77	SB 44+63	S10L	3	1	3.0	

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



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SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-1

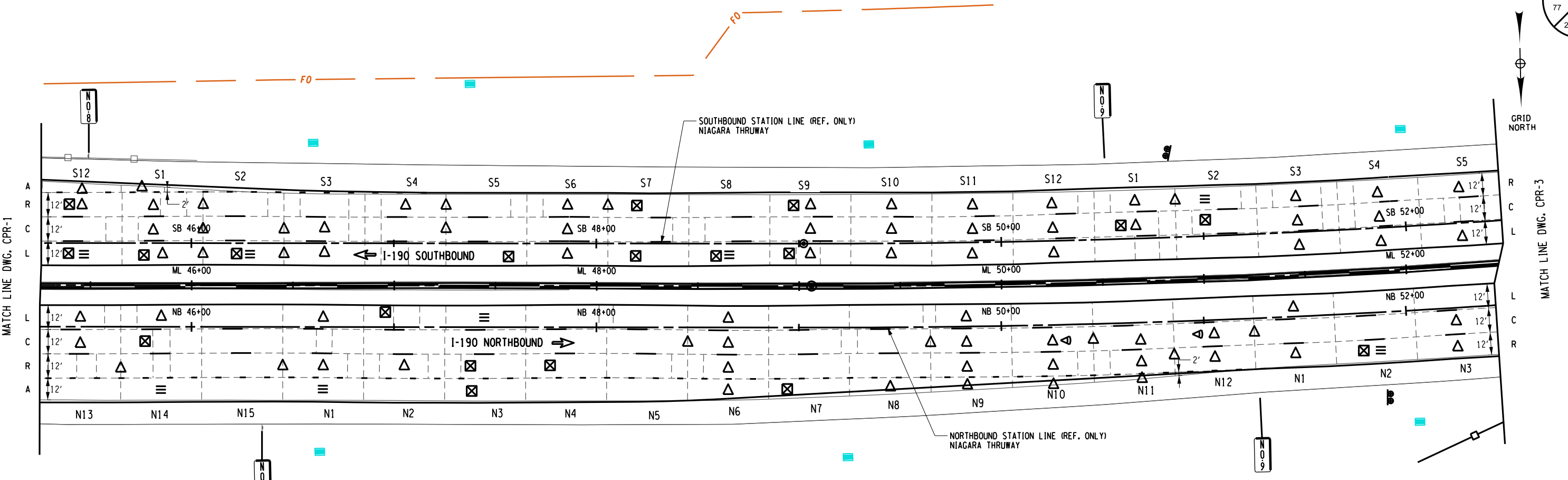
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH



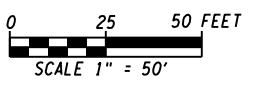
- NOTES:
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-2.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

PARTIAL DEPTH REPAIR						
LOCATION			REPAIR DIMENSIONS			REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	
900.79	NB 45+85	N14R	1	1	1.0	
900.81	NB 47+04	N2L	3	1	3.0	
900.82	NB 47+45	N3A	3	1	3.0	
900.82	NB 47+45	N3R	1	1	1.0	
900.83	NB 47+84	N4R	1	1	1.0	
900.85	NB 49+04	N7A	1	1	1.0	
900.91	NB 51+84	N2C	1	1	1.0	
900.80	SB 45+43	S12R	2	2	4.0	
900.80	SB 45+44	S12L	3	1	3.0	2 SEPARATE REPAIRS (2'x 1' & 1'x 1')
900.81	SB 45+82	S1L	2	1	2.0	
900.81	SB 46+25	S2L	3	1	3.0	
900.84	SB 47+45	S5L	2.5	1	2.5	2 SEPARATE REPAIRS (1.5'x 1' & 1'x 1')
900.85	SB 48+26	S7R	2	1	2.0	
900.85	SB 48+26	S7L	3	1	3.0	2 SEPARATE REPAIRS (2'x 1' & 1'x 1')
900.86	SB 48+65	S8L	1	1	1.0	
900.87	SB 49+12	S9R	2	1.5	3.0	
900.87	SB 49+12	S9L	2	1	2.0	
900.90	SB 50+63	S1C	3	2	6.0	
900.91	SB 51+06	S2C	4	2	8.0	

DOWEL BAR RETROFIT REPAIRS						
LOCATION			REPAIR ITEMS			REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
900.80	NB 45+83	N14A	12	8	12	
900.81	NB 46+65	N1A	12	8	12	
900.82	NB 47+45	N3L	12	8	12	
900.91	NB 51+84	N2R	12	8	12	
900.80	SB 45+46	S12L	12	8	12	
900.81	SB 46+27	S2L	12	8	12	
900.86	SB 48+66	S8L	12	8	12	
900.91	SB 51+07	S2R	12	8	12	

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-2

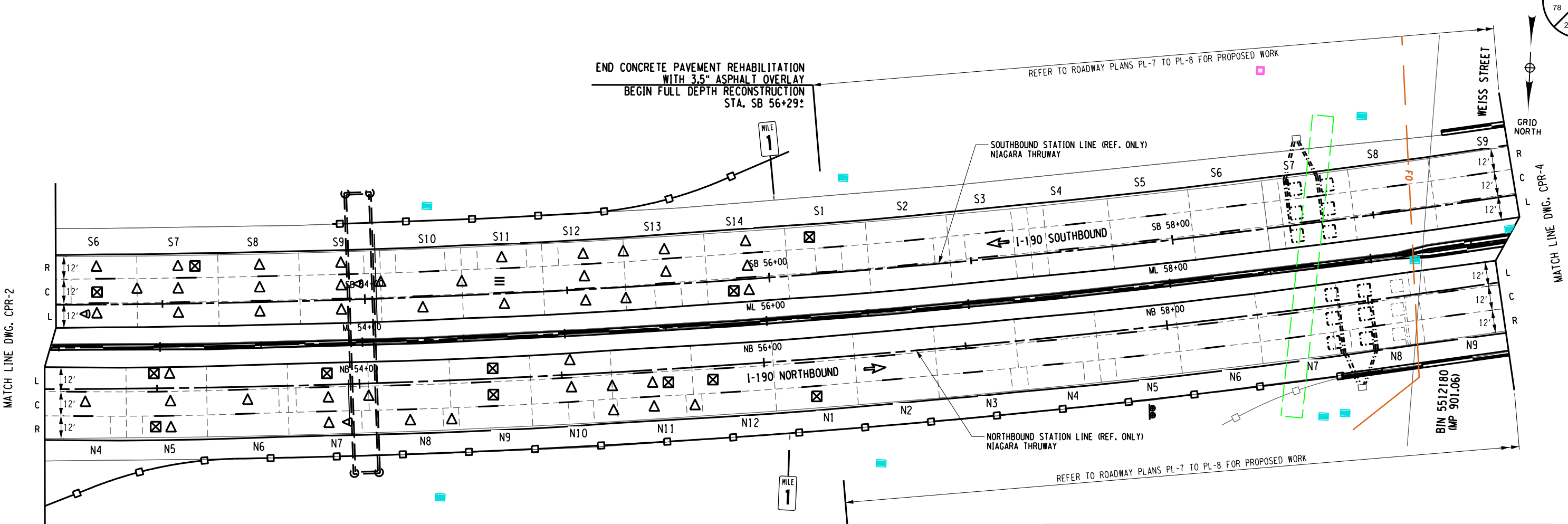
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: DMARSH



- NOTES:
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPR-3.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

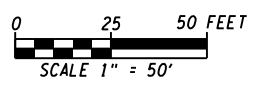
END CONCRETE PAVEMENT REHABILITATION WITH 3.5" ASPHALT OVERLAY  
BEGIN FULL DEPTH RECONSTRUCTION STA. NB 56+29'

DOWEL BAR RETROFIT REPAIRS						
LOCATION			REPAIR ITEMS			REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
900.98	SB 54+69	S11C	12	8	12	

PARTIAL DEPTH REPAIR						
MILE MARKER	STATION	SLAB NUMBER	REPAIR DIMENSIONS		PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	REMARKS
			LENGTH (FT)	WIDTH (FT)		
900.93	NB 53+04	N5R	2	2	4.0	
900.93	NB 53+04	N5L	1	1	1.0	
900.95	NB 53+85	N7L	2	1	2.0	
900.96	NB 54+63	N9L	4	1	4.0	2 SEPARATE REPAIRS (3'x 1' & 1'x 1')
900.96	NB 54+63	N9C	5	1	5.0	2 SEPARATE REPAIRS (2'x 2' & 1'x 1')
900.98	NB 55+42	N11C	1	1	1.0	
900.99	NB 55+83	N12C	3	2	6.0	
901.00	NB 56+23	N1R	2	1	2.0	2 SEPARATE REPAIRS (1'x 1' & 1'x 1')
900.94	SB 52+67	S6C	3	2	6.0	
900.94	SB 53+10	S7R	2	1	2.0	
901.00	SB 55+88	S14L	1	1	1.0	
901.00	SB 56+30	S1R	1	1	1.0	
901.00	SB 56+30	S1C	1	1	1.0	

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



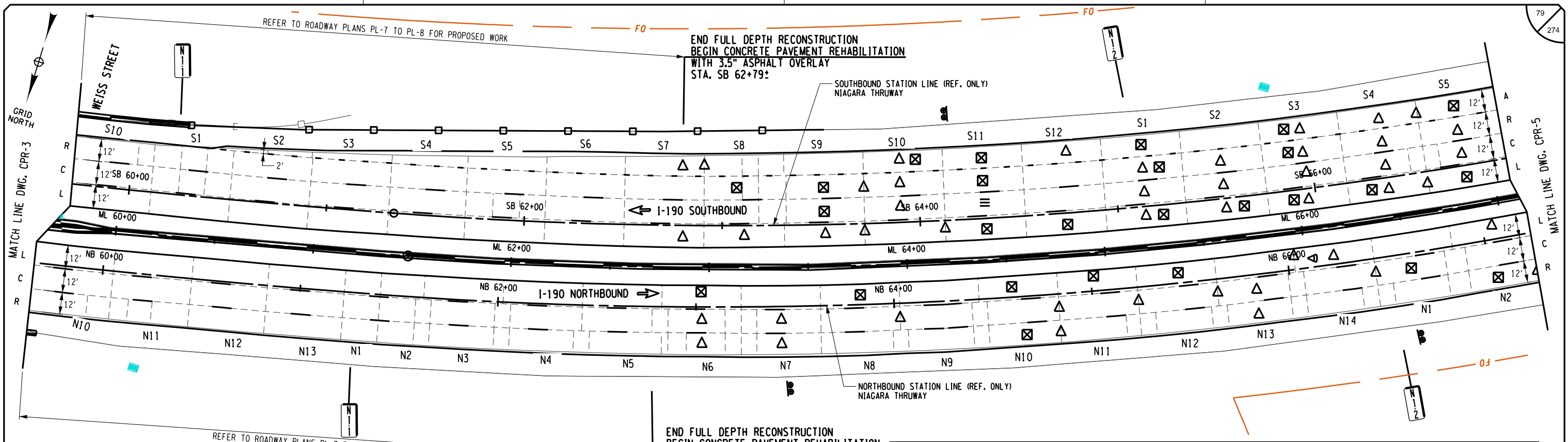
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-3

J. KLEINMANN  
 CHECKED BY:  
 P. AMICONE  
 DRAFTED BY:  
 J. KLEINMANN  
 CHECKED BY:  
 P. AMICONE  
 DESIGNED BY:  
 D. MARSH



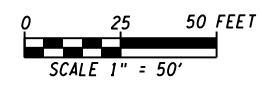
- NOTES:**
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-4.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

DOWEL BAR RETROFIT REPAIRS						
LOCATION				REPAIR ITEMS		REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
901.18	SB 64+33	S11C	12	8	12	

PARTIAL DEPTH REPAIR						
MILE MARKER	STATION	SLAB NUMBER	REPAIR DIMENSIONS		PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	REMARKS
			LENGTH (FT)	WIDTH (FT)		
901.03	NB 63+02	N6L	2	1	2.0	2 SEPARATE REPAIRS (1'x 1' & 1'x 1')
901.05	NB 63+83	N8L	12	1	12.0	5 SEPARATE REPAIRS ((4)-2'x 1' & 2'x 2')
901.06	NB 64+62	N10R	3	1	3.0	2 SEPARATE REPAIRS (2'x 1' & 1'x 1')
901.06	NB 64+62	N10L	1	1	1.0	
901.07	NB 65+02	N11L	1.5	1	1.5	
901.08	NB 65+42	N12L	1	1	1.0	
901.20	NB 66+63	N1C	2	2	4.0	
901.21	NB 67+05	N2R	2	2	4.0	
901.15	SB 62+70	S7C	2	1	2.0	2 SEPARATE REPAIRS (1'x 1' & 1'x 1')
901.15	SB 63+11	S8R	2	1	2.0	
901.16	SB 63+53	S9R	9	1	9.0	2 SEPARATE REPAIRS (8'x 1' & 1'x 1')
901.16	SB 63+53	S9C	3	1	3.0	
901.17	SB 63+93	S10A	1	1	1.0	2 SEPARATE REPAIRS (8'x 1' & 1'x 1')
901.18	SB 64+33	S11A	4	1	4.0	2 SEPARATE REPAIRS (2'x 1' & 2'x 1')
901.18	SB 64+33	S11R	3	1	3.0	2 SEPARATE REPAIRS (2'x 1' & 1'x 1')
901.18	SB 64+33	S11L	12	1	12.0	2 SEPARATE REPAIRS (10'x 1' & 2'x 1')
901.19	SB 64+73	S12L	22	1	22.0	2 SEPARATE REPAIRS (18'x 1' & (2)-2'x 1')
901.20	SB 65+14	S1A	2	1	2.0	
901.20	SB 65+14	S1R	2	2	4.0	
901.20	SB 65+14	S1L	2	1	2.0	
901.21	SB 65+55	S2L	5	1	5.0	
901.22	SB 65+95	S3A	3	1	3.0	
901.22	SB 65+95	S3R	1	1	1.0	
901.22	SB 65+95	S3L	1	1	1.0	
901.22	SB 66+36	S4L	5	1	5.0	2 SEPARATE REPAIRS (2.5'x 1' & 2.5'x 1')
901.23	SB 66+76	S5A	3	2	6.0	
901.23	SB 66+76	S5L	16	1	16.0	3 SEPARATE REPAIRS (5'x 1, 2.5'x 1' & 8.5'x 1')

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE

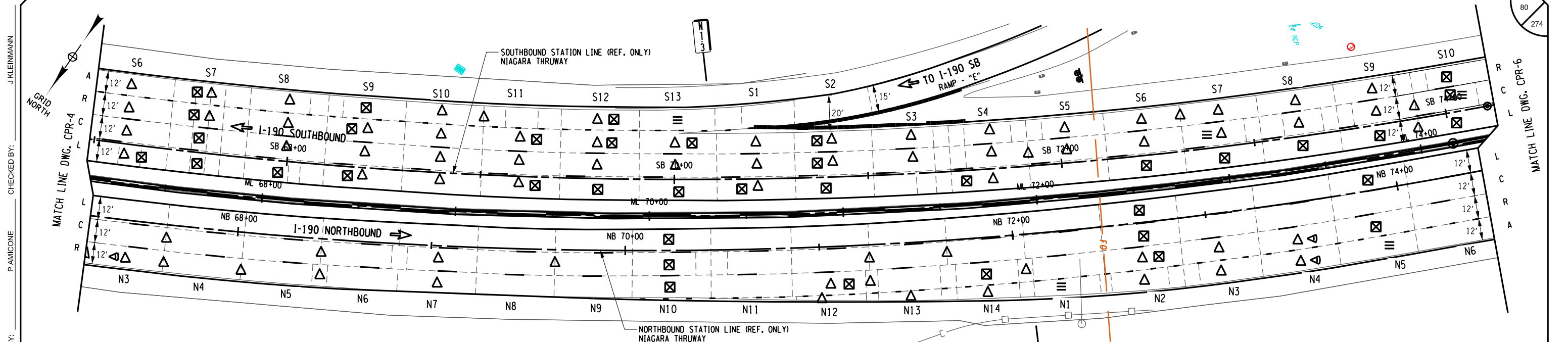


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SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-4



CHECKED BY: J. KLEINMANN  
 DRAFTED BY: P. AMICONE  
 CHECKED BY: J. KLEINMANN  
 CHECKED BY: P. AMICONE  
 DESIGNED BY: D. MARSH  
 DESIGN SUPERVISOR:

**PARTIAL DEPTH REPAIR**

LOCATION		REPAIR DIMENSIONS				REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	PD REPAIRS HES CONC SAW CUTTING ITEM 502.4220018 (SF)	
901.27	NB 70+24	N10L	1	1	1.0	
901.27	NB 70+24	N10C	3	2	6.0	2 SEPARATE REPAIRS (2'x 1' & 2'x 2')
901.27	NB 70+24	N10R	1	1	1.0	
901.29	NB 71+06	N12L	1	1	1.0	
901.30	NB 71+83	N14R	4	1	4.0	2 SEPARATE REPAIRS (2'x 1' & 2'x 1')
901.31	NB 72+66	N2R	7	1	7.0	
901.31	NB 72+66	N2C	2	1	2.0	
901.31	NB 72+66	N2L	10	1	10.0	
901.33	NB 73+85	N5L	1	1	1.0	
901.33	NB 73+85	N5R	3	2	6.0	
901.24	SB 67+17	S6L	6	1	6.0	2 SEPARATE REPAIRS (4'x 1' & 6'x 1')
901.25	SB 67+57	S7A	1	1	1.0	
901.25	SB 67+57	S7R	1	1	1.0	
901.25	SB 67+57	S7C	5.5	1	5.5	2 SEPARATE REPAIRS (4.5'x 1' & 1'x 1')
901.25	SB 67+57	S7L	15	1	15.0	2 SEPARATE REPAIRS (10'x 1' & 5'x 1')
901.26	SB 67+98	S8L	36	1	36.0	3 SEPARATE REPAIRS (22'x 1, 13'x 1' & 1'x 1')
901.26	SB 68+38	S9A	3	1	3.0	
901.26	SB 68+38	S9R	13	1	13.0	2 SEPARATE REPAIRS (10'x 1' & 3'x 1')
901.26	SB 68+38	S9L	13	1	13.0	
901.28	SB 69+20	S11R	7	1	7.0	
901.28	SB 69+20	S11L	3	1	3.0	3 SEPARATE REPAIRS (1'x 1, 1'x 1' & 1'x 1')
901.29	SB 69+61	S12L	7	1	7.0	2 SEPARATE REPAIRS (3'x 2' & 1'x 1')
901.29	SB 69+61	S12R	6	1	6.0	
901.29	SB 69+61	S12A	2	1	2.0	
901.29	SB 70+02	S13L	16.5	1	16.5	MULTIPLE SIZE REPAIRS AND LOCATIONS

- NOTES:**
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWGS. CPRT-5 AND CPRT-6.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

**PARTIAL DEPTH REPAIR**

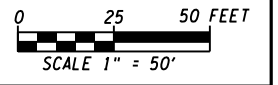
LOCATION		REPAIR DIMENSIONS			REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	
901.29	SB 70+02	S13R	1	1	1.0
901.30	SB 70+42	S1L	5	1	5.0
901.31	SB 70+82	S2R	1	1	1.0
901.31	SB 70+82	S2C	1	1	1.0
901.31	SB 70+82	S2L	9	1	9.0
901.33	SB 71+63	S4L	10	1	10.0
901.34	SB 72+43	S6L	17	1	17.0
901.35	SB 72+83	S7L	5	1	5.0
901.36	SB 73+23	S8L	1	1	1.0
901.37	SB 73+65	S9L	1	1	1.0
901.37	SB 74+05	S10R	1	1	1.0
901.37	SB 74+05	S10C	1	1	1.0
901.37	SB 74+05	S10L	2	1	2.0

**DOWEL BAR RETROFIT REPAIRS**

LOCATION		REPAIR ITEMS				REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
901.30	NB 71+98	N1A	9	6	9	
901.33	NB 73+85	N5A	12	8	12	
901.30	SB 70+01	S13A	12	8	12	
901.35	SB 72+82	S7C	12	8	12	
901.37	SB 74+04	S10C	12	8	12	

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



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SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

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**REVISIONS**

DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY**

**Thruway Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-5



CHECKED BY: J. KLEINMANN

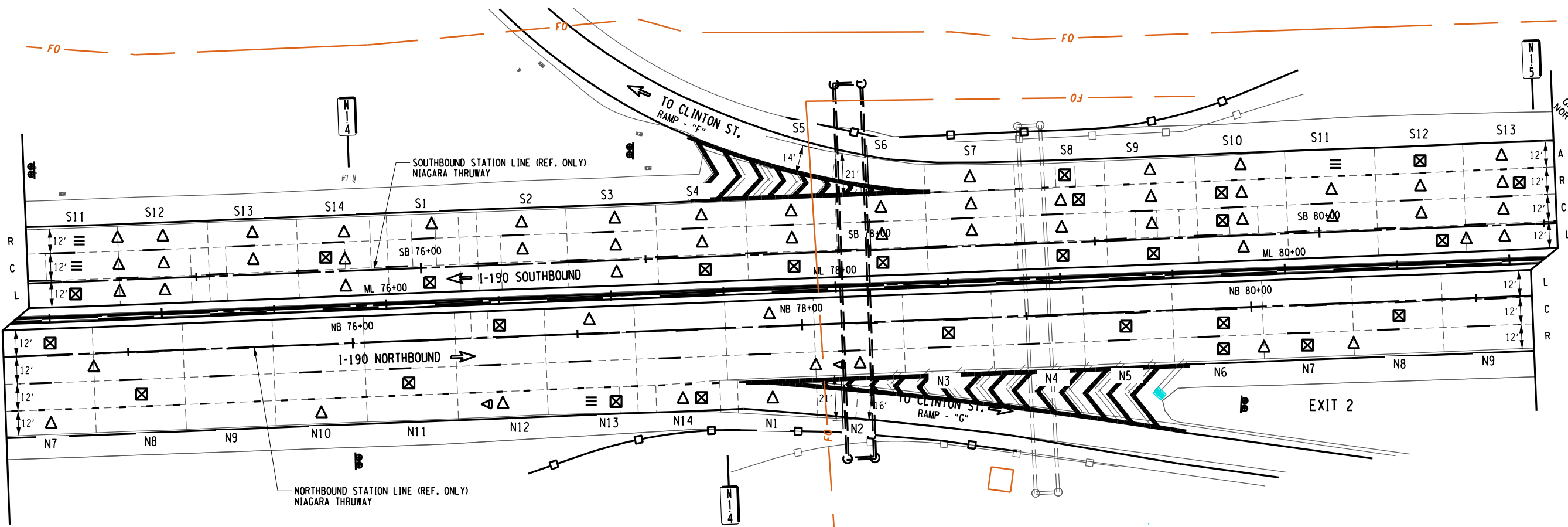
DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

81  
274



MATCH LINE DWG. CPR-5

MATCH LINE DWG. CPR-7

NOTES:

- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPR-7.
- LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

PARTIAL DEPTH REPAIR

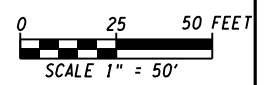
LOCATION		REPAIR DIMENSIONS			PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)		
901.35	NB 74+63	N7L	2	1	2.0	
901.36	NB 75+04	N8R	1	1	1.0	
901.38	NB 76+25	N11R	3	1	3.0	
901.39	NB 76+65	N12L	2	1	2.0	
901.39	NB 77+05	N13A	2	1	2.0	
901.40	NB 77+45	N14A	3	1	3.0	3 SEPARATE REPAIRS (1'x 1', 1'x 1' & 1'x 1')
901.42	NB 78+65	N3C	2	2	4.0	
901.43	NB 79+45	N5C	5	1	5.0	2 SEPARATE REPAIRS (2'x 2' & 1'x 1')
901.44	NB 79+85	N6C	1	1	1.0	
901.44	NB 79+85	N6R	1	1	1.0	
901.45	NB 80+24	N7R	1	1	1.0	
901.46	NB 80+64	N8C	1	1	1.0	
901.38	SB 74+43	S11L	1	1	1.0	
901.40	SB 75+67	S14C	1	1	1.0	
901.41	SB 76+06	S1L	5	1	5.0	
901.43	SB 77+25	S4L	1	1	1.0	
901.44	SB 77+65	S5L	5	1	5.0	
901.44	SB 78+05	S6L	1	1	1.0	
901.46	SB 78+85	S8L	2	1	2.0	
901.46	SB 78+85	S8R	2	2	4.0	
901.46	SB 78+85	S8A	2	1	2.0	
901.47	SB 79+26	S9L	3	1	3.0	
901.47	SB 79+64	S10C	7	1	7.0	2 SEPARATE REPAIRS (3'x 2' & 1'x 1')
901.47	SB 79+64	S10R	3	2	6.0	
901.49	SB 80+46	S12A	3	2	6.0	
901.49	SB 80+46	S12L	1	1	1.0	
901.50	SB 80+85	S13R	1	1	1.0	

DOWEL BAR RETROFIT REPAIRS

LOCATION		REPAIR ITEMS			REMARKS	
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)		FILL CRACKS ITEM 502.90010018 (LF)
901.39	NB 77+06	N13A	12	8	12	
901.38	SB 74+43	S11R	12	8	12	
901.38	SB 74+44	S11C	12	8	12	
901.48	SB 80+05	S11R	12	8	12	

LEGEND

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority**

TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

CONTRACT NUMBER:  
TAN 24-12

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

DATE:  
MAY 2024

TITLE OF DRAWING  
CONCRETE PAVEMENT  
REPAIR PLANS

DRAWING NUMBER:  
CPR-6

CHECKED BY: J. KLEINMANN

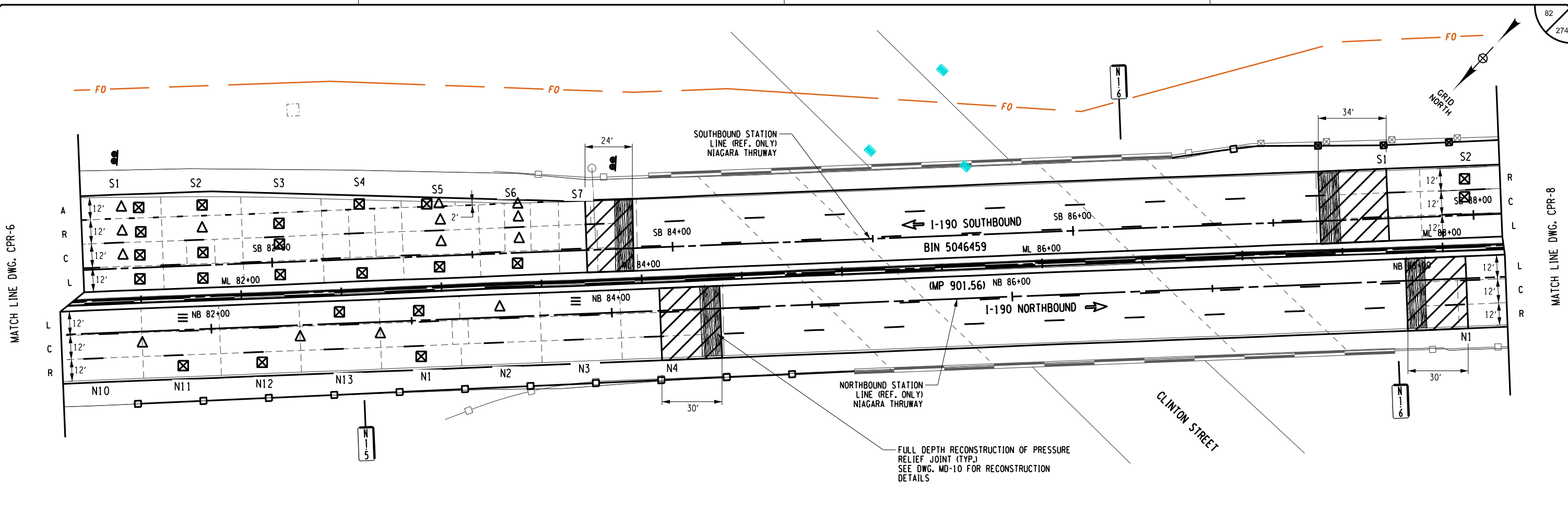
DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

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274



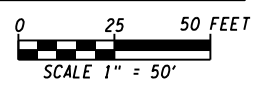
- NOTES:
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-8.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

DOWEL BAR RETROFIT REPAIRS						
LOCATION			REPAIR ITEMS			REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
901.48	NB 81+85	N11L	12	8	12	
901.52	NB 83+84	N3L	12	8	12	
901.53	NB 84+25	N4L	12	8	12	

PARTIAL DEPTH REPAIR						
LOCATION		REPAIR DIMENSIONS			PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)		
901.48	NB 81+85	N11R	1	1	1.0	
901.49	NB 82+23	N12R	1	1	1.0	
901.49	NB 82+64	N13L	1	1	1.0	
901.51	NB 83+04	N1L	3	1	3.0	
901.51	NB 83+04	N1R	1	1	1.0	
901.51	SB 81+26	S1A	2	2	4.0	
901.51	SB 81+26	S1R	2	1	2.0	
901.51	SB 81+26	S1C	2	1	2.0	
901.51	SB 81+26	S1L	5	3	15.0	
901.51	SB 81+64	S2C	12	1	12.0	
901.51	SB 81+64	S2L	4	1	4.0	2 SEPARATE REPAIRS (3'x 1' & 1'x 1')
901.51	SB 81+64	S2A	2	2	4.0	
901.52	SB 82+04	S3R	3	1	3.0	
901.52	SB 82+04	S3C	2	2	4.0	
901.52	SB 82+04	S3L	1	1	1.0	
901.53	SB 82+46	S4A	1	1	1.0	
901.53	SB 82+46	S4L	3	2	6.0	
901.54	SB 82+84	S5A	1	1	1.0	
901.54	SB 82+84	S5L	7	1	7.0	
901.54	SB 83+23	S6L	34	1	34.0	2 SEPARATE REPAIRS (30'x 1' & 4'x 1')
901.63	SB 87+95	S2R	7	1	7.0	
901.63	SB 87+95	S2C	12	1	12.0	2 SEPARATE REPAIRS (10'x 1' & 2'x 1')

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



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SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-7

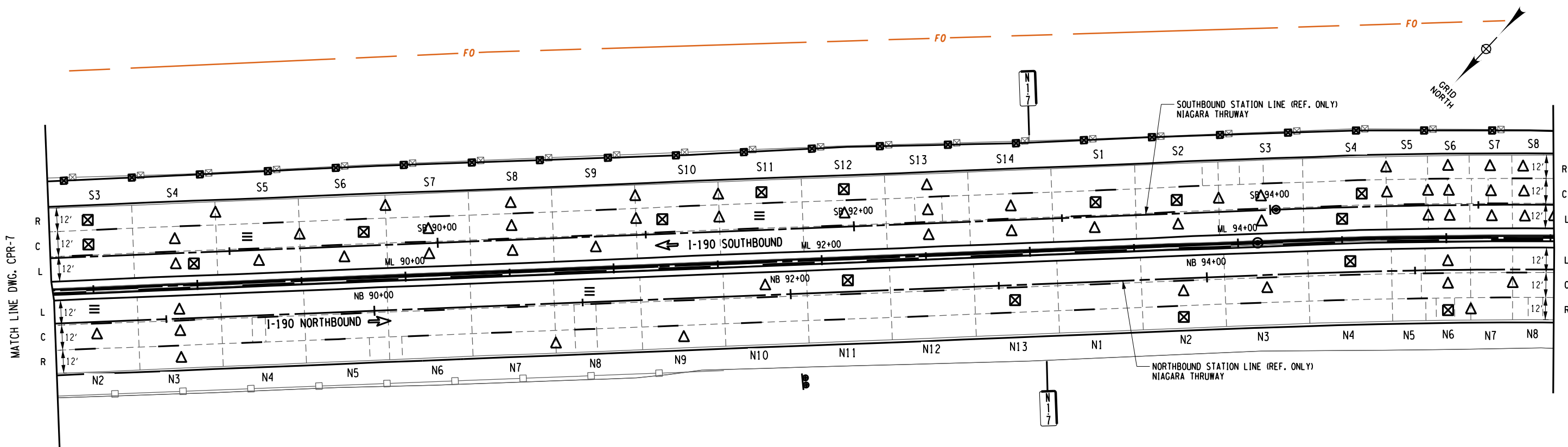
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH



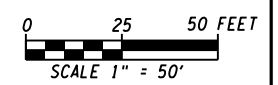
- NOTES:**
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPR1-9.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

PARTIAL DEPTH REPAIR					
LOCATION			REPAIR DIMENSIONS		REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	
901.68	NB 92+28	N11L	3	1	3.0
901.70	NB 93+08	N13C	1	1	1
901.71	NB 93+89	N2R	3	1	3
901.73	NB 94+67	N4L	1	1	1
901.74	NB 95+15	N6R	3	1	3
901.64	SB 88+33	S3R	5	1	5
901.64	SB 88+33	S3C	1	1	1
901.65	SB 88+75	S4L	1	1	1
901.66	SB 89+55	S6C	1	1	1
901.69	SB 91+14	S10C	3	2	6
901.70	SB 91+56	S11R	6	1	6
901.71	SB 91+95	S12R	2	1	2
901.71	SB 93+17	S1C	3	2	6
901.71	SB 93+55	S2C	3	2	6
901.73	SB 94+37	S4L	1	1	1
901.73	SB 94+37	S4C	3	1	3

DOWEL BAR RETROFIT REPAIRS						
LOCATION				REPAIR ITEMS		REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
901.61	NB 88+67	N2L	12	8	12	
901.66	NB 91+05	N8L	12	8	12	
901.65	SB 89+15	S5C	12	8	12	
901.70	SB 91+55	S11C	12	8	12	

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-8

CHECKED BY: J. KLEINMANN

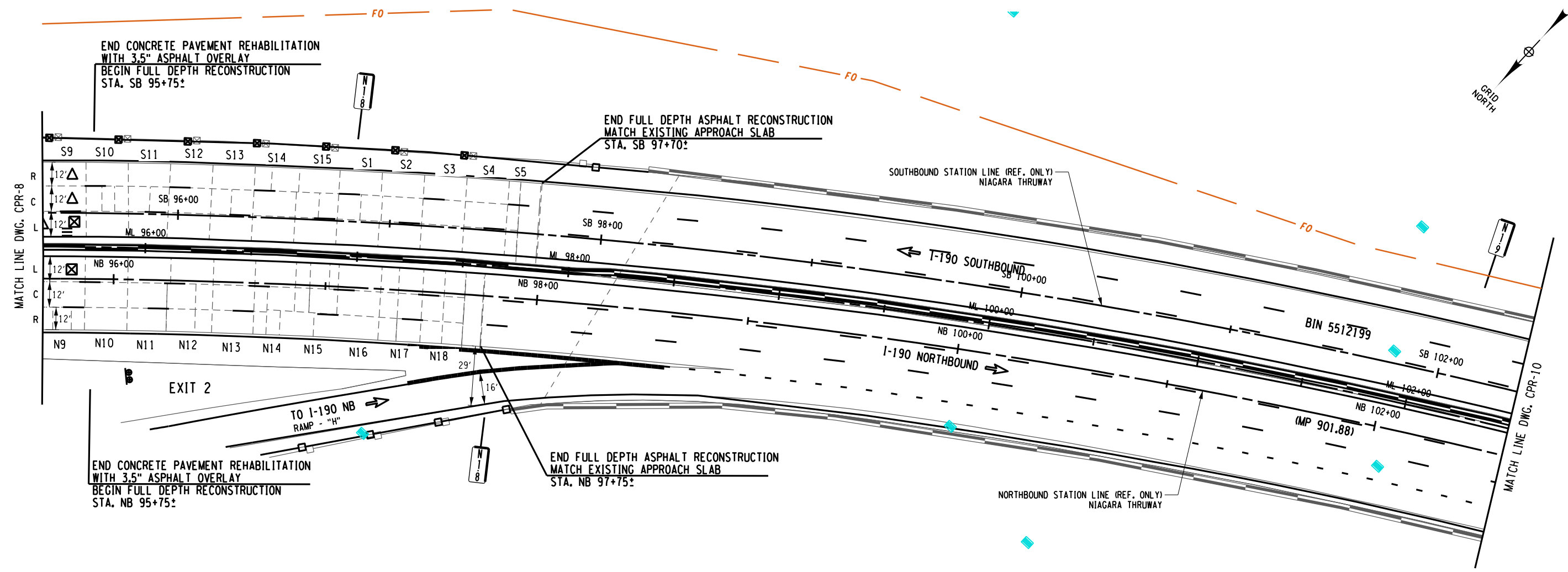
DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

DESIGNED BY: D. MARSH

84  
274



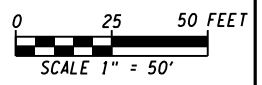
NOTES:

1. FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-9.
2. LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

DOWEL BAR RETROFIT REPAIRS						
LOCATION			REPAIR ITEMS			REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
901.75	SB 95+47	S9L	12	8	12	

PARTIAL DEPTH REPAIR						
LOCATION			REPAIR DIMENSIONS			REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	
901.75	NB 95+76	N9L	2	1	2.0	
901.75	SB 95+47	S9L	2	2	4.0	

LEGEND	
	FULL DEPTH REPAIR
	PARTIAL DEPTH REPAIR
	DOWEL BAR RETROFIT
	HINGE JOINT
	CLEAN AND FILL CRACKS
	LEFT LANE
	CENTER LANE
	RIGHT LANE
	AUXILIARY LANE



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority**

2025 Mid-Range Tower  
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TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-9

CHECKED BY: J. KLEINMANN

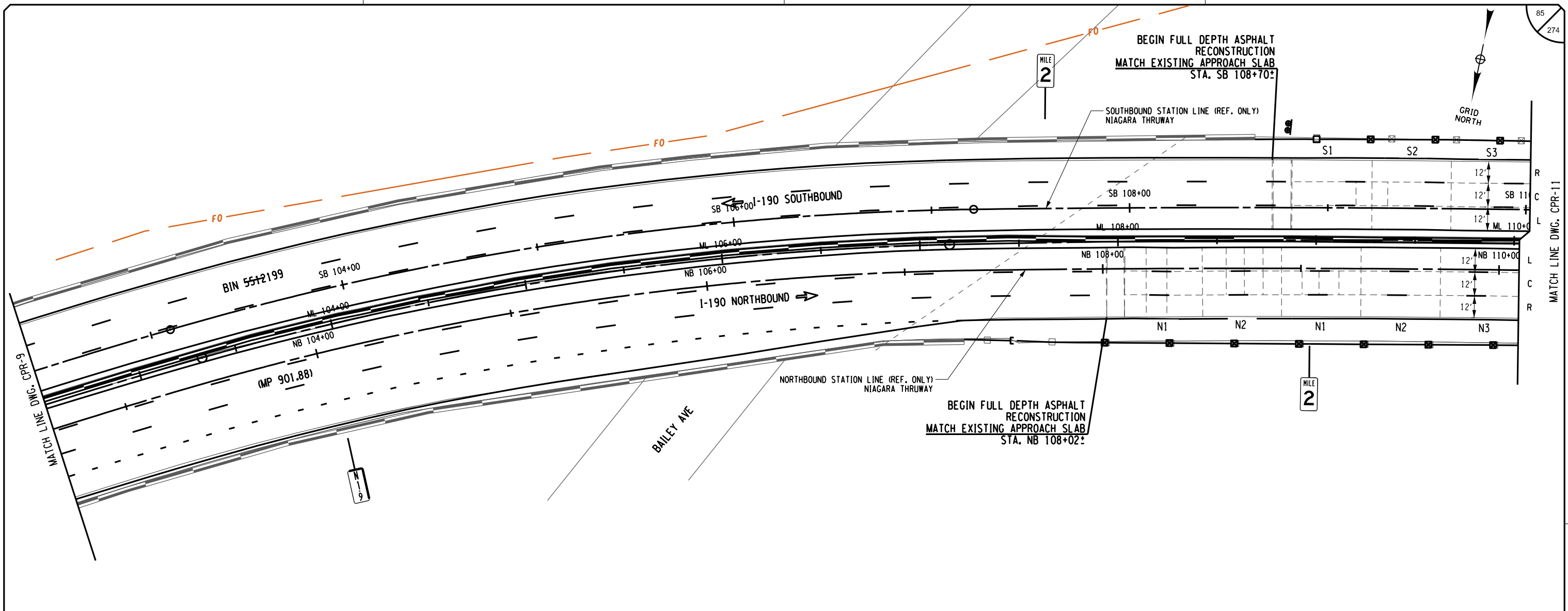
DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

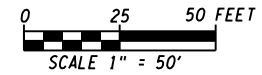
DESIGN SUPERVISOR: D. MARSH

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274



NOTES:  
 1. THERE ARE NO CONCRETE PAVEMENT REPAIRS ON THIS SHEET.

LEGEND	
▲	FULL DEPTH REPAIR
⊠	PARTIAL DEPTH REPAIR
≡	DOWEL BAR RETROFIT
◁▷	HINGE JOINT
	CLEAN AND FILL CRACKS
L	LEFT LANE
C	CENTER LANE
R	RIGHT LANE
A	AUXILIARY LANE



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-10

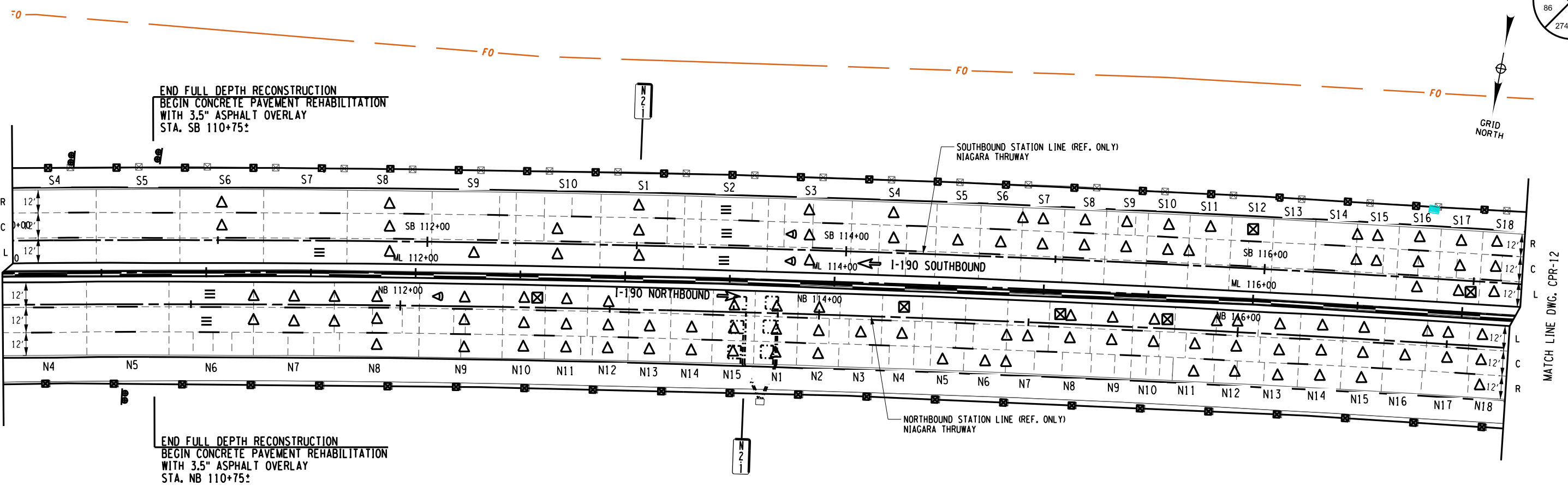
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH



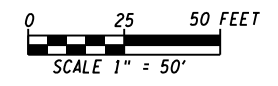
- NOTES:
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWGS. CPRT-10 AND CPRT-11.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

PARTIAL DEPTH REPAIR						
LOCATION			REPAIR DIMENSIONS			REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	
902.06	NB 112+59	N10L	6	1	6.0	2 SEPARATE REPAIRS (5'x 1' & 1'x 1')
902.07	NB 113+19	N13L	1	1	1.0	
902.11	NB 114+40	N4L	1	1	1.0	
902.13	NB 115+18	N8L	5	1	5.0	2 SEPARATE REPAIRS (4'x 1' & 1'x 1')
902.14	NB 115+60	N10L	6	1	6.0	2 SEPARATE REPAIRS (4'x 1' & 2'x 1')
902.16	SB 116+04	S12R	1	1	1.0	
902.17	SB 117+05	S17L	2	1	2.0	

DOWEL BAR RETROFIT REPAIRS						
LOCATION			REPAIR ITEMS			REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
902.04	NB 111+09	N6L	12	8	12	
902.04	NB 111+09	N6C	12	8	12	
902.07	SB 111+41	S7L	12	8	12	
902.11	SB 113+43	S2R	12	8	12	
902.11	SB 113+43	S2C	12	8	12	
902.11	SB 113+43	S2L	12	8	12	

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-11

CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

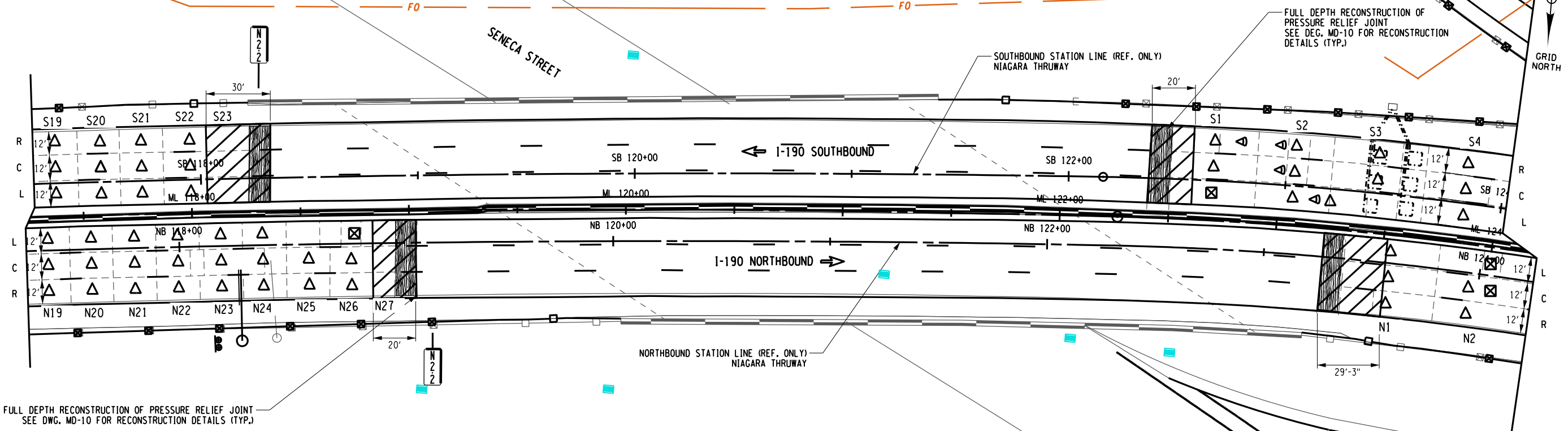
DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

87  
274

MATCH LINE DWG. CPR-11

MATCH LINE DWG. CPR-13

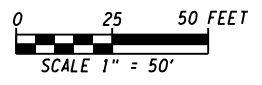


- NOTES:
- FOR FULL DEPTH CONCRETE REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-11.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

PARTIAL DEPTH REPAIR						
LOCATION			REPAIR DIMENSIONS			REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	
902.20	NB 118+79	N26L	1	1	1.0	
902.29	NB 124+03	N2L	1	1	1.0	
902.29	NB 124+03	N2C	2	1	2.0	
902.28	SB 122+66	S1L	7	1	7.0	2 SEPARATE REPAIRS (3'x 1' & 2'x 2')

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-12

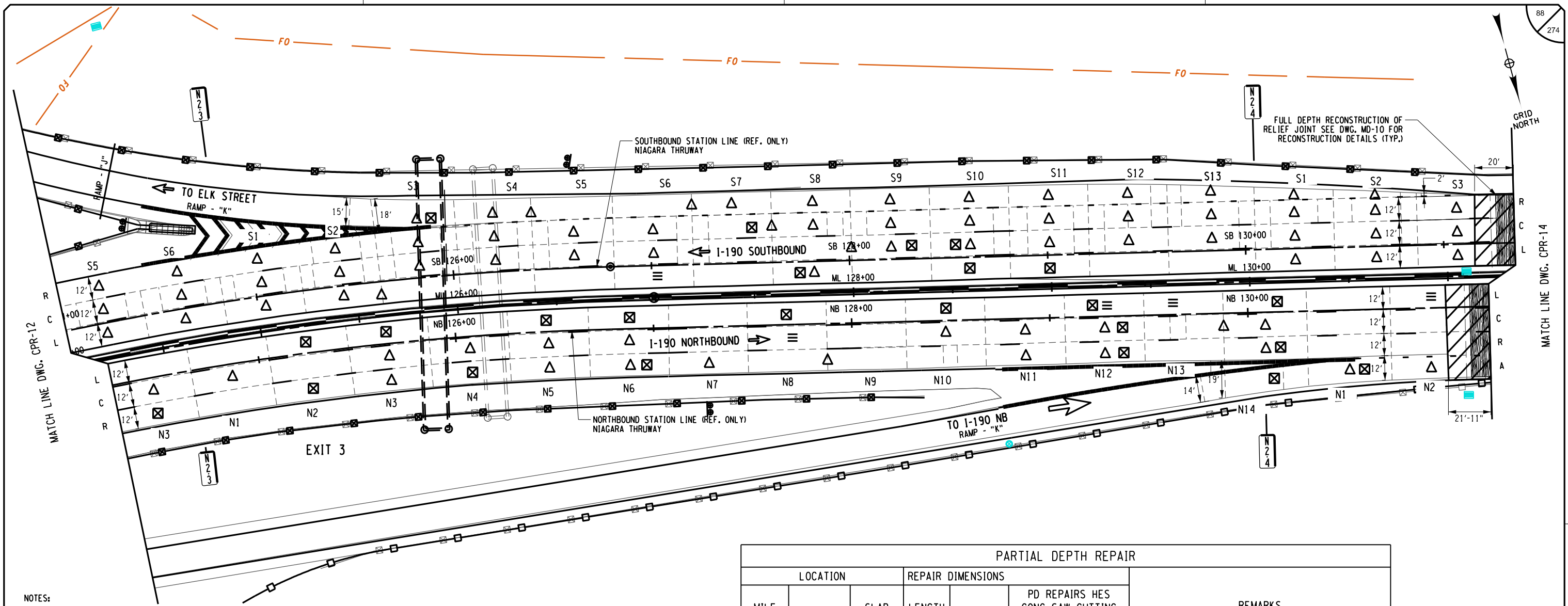
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH



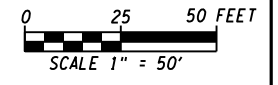
- NOTES:**
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-12.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

DOWEL BAR RETROFIT REPAIRS						
LOCATION				REPAIR ITEMS		REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
902.36	NB 127+67	N8C	12	8	12	
902.39	NB 129+35	N12L	12	8	12	
902.40	NB 129+66	N13L	12	8	12	
902.42	NB 130+93	N2L	12	8	12	
902.34	SB 127+03	S6L	12	8	12	

PARTIAL DEPTH REPAIR						
MILE MARKER	STATION	SLAB NUMBER	REPAIR DIMENSIONS		PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	REMARKS
			LENGTH (FT)	WIDTH (FT)		
902.30	NB 124+45	N3R	8	1	8.0	
902.31	NB 125+25	N2L	9	1	9.0	2 SEPARATE REPAIRS (8'x 1' & 1'x 1')
902.31	NB 125+25	N2R	2	2	4.0	
902.32	NB 125+66	N3L	2	1	2.0	
902.33	NB 126+06	N4R	1	1	1.0	
902.33	NB 126+46	N5L	3	1	3.0	
902.34	NB 126+87	N6L	1	1	1.0	
902.34	NB 126+87	N6R	7	1	7.0	
902.36	NB 127+68	N8L	1	1	1.0	
902.37	NB 128+48	N10L	15	1	15.0	2 SEPARATE REPAIRS (12'x 1' & 3'x 1')
902.39	NB 129+27	N12L	14	1	14.0	3 SEPARATE REPAIRS (12'x 1', 1'x 1' & 1'x 1')
902.39	NB 129+27	N12C	1	1	1.0	
902.39	NB 129+27	N12R	8	1	8.0	2 SEPARATE REPAIRS (2'x 2' & 4'x 1')
902.40	NB 130+07	N14L	4	1	4.0	2 SEPARATE REPAIRS (3'x 1' & 1'x 1')
902.40	NB 130+07	N14R	2	1	2.0	
902.40	NB 130+07	N14A	30	1	30.0	3 SEPARATE REPAIRS (12'x 1', 2'x 2' & 14'x 1')
902.41	NB 130+51	N1A	3	1	3.0	
902.32	SB 125+82	S3A	2	1	2.0	
902.35	SB 127+41	S7C	2.5	1.5	3.8	
902.36	SB 127+81	S8L	1	1	1.0	
902.36	SB 128+21	S9C	2	1	2.0	
902.37	SB 128+60	S10C	2.5	1.5	3.8	
902.37	SB 128+60	S10L	2	2	4.0	
902.38	SB 129+00	S11L	8	1	8.0	

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ∇ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ SIGNATURE: \_\_\_\_\_  
 STAMP: \_\_\_\_\_ STAMP: \_\_\_\_\_

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY** | **Thruway Authority**

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 CONTRACT NUMBER: TAN 24-12

LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 DATE: MAY 2024

TITLE OF DRAWING: CONCRETE PAVEMENT REPAIR PLANS  
 DRAWING NUMBER: CPR-13



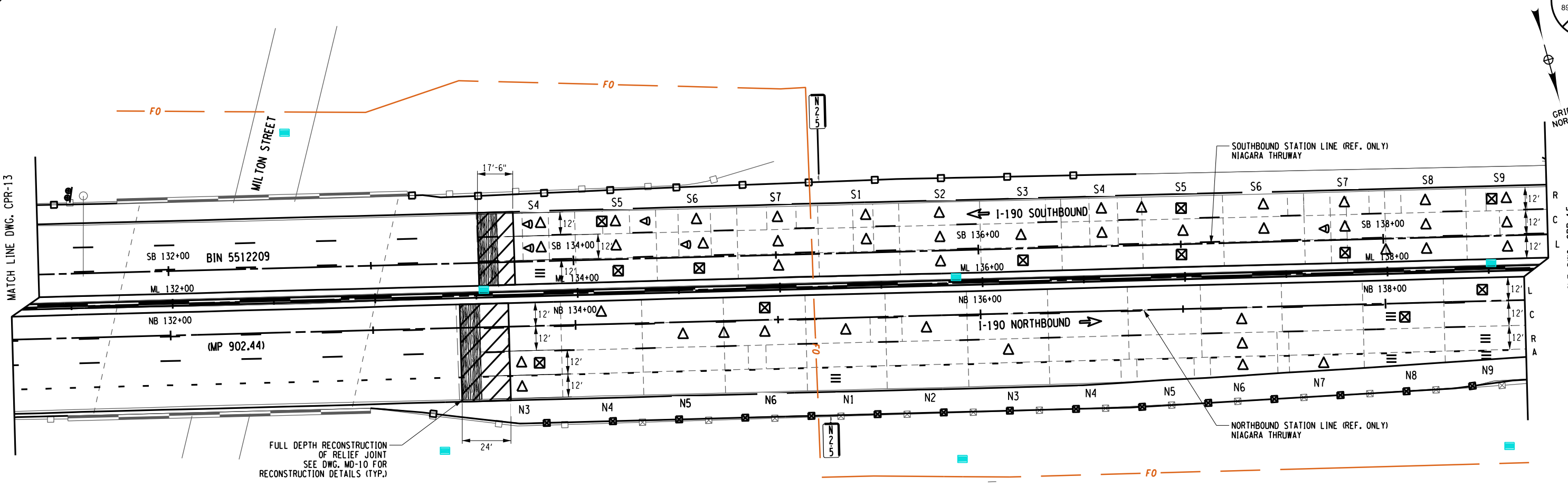
CHECKED BY: J.KLEINMANN

DRAFTED BY: P.AMICONE

CHECKED BY: J.KLEINMANN

DESIGNED BY: P.AMICONE

DESIGN SUPERVISOR: D.MARSH



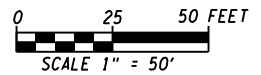
- NOTES:**
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPR-13.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

DOWEL BAR RETROFIT REPAIRS						
MILE MARKER	LOCATION		SLAB WIDTH (FT)	REPAIR ITEMS		REMARKS
	STATION	LANE		RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
902.50	NB 135+34	N1A	12	8	12	
902.55	NB 138+08	N8C	12	8	12	
902.55	NB 138+08	N8A	7	4	7	
902.56	NB 138+49	N9R	12	8	12	
902.56	NB 138+49	N9A	6	4	6	
902.47	SB 133+81	S4L	12	8	12	

PARTIAL DEPTH REPAIR						
MILE MARKER	LOCATION		REPAIR DIMENSIONS			REMARKS
	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	
902.47	NB 133+72	N3C	5	2	10.0	
902.49	NB 134+93	N6L	1	1	1.0	
902.55	NB 138+08	N8C	1	1	1.0	
902.56	NB 138+48	N9L	5	1	5.0	
902.48	SB 134+20	S5R	1	1	1.0	
902.48	SB 134+20	S5L	2	1	2.0	2 SEPARATE REPAIRS (1'x 1' & 1'x 1')
902.49	SB 134+60	S6L	1	1	1.0	
902.52	SB 136+21	S3L	1	1	1.0	
902.53	SB 137+02	S5R	1.5	1.5	2.3	
902.53	SB 137+02	S5L	1	1	1.0	
902.55	SB 137+81	S7L	1.5	1	1.5	
902.56	SB 138+62	S9R	1	1	1.0	

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

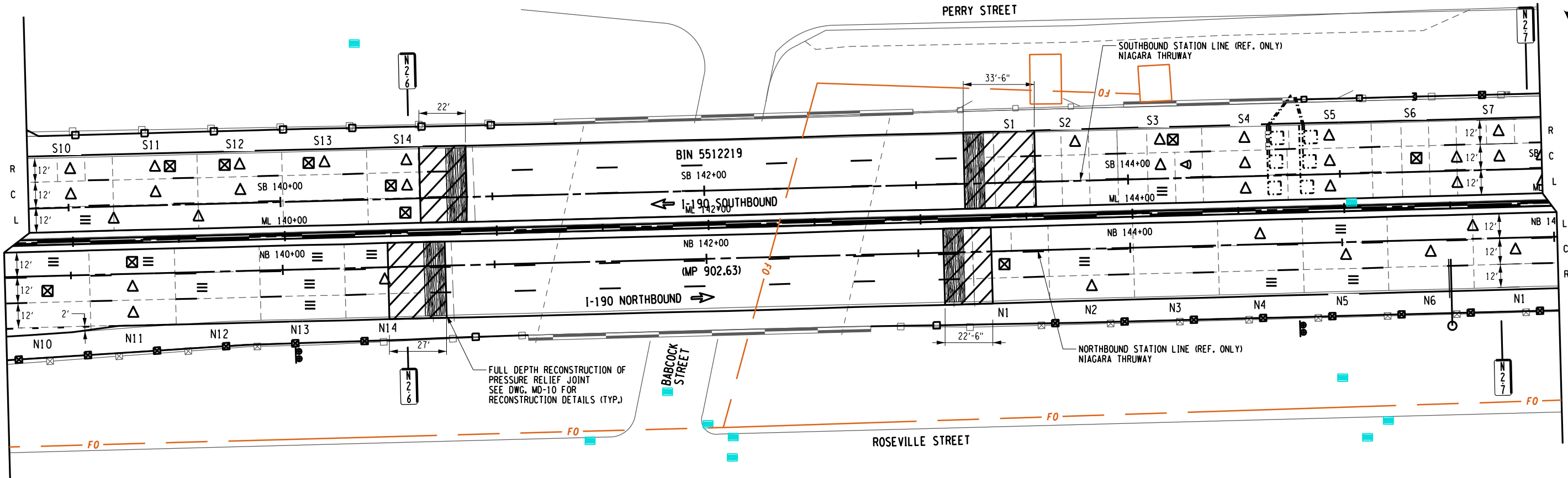
SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-14

CHECKED BY: J. KLEINMANN  
DRAFTED BY: P. AMICONE  
DESIGNED BY: P. AMICONE  
DESIGN SUPERVISOR: D. MARSH



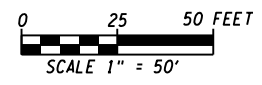
NOTES:  
1. FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPR-14.  
2. LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

MILE MARKER	LOCATION			REPAIR ITEMS		REMARKS
	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
902.57	NB 138+88	N10L	12	8	12	
902.58	NB 139+29	N11L	12	8	12	
902.58	NB 139+68	N12C	12	8	12	
902.59	NB 140+09	N13L	12	8	12	
902.59	NB 140+09	N13C	12	8	12	
902.59	NB 140+09	N13R	12	8	12	
902.60	NB 140+48	N14L	12	8	12	
902.66	NB 143+80	N2C	12	8	12	
902.68	NB 144+60	N4R	12	8	12	
902.69	NB 145+02	N5L	12	8	12	
902.69	NB 145+02	N5R	6	4	6	
902.67	SB 144+15	S3L	12	8	12	
902.57	SB 139+01	S10L	12	8	12	

LOCATION		REPAIR DIMENSIONS			REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	
902.57	NB 138+88	N10C	2	1	2.0
902.58	NB 139+28	N11L	5	1	5.0
902.65	NB 143+41	N1C	4	4	16.0
902.58	SB 139+41	S11R	1	1	1.0
902.59	SB 139+80	S12R	1	1	1.0
902.59	SB 140+21	S13R	1	1	1.0
902.60	SB 140+60	S14C	1	1	1.0
902.60	SB 140+60	S14L	3	1	3.0
902.67	SB 144+14	S3R	1	1	1.0
902.69	SB 145+36	S6C	2	1	2.0

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

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DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY Thruway Authority

2018 Mark, Place Tower, Suite, WI 14220-3722, 516.483.4800 - www.thruwayauthority.com

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-15

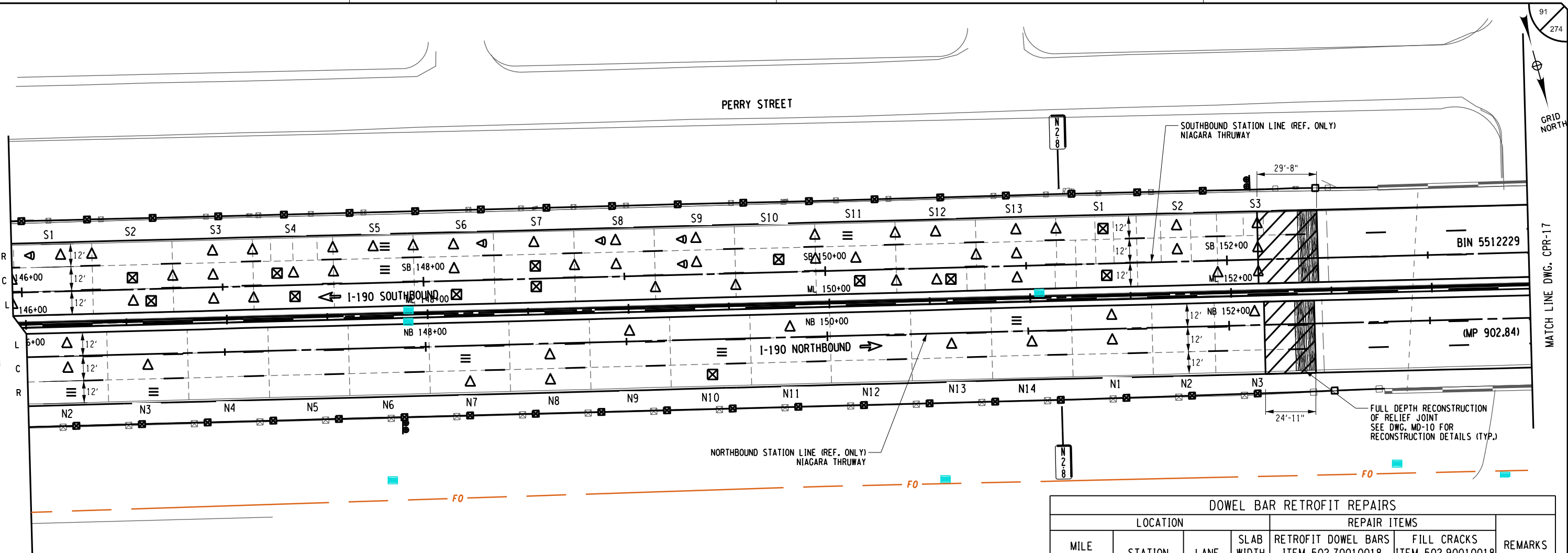
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH



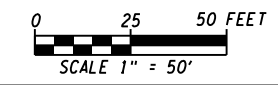
- NOTES:
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-15.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

DOWEL BAR RETROFIT REPAIRS						
LOCATION			REPAIR ITEMS		REMARKS	
MILE MARKER	STATION	LANE	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)		
902.71	NB 146+21	N2R	12	8	12	
902.72	NB 146+60	N3R	12	8	12	
902.75	NB 148+21	N7C	12	8	12	
902.77	NB 149+41	N10C	12	8	12	
902.80	NB 151+03	N14L	12	8	12	
902.74	SB 147+77	S5R	12	8	12	
902.74	SB 147+77	S5C	12	8	12	
902.78	SB 150+17	S11R	12	8	12	

PARTIAL DEPTH REPAIR						
LOCATION		REPAIR DIMENSIONS				REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	
902.77	NB 149+40	N10R	4.5	1.5	6.8	
902.71	SB 146+55	S2C	4	1	4.0	2 SEPARATE REPAIRS (3'x 1' & 1'x 1')
902.71	SB 146+55	S2L	1	1	1.0	
902.73	SB 147+34	S4C	1.5	1	1.5	
902.73	SB 147+34	S4L	1	1	1.0	
902.74	SB 148+15	S6L	7	1	7.0	
902.75	SB 148+55	S7C	1	1	1.0	
902.75	SB 148+55	S7L	12	1	12.0	2 SEPARATE REPAIRS (6'x 1' & 6'x 1')
902.77	SB 149+73	S10C	6	1	6.0	
902.78	SB 150+14	S11L	1	1	1.0	
902.79	SB 150+55	S12L	6	1	6.0	
902.80	SB 151+34	S1R	2.5	1	2.5	2 SEPARATE REPAIRS (1.5'x 1' & 1'x 1')
902.80	SB 151+34	S1L	1	1	1.0	

**LEGEND**

- △ FULL DEPTH REPAIR
- ☒ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority**

TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
**CONCRETE PAVEMENT REPAIR PLANS**

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
CPR-16

CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

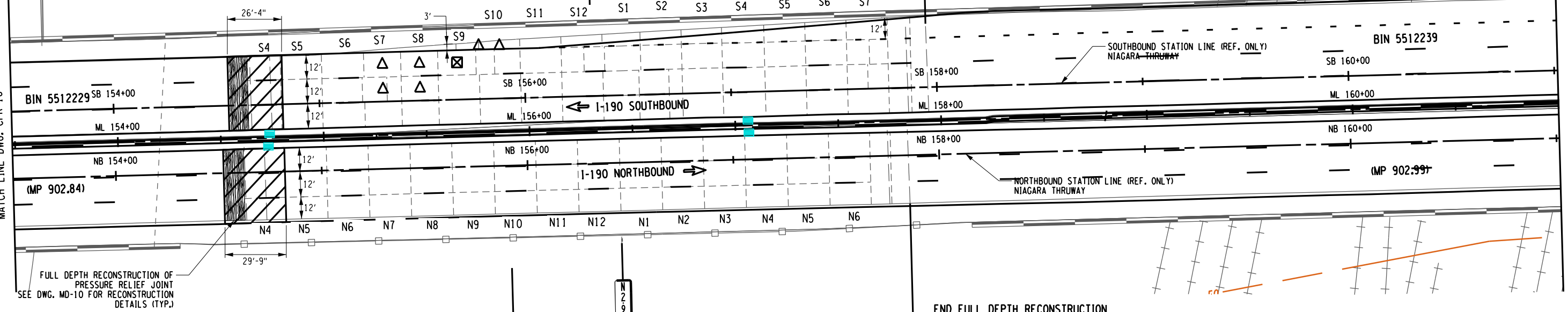
DESIGN SUPERVISOR: D. MARSH

END CONCRETE PAVEMENT REHABILITATION WITH 3.5" ASPHALT OVERLAY BEGIN FULL DEPTH RECONSTRUCTION STA. SB 155+92±

END FULL DEPTH RECONSTRUCTION MATCH EXISTING APPROACH SLAB STA. SB 157+95±

END CONCRETE PAVEMENT REHABILITATION WITH 3.5" ASPHALT OVERLAY BEGIN FULL DEPTH RECONSTRUCTION STA. SB 155+92±

END FULL DEPTH RECONSTRUCTION MATCH EXISTING APPROACH SLAB STA. NB 157+85±

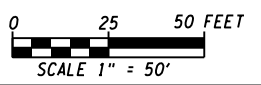


- NOTES:
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-16.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

PARTIAL DEPTH REPAIR						
MILE MARKER	LOCATION		REPAIR DIMENSIONS			REMARKS
	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	
902.87	NB 154+72	N4C	5	1	5.0	
902.87	NB 154+72	N4R	2	1	2.0	
902.89	SB 155+68	S9R	1	1	1.0	

**LEGEND**

- △ FULL DEPTH REPAIR
- ☒ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY Thruway Authority

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 CONTRACT NUMBER: TAN 24-12  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 DATE: MAY 2024  
 TITLE OF DRAWING: CONCRETE PAVEMENT REPAIR PLANS  
 DRAWING NUMBER: CPR-17

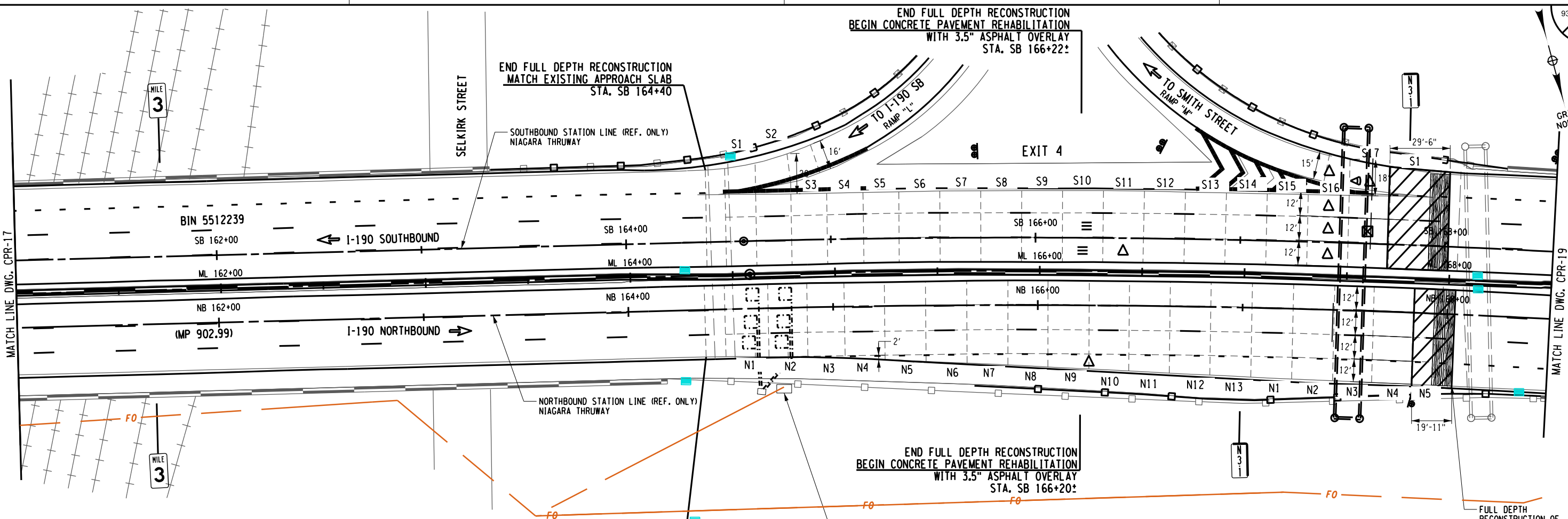
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

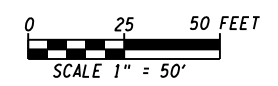


- NOTES:**
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-16.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

PARTIAL DEPTH REPAIR						
MILE MARKER	STATION	SLAB NUMBER	REPAIR DIMENSIONS		PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	REMARKS
			LENGTH (FT)	WIDTH (FT)		
903.11	SB 167+61	S17C	1	1	1.0	

**LEGEND**

- ▲ FULL DEPTH REPAIR
- ▣ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY

**Thruway Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-18

CHECKED BY: J. KLEINMANN

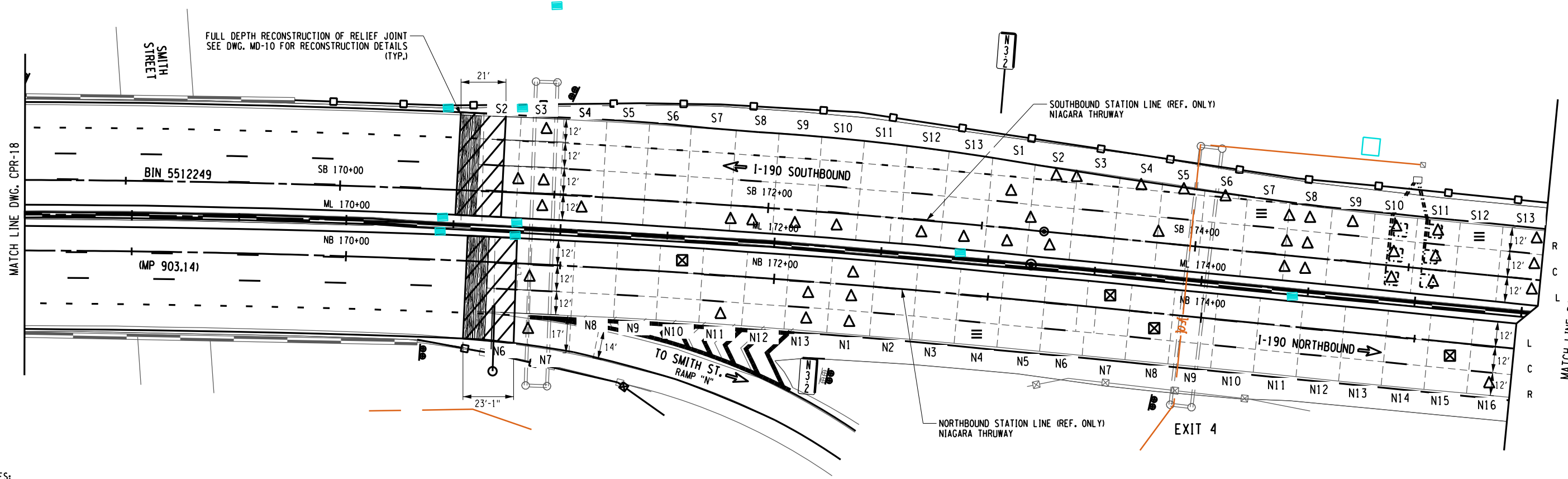
DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

DESIGNED BY: D. MARSH

GRID NORTH



- NOTES:**
1. FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-17.
  2. LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

DOWEL BAR RETROFIT REPAIRS						
LOCATION				REPAIR ITEMS		REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
903.21	NB 172+96	N4R	12	8	12	
903.22	SB 174+33	S7R	12	8	12	
903.24	SB 175+31	S12R	12	8	12	

LEGEND	
	FULL DEPTH REPAIR
	PARTIAL DEPTH REPAIR
	DOWEL BAR RETROFIT
	HINGE JOINT
	CLEAN AND FILL CRACKS
	LEFT LANE
	CENTER LANE
	RIGHT LANE
	AUXILIARY LANE

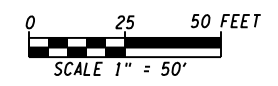
PARTIAL DEPTH REPAIR						
LOCATION			REPAIR DIMENSIONS			REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	
903.18	NB 170+96	N6L	1	1	1.0	
903.19	NB 171+55	N10L	2	1	2.0	
903.22	NB 173+56	N7L	2	1	2.0	
903.29	NB 176+77	N8C	3	2	6.0	
903.25	NB 175+16	N15C	2	1	2.0	2 SEPARATE REPAIRS (1'x 1' & 1'x 1')

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-19



CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

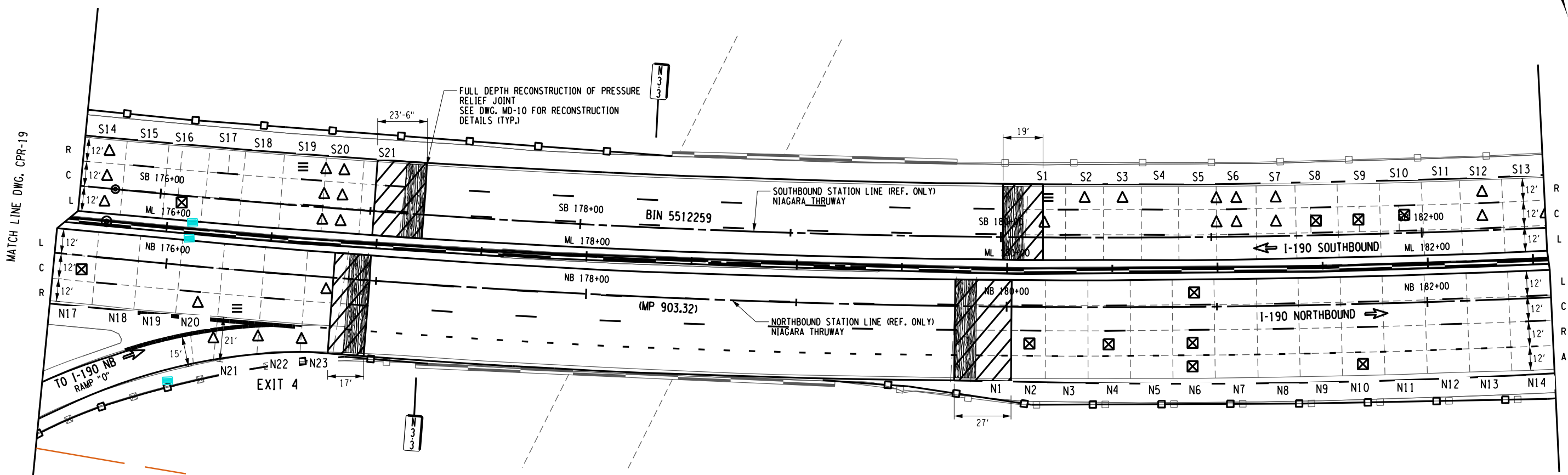
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

95  
274

GRID NORTH



- NOTES:
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-17.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

DOWEL BAR RETROFIT REPAIRS						
LOCATION				REPAIR ITEMS		REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
903.28	NB 176+35	N21R	12	8	12	
903.27	SB 176+68	S19R	12	8	12	
903.34	SB 180+21	S1R	12	8	12	

PARTIAL DEPTH REPAIR						
LOCATION			REPAIR DIMENSIONS			REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	
903.27	NB 175+57	N17C	2	1	2.0	
903.35	NB 180+10	N2R	4	1	4.0	3 SEPARATE REPAIRS (1'x 1', 1'x 1' & 2'x 1')
903.36	NB 180+48	N4R	2	1	2.0	2 SEPARATE REPAIRS ((2) - 1'x 1')
903.37	NB 180+89	N6R	1	1	1.0	
903.36	NB 180+89	N6A	1	1	1.0	
903.36	NB 180+89	N6L	1	1	1.0	
903.38	NB 181+69	N10A	1	1	1.0	
903.26	SB 176+10	S16L	1	1	1.0	
903.36	SB 181+51	S8C	4	1	4.0	4 SEPARATE REPAIRS ((4) - 1'x 1')
903.36	SB 181+70	S9C	4	1	4.0	4 SEPARATE REPAIRS ((4) - 1'x 1')
903.37	SB 181+90	S10C	2	1	2.0	2 SEPARATE REPAIRS ((2) - 1'x 1')

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



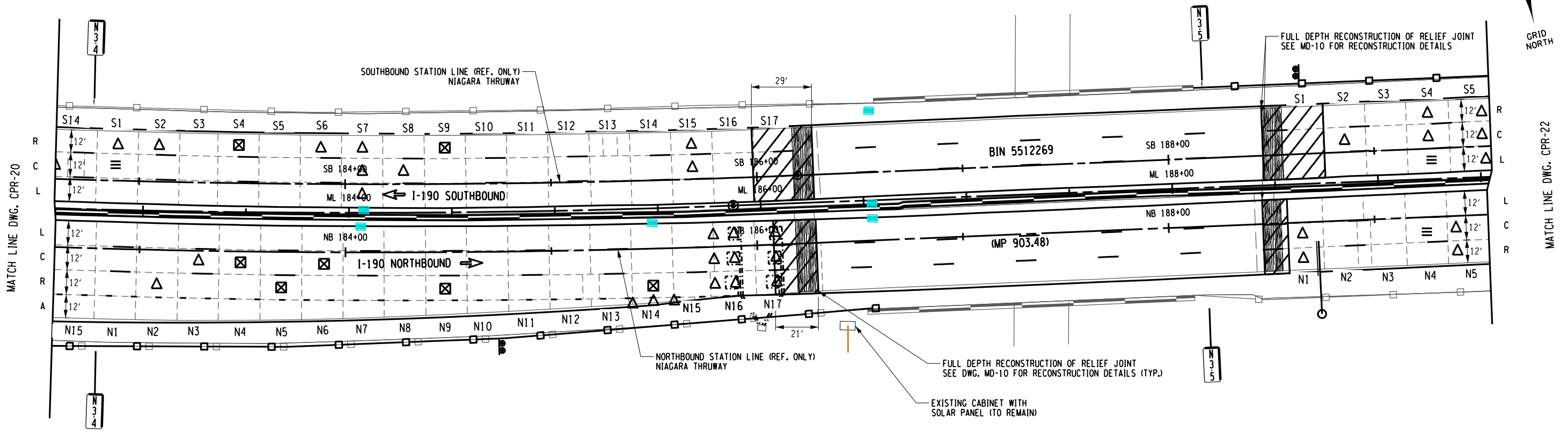
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-20

CHECKED BY: J. KLEINMANN  
 DRAFTED BY: P. AMICONE  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: P. AMICONE  
 DESIGN SUPERVISOR: D. MARSH



MATCH LINE DWG. CPR-20

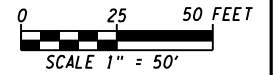
MATCH LINE DWG. CPR-22

- NOTES:**
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-18.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

DOWEL BAR RETROFIT REPAIRS						
LOCATION			REPAIR ITEMS		REMARKS	
MILE MARKER	STATION	LANE	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)		
903.52	NB 189+25	N4C	12	8	12	
903.40	SB 182+90	S1C	12	8	12	
903.52	SB 189+26	S4L	12	8	12	

PARTIAL DEPTH REPAIR					
LOCATION		REPAIR DIMENSIONS			REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	
903.41	NB 183+48	N4C	1	1	1.0
903.42	NB 183+68	N5R	1	1	1.0
903.42	NB 183+89	N6C	1	1	1.0
903.43	NB 184+49	N9R	2	1	2.0
903.45	NB 185+49	N14R	2	1	2.0
903.41	SB 183+48	S4R	1	1	1.0
903.43	SB 184+48	S9R	1	1	1.0

LEGEND	
	FULL DEPTH REPAIR
	PARTIAL DEPTH REPAIR
	DOWEL BAR RETROFIT
	HINGE JOINT
	CLEAN AND FILL CRACKS
<b>L</b>	LEFT LANE
<b>C</b>	CENTER LANE
<b>R</b>	RIGHT LANE
<b>A</b>	AUXILIARY LANE



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

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TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-21



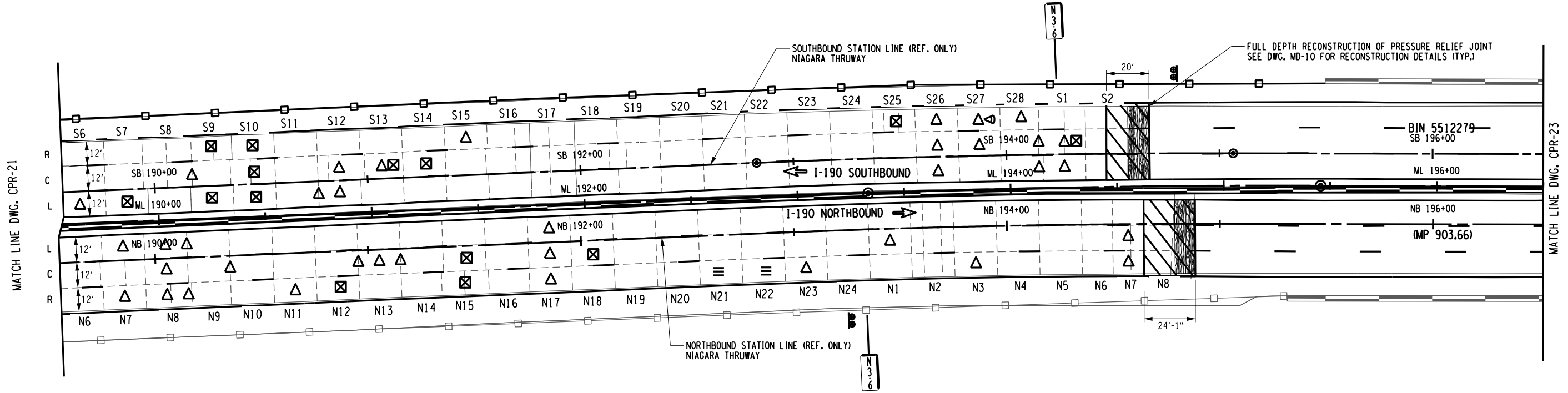
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH



NOTES:

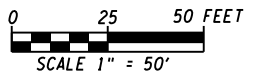
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-18.
- LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

DOWEL BAR RETROFIT REPAIRS						
LOCATION			REPAIR ITEMS			REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
903.58	NB 192+66	N21R	12	8	12	
903.59	NB 192+85	N22R	12	8	12	

PARTIAL DEPTH REPAIR						
LOCATION		REPAIR DIMENSIONS			REMARKS	
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)		PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)
903.55	NB 190+85	N12R	2	1	2.0	
903.56	NB 191+43	N15C	1	1	1.0	
903.56	NB 191+43	N15R	3	1	3.0	3 SEPARATE REPAIRS ((1) - 1'x 1')
903.57	NB 192+04	N18C	1	1	1.0	
903.43	SB 189+87	S7L	1.5	1	1.5	
903.44	SB 190+26	S9R	1.5	1	1.5	
903.44	SB 190+26	S9L	1	1	1.0	
903.44	SB 190+46	S10C	5	1	5.0	4 SEPARATE REPAIRS ((2) - 1'x 1' & (2) - 1.5'x 1')
903.44	SB 190+46	S10R	1.5	1	1.5	
903.44	SB 190+46	S10L	1	1	1.0	
903.45	SB 191+06	S13C	1.5	1	1.5	
903.46	SB 191+27	S14C	1	1	1.0	
903.50	SB 193+47	S25R	1	1	1.0	
903.60	SB 194+28	S1C	1	1	1.0	

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-22

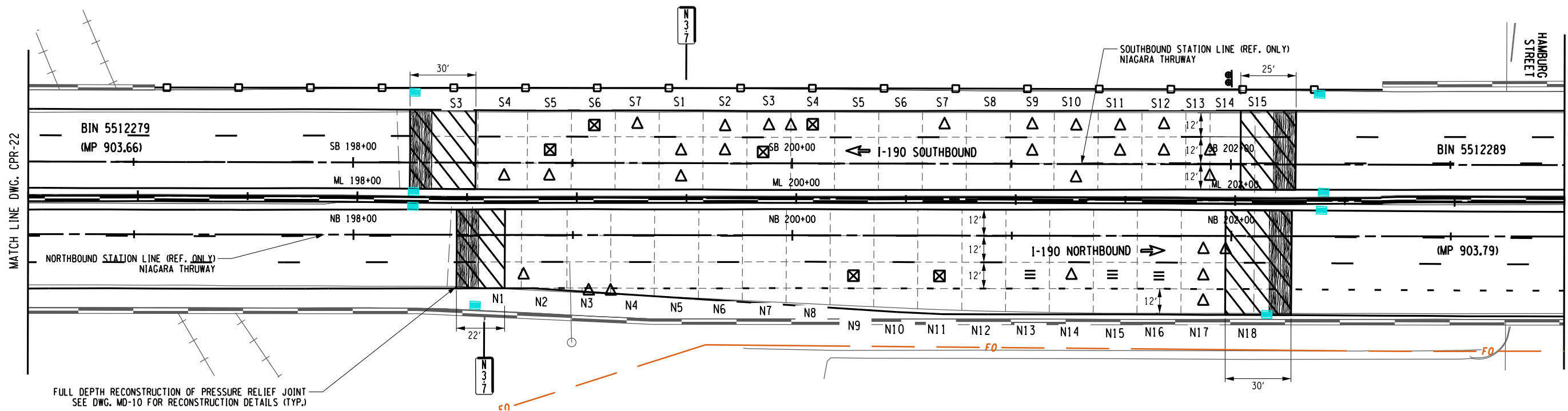
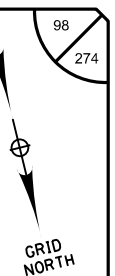
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH



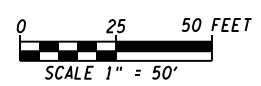
NOTES:

- 1. FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-19.
- 2. LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

DOWEL BAR RETROFIT REPAIRS						
LOCATION			REPAIR ITEMS			REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
903.75	NB 201+08	N13R	12	8	12	
903.75	NB 201+48	N15R	12	8	12	
903.76	NB 201+67	N16R	12	8	12	

PARTIAL DEPTH REPAIR						
LOCATION		REPAIR DIMENSIONS			PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)		
903.73	NB 200+27	N9R	1	1	1.0	
903.74	NB 200+67	N11R	1	1	1.0	
903.77	NB 202+07	N18C	1	1	1.0	
903.69	SB 198+88	S5C	1	1	1.0	
903.69	SB 199+09	S6R	1	1	1.0	
903.71	SB 199+90	S3C	1	1	1.0	
903.71	SB 200+09	S4R	1	1	1.0	

LEGEND	
	FULL DEPTH REPAIR
	PARTIAL DEPTH REPAIR
	DOWEL BAR RETROFIT
	HINGE JOINT
	CLEAN AND FILL CRACKS
	LEFT LANE
	CENTER LANE
	RIGHT LANE
	AUXILIARY LANE



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ SIGNATURE: \_\_\_\_\_  
 STAMP: \_\_\_\_\_ STAMP: \_\_\_\_\_

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-23

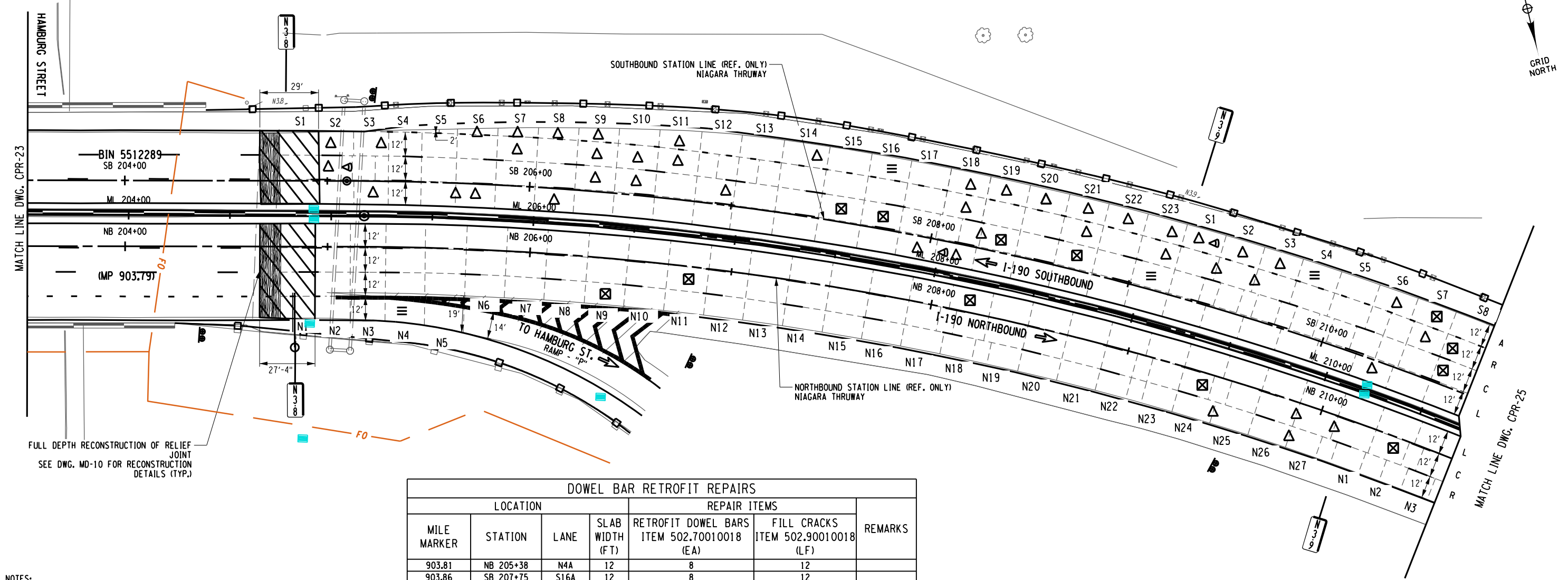
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH



FULL DEPTH RECONSTRUCTION OF RELIEF JOINT  
SEE DWG. MD-10 FOR RECONSTRUCTION DETAILS (TYP.)

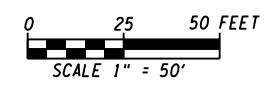
DOWEL BAR RETROFIT REPAIRS						
LOCATION			REPAIR ITEMS			REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	FILL CRACKS ITEM 502.90010018 (LF)	
903.81	NB 205+38	N4A	12	8	12	
903.86	SB 207+75	S16A	12	8	12	
903.88	SB 209+12	S23C	12	8	12	
903.91	SB 209+89	S4A	12	8	12	

- NOTES:
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-19.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

PARTIAL DEPTH REPAIR						
LOCATION			REPAIR DIMENSIONS		PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)		
903.83	NB 206+39	N9R	1	1	1.0	
903.84	NB 206+78	N11C	1	1	1.0	
903.86	NB 208+18	N18L	2	1	2.0	
903.89	NB 209+39	N24C	2	1	2.0	
903.90	NB 210+38	N2C	2	1	2.0	
903.85	SB 207+54	S15C	1	1	1.0	
903.86	SB 207+74	S16C	1	1	1.0	
903.87	SB 208+32	S19C	1.5	1.5	2.3	
903.87	SB 208+72	S21C	2	1	2.0	
903.92	SB 210+47	S7A	1	1	1.0	
903.93	SB 210+66	S8R	1	1	1.0	
903.93	SB 210+66	S8C	1	1	1.0	

**LEGEND**

- △ FULL DEPTH REPAIR
- ⊠ PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

STATE OF NEW YORK  
JOSEPH W. KLEINMANN  
LICENSED PROFESSIONAL ENGINEER  
EXCELLENCE  
092577

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-24

CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

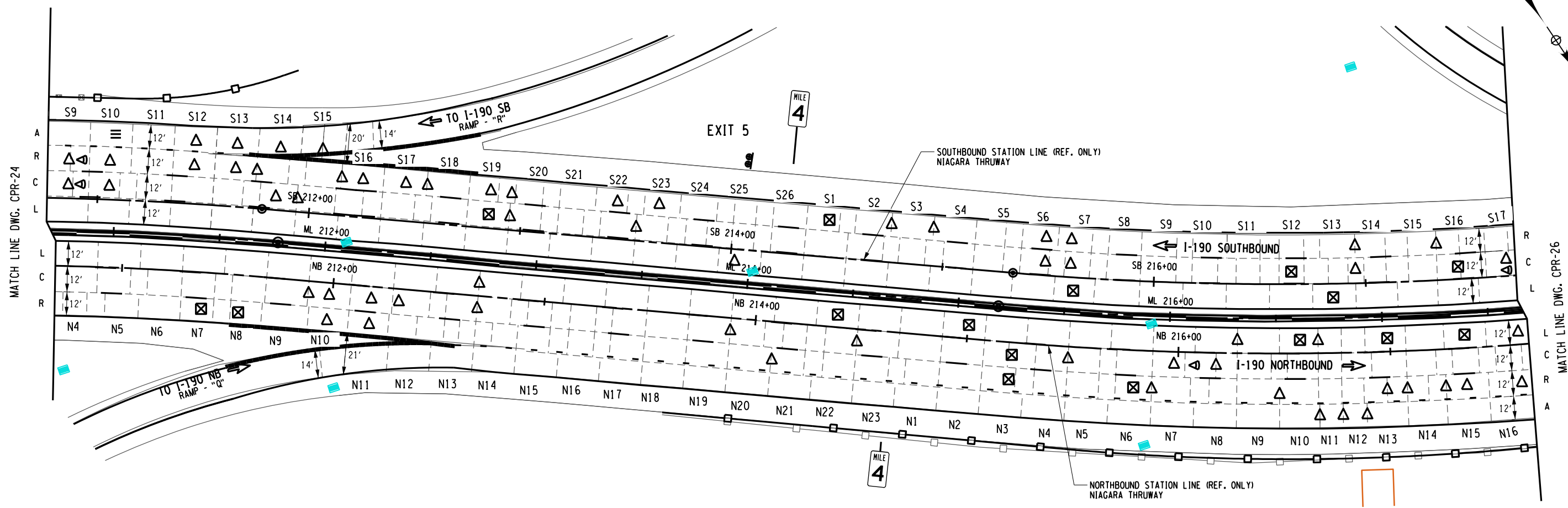
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

100  
274

GRID  
NORTH



MATCH LINE DWG. CPR-24

MATCH LINE DWG. CPR-26

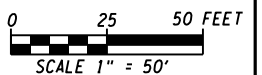
DOWEL BAR RETROFIT REPAIRS					
LOCATION			REPAIR ITEMS		REMARKS
MILE MARKER	STATION	LANE	SLAB WIDTH (FT)	RETROFIT DOWEL BARS ITEM 502.70010018 (EA)	
903.93	SB 211+05	S10A	12	8	12

PARTIAL DEPTH REPAIR						
LOCATION		REPAIR DIMENSIONS			REMARKS	
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)		
903.92	NB 211+39	N7R	2	1	2.0	
903.93	NB 211+58	N8R	2	1	2.0	
903.98	NB 214+38	N22L	2	1	2.0	2 SEPARATE REPAIRS (1'x 1')
904.01	NB 214+99	N2L	2	1	2.0	2 SEPARATE REPAIRS (1'x 1')
904.01	NB 215+18	N3C	1	1	1.0	
904.01	NB 215+18	N3R	8	1	8.0	
904.02	NB 215+78	N6R	2	1	2.0	
904.04	NB 216+57	N10L	7	1	7.0	2 SEPARATE REPAIRS (1'x 1' & 6'x 1')
904.04	NB 216+98	N13L	4	1	4.0	2 SEPARATE REPAIRS (2'x 1' & 2'x 2')
904.05	NB 217+36	N15L	2	1	2.0	
903.97	SB 212+84	S19C	3	2	6.0	
904.00	SB 214+43	S1R	1	1	1.0	
904.03	SB 215+63	S7L	1	1	1.0	
904.05	SB 216+65	S12C	1	1	1.0	
904.05	SB 216+85	S13L	4	1	4.0	
904.06	SB 217+44	S16C	2	1	2.0	

- NOTES:
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPRT-20.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

**LEGEND**

- △ FULL DEPTH REPAIR
- PARTIAL DEPTH REPAIR
- ≡ DOWEL BAR RETROFIT
- ◁ HINGE JOINT
- | CLEAN AND FILL CRACKS
- L LEFT LANE
- C CENTER LANE
- R RIGHT LANE
- A AUXILIARY LANE



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-25

CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

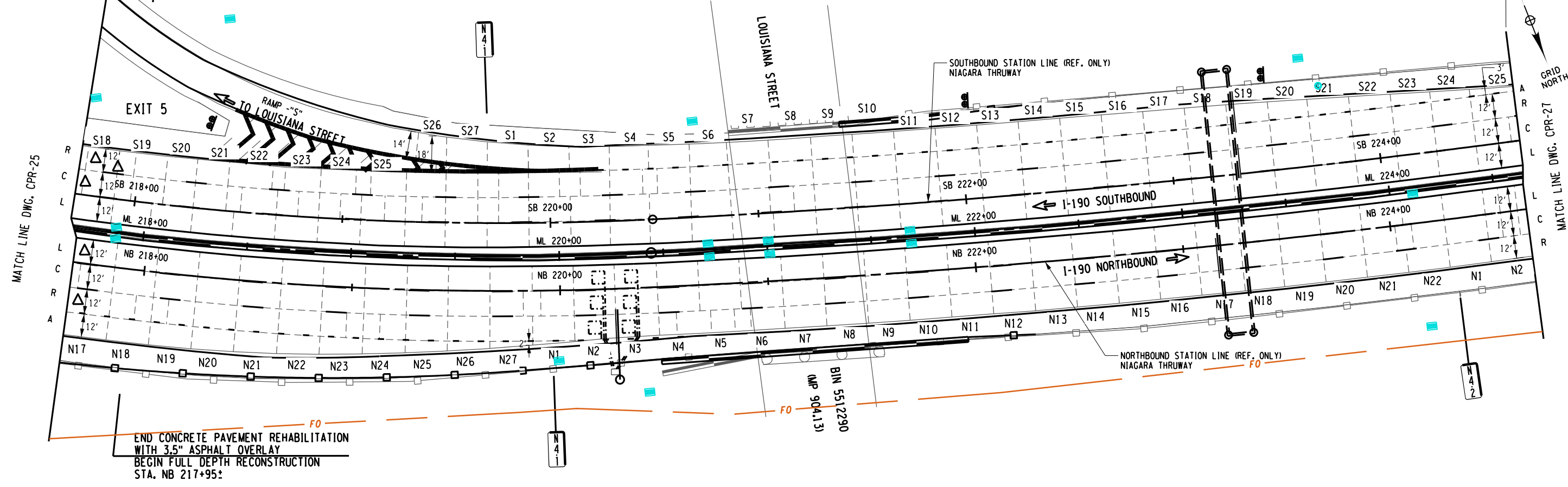
CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

END CONCRETE PAVEMENT REHABILITATION  
WITH 3.5" ASPHALT OVERLAY  
BEGIN FULL DEPTH RECONSTRUCTION  
STA. SB 217+95±

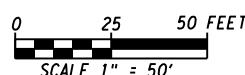
REFER TO ROADWAY PLANS FOR PROPOSED WORK



END CONCRETE PAVEMENT REHABILITATION  
WITH 3.5" ASPHALT OVERLAY  
BEGIN FULL DEPTH RECONSTRUCTION  
STA. NB 217+95±

- NOTES:
- FOR FULL DEPTH CONCRETE PAVEMENT REPAIRS SEE FULL DEPTH REPAIR TABLE ON DWG. CPR-21.
  - LIMITS OF CONCRETE REPAIRS SHOWN ON THE PLANS ARE APPROXIMATE AND EXACT LIMITS OF REPAIRS SHALL BE LAID OUT IN THE FIELD PRIOR TO PERFORMING ANY REPAIR WORK. PAYMENT FOR THE LAYOUT UNDER ITEM 502.10010018.

LEGEND	
	FULL DEPTH REPAIR
	PARTIAL DEPTH REPAIR
	DOWEL BAR RETROFIT
	HINGE JOINT
	CLEAN AND FILL CRACKS
L	LEFT LANE
C	CENTER LANE
R	RIGHT LANE
A	AUXILIARY LANE



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

PARTIAL DEPTH REPAIR						
LOCATION			REPAIR DIMENSIONS			REMARKS
MILE MARKER	STATION	SLAB NUMBER	LENGTH (FT)	WIDTH (FT)	PD REPAIRS HES CONC SAW CUTTING ITEM 502.42200018 (SF)	
904.07	SB 217+99	S19C	2	1	2.0	2 SEPARATE REPAIRS (2) - 1'x 1'

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-26



CHECKED BY: J. KLEINMANN

DRAFTED BY: P. AMICONE

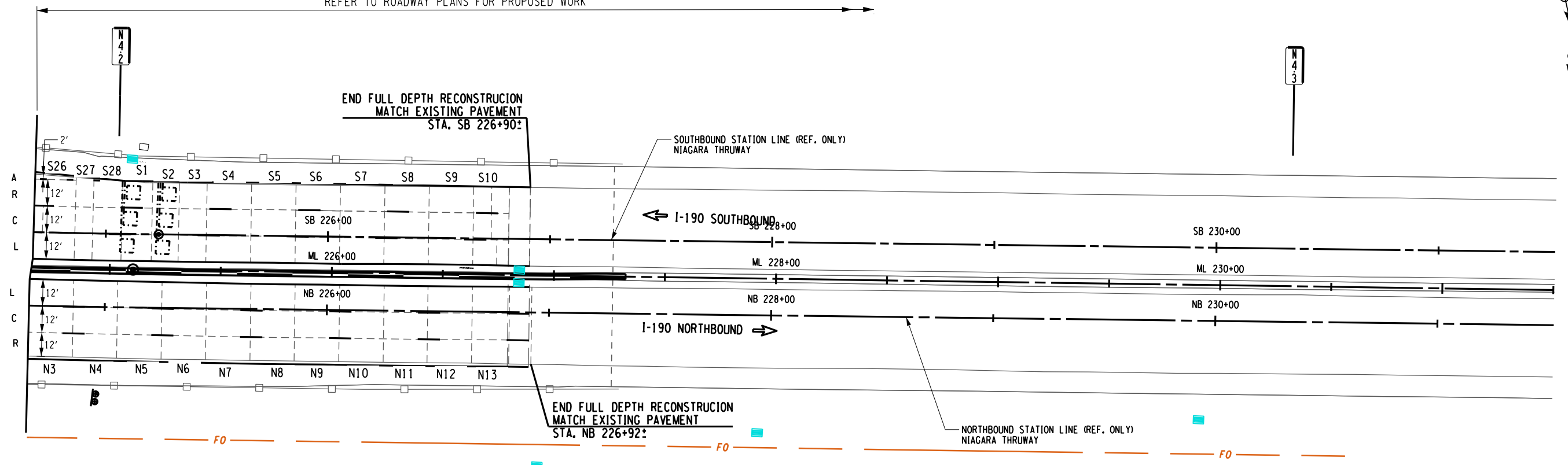
CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

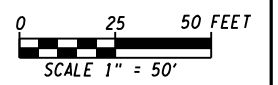
REFER TO ROADWAY PLANS FOR PROPOSED WORK

MATCH LINE DWG. PL-26



NOTES:  
1. THERE ARE NO CONCRETE PAVEMENT REPAIRS FOR THIS SHEET.

LEGEND	
	FULL DEPTH REPAIR
	PARTIAL DEPTH REPAIR
	DOWEL BAR RETROFIT
	HINGE JOINT
	CLEAN AND FILL CRACKS
L	LEFT LANE
C	CENTER LANE
R	RIGHT LANE
A	AUXILIARY LANE



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR PLANS	DRAWING NUMBER: CPR-27

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

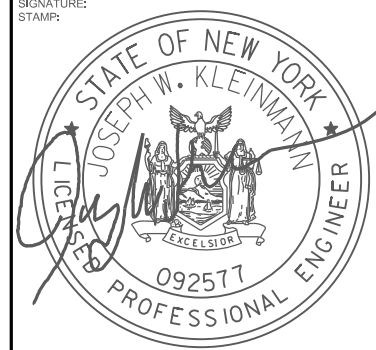
FULL DEPTH REPAIR

MILE MARKER	START SLAB	LOCATION				REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS						
		START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)				
900.76	S8	SB 43+82	S9	SB 44+07	A	25.00	12.00	300.0	33.3	300.0						22		12.0	9.3							
900.76	S8	SB 43+82	S8	SB 43+90	L	8.00	12.00	96.0	10.7	96.0						22			3.0							
900.76	S9	SB 44+20	S9	SB 44+28	C	8.00	12.00	96.0	10.7	96.0						22			3.0		8.0				8.0	
900.76	S9	SB 44+20	S9	SB 44+28	L	8.00	12.00	96.0	10.7	96.0						22			3.0							
900.77	S9	SB 44+45	S11	SB 44+87	A	42.00	8.00	336.0	37.3	336.0						14			10.4		16.0			16.0		
900.77	S10	SB 44+64	S10	SB 44+79	C	15.00	12.00	180.0	20.0	180.0	4.4	6.7	0.7													
900.77	S10	SB 44+64	S10	SB 44+79	L	15.00	12.00	180.0	20.0	180.0						22			5.6							
900.78	S11	SB 44+93	S11	SB 45+13	R	20.00	12.00	240.0	26.7	240.0						22			7.4		12.0	20.0	12.0	20.0		
900.78	S11	SB 44+93	S11	SB 45+13	C	20.00	12.00	240.0	26.7	240.0	5.8	9.0	0.9													
900.78	S11	SB 45+03	S11	SB 45+13	L	10.00	12.00	120.0	13.3	120.0						22			3.7							
900.77	N9	NB 44+01	N10	NB 44+09	R	8.00	12.00	96.0	10.7	96.0						22			3.0							
900.77	N10	NB 44+36	N11	NB 44+74	R	38.00	12.00	456.0	50.7	456.0						22	7	12.0	14.1		12.0			12.0		
900.77	N11	NB 44+59	N12	NB 44+89	C	30.00	12.00	360.0	40.0	360.0	8.8	13.5	1.4													

FULL DEPTH REPAIR

MILE MARKER	START SLAB	LOCATION				REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS						
		START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)				
900.79	S11	SB 45+13	S1	SB 45+87	A	74.00	4.00	296.0	32.9	296.0						3			9.1		16.0	13.0	16.0	13.0		
900.80	S12	SB 45+43	S12	SB 45+58	R	13.00	12.00	156.0	17.3	156.0						22			4.8							
900.81	S1	SB 45+83	S2	SB 46+10	R	27.00	12.00	324.0	36.0	324.0						22			10.0		12.0	27.0	12.0	27.0		
900.81	S1	SB 45+83	S2	SB 46+10	C	27.00	12.00	324.0	36.0	324.0	7.9	12.1	1.3													
900.81	S1	SB 45+97	S2	SB 46+10	L	13.00	12.00	156.0	17.3	156.0						22			4.8							
900.82	S2	SB 46+41	S3	SB 46+49	L	8.00	12.00	96.0	10.7	96.0						22			3.0							
900.82	S3	SB 46+63	S3	SB 46+71	L	8.00	12.00	96.0	10.7	96.0						22			3.0							
900.82	S2	SB 46+71	S3	SB 46+49	C	8.00	12.00	96.0	10.7	96.0						22			3.0		8.0				8.0	
900.82	S3	SB 46+71	S3	SB 46+79	C	8.00	12.00	96.0	10.7	96.0						22			3.0							
900.83	S4	SB 46+93	S4	SB 47+01	R	8.00	12.00	96.0	10.7	96.0						22			3.0							
900.83	S4	SB 47+22	S5	SB 47+30	R	8.00	12.00	96.0	10.7	96.0						22			3.0		8.0				8.0	
900.83	S4	SB 47+22	S5	SB 47+30	C	8.00	12.00	96.0	10.7	96.0						22			3.0							
900.84	S6	SB 47+73	S7	SB 48+10	R	40.00	12.00	480.0	53.3	480.0						22			14.8		24.0	28.0	24.0	28.0		
900.84	S6	SB 47+73	S6	SB 47+97	C	28.00	12.00	336.0	37.3	336.0	8.2	12.6	1.3													
900.84	S6	SB 47+73	S6	SB 47+84	L	11.00	12.00	132.0	14.7	132.0						22			4.1							
900.87	S9	SB 48+98	S9	SB 49+13	R	15.00	12.00	180.0	20.0	180.0						22			5.6		15.0				15.0	
900.87	S9	SB 48+98	S9	SB 49+13	C	15.00	12.00	180.0	20.0	180.0	4.4	6.7	0.7													
900.87	S9	SB 48+98	S9	SB 49+13	L	15.00	12.00	180.0	20.0	180.0						22			5.6							
900.87	S10	SB 49+34	S12	SB 50+09	L	75.00	12.00	900.0	100.0	900.0						22			27.8		48.0		48.0			
900.88	S10	SB 49+49	S12	SB 50+09	R	60.00	12.00	720.0	80.0	720.0						22			22.2		36.0	60.0	36.0	60.0		
900.88	S10	SB 49+49	S12	SB 50+09	C	60.00	12.00	720.0	80.0	720.0	17.5	27.0	2.8													
900.89	S12	SB 50+24	S12	SB 50+39	R	15.00	12.00	180.0	20.0	180.0						22			5.6		15.0				15.0	

ALTERED ON:	AFFIXED ON:	ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS
	5/10/2024	304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
		404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
		404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF
		407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF
		502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
		502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF
		502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA			



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING: CONCRETE PAVEMENT REPAIR TABLE	DRAWING NUMBER: CPRT-1

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY



CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

FULL DEPTH REPAIR																						
MILE MARKER	START SLAB	LOCATION				REPAIR DIMENSIONS			REPAIR ITEMS										REMARKS			
		START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)		ITEM 502.38010018 (LF)	ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)
900.89	S12	SB 50+24	S12	SB 50+39	C	15.00	12.00	180.0	20.0	180.0	4.4	6.7	0.7									
900.89	S12	SB 50+24	S12	SB 50+39	L	15.00	12.00	180.0	20.0	180.0				22				5.6				
900.90	S1	SB 50+64	S2	SB 50+88	R	23.00	12.00	276.0	30.7	276.0				22				8.5	12.0	15.0	12.0	15.0
900.90	S1	SB 50+64	S2	SB 50+79	C	15.00	12.00	180.0	20.0	180.0	4.4	6.7	0.7									
900.92	S3	SB 51+45	S3	SB 51+60	R	15.00	12.00	180.0	20.0	180.0				22				5.6		15.0		15.0
900.92	S3	SB 51+45	S3	SB 51+60	C	15.00	12.00	180.0	20.0	180.0	4.4	6.7	0.7									
900.92	S3	SB 51+45	S3	SB 51+60	L	15.00	12.00	180.0	20.0	180.0				22				5.6				15.0
900.92	S4	SB 51+83	S4	SB 51+91	R	8.00	12.00	96.0	10.7	96.0				22	3			3.0				
900.92	S4	SB 51+83	S4	SB 51+91	C	8.00	12.00	96.0	10.7	96.0				22				3.0		8.0		8.0
900.92	S4	SB 51+83	S4	SB 51+91	L	8.00	12.00	96.0	10.7	96.0				22				3.0				
900.93	S5	SB 52+19	S5	SB 52+27	R	8.00	12.00	96.0	10.7	96.0				22				3.0				
900.93	S5	SB 52+19	S5	SB 52+27	L	8.00	12.00	96.0	10.7	96.0				22				3.0				
900.78	N13	NB 45+43	N13	NB 45+53	C	10.00	12.00	120.0	13.3	120.0				22				3.7				
900.78	N13	NB 45+53	N14	NB 45+81	L	28.00	12.00	336.0	37.3	336.0				22				10.4	12.0		12.0	
900.79	N13	NB 45+59	N14	NB 45+69	R	10.00	12.00	120.0	13.3	120.0				22				3.7				
900.80	N15	NB 46+41	N1	NB 46+49	R	8.00	12.00	96.0	10.7	96.0				22				3.0				
900.81	N1	NB 46+62	N1	NB 46+69	L	8.00	12.00	96.0	10.7	96.0				22				3.0				
900.81	N1	NB 46+73	N2	NB 46+94	R	21.00	12.00	252.0	28.0	252.0				22				7.8	12.0		12.0	
900.84	N5	NB 48+41	N6	NB 48+49	C	8.00	12.00	96.0	10.7	96.0				22				3.0				
900.84	N6	NB 48+61	N6	NB 48+69	L	8.00	12.00	96.0	10.7	96.0				22				3.0				
900.84	N6	NB 48+61	N6	NB 48+69	C	8.00	12.00	96.0	10.7	96.0				22				3.0		8.0		8.0
900.84	N6	NB 48+61	N6	NB 48+69	R	8.00	12.00	96.0	10.7	96.0				22	3			3.0				
900.84	N6	NB 48+61	N6	NB 48+69	A	8.00	12.00	96.0	10.7	96.0				22				3.0		8.0		8.0
900.86	N8	NB 49+47	N8	NB 49+55	A	8.00	8.00	64.0	7.1	64.0				14				2.0				
900.86	N8	NB 49+61	N9	NB 49+89	C	28.00	12.00	336.0	37.3	336.0	8.2	12.6	1.3									
900.87	N9	NB 49+77	N9	NB 49+89	L	12.00	12.00	144.0	16.0	144.0				22				4.4				
900.87	N9	NB 49+77	N9	NB 49+89	R	12.00	12.00	144.0	16.0	144.0				22	5			4.4				
900.87	N9	NB 49+77	N9	NB 49+89	A	12.00	7.00	84.0	9.3	84.0				12				2.6		7.0		7.0
900.87	N10	NB 50+17	N11	NB 50+46	C	30.00	12.00	360.0	40.0	360.0	8.8	13.5	1.4									
900.87	N10	NB 50+17	N10	NB 50+29	R	12.00	12.00	144.0	16.0	144.0				22				4.4				
900.87	N10	NB 50+17	N10	NB 50+29	A	12.00	8.00	96.0	10.7	96.0				14				3.0		8.0		8.0
900.88	N11	NB 50+52	N12	NB 51+27	R	36.00	12.00	432.0	48.0	432.0				22	6			13.3	24.0		24.0	
900.88	N11	NB 50+52	N12	NB 50+84	A	33.00	3.00	99.0	11.0	99.0				2				3.1	6.0	33.0	6.0	33.0
900.88	N11	NB 50+74	N12	NB 50+88	C	53.00	12.00	636.0	70.7	636.0	15.5	23.8	2.5									
900.89	N12	NB 51+14	N1	NB 51+40	R	26.00	12.00	312.0	34.7	312.0				22				9.6	12.0	13.0	12.0	13.0
900.90	N1	NB 51+42	N1	NB 51+50	L	8.00	12.00	96.0	10.7	96.0				22				3.0				
900.92	N3	NB 52+18	N3	NB 52+30	C	12.00	12.00	144.0	16.0	144.0	3.5	5.4	0.6									
900.92	N3	NB 52+18	N3	NB 52+30	R	12.00	12.00	144.00	16.0	144.0				22				4.4		12.0		12.0

SEE PLAN ON DWG. CPR-2

ALTERED ON:	AFFIXED ON:	5/10/2024																																																
SIGNATURE: STAMP:	SIGNATURE: STAMP:																																																	
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CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

FULL DEPTH REPAIR																								
MILE MARKER	START SLAB	LOCATION				REPAIR DIMENSIONS			REPAIR ITEMS										REMARKS					
		START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)		ITEM 502.38010018 (LF)	ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)		
900.94	S5	SB 52+39	S6	SB 52+55	R	15.00	12.00	180.0	20.0	180.0					22	7		5.6						
900.94	S5	SB 52+43	S6	SB 52+72	L	28.00	12.00	336.0	37.3	336.0					22		12.0	10.4						
900.94	S6	SB 52+84	S7	SB 53+12	R	28.00	12.00	336.0	37.3	336.0					22			10.4	12.0	28.0	12.0		28.0	
900.94	S6	SB 52+84	S7	SB 53+12	C	28.00	12.00	336.0	37.3	336.0	8.2	12.6	1.3											
900.95	S7	SB 52+98	S7	SB 53+12	L	14.00	12.00	168.0	18.7	168.0					22			5.2						
900.95	S8	SB 53+40	S8	SB 53+54	L	14.00	12.00	168.0	18.7	168.0					22			5.2						
900.96	S8	SB 53+46	S8	SB 53+54	R	8.00	12.00	96.0	10.7	96.0					22			3.0						
900.96	S8	SB 53+46	S8	SB 53+54	C	8.00	12.00	96.0	10.7	96.0					22			3.0						
900.96	S9	SB 53+82	S9	SB 53+90	R	8.00	12.00	96.0	10.7	96.0					22			3.0		8.0			8.0	
900.96	S9	SB 53+82	S10	SB 54+16	C	34.00	12.00	408.0	45.3	408.0	9.9	15.3	1.6											
900.96	S9	SB 53+82	S9	SB 53+90	L	8.00	12.00	96.0	10.7	96.0					22			3.0						
900.97	S10	SB 54+27	S10	SB 54+35	L	8.00	12.00	96.0	10.7	96.0					22			3.0						
900.97	S10	SB 54+44	S11	SB 54+52	C	8.00	12.00	96.0	10.7	96.0					22			3.0						
900.98	S11	SB 54+62	S11	SB 54+70	R	8.00	12.00	96.0	10.7	96.0					22			3.0						
900.98	S11	SB 54+70	S11	SB 54+78	L	8.00	12.00	96.0	10.7	96.0					22			3.0						
900.98	S12	SB 55+01	S12	SB 55+13	L	12.00	12.00	144.0	16.0	144.0					22			4.4						
900.99	S12	SB 55+05	S12	SB 55+13	R	8.00	12.00	96.0	10.7	96.0					22			3.0		8.0			8.0	
900.99	S12	SB 55+05	S12	SB 55+13	C	8.00	12.00	96.0	10.7	96.0					22	3		3.0						
900.99	S12	SB 55+25	S13	SB 55+33	R	8.00	12.00	96.0	10.7	96.0					22			3.0						
900.99	S12	SB 55+25	S13	SB 55+33	L	8.00	12.00	96.0	10.7	96.0					22			3.0						
900.99	S13	SB 55+51	S13	SB 55+59	R	8.00	12.00	96.0	10.7	96.0					22			3.0		8.0			8.0	
900.99	S13	SB 55+51	S13	SB 55+59	C	8.00	12.00	96.0	10.7	96.0					22			3.0						
901.00	S14	SB 55+82	S14	SB 55+97	C	12.00	12.00	144.0	16.0	144.0	3.5	5.4	0.6					4.4						
901.00	S14	SB 55+85	S14	SB 55+97	R	12.00	12.00	144.0	16.0	144.0					22			4.4						
901.00	S14	SB 55+85	S14	SB 55+97	L	12.00	12.00	144.0	16.0	144.0					22			4.4						
900.93	N4	NB 52+58	N4	NB 52+66	C	8.00	12.00	96.0	10.7	96.0					22			3.0						
900.93	N5	NB 53+00	N5	NB 53+12	R	12.00	12.00	144.0	16.0	144.0					22	5		4.4						
900.93	N5	NB 53+01	N5	NB 53+13	C	12.00	12.00	144.0	16.0	144.0	3.5	5.4	0.6					4.4						
900.93	N5	NB 53+01	N5	NB 53+13	L	12.00	12.00	144.0	16.0	144.0					22			4.4						
900.94	N6	NB 53+53	N7	NB 53+77	C	24.00	12.00	288.0	32.0	288.0	7.0	10.8	1.1											
900.95	N7	NB 53+84	N9	NB 54+51	R	67.00	12.00	804.0	89.3	804.0					22	3	12.0	24.8	36.0			36.0		
900.95	N7	NB 54+00	N8	NB 54+08	C	8.00	12.00	96.0	10.7	96.0					22			3.0						
900.97	N10	NB 55+02	N10	NB 55+10	L	8.00	12.00	96.0	10.7	96.0					22			3.0						
900.97	N10	NB 55+02	N10	NB 55+10	C	8.00	12.00	96.0	10.7	96.0					22			3.0		8.0			8.0	
900.98	N10	NB 55+24	N11	NB 55+39	C	15.00	12.00	180.0	20.0	180.0	4.4	6.7	0.7											
900.98	N10	NB 55+24	N12	NB 55+69	R	46.00	12.00	552.0	61.3	552.0					22	7		17.0	36.0			36.0		

SEE PLAN ON DWG. CPR-3

ALTERED ON:	AFFIXED ON:	ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS
	5/10/2024	304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
		404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
		404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF
		407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF
		502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
		502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF
		502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA			

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



NEW YORK STATE OF OPPORTUNITY Thruway Authority

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR TABLE	DRAWING NUMBER: CPRT-3

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

FULL DEPTH REPAIR																						
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS		
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)
901.15	S7	SB 62+79	S8	SB 62+97	A	18.0	12.0	216.0	24.0	216.0					22			6.7	12.0		12.0	
901.17	S9	SB 63+71	S10	SB 63+99	R	28.0	12.0	336.0	37.3	336.0					22	6		10.4	12.0		12.0	
901.17	S10	SB 63+85	S10	SB 63+99	A	14.0	12.0	168.0	18.7	168.0					22			5.2		14.0		14.0
901.17	S10	SB 63+85	S10	SB 63+99	C	14.0	12.0	168.0	18.7	168.0	4.1	6.3	0.7									
901.17	S10	SB 64+09	S11	SB 64+17	L	8.0	12.0	96.0	10.7	96.0					22			3.0				
901.18	S12	SB 64+69	S12	SB 64+77	A	8.0	12.0	96.0	10.7	96.0					22			3.0				
901.19	S1	SB 65+08	S1	SB 65+20	R	13.0	12.0	156.0	17.3	156.0					22	6		4.8				
901.19	S1	SB 65+08	S1	SB 65+20	C	13.0	12.0	156.0	17.3	156.0	3.8	5.8	0.6									
901.19	S1	SB 65+08	S1	SB 65+20	L	13.0	12.0	156.0	17.3	156.0					22			4.8				
901.21	S2	SB 65+47	S2	SB 65+55	R	8.0	12.0	96.0	10.7	96.0					22	3		3.0				
901.21	S2	SB 65+47	S2	SB 65+55	C	8.0	12.0	96.0	10.7	96.0					22			3.0		8.0		8.0
901.21	S2	SB 65+48	S2	SB 65+56	L	8.0	12.0	96.0	10.7	96.0					22			3.0				
901.21	S2	SB 65+75	S3	SB 66+02	L	28.0	12.0	336.0	37.3	336.0					22			10.4	12.0		12.0	
901.22	S3	SB 65+89	S3	SB 66+02	A	13.0	12.0	156.0	17.3	156.0					22			4.8		13.0		13.0
901.22	S3	SB 65+89	S3	SB 66+02	R	13.0	12.0	156.0	17.3	156.0					22	6		4.8				
901.22	S3	SB 65+89	S3	SB 66+02	C	13.0	12.0	156.0	17.3	156.0	3.8	5.8	0.6									
901.22	S4	SB 66+36	S5	SB 66+59	A	22.0	12.0	264.0	29.3	264.0					22			8.1	12.0	11.0	12.0	11.0
901.22	S4	SB 66+37	S4	SB 66+48	R	11.0	12.0	132.0	14.7	132.0					22	6		4.1				
901.22	S4	SB 66+37	S4	SB 66+48	C	11.0	12.0	132.0	14.7	132.0					22			4.1				
901.23	S4	SB 66+48	S5	SB 66+59	L	11.0	12.0	132.0	14.7	132.0					22			4.1				
901.23	S5	SB 66+73	S5	SB 66+81	R	8.0	12.0	96.0	10.7	96.0					22	3		3.0				
901.23	S5	SB 66+73	S5	SB 66+82	C	8.0	12.0	96.0	10.7	96.0					22			3.0				
901.13	N6	NB 62+79	N7	NB 63+33	C	54.00	12.00	648.0	72.0	648.0	15.8	24.3	2.5									
901.13	N6	NB 63+13	N7	NB 63+33	R	20.00	12.00	240.0	26.7	240.0					22			7.4	12.0	20.0	12.0	20.0
901.15	N8	NB 63+99	N9	NB 64+07	C	8.00	12.00	96.0	10.7	96.0					22			3.0				
901.17	N10	NB 64+79	N11	NB 64+87	C	8.00	12.00	96.0	10.7	96.0					22			3.0				
901.17	N10	NB 64+79	N11	NB 64+87	R	8.00	12.00	96.0	10.7	96.0					22			3.0		8.0		8.0
901.17	N11	NB 65+21	N12	NB 65+31	C	10.00	12.00	120.0	13.3	120.0					22			3.7				
901.18	N12	NB 65+59	N13	NB 65+67	C	8.00	12.00	96.0	10.7	96.0					22			3.0				
901.19	N13	NB 65+87	N13	NB 66+18	C	13.00	12.00	156.0	17.3	156.0	3.8	5.8	0.6									
901.19	N13	NB 65+87	N13	NB 65+99	R	13.00	12.00	156.0	17.3	156.0					22			4.8		13.0		13.0
901.19	N13	NB 65+99	N14	NB 65+99	L	19.00	12.00	228.0	25.3	228.0					22		12.0	7.0				
901.20	N14	NB 66+39	N1	NB 66+47	C	8.00	12.00	96.0	10.7	96.0					22			3.0				
901.21	N2	NB 67+01	N2	NB 67+09	L	8.00	12.00	96.0	10.7	96.0					22			3.0				
901.21	N2	NB 67+19	N3	NB 67+39	R	20.0	12.0	240.0	26.7	240.0					22		12.0	7.4				

SEE PLAN ON DWG. CPR-4

ALTERED ON:	AFFIXED ON: 5/10/2024	ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS
SIGNATURE: STAMP:		304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
		404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
		404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF
		407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF
		502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
		502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF
		502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA			

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority**

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2

TITLE OF DRAWING: CONCRETE PAVEMENT REPAIR TABLE

CONTRACT NUMBER: TAN 24-12

DATE: MAY 2024

DRAWING NUMBER: CPRT-4

GMA  
2024 Mark "Trade Tower"  
Sullivan, WI 54223-3723  
618.465.4897 - www.gmaibms.com

CHECKED BY: J.KLEINMANN

DRAFTED BY: N.HOY

CHECKED BY: J.KLEINMANN

DESIGNED BY: P.AMICONE

DESIGN SUPERVISOR: D.MARSH

FULL DEPTH REPAIR																							
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS			
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)	
901.24	S6	SB 67+20	S6	SB 67+30	A	10.0	12.0	120.0	13.3	120.0							22			3.7		10.0	10.0
901.24	S6	SB 67+20	S6	SB 67+31	R	10.0	12.0	120.0	13.3	120.0							22	4		3.7			
901.24	S6	SB 67+21	S6	SB 67+31	C	10.0	12.0	120.0	13.3	120.0							22			3.7		10.0	10.0
901.24	S6	SB 67+21	S6	SB 67+31	L	10.0	12.0	120.0	13.3	120.0							22			3.7			
901.24	S7	SB 67+44	S7	SB 67+66	R	22.0	12.0	264.0	29.3	264.0							22			8.1	12.0	12.0	
901.25	S7	SB 67+56	S8	SB 67+79	A	23.0	12.0	276.0	30.7	276.0							22			8.5	12.0	10.0	12.0
901.25	S8	SB 67+89	S8	SB 68+03	C	14.0	12.0	168.0	18.7	168.0	4.1	6.3	0.7										
901.25	S8	SB 67+95	S8	SB 68+03	A	8.0	12.0	96.0	10.7	96.0							22			3.0		8.0	8.0
901.25	S8	SB 67+95	S8	SB 68+03	R	8.0	12.0	96.0	10.7	96.0							22			3.0			
901.26	S9	SB 68+33	S10	SB 68+61	C	28.0	12.0	336.0	37.3	336.0	8.2	12.6	1.3										
901.26	S9	SB 68+33	S10	SB 68+61	L	28.0	12.0	336.0	37.3	336.0							22			10.4	12.0	28.0	12.0
901.26	S9	SB 68+47	S10	SB 68+61	R	14.0	12.0	168.0	18.7	168.0							22			5.2			
901.27	S10	SB 68+75	S11	SB 69+02	A	27.0	12.0	324.0	36.0	324.0							22			10.0	12.0	13.0	12.0
901.27	S10	SB 68+75	S10	SB 68+88	R	13.0	12.0	156.0	17.3	156.0							22	6		4.8			
901.27	S10	SB 68+75	S10	SB 68+88	C	13.0	12.0	156.0	17.3	156.0	3.8	5.8	0.6										
901.27	S10	SB 68+76	S11	SB 69+02	L	27.0	12.0	324.0	36.0	324.0							22			10.0	12.0		12.0
901.28	S11	SB 69+16	S11	SB 69+24	R	8.0	12.0	96.0	10.7	96.0							22			3.0			
901.28	S11	SB 69+16	S11	SB 69+24	C	8.0	12.0	96.0	10.7	96.0							22			3.0			
901.28	S11	SB 69+16	S11	SB 69+29	L	13.0	12.0	156.0	17.3	156.0							22			4.8			
901.29	S12	SB 69+61	S12	SB 69+70	A	8.0	12.0	96.0	10.7	96.0							22			3.0		8.0	8.0
901.29	S12	SB 69+62	S12	SB 69+70	R	8.0	12.0	96.0	10.7	96.0							22	3		3.0			
901.29	S12	SB 69+62	S12	SB 69+70	C	8.0	12.0	96.0	10.7	96.0							22			3.0			
901.29	S13	SB 70+02	S13	SB 70+10	R	8.0	12.0	96.0	10.7	96.0							22	3		3.0			
901.29	S13	SB 70+02	S13	SB 70+10	C	8.0	12.0	96.0	10.7	96.0							22			3.0			
901.30	S1	SB 70+38	S1	SB 70+46	R	8.0	12.0	96.0	10.7	96.0							22			3.0		8.0	8.0
901.30	S1	SB 70+38	S1	SB 70+46	C	8.0	12.0	96.0	10.7	96.0							22	3		3.0			
901.30	S1	SB 70+38	S1	SB 70+46	L	8.0	12.0	96.0	10.7	96.0							22			3.0			
901.31	S2	SB 70+78	S2	SB 70+87	R	8.0	12.0	96.0	10.7	96.0							22			3.0		8.0	8.0
901.31	S2	SB 70+78	S2	SB 70+86	C	8.0	12.0	96.0	10.7	96.0							22			3.0			
901.32	S3	SB 71+21	S4	SB 71+65	R	44.0	12.0	528.0	58.7	528.0							22			16.3	24.0	44.0	24.0
901.32	S3	SB 71+21	S4	SB 71+65	C	44.0	12.0	528.0	58.7	528.0	12.9	19.8	2.1										
901.33	S4	SB 71+57	S4	SB 71+65	L	8.0	12.0	96.0	10.7	96.0							22			3.0			
901.33	S4	SB 71+79	S5	SB 71+87	C	8.0	12.0	96.0	10.7	96.0							22			3.0			
901.33	S5	SB 72+01	S5	SB 72+09	R	8.0	12.0	96.0	10.7	96.0							22			3.0		8.0	8.0
901.33	S5	SB 72+02	S5	SB 72+10	C	8.0	12.0	96.0	10.7	96.0							22			3.0			
901.34	S6	SB 72+39	S6	SB 72+47	R	8.0	12.0	96.0	10.7	96.0							22			3.0		8.0	8.0
901.34	S6	SB 72+39	S6	SB 72+47	C	8.0	12.0	96.0	10.7	96.0							22			3.0			
901.35	S6	SB 72+59	S7	SB 72+67	R	8.0	12.0	96.0	10.7	96.0							22			3.0			
901.35	S7	SB 72+89	S7	SB 72+97	R	8.0	12.0	96.0	10.7	96.0							22			3.0		8.0	8.0
901.35	S7	SB 72+89	S7	SB 72+97	C	8.0	12.0	96.0	10.7	96.0							22			3.0			
901.36	S8	SB 73+15	S9	SB 73+67	R	52.0	12.0	624.0	69.3	624.0							22			19.3	36.0	23.0	36.0
901.36	S8	SB 73+15	S8	SB 73+39	C	23.0	12.0	276.0	30.7	276.0	6.7	10.3	1.1										
901.36	S9	SB 73+59	S9	SB 73+67	C	8.0	12.0	96.0	10.7	96.0							22			3.0			

SEE PLAN ON DWG. CPR-5

ALTERED ON:	AFFIXED ON:
SIGNATURE:	SIGNATURE:
STAMP:	STAMP:

5/10/2024

ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS
304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF
407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF
502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF
502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA			

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT	I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER:	TAN 24-12
LOCATION OF PROJECT	I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE:	MAY 2024
TITLE OF DRAWING	CONCRETE PAVEMENT REPAIR TABLE	DRAWING NUMBER:	CPRT-5

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

FULL DEPTH REPAIR																						
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS		
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)
901.37	S9	SB 73+81	S10	SB 73+89	C	8.0	12.0	96.0	10.7	96.0				22	3							
901.37	S9	SB 73+81	S10	SB 73+89	L	8.0	12.0	96.0	10.7	96.0				22			3.0					
901.22	N3	NB 67+59	N4	NB 67+67	C	8.0	12.0	96.0	10.7	96.0				22			3.0					
901.22	N3	NB 67+59	N4	NB 67+67	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0			8.0
901.23	N4	NB 67+99	N5	NB 68+07	R	8.0	12.0	96.0	10.7	96.0				22			3.0					
901.24	N5	NB 68+39	N6	NB 68+48	C	10.0	12.0	120.0	13.3	120.0				22			3.7					
901.24	N5	NB 68+39	N6	NB 68+48	R	10.0	12.0	120.0	13.3	120.0				22			3.7		8.0			8.0
901.25	N7	NB 69+03	N7	NB 69+11	C	8.0	12.0	96.0	10.7	96.0				22			3.0					
901.25	N7	NB 69+03	N7	NB 69+11	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0			8.0
901.26	N8	NB 69+60	N9	NB 69+68	C	8.0	12.0	96.0	10.7	96.0				22			3.0					
901.28	N12	NB 70+85	N14	NB 71+89	A	104.0	12.0	1248.0	138.7	1248.0				11			38.5	72.0	30.0	72.0		30.0
901.29	N12	NB 71+00	N13	NB 71+30	R	30.0	12.0	360.0	40.0	360.0				22	7		11.1	12.0			12.0	
901.29	N12	NB 71+16	N13	NB 71+30	C	15.0	12.0	180.0	20.0	180.0	4.4	6.7	0.7									
901.30	N13	NB 71+62	N14	NB 71+70	C	8.0	12.0	96.0	10.7	96.0				22			3.0					
901.30	N14	NB 72+00	N1	NB 72+10	R	10.0	12.0	120.0	13.3	120.0				22			3.7					
901.31	N2	NB 72+58	N3	NB 73+09	A	52.0	12.0	624.0	69.3	624.0				22			19.3	36.0	26.0	36.0		26.0
901.31	N2	NB 72+71	N3	NB 72+96	R	26.0	12.0	312.0	34.7	312.0				22			9.6	12.0		12.0		
901.32	N4	NB 73+37	N4	NB 73+57	R	20.0	12.0	240.0	26.7	240.0				22			12.0	7.4				
901.32	N4	SB 73+37	N4	SB 73+57	A	20.0	12.0	240.0	26.7	240.0				22			12.0	7.4		20.0		20.0

SEE PLAN ON DWG. CPR-5

ALTERED ON:	AFFIXED ON: 5/10/2024	ITEM 304.00010018	DESCRIPTION FINE GRADING OF EXISTING SUBBASE	UNITS SF	ITEM 502.34010018	DESCRIPTION TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	UNITS LF	
SIGNATURE:		404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY	
STAMP:		404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF	
		407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF	
		502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE(PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF	
		502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA				
		502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.		REVISIONS				TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION		CONTRACT NUMBER: TAN 24-12
		DATE DESCRIPTION BY SYM.				LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2		DATE: MAY 2024
						TITLE OF DRAWING: CONCRETE PAVEMENT REPAIR TABLE		DRAWING NUMBER: CPRT-6

CHECKED BY: J.KLEINMANN

DRAFTED BY: N.HOY

CHECKED BY: J.KLEINMANN

DESIGNED BY: P.AMICONE

DESIGN SUPERVISOR: D.MARSH

FULL DEPTH REPAIR																					
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS										REMARKS		
mile marker	start slab	start station	end slab	end station	lane	length (ft)	width (ft)	area (sf)	item 502.31010018 (SY)	item 304.00010018 (SF)	item 404.1959 (TON)	item 404.3779 (TON)	item 407.01040009 (GAL)	item 502.32050025 (EA)	item 502.33010018 (EA)	item 502.34010018 (LF)	item 502.36010018 (CY)	item 502.37010018 (LF)		item 502.38010018 (LF)	item 502.90010018 (LF)
901.38	S11	SB 74+62	S12	SB 74+70	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0
901.38	S11	SB 74+62	S12	SB 74+70	C	8.0	12.0	96.0	10.7	96.0				22	3		3.0				
901.38	S11	SB 74+62	S12	SB 74+70	L	8.0	12.0	96.0	10.7	96.0				22			3.0				
901.39	S12	SB 74+84	S13	SB 75+07	R	23.0	12.0	276.0	30.7	276.0				22			8.5	12.0	15.0	12.0	15.0
901.39	S12	SB 74+84	S12	SB 74+99	C	15.0	12.0	180.0	20.0	180.0	4.4	6.7	0.7								
901.39	S12	SB 74+84	S12	SB 74+99	L	15.0	12.0	180.0	20.0	180.0				22			5.6				
901.39	S13	SB 75+19	S13	SB 75+27	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0
901.39	S13	SB 75+19	S13	SB 75+27	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
901.39	S13	SB 75+42	S14	SB 75+72	R	30.0	12.0	360.0	40.0	360.0				22			11.1	12.0	30.0	12.0	30.0
901.39	S13	SB 75+42	S14	SB 75+72	C	30.0	12.0	360.0	40.0	360.0	8.8	13.5	1.4								
901.40	S14	SB 75+57	S14	SB 75+72	L	15.0	12.0	180.0	20.0	180.0				22			5.6				
901.41	S1	SB 76+16	S3	SB 76+92	R	76.0	12.0	912.0	101.3	912.0				22			28.1	60.0	50.0	60.0	50.0
901.41	S2	SB 76+35	S3	SB 76+92	C	58.0	12.0	696.0	77.3	696.0	17.0	26.1	2.7								
901.42	S3	SB 76+81	S3	SB 76+92	L	11.0	12.0	132.0	14.7	132.0				22			4.1				
901.43	S4	SB 77+26	S5	SB 77+46	R	20.0	12.0	240.0	26.7	240.0				22			7.4	12.0	10.0	12.0	10.0
901.43	S4	SB 77+26	S5	SB 77+36	C	10.0	12.0	120.0	13.3	120.0				22			3.7				
901.44	S5	SB 77+61	S5	SB 77+73	R	12.0	12.0	144.0	16.0	144.0				22			4.4		12.0		12.0
901.44	S5	SB 77+61	S5	SB 77+73	C	12.0	12.0	144.0	16.0	144.0	3.5	5.4	0.6								
901.44	S6	SB 77+98	S7	SB 78+47	R	49.0	12.0	588.0	65.3	588.0				22			18.1	36.0	49.0	36.0	49.0
901.44	S6	SB 77+98	S7	SB 78+47	C	49.0	12.0	588.0	65.3	588.0	14.3	22.0	2.3								
901.45	S7	SB 78+37	S7	SB 78+47	A	10.0	12.0	120.0	13.3	120.0				22	4		3.7				
901.46	S8	SB 78+79	S8	SB 78+87	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0
901.46	S8	SB 78+79	S8	SB 78+87	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
901.47	S9	SB 79+37	S10	SB 79+52	A	15.0	12.0	180.0	20.0	180.0				22			5.6		15.0		15.0
901.47	S9	SB 79+36	S10	SB 79+52	R	15.0	12.0	180.0	20.0	180.0				22	7		5.6				
901.47	S9	SB 79+37	S10	SB 79+52	C	15.0	12.0	180.0	20.0	180.0	4.4	6.7	0.7								
901.47	S10	SB 79+52	S10	SB 79+61	L	9.0	12.0	108.0	12.0	108.0				22			3.3				
901.48	S11	SB 80+00	S11	SB 80+13	R	13.0	12.0	156.0	17.3	156.0				22	5		4.8				
901.48	S11	SB 80+00	S11	SB 80+13	C	13.0	12.0	156.0	17.3	156.0	3.8	5.8	0.6								
901.49	S12	SB 80+40	S12	SB 80+52	R	12.0	12.0	144.0	16.0	144.0				22	5		4.4				
901.49	S12	SB 80+40	S1	SB 81+36	C	96.0	12.0	1152.0	128.0	1152.0	28.1	43.1	4.5								
901.49	S12	SB 80+63	S13	SB 80+74	L	11.0	12.0	132.0	14.7	132.0				22			4.1				
901.50	S13	SB 80+74	S13	SB 80+86	A	12.0	12.0	144.0	16.0	144.0				22			4.4		12.0		12.0
901.50	S13	SB 80+86	S13	SB 80+99	R	13.0	12.0	156.0	17.3	156.0				22	6		4.8				
901.35	N7	NB 74+55	N7	NB 74+63	A	8.0	12.0	96.0	10.7	96.0				22			3.0				
901.35	N7	NB 74+80	N8	NB 74+88	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
901.37	N10	NB 75+78	N10	NB 75+86	A	8.0	12.0	96.0	10.7	96.0				22			3.0				
901.38	N11	NB 76+46	N12	NB 76+66	A	21.0	12.0	252.0	28.0	252.0				22			12.0	7.8			
901.39	N13	NB 76+97	N13	NB 77+05	L	8.0	12.0	96.0	10.7	96.0				22			3.0				
901.40	N14	NB 77+53	N14	NB 77+63	A	10.0	12.0	120.0	13.3	120.0				22			3.7				
901.40	N1	NB 77+79	N1	NB 77+94	L	15.0	12.0	180.0	20.0	180.0				22			5.6				
901.40	N1	NB 77+82	N1	NB 77+92	A	10.0	12.0	120.0	13.3	120.0				22			3.7				
901.41	N1	NB 78+04	N2	NB 78+30	R	26.0	12.0	312.0	34.7	312.0				22			12.0	9.6			
901.44	N6	NB 80+01	N7	NB 80+09	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
901.45	N7	NB 80+41	N8	NB 80+49	R	8.0	12.0	96.0	10.7	96.0				22			3.0				

SEE PLAN ON DWG. CPR-6

ALTERED ON:	AFFIXED ON: 5/10/2024	ITEM		DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS	
SIGNATURE: STAMP:	SIGNATURE: STAMP:	304.00010018	FINE GRADING OF EXISTING SUBBASE		SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF	
		404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION		TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY	
		404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION		TON	502.37010018	TRANSVERSE JOINTS	LF	
		407.01040009	NON-TRACKING TACK COAT		GAL	502.38010018	LONGITUDINAL JOINTS	LF	
		502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT		SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE(PCC) PAVEMENT, ASTM D 6690 TYPE IV		LF
		502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS		EA		SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER		LF
		502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS		EA				
		IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE. THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.		REVISIONS		DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING: CONCRETE PAVEMENT REPAIR TABLE	DRAWING NUMBER: CPRT-7

CHECKED BY: J.KLEINMANN


DRAFTED BY: N.HOY

CHECKED BY: J.KLEINMANN

DESIGNED BY: P.AMICONE

DESIGN SUPERVISOR: D.MARSH

FULL DEPTH REPAIR																									
MILE MARKER	START SLAB	LOCATION				REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS					
		START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)			
901.50	S1	SB 81+12	S1	SB 81+23	A	11.0	12.0	132.0	14.7	132.0							22			4.1					
901.50	S1	SB 81+12	S1	SB 81+23	R	11.0	12.0	132.0	14.7	132.0							22			4.1					
901.51	S2	SB 81+62	S2	SB 81+72	R	10.0	12.0	120.0	13.3	120.0							22			3.7					
901.53	S5	SB 82+76	S6	SB 83+31	C	54.0	12.0	648.0	72.0	648.0	15.8	24.3	2.5												
901.54	S5	SB 82+91	S5	SB 83+19	A	10.0	3.0	30.0	3.3	30.0							2			0.9					
901.54	S5	SB 82+91	S6	SB 83+31	R	39.0	12.0	468.0	52.0	468.0							22			14.4	24.0	39.0	24.0	39.0	
901.48	N10	NB 81+61	N11	NB 81+69	C	8.0	12.0	96.0	10.7	96.0							22			3.0					
901.49	N12	NB 82+40	N13	NB 82+48	C	8.0	12.0	96.0	10.7	96.0							22			3.0					
901.50	N13	NB 82+80	N1	NB 82+88	C	8.0	12.0	96.0	10.7	96.0							22			3.0					
901.51	N2	NB 83+38	N2	NB 83+46	L	8.0	12.0	96.0	10.7	96.0							22			3.0					
901.60	N1	NB 88+06	N1	NB 88+32	L	26.0	12.0	312.0	34.7	312.0							11			9.6	12.0		12.0		

ALTERED ON:	AFFIXED ON: 5/10/2024	ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS
SIGNATURE: STAMP:		304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
		404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
		404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF
		407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF
		502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE(PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
		502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA			
		502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.		REVISIONS		DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR TABLE	DRAWING NUMBER: CPRT-8

YD

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

FULL DEPTH REPAIR																					
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS												REMARKS
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)	ITEM 502.90010018 (LF)	
901.65	S4	SB 88+68	S4	SB 88+83	C	14.0	12.0	168.0	18.7	168.0	4.1	6.3	0.7								
901.65	S4	SB 88+69	S4	SB 88+83	L	14.0	12.0	168.0	18.7	168.0				22				5.2			
901.65	S4	SB 88+90	S5	SB 88+98	R	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.66	S5	SB 89+30	S6	SB 89+38	C	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.66	S5	SB 89+29	S7	SB 89+80	L	50.0	12.0	600.0	66.7	600.0				22			18.5	48.0		48.0	
901.67	S6	SB 89+72	S7	SB 89+80	R	8.0	12.0	96.0	10.7	96.0				22				3.0	8.0		8.0
901.67	S7	SB 90+05	S8	SB 90+20	C	15.0	12.0	180.0	20.0	180.0	4.4	6.7	0.7								
901.67	S7	SB 90+05	S8	SB 90+20	L	15.0	12.0	180.0	20.0	180.0				22				5.6			
901.68	S8	SB 90+29	S8	SB 90+39	R	10.0	12.0	120.0	13.3	120.0				22				3.7			
901.68	S8	SB 90+29	S8	SB 90+39	C	10.0	12.0	120.0	13.3	120.0				22				3.7			
901.68	S9	SB 90+66	S9	SB 90+74	L	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.69	S9	SB 90+92	S10	SB 91+00	R	8.0	12.0	96.0	10.7	96.0				22				3.0	8.0		8.0
901.69	S9	SB 90+92	S10	SB 91+00	C	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.69	S10	SB 91+27	S11	SB 91+40	R	13.0	12.0	156.0	17.3	156.0				22				4.8			
901.70	S10	SB 91+32	S11	SB 91+40	C	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.71	S12	SB 91+89	S14	SB 92+82	C	93.0	12.0	1116.0	124.0	1116.0	27.2	41.8	4.3								
901.71	S13	SB 92+27	S13	SB 92+39	R	12.0	12.0	144.0	16.0	144.0				22				4.4	12.0		12.0
901.72	S13	SB 92+35	S13	SB 92+43	L	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.72	S14	SB 92+69	S14	SB 92+82	L	13.0	12.0	156.0	17.3	156.0				22				4.8			
901.73	S1	SB 93+12	S1	SB 93+27	L	15.0	12.0	180.0	20.0	180.0				22				5.6			
901.71	S2	SB 93+50	S2	SB 93+58	L	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.72	S2	SB 93+75	S3	SB 94+01	C	27.0	12.0	324.0	36.0	324.0	7.9	12.1	1.3								
901.72	S2	SB 93+75	S3	SB 94+01	L	27.0	12.0	324.0	36.0	324.0				22			10.0	12.0		12.0	
901.73	S4	SB 94+51	S5	SB 94+61	R	10.0	12.0	120.0	13.3	120.0				22				3.7	10.0		10.0
901.73	S4	SB 94+51	S5	SB 94+61	C	10.0	12.0	120.0	13.3	120.0				22				3.7			
901.74	S5	SB 94+74	S6	SB 94+88	C	14.0	12.0	168.0	18.7	168.0	4.1	6.3	0.7								
901.74	S5	SB 94+74	S6	SB 94+88	L	14.0	12.0	168.0	18.7	168.0				22				5.2			
901.74	S6	SB 94+82	S6	SB 94+90	R	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.74	S7	SB 95+01	S10	SB 95+57	R	58.0	12.0	696.0	77.3	696.0				22			21.5	48.0	57.0	48.0	57.0
901.74	S7	SB 95+01	S10	SB 95+57	C	58.0	12.0	696.0	77.3	696.0	17.0	26.1	2.7								
901.74	S7	SB 95+01	S9	SB 95+37	L	44.0	12.0	528.0	58.7	528.0				22			16.3	24.0		24.0	
901.61	N2	NB 88+61	N2	NB 88+69	C	8.0	12.0	96.0	10.7	96.0				22	3			3.0			
901.62	N3	NB 89+00	N3	NB 89+08	L	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.62	N3	NB 89+00	N3	NB 89+08	C	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.62	N3	NB 89+00	N3	NB 89+08	R	8.0	12.0	96.0	10.7	96.0				22				3.0	8.0		8.0
901.65	N7	NB 90+50	N7	NB 90+58	R	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.67	N9	NB 91+41	N9	NB 91+49	R	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.67	N10	NB 91+76	N10	NB 91+84	L	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.71	N2	NB 93+87	N2	NB 93+95	C	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.72	N3	NB 94+18	N3	NB 94+26	C	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.74	N6	NB 95+11	N6	NB 95+19	L	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.74	N6	NB 95+11	N6	NB 95+19	C	8.0	12.0	96.0	10.7	96.0				22	3			3.0			
901.74	N6	NB 95+12	N7	NB 95+20	R	8.0	12.0	96.0	10.7	96.0				22				3.0			
901.74	N7	NB 95+26	N8	NB 95+34	C	8.0	12.0	96.0	10.7	96.0				22				3.0			

SEE PLAN ON DWG. CPR-8

ALTERED ON:	AFFIXED ON:	ITEM DESCRIPTION UNITS		ITEM DESCRIPTION UNITS	
SIGNATURE:	5/10/2024	304.00010018 FINE GRADING OF EXISTING SUBBASE SF	502.34010018 TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT LF		
STAMP:		404.1959 19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION TON	502.36010018 PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS CY		
		404.3779 37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION TON	502.37010018 TRANSVERSE JOINTS LF		
		407.01040009 NON-TRACKING TACK COAT GAL	502.38010018 LONGITUDINAL JOINTS LF		
		502.31010018 FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT SY	502.90010018 CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT, ASTM D 6690 TYPE IV LF		
		502.32050025 DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS EA	502.9310 SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER LF		
		502.33010018 DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS EA			
		IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.			
		REVISIONS			
		DATE	DESCRIPTION	BY	SYM.
		NEW YORK STATE OF OPPORTUNITY		Thruway Authority	
				TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION	
				CONTRACT NUMBER: TAN 24-12	
				DATE: MAY 2024	
				DRAWING NUMBER: CPRT-9	
				TITLE OF DRAWING: CONCRETE PAVEMENT REPAIR TABLE	

CHECKED BY: J.KLEINMANN

DRAFTED BY: N.HOY

CHECKED BY: J.KLEINMANN

DESIGNED BY: P.AMICONE

DESIGN SUPERVISOR: D.MARSH

ITEM	DESCRIPTION	UNITS
304.00010018	FINE GRADING OF EXISTING SUBBASE	SF
404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON
404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY
502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA
502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA
502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
502.37010018	TRANSVERSE JOINTS	LF
502.38010018	LONGITUDINAL JOINTS	LF
502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF

**FULL DEPTH REPAIR**

LOCATION			REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS						
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)	ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)		
902.06	S6	SB 110+93	S6	SB 111+08	C	15.0	12.0	180.0	20.0	180.0	4.4	6.7	0.7										
902.06	S6	SB 111+00	S6	SB 111+08	R	8.0	12.0	96.0	10.7	96.0				22				3.0					
902.08	S8	SB 111+76	S8	SB 111+91	R	15.0	12.0	180.0	20.0	180.0				22				5.6		15.0			15.0
902.08	S8	SB 111+76	S8	SB 111+91	C	15.0	12.0	180.0	20.0	180.0	4.4	6.7	0.7										
902.08	S8	SB 111+76	S8	SB 111+91	L	15.0	12.0	180.0	20.0	180.0				22				5.6					
902.09	S9	SB 112+26	S9	SB 112+34	L	8.0	12.0	96.0	10.7	96.0				22				3.0					
902.09	S10	SB 112+56	S10	SB 112+69	C	13.0	12.0	156.0	17.3	156.0	3.8	5.8	0.6										
902.09	S10	SB 112+56	S10	SB 112+69	L	13.0	12.0	156.0	17.3	156.0				22				4.8		13.0			13.0
902.10	S1	SB 112+93	S1	SB 113+04	C	11.0	12.0	132.0	14.7	132.0				22				4.1					
902.10	S1	SB 112+97	S1	SB 113+04	R	8.0	12.0	96.0	10.7	96.0				22	3			3.0					
902.10	S1	SB 112+96	S1	SB 113+04	L	8.0	12.0	96.0	10.7	96.0				22				3.0					
902.11	S3	SB 113+68	S3	SB 113+86	C	18.0	12.0	216.0	24.0	216.0	5.3	8.1	0.8										
902.11	S3	SB 113+68	S3	SB 113+86	L	18.0	12.0	216.0	24.0	216.0				22		12		6.7	12.0			12.0	
902.11	S3	SB 113+77	S3	SB 113+86	R	9.0	12.0	108.0	12.0	108.0				22				3.3					
902.12	S3	SB 113+95	S4	SB 114+03	R	8.0	12.0	96.0	10.7	96.0				22				3.0					
902.12	S4	SB 114+15	S4	SB 114+30	C	15.0	12.0	180.0	20.0	180.0	4.4	6.7	0.7										
902.12	S4	SB 114+22	S4	SB 114+30	R	8.0	12.0	96.0	10.7	96.0				22				3.0					
902.13	S4	SB 114+42	S5	SB 114+50	R	8.0	12.0	96.0	10.7	96.0				22				3.0					
902.13	S6	SB 114+70	S8	SB 115+17	C	48.0	12.0	576.0	64.0	576.0	14.0	21.6	2.2										
902.13	S6	SB 114+82	S8	SB 115+17	R	36.0	12.0	432.0	48.0	432.0				22				13.3	24.0	36.0	24.0	36.0	
902.14	S9	SB 115+36	S11	SB 115+74	R	38.0	12.0	456.0	50.7	456.0				22				14.1	24.0	28.0	24.0	28.0	
902.14	S9	SB 115+36	S11	SB 115+64	C	28.0	12.0	336.0	37.3	336.0	8.2	12.6	1.3										
902.16	S14	SB 116+42	S22	SB 118+03	R	160.0	12.0	1920.0	213.3	1920.0	46.8	71.9	7.5	11				59.3	156.0			156.0	
902.17	S17	SB 116+94	S22	SB 118+03	L	108.0	12.0	1296.0	144.0	1296.0				11				40.0	84.0			84.0	
902.04	N6	NB 111+29	N8	NB 111+70	L	42.0	12.0	504.0	56.0	504.0				22				15.6	24.0			24.0	
900.15	N6	NB 11+29	N8	NB 111+70	C	42.0	12.0	504.0	56.0	504.0	12.3	18.9	2.0										
902.05	N8	NB 111+70	N8	NB 112+00	R	30.0	12.0	360.0	40.0	360.0				22	7			11.1	12.0			12.0	
900.16	N8	NB 11+85	N8	NB 111+93	L	8.0	12.0	96.0	10.7	96.0				22				3.0					
902.05	N8	NB 111+85	N8	NB 111+93	C	8.0	12.0	96.0	10.7	96.0				22				3.0		8.0		8.0	
902.06	N8	NB 112+06	N10	NB 112+53	L	47.0	12.0	564.0	62.7	564.0				22		12		17.4	24.0			24.0	
902.06	N8	NB 112+08	N4	NB 114+33	C	226.0	12.0	2712.0	301.3	2712.0	66.1	101.6	10.5										
902.06	N9	NB 112+17	N2	NB 113+96	R	180.0	12.0	2160.0	240.0	2160.0				22				66.7	144.0	165.0	144.0	165.0	
902.07	N11	NB 112+83	N12	NB 112+91	L	8.0	12.0	96.0	10.7	96.0				22				3.0					
902.08	N12	NB 113+05	N13	NB 113+13	L	8.0	12.0	96.0	10.7	96.0				22				3.0					
902.09	N15	NB 113+58	N2	NB 114+03	L	45.0	12.0	540.0	60.0	540.0				22				16.7	24.0			24.0	
902.12	N5	NB 114+53	N7	NB 114+91	R	38.0	12.0	456.0	50.7	456.0				22				14.1	24.0			24.0	
902.12	N6	NB 114+89	N9	NB 115+34	C	45.0	12.0	540.0	60.0	540.0	13.2	20.2	2.1										
902.13	N8	NB 115+26	N9	NB 115+34	L	8.0	12.0	96.0	10.7	96.0				22				3.0					
902.13	N9	NB 115+49	N10	NB 115+57	L	8.0	12.0	96.0	10.7	96.0				22				3.0					
902.13	N9	NB 115+49	N10	NB 115+57	C	8.0	12.0	96.0	10.7	96.0				22				3.0		8.0		8.0	
902.14	N11	NB 115+75	N14	NB 116+44	C	70.0	12.0	840.0	93.3	840.0	20.5	31.5	3.3										
902.14	N11	NB 115+75	N14	NB 116+44	R	70.0	12.0	840.0	93.3	840.0				22				25.9	48.0	70.0	48.0	70.0	
902.14	N11	NB 115+86	N14	NB 116+44	L	59.0	12.0	708.0	78.7	708.0				22				21.9	36.0			36.0	
900.17	N15	NB 11+56	N15	NB 116+64	L	8.0	12.0	96.0	10.7	96.0				22				3.0					
902.16	N15	NB 116+56	N15	NB 116+64	C	8.0	12.0	96.0	10.7	96.0				22	3			3.0					
902.16	N15	NB 116+56	N15	NB 116+64	R	8.0	12.0	96.0	10.7	96.0				22				3.0		8.0		8.0	
902.16	N16	NB 116+76	N26	NB 118+89	C	200.0	12.0	2400.0	266.7	2400.0	58.5	89.9	9.3										
902.16	N16	NB 116+88	N24	NB 118+49	L	156.0	12.0	1872.0	208.0	1872.0				22				57.8	120.0			120.0	
902.17	N17	NB 117+10	N26	NB 118+89	R	180.0	12.00	2160.0	240.0	2160.0				22				66.7	120.0	180.0	120.0	180.0	

SEE PLAN ON DWG. CPR-11

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

**STATE OF NEW YORK  
JOSEPH W. KLEINMANN  
LICENSED PROFESSIONAL ENGINEER  
EXCELLENCE  
092577**

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY**  
**Thruway Authority**

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**TITLE OF PROJECT**  
I-190 HIGHWAY REHABILITATION

**LOCATION OF PROJECT**  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

**TITLE OF DRAWING**  
CONCRETE PAVEMENT REPAIR TABLE

**CONTRACT NUMBER:** TAN 24-12

**DATE:** MAY 2024

**DRAWING NUMBER:** CPRT-10



FULL DEPTH REPAIR

MILE MARKER	START SLAB	LOCATION				REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS		
		START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)
902.28	S1	SB 122+57	S2	SB 123+01	R	44.5	12.0	534.0	59.3	534.0				11		24	16.5	24.0	30.0	24.0	30.0	
902.29	S1	SB 122+85	S2	SB 123+01	C	16.0	12.0	192.0	21.3	192.0	4.7	7.2	0.7									
902.29	S2	SB 122+98	S3	SB 123+22	L	25.0	12.0	300.0	33.3	300.0				22		12	9.3					
902.29	S3	SB 123+27	S3	SB 123+39	R	12.0	12.0	144.0	16.0	144.0				22			4.4		12.0		12.0	
902.29	S3	SB 123+27	S3	SB 123+39	C	12.0	12.0	144.0	16.0	144.0	3.5	5.4	0.6									
902.30	S4	SB 123+64	S2	SB 125+56	R	194.0	12.0	2328.0	258.7	2328.0				22			71.9	156.0	194.0	156.0	194.0	
902.30	S4	SB 123+64	S2	SB 125+56	C	194.0	12.0	2328.0	258.7	2328.0	56.7	87.2	9.1									
902.31	S4	SB 123+98	S5	SB 124+06	L	8.0	12.0	96.0	10.7	96.0				22			3.0					
902.28	N1	NB 123+56	N2	NB 123+82	L	26.0	12.0	312.0	34.7	312.0				11			9.6	24.0		24.0		
902.28	N1	NB 123+56	N2	NB 123+82	C	26.0	12.0	312.0	34.7	312.0	7.6	11.7	1.2									
902.28	N1	NB 123+56	N2	NB 123+82	R	10.0	12.0	120.0	13.3	120.0				22	12		3.7					
902.29	N2	NB 123+96	N2	NB 124+04	C	8.0	12.0	96.0	10.7	96.0				22			3.0					
902.29	N2	NB 123+96	N2	NB 124+04	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0	

SEE PLAN ON DWG. CPR-12

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

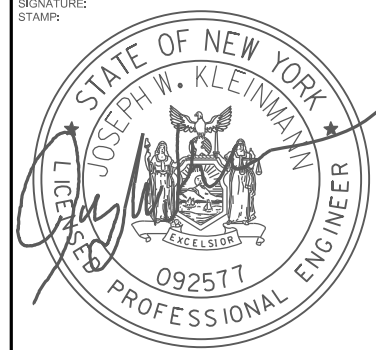
DESIGN SUPERVISOR: D. MARSH

ALTERED ON: \_\_\_\_\_

AFFIXED ON: **5/10/2024**

SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_



ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS
304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF
407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF
502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF
502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA			

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2

TITLE OF DRAWING: CONCRETE PAVEMENT REPAIR TABLE

CONTRACT NUMBER: TAN 24-12

DATE: MAY 2024

DRAWING NUMBER: CPRT-11

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

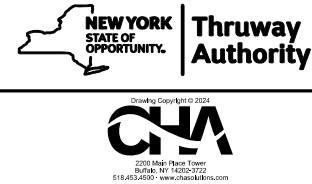
FULL DEPTH REPAIR																				REMARKS			
LOCATION					REPAIR DIMENSIONS				REPAIR ITEMS														
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)	
902.29	S5	SB 124+39	S6	SB 124+47	L	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.29	S6	SB 124+57	S6	SB 124+65	L	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.30	S1	SB 124+97	S1	SB 125+06	L	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.31	S1	SB 125+23	S2	SB 125+31	L	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.31	S2	SB 125+56	S3	SB 125+67	L	11.0	12.0	132.0	14.7	132.0				22			4.1						
902.32	S3	SB 125+70	S3	SB 125+96	R	26.0	12.0	312.0	34.7	312.0				22			9.6	12.0	26.0	12.0	26.0		
902.32	S3	SB 125+70	S3	SB 125+96	C	26.0	12.0	312.0	34.7	312.0	7.6	11.7	1.2										
902.32	S3	SB 125+83	S3	SB 125+91	A	8.0	14.5	116.0	12.9	116.0				27			3.6						
902.32	S4	SB 126+10	S7	SB 127+22	R	114.0	12.0	1368.0	152.0	1368.0				22	48		42.2	96.0		96.0			
902.32	S4	SB 126+10	S7	SB 127+22	C	114.0	12.0	1368.0	152.0	1368.0	33.3	51.2	5.3										
902.33	S4	SB 126+21	S5	SB 126+41	A	36.0	13.5	486.0	54.0	486.0				25			15.0	27.0	36.0	27.0	36.0		
902.34	S6	SB 127+19	S8	SB 127+71	A	52.0	12.0	624.0	69.3	624.0				22			19.3	36.0	14.0	36.0	14.0		
902.35	S7	SB 127+57	S9	SB 128+10	R	53.0	12.0	636.0	70.7	636.0				22		12	19.6	24.0		24.0			
902.34	S8	SB 127+08	S8	SB 127+88	L	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.36	S8	SB 127+97	S9	SB 128+10	A	13.0	12.0	156.0	17.3	156.0				22			4.8		13.0			13.0	
902.36	S8	SB 127+97	S9	SB 128+10	C	13.0	12.0	156.0	17.3	156.0	3.8	5.8	0.6										
902.37	S9	SB 128+35	S10	SB 128+43	R	8.0	12.0	96.0	10.7	96.0				22	3		3.0						
902.37	S9	SB 128+35	S10	SB 128+43	C	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.37	S10	SB 128+72	S11	SB 129+30	R	60.0	12.0	720.0	80.0	720.0				11	25		22.2	48.0		48.0			
902.37	S10	SB 128+72	S12	SB 129+30	C	60.0	12.0	720.0	80.0	720.0	17.5	27.0	2.8										
902.37	S10	SB 128+76	S11	SB 128+84	A	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.38	S12	SB 129+32	S3	SB 130+89	A	158.0	7.0	1106.0	122.9	1106.0				6			34.1	84.0	158.0	84.0	158.0		
902.30	N3	NB 124+42	N3	NB 124+50	C	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.30	N1	NB 124+82	N1	NB 124+90	C	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.32	N3	NB 125+61	N3	NB 125+75	C	15.0	12.0	180.0	20.0	180.0	4.4	6.7	0.7										
902.32	N3	NB 125+61	N3	NB 125+75	R	15.0	12.0	180.0	20.0	180.0				22			5.6		15.0		15.0		
902.33	N4	NB 126+04	N4	NB 126+12	C	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.33	N5	NB 126+44	N5	NB 126+59	C	15.0	12.0	180.0	20.0	180.0	4.4	6.7	0.7										
902.33	N5	NB 126+44	N5	NB 126+59	R	15.0	12.0	180.0	20.0	180.0				22			5.6		15.0		15.0		
902.34	N6	NB 126+83	N6	NB 126+91	C	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.34	N6	NB 126+83	N6	NB 126+91	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0		
902.35	N7	NB 127+24	N7	NB 127+32	R	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.36	N8	NB 127+83	N9	NB 127+91	R	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.37	N10	NB 128+45	N10	NB 128+53	C	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.38	N11	NB 128+84	N11	NB 128+92	C	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.38	N11	NB 128+84	N11	NB 128+92	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0		
902.39	N12	NB 129+24	N12	NB 129+32	C	8.0	12.0	96.0	10.7	96.0				22			3.0						
902.39	N12	NB 129+24	N12	NB 129+32	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0		

SEE PLAN ON DWG. CPR-13

ALTERED ON:	AFFIXED ON:	ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS
	5/10/2024	304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
		404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
		404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF
		407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF
		502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
		502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF
		502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA			

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR TABLE	DRAWING NUMBER: CPR-12

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

FULL DEPTH REPAIR																								
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS												REMARKS			
MP	SL	SS	ES	ES	LANE	LF	WF	AF	IT502.31010018 (SY)	IT304.00010018 (SF)	IT404.1959 (TON)	IT404.3779 (TON)	IT407.01040009 (GAL)	IT502.32050025 (EA)	IT502.33010018 (EA)	IT502.34010018 (LF)	IT502.36010018 (CY)	IT502.37010018 (LF)	IT502.38010018 (LF)	IT502.90010018 (LF)		IT502.9310 (LF)		
902.47	S4	SB 133+71	S4	SB 133+89	R	18.0	12.0	216.0	24.0	216.0				22		12	6.7		18.0		18.0			
902.47	S4	SB 133+71	S4	SB 133+89	C	18.0	12.0	216.0	24.0	216.0	5.3	8.1	0.8											
902.48	S5	SB 134+24	S7	SB 134+69	R	88.0	12.0	1056.0	117.3	1056.0				22		12	32.6	60.0	10.0	60.0	10.0			
902.48	S5	SB 134+24	S5	SB 134+34	C	10.0	12.0	120.0	13.3	120.0				22			3.7							
902.49	S6	SB 134+51	S6	SB 135+12	C	18.0	12.0	216.0	24.0	216.0	5.3	8.1	0.8											
902.50	S7	SB 134+98	S7	SB 135+06	L	8.0	12.0	96.0	10.7	96.0				22			3.0							
902.50	S7	SB 135+02	S8	SB 135+22	C	20.0	12.0	240.0	26.7	240.0	5.8	9.0	0.9											
902.50	S1	SB 135+37	S2	SB 135+62	R	25.0	12.0	300.0	33.3	300.0				22			9.3	12.0	12.0	12.0	12.0			
902.50	S1	SB 135+37	S1	SB 135+49	C	12.0	12.0	144.0	16.0	144.0	3.5	5.4	0.6											
902.51	S2	SB 135+79	S2	SB 135+89	R	10.0	12.0	120.0	13.3	120.0				22			3.7		10.0		10.0			
902.51	S2	SB 135+79	S2	SB 135+89	C	10.0	12.0	120.0	13.3	120.0				22	4		3.7							
902.51	S2	SB 135+79	S2	SB 135+89	L	10.0	12.0	120.0	13.3	120.0				22			3.7							
902.52	S3	SB 136+15	S3	SB 136+23	C	8.0	12.0	96.0	10.7	96.0				22			3.0							
902.53	S4	SB 136+54	S4	SB 136+63	R	9.0	12.0	108.0	12.0	108.0				22			3.3		9.0		9.0			
902.53	S4	SB 136+54	S4	SB 136+63	C	9.0	12.0	108.0	12.0	108.0				22			3.3							
902.53	S4	SB 136+77	S5	SB 136+85	R	8.0	12.0	96.0	10.7	96.0				22			3.0							
902.53	S5	SB 136+98	S5	SB 137+06	C	8.0	12.0	96.0	10.7	96.0				22			3.0							
902.54	S6	SB 137+35	S6	SB 137+43	C	8.0	12.0	96.0	10.7	96.0				22			3.0							
902.54	S6	SB 137+43	S6	SB 137+54	R	11.0	12.0	132.0	14.7	132.0				22			4.1							
902.54	S6	SB 137+59	S7	SB 137+75	C	16.0	12.0	192.0	21.3	192.0	4.7	7.2	0.7											
902.55	S7	SB 137+81	S7	SB 137+95	R	14.0	12.0	168.0	18.7	168.0				22			5.2							
902.55	S7	SB 137+97	S8	SB 138+05	C	8.0	12.0	96.0	10.7	96.0				22	3		3.0							
902.55	S7	SB 137+97	S8	SB 138+05	L	8.0	12.0	96.0	10.7	96.0				22			3.0							
902.56	S8	SB 138+17	S9	SB 138+52	R	35.0	12.0	420.0	46.7	420.0				22			13.0	24.0	12.0	24.0	12.0			
902.56	S8	SB 138+17	S8	SB 138+29	C	12.0	12.0	144.0	16.0	144.0	3.5	5.4	0.6											
902.56	S8	SB 138+17	S8	SB 138+29	L	12.0	12.0	144.0	16.0	144.0				22			4.4							
902.56	S9	SB 138+49	S9	SB 138+65	C	16.0	12.0	192.0	21.3	192.0	4.7	7.2	0.7											
902.56	S9	SB 138+49	S9	SB 138+57	L	8.0	12.0	96.0	10.7	96.0				22			3.0							
902.57	S9	SB 138+77	S10	SB 138+85	C	8.0	12.0	96.0	10.7	96.0				22			3.0							
902.55	S9	SB 138+08	S10	SB 139+11	R	31.0	12.0	372.0	41.3	372.0				22			11.5	24.0		24.0				
902.47	N3	NB 133+53	N3	NB 133+68	R	15.0	12.0	180.0	20.0	180.0				11			5.6							
902.47	N3	NB 133+68	N3	NB 133+78	A	10.0	12.0	120.0	13.3	120.0				22			3.7							
902.48	N4	NB 134+05	N4	NB 134+13	L	8.0	12.0	96.0	10.7	96.0				22			3.0							
902.48	N5	NB 134+52	N6	NB 134+74	C	45.0	12.0	540.0	60.0	540.0	13.2	20.2	2.1											
902.50	N1	NB 135+24	N2	NB 135+63	C	39.0	12.0	468.0	52.0	468.0	11.4	17.5	1.8											
902.52	N3	NB 136+09	N3	NB 136+17	R	8.0	12.0	96.0	10.7	96.0				22			3.0							
902.54	N6	NB 137+29	N6	NB 137+37	C	8.0	12.0	96.0	10.7	96.0				22			3.0							
902.54	N6	NB 137+29	N6	NB 137+37	R	8.0	12.0	96.0	10.7	96.0				22	3		3.0							
902.54	N6	NB 137+29	N6	NB 137+37	A	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0			
902.54	N7	NB 137+62	N7	NB 137+70	A	8.0	8.00	64.00	7.1	64.0				14			2.0							

SEE PLAN ON DWG. CPR-14

ALTERED ON: [Blank]

AFFIXED ON: 5/10/2024

SIGNATURE: [Blank]

STAMP: [Blank]

SIGNATURE: [Blank]

STAMP:

ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS
304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF
407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF
502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF
502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA			

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority**

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2

TITLE OF DRAWING: CONCRETE PAVEMENT REPAIR TABLE

CONTRACT NUMBER: TAN 24-12

DATE: MAY 2024

DRAWING NUMBER: CPRT-13

CHECKED BY: J.KLEINMANN

DRAFTED BY: N.HOY

CHECKED BY: J.KLEINMANN

DESIGNED BY: P.AMICONE

DESIGN SUPERVISOR: D.MARSH

FULL DEPTH REPAIR																					
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS	
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)
902.57	S10	SB 138+96	S10	SB 139+04	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.57	S10	SB 139+17	S11	SB 139+25	L	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.58	S11	SB 139+36	S11	SB 139+46	R	10.0	12.0	120.0	13.3	120.0				22			3.7		10.0		10.0
902.58	S11	SB 139+36	S11	SB 139+46	C	10.0	12.0	120.0	13.3	120.0				22			3.7				
902.58	S11	SB 139+60	S12	SB 139+84	C	24.0	12.0	288.0	32.0	288.0	7.0	10.8	1.1								
902.58	S11	SB 139+60	S12	SB 139+68	L	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.59	S12	SB 139+72	S12	SB 139+84	R	12.0	12.0	144.0	16.0	144.0				22			4.4		12.0		12.0
902.59	S13	SB 140+16	S13	SB 140+24	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.60	S13	SB 140+37	S14	SB 140+45	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.60	S14	SB 140+59	S14	SB 140+66	R	20.0	12.0	240.0	26.7	240.0				22		12	7.4		20.0		20.0
902.60	S14	SB 140+59	S14	SB 140+66	C	20.0	12.0	240.0	26.7	240.0	5.8	9.0	0.9								
902.66	S2	SB 143+68	S3	SB 144+18	R	51.0	12.0	612.0	68.0	612.0				22			18.9	36.0		36.0	
902.67	S3	SB 144+08	S4	SB 144+36	C	28.0	12.0	336.0	37.3	336.0	8.2	12.6	1.3								
902.67	S4	SB 144+51	S5	SB 144+95	R	44.0	12.0	528.0	58.7	528.0				22			16.3	36.0	44.0	36.0	44.0
902.67	S4	SB 144+51	S5	SB 144+95	C	44.0	12.0	528.0	58.7	528.0	12.9	19.8	2.1								
902.68	S4	SB 144+65	S5	SB 144+95	L	30.0	12.0	360.0	40.0	360.0				22			11.1	24.0		24.0	
902.69	S6	SB 145+51	S7	SB 145+59	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.69	S6	SB 145+51	S7	SB 145+59	L	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.70	S7	SB 145+75	S1	SB 146+18	R	43.0	12.0	516.0	57.3	516.0				22		12	15.9	24.0	8.0	24.0	8.0
902.70	S7	SB 145+77	S1	SB 145+99	C	22.0	12.0	264.0	29.3	264.0	6.4	9.9	1.0								
902.70	S7	SB 145+91	S1	SB 145+99	L	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.58	N11	NB 139+27	N1	NB 139+35	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.58	N11	NB 139+27	N1	NB 139+35	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0
902.60	N14	NB 140+40	N14	NB 140+48	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.66	N2	NB 143+78	N2	NB 143+86	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.68	N4	NB 144+55	N4	NB 144+63	L	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.68	N5	NB 145+02	N5	NB 145+10	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.69	N6	NB 145+34	N6	NB 145+42	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.69	N6	NB 145+58	N1	NB 145+66	L	8.0	12.0	96.0	10.7	96.0				22			3.0				
902.70	N1	NB 145+72	N1	NB 145+79	C	8.0	12.0	96.0	10.7	96.0				22			3.0				

SEE PLAN ON DWG. CPR-15

ALTERED ON:	AFFIXED ON:	ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS
	5/10/2024	304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
		404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
		404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF
		407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF
		502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE(PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
		502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF
		502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA			



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR TABLE	DRAWING NUMBER: CPRT-14

CHECKED BY: J.KLEINMANN  
DRAFTED BY: N.HOY  
CHECKED BY: J.KLEINMANN  
DESIGNED BY: P. AMICONE  
DESIGN SUPERVISOR: D.MARSH

ITEM	DESCRIPTION	UNITS
304.00010018	FINE GRADING OF EXISTING SUBBASE	SF
404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON
404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY
502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA
502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA
502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
502.37010018	TRANSVERSE JOINTS	LF
502.38010018	LONGITUDINAL JOINTS	LF
502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE(PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF


FULL DEPTH REPAIR																						
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS		
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)
902.71	S1	SB 146+31	S2	SB 146+39	R	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.71	S2	SB 146+53	S2	SB 146+61	L	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.72	S2	SB 146+71	S3	SB 146+79	C	8.0	12.0	96.0	10.7	96.0					22	3		3.0				
902.72	S3	SB 146+92	S3	SB 147+02	R	10.0	12.0	120.0	13.3	120.0					22			3.7		10.0		10.0
902.72	S3	SB 146+92	S3	SB 147+02	C	10.0	12.0	120.0	13.3	120.0					22			3.7				
902.72	S3	SB 146+92	S3	SB 147+02	L	10.0	12.0	120.0	13.3	120.0					22			3.7				
902.72	S3	SB 147+10	S4	SB 147+18	R	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.72	S3	SB 147+10	S4	SB 147+18	L	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.73	S4	SB 147+51	S5	SB 147+61	C	10.0	12.0	120.0	13.3	120.0					22			3.7				
902.73	S4	SB 147+54	S5	SB 147+61	R	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.74	S5	SB 147+88	S6	SB 147+96	R	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.74	S6	SB 148+03	S6	SB 148+15	C	12.0	12.0	144.0	16.0	144.0	3.5	5.4	0.6									
902.74	S6	SB 148+23	S7	SB 148+43	R	20.0	12.0	240.0	26.7	240.0					22		12	7.4	12.0		12.0	
902.75	S7	SB 148+65	S8	SB 148+95	R	30.0	12.0	360.0	40.0	360.0					22		12	11.1	12.0		12.0	
902.75	S7	SB 148+68	S8	SB 148+76	C	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.76	S8	SB 148+95	S8	SB 149+03	C	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.76	S8	SB 149+12	S9	SB 149+36	R	24.0	12.0	288.0	32.0	288.0					22		12	8.9		24.0		24.0
902.76	S8	SB 149+12	S9	SB 149+36	C	24.0	12.0	288.0	32.0	288.0	7.0	10.8	1.1									
902.76	S8	SB 149+12	S9	SB 149+44	L	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.77	S9	SB 149+44	S10	SB 149+56	L	12.0	12.0	144.0	16.0	144.0					22			4.4				
902.78	S10	SB 149+91	S11	SB 149+99	R	8.0	12.0	96.0	10.7	96.0					22			3.0		8.0		8.0
902.78	S10	SB 149+91	S11	SB 149+99	C	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.78	S11	SB 150+18	S11	SB 150+26	C	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.78	S11	SB 150+31	S12	SB 150+39	R	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.79	S11	SB 150+61	S12	SB 150+39	L	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.79	S12	SB 150+52	S12	SB 150+52	R	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.79	S12	SB 150+52	S12	SB 150+60	L	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.79	S12	SB 150+60	S13	SB 150+76	R	8.0	12.0	96.0	10.7	96.0					22			3.0		8.0		8.0
902.79	S12	SB 150+68	S13	SB 150+76	C	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.79	S13	SB 150+86	S13	SB 150+99	C	13.0	12.0	156.0	17.3	156.0	3.8	5.8	0.6									
902.79	S13	SB 150+86	S13	SB 150+99	L	13.0	12.0	156.0	17.3	156.0					22			4.8				
902.80	S13	SB 150+91	S13	SB 150+99	R	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.80	S13	SB 151+09	S1	SB 151+17	R	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.81	S2	SB 151+66	S3	SB 152+16	R	50.0	12.0	600.0	66.7	600.0					22		18.5	36.0	70.0	36.0	70.0	
902.81	S2	SB 151+66	S3	SB 152+16	C	50.0	12.0	600.0	66.7	600.0	14.6	22.5	2.3									
902.81	S2	SB 151+94	S3	SB 152+16	L	22.0	12.0	264.0	29.3	264.0					22			8.1	12.0		12.0	
902.71	N2	NB 146+22	N2	NB 146+30	L	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.71	N2	NB 146+22	N2	NB 146+30	C	8.0	12.0	96.0	10.7	96.0					22			3.0		8.0		8.0
902.72	N3	NB 146+60	N3	NB 146+68	C	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.75	N7	NB 148+15	N7	NB 148+23	R	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.75	N8	NB 148+55	N8	NB 148+71	C	16.0	12.0	192.0	21.3	192.0	4.7	7.2	0.7									
902.75	N8	NB 148+55	N8	NB 148+63	R	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.76	N9	NB 149+00	N9	NB 149+08	L	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.78	N11	NB 149+75	N11	NB 149+33	L	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.79	N13	NB 150+53	N13	NB 150+61	C	8.0	12.0	96.0	10.7	96.0					22			3.0				
902.80	N14	NB 150+92	N14	NB 151+06	C	14.0	12.0	168.0	18.7	168.0	4.1	6.3	0.7									
902.80	N1	NB 151+35	N1	NB 151+47	L	12.0	12.0	144.0	16.0	144.0					22			4.4				
902.80	N1	NB 151+35	N1	NB 151+47	C	12.0	12.0	144.0	16.0	144.0	3.5	5.4	0.6									
902.82	N3	NB 152+01	N3	NB 152+16	L	15.00	12.00	180.00	20.0	180.0					22			5.6				

SEE PLAN ON DWG. CPR-16

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.




TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
CONCRETE PAVEMENT  
REPAIR TABLE

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
CPR-15

CHECKED BY: J.KLEINMANN

DRAFTED BY: N.HOY

CHECKED BY: J.KLEINMANN

DESIGNED BY: P.AMICONE

DESIGN SUPERVISOR: D.MARSH

**FULL DEPTH REPAIR**

MILE MARKER	LOCATION					REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS		
	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)
902.88	S7	SB 155+30	S8	SB 155+50	R	20.0	12.0	240.0	26.7	240.0				22			7.4	12.0	11.0	12.0	11.0	
902.88	S7	SB 155+39	S8	SB 155+50	C	11.0	12.0	132.0	14.7	132.0				22			4.1					
902.89	S9	SB 155+77	S3	SB 156+87	A	110.0	7.0	770.0	85.6	770.0				12			23.8			49.0		
902.89	S11	SB 156+08	S12	SB 156+19	R	11.0	12.0	132.0	14.7	132.0				22			4.1					
902.89	S11	SB 156+11	S12	SB 156+19	C	8.0	12.0	96.0	10.7	96.0				22			3.0					
902.91	S2	SB 156+63	S2	SB 156+71	R	8.0	12.0	96.0	10.7	96.0				22			3.0					
902.91	S4	SB 157+02	S4	SB 157+10	A	8.0	9.0	72.0	8.0	72.0				16			2.2					
902.91	S4	SB 157+02	S4	SB 157+10	C	8.0	12.0	96.0	10.7	96.0				22			3.0					
902.91	S4	SB 157+02	S4	SB 157+10	L	8.0	12.0	96.0	10.7	96.0				22			3.0					
902.92	S5	SB 157+23	S6	SB 157+51	A	28.0	10.0	280.0	31.1	280.0				18			8.6	10.0		10.0		
902.92	S5	SB 157+23	S5	SB 157+31	R	8.0	12.0	96.0	10.7	96.0				22			3.0					
902.92	S7	SB 157+63	S7	SB 157+71	A	8.0	12.0	96.0	10.7	96.0				22			3.0					

**FULL DEPTH REPAIR**

MILE MARKER	LOCATION					REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS		
	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)
903.09	S11	SB 166+39	S11	SB 166+77	L	8.0	12.0	96.0	10.7	96.0				22			3.0					
903.11	S15	SB 167+32	S16	SB 167+43	R	11.0	12.0	132.0	14.7	132.0				22			4.1		11.0			11.0
903.11	S15	SB 167+32	S16	SB 167+43	C	11.0	12.0	132.0	14.7	132.0				22	5		4.1					
903.11	S15	SB 167+32	S16	SB 167+43	L	11.0	12.0	132.0	14.7	132.0				22			4.1					
903.11	S16	SB 167+41	S17	SB 167+83	A	31.0	15.0	465.0	51.7	465.0				28		12	14.4	18.0		18.0		
903.11	S17	SB 167+70	S16	SB 167+52	R	20.0	12.0	240.0	26.7	240.0				22		12	7.4		22.0			22.0
903.11	S17	SB 167+71	S16	SB 167+52	C	20.0	12.0	240.0	26.7	240.0	5.8	9.0	0.9									
903.11	S17	SB 167+71	S16	SB 167+52	L	20.0	12.0	240.0	26.7	240.0				22		12	7.4					
903.05	N1	NB 164+47	N3	NB 164+88	L	42.0	12.0	504.0	56.0	504.0				11			15.6	24.0		24.0		
903.05	N1	NB 164+47	N3	NB 164+88	C	42.0	12.0	504.0	56.0	504.0	12.3	18.9	2.0									
903.05	N1	NB 164+47	N3	NB 164+88	R	42.0	12.0	504.0	56.0	504.0				11			15.6	24.0	42.0	24.0	42.0	42.0
903.07	N8	NB 165+25	N10	NB 166+26	A	26.0	8.0	208.0	23.1	208.0				7.0			6.4	8.0		8.0		

ALTERED ON:	AFFIXED ON:	ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS
	5/10/2024	304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
		404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
		404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF
		407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF
		502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE(PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
		502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF
		502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA			



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**REVISIONS**

DATE	DESCRIPTION	BY	SYM.

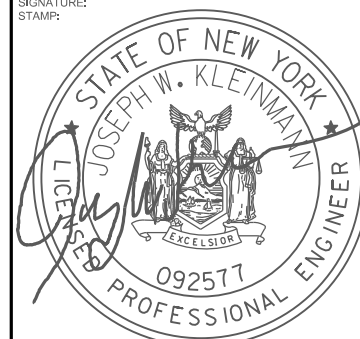


TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR TABLE	DRAWING NUMBER: CPRT-16

ITEM	DESCRIPTION	UNITS
304.00010018	FINE GRADING OF EXISTING SUBBASE	SF
404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON
404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY
502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA
502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA
502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
502.37010018	TRANSVERSE JOINTS	LF
502.38010018	LONGITUDINAL JOINTS	LF
502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE(PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF


MILE MARKER	LOCATION					REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS		
	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)
903.16	S2	SB 170+72	S4	SB 171+17	L	45.0	12.0	540.0	60.0	540.0				22			16.7	24.0		24.0		
903.16	S2	SB 170+82	S3	SB 170+95	C	13.0	12.0	156.0	17.3	156.0	3.8	5.8	0.6									
903.16	S3	SB 170+89	S3	SB 170+97	A	8.0	12.0	96.0	10.7	96.0				22			3.0					
903.18	S7	SB 171+82	S10	SB 172+36	L	54.0	12.0	648.0	72.0	648.0				22			20.0	36.0		36.0		
903.19	S11	SB 172+62	S12	SB 172+72	L	10.0	12.0	120.0	13.3	120.0				22			3.7					
903.20	S13	SB 172+87	S13	SB 172+96	L	8.0	12.0	96.0	10.7	96.0				22			3.0					
903.20	S1	SB 173+10	S2	SB 173+23	L	13.0	12.0	156.0	17.3	156.0				22			4.8					
903.20	S1	SB 173+10	S1	SB 173+23	C	8.0	12.0	96.0	10.7	96.0				22			3.0					
903.20	S2	SB 173+28	S3	SB 173+43	A	15.0	8.0	120.0	13.3	120.0				14			3.7					
903.21	S4	SB 173+68	S6	SB 174+18	A	50.0	4.5	225.0	25.0	225.0				3.5			6.9	13.5		13.5		
903.21	S4	SB 173+78	S5	SB 173+86	C	8.0	12.0	96.0	10.7	96.0				22			3.0					
903.23	S7	SB 174+41	S8	SB 174+53	R	12.0	12.0	144.0	16.0	144.0				22			4.4	12.0		12.0		
903.23	S7	SB 174+41	S8	SB 174+53	C	12.0	12.0	144.0	16.0	144.0	3.5	5.4	0.6									
903.23	S7	SB 174+41	S8	SB 174+53	L	12.0	12.0	144.0	16.0	144.0				22			4.4					
903.23	S9	SB 174+68	S9	SB 174+76	R	8.0	12.0	96.0	10.7	96.0				22			3.0					
903.23	S10	SB 174+88	S11	SB 175+16	R	28.0	12.0	336.0	37.3	336.0				22			10.4	12.0	28.0	12.0	28.0	
903.23	S10	SB 174+88	S11	SB 175+16	C	28.0	12.0	336.0	37.3	336.0	8.2	12.6	1.3									
903.23	S10	SB 174+88	S11	SB 175+16	L	28.0	12.0	336.0	37.3	336.0				22			10.4	12.0		12.0		
903.25	S13	SB 175+61	S14	SB 175+76	R	15.0	12.0	180.0	20.0	180.0				22			5.6		15.0		15.0	
903.25	S13	SB 175+60	S14	SB 175+76	C	15.0	12.0	180.0	20.0	180.0	4.4	6.7	0.7									
903.25	S13	SB 175+61	S14	SB 175+76	L	15.0	12.0	180.0	20.0	180.0				22			5.6					
903.17	N6	NB 170+80	N7	NB 170+87	A	8.0	12.0	96.0	10.7	96.0				11			3.0		13.0		13.0	
903.17	N6	NB 170+79	N7	NB 170+87	C	8.0	12.0	96.0	10.7	96.0				22			3.0					
903.19	N11	NB 171+72	N11	NB 171+80	R	8.0	12.0	96.0	10.7	96.0				22			3.0					
903.20	N13	NB 172+20	N1	NB 172+41	C	20.0	12.0	240.0	26.7	240.0	5.8	9.0	0.9									
903.20	N13	NB 172+20	N1	NB 172+41	R	20.0	12.0	240.0	26.7	240.0				22			7.4	12.0	20.0	12.0	20.0	
903.24	N1	NB 172+31	N1	NB 172+40	L	8.0	12.0	96.0	10.7	96.0				22			3.0					
903.26	N16	NB 175+33	N6	NB 175+41	R	8.0	12.0	96.0	10.7	96.0				22			3.0					

MILE MARKER	LOCATION					REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS		
	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)
903.27	S19	SB 176+76	S20	SB 176+85	R	9.0	12.0	108.0	12.0	108.0				22			3.3		9.0		9.0	
903.27	S19	SB 176+76	S20	SB 176+85	C	9.0	12.0	108.0	12.0	108.0				22	4		3.3					
903.27	S19	SB 176+76	S20	SB 176+85	L	9.0	12.0	108.0	12.0	108.0				22			3.3					
903.33	S1	SB 180+16	S1	SB 180+25	C	8.0	12.0	96.0	10.7	96.0				22			3.0					
903.34	S2	SB 180+39	S3	SB 180+61	R	22.0	12.0	264.0	29.3	264.0				22		12	8.1					
903.35	S5	SB 181+02	S7	SB 181+34	R	32.0	12.0	384.0	42.7	384.0				22			11.9	24.0	32.0	24.0	32.0	
903.35	S5	SB 181+01	S7	SB 181+34	C	32.0	12.0	384.0	42.7	384.0	9.4	14.4	1.5									
903.37	S12	SB 182+25	S12	SB 182+33	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0	
903.38	S12	SB 182+52	S12	SB 182+33	C	8.0	12.0	96.0	10.7	96.0				22			3.0					
903.38	S13	SB 182+58	S14	SB 182+66	C	8.0	12.0	96.0	10.7	96.0				22			3.0					
903.27	N20	NB 176+13	N20	NB 176+21	R	8.0	12.0	96.0	10.7	96.0				22			3.0					
903.28	N20	NB 176+25	N23	NB 176+68	A	42.0	22.0	924.0	102.7	924.0				42			28.5	44.0		44.0		
903.28	N23	NB 176+73	N23	NB 176+81	C	8.0	12.0	96.0	10.7	96.0				22			3.0					

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.


REVISIONS			
DATE	DESCRIPTION	BY	SYM.



**NEW YORK**  
STATE OF  
OPPORTUNITY.

**Thruway Authority**

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2025 Mark Road Tower  
Bldg. WJ 1020-3722  
518.453.4897 www.qma.com

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION  LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	CONTRACT NUMBER: TAN 24-12  DATE: MAY 2024
TITLE OF DRAWING <b>CONCRETE PAVEMENT REPAIR TABLE</b>	DRAWING NUMBER: CPRT-17

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

FULL DEPTH REPAIR																					
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS	
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)
903.39	S1	SB 182+85	S1	SB 182+93	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.41	S2	SB 183+05	S2	SB 183+13	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.42	S6	SB 183+84	S6	SB 183+82	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.42	S7	SB 184+06	S8	SB 184+19	R	13.0	12.0	156.0	17.3	156.0				22			4.8		13.0		13.0
903.42	S7	SB 184+06	S8	SB 184+32	C	26.0	12.0	312.0	34.7	312.0	7.6	11.7	1.2								
903.42	S7	SB 184+06	S8	SB 184+19	L	13.0	12.0	156.0	17.3	156.0				22			4.8				
903.45	S15	SB 185+65	S15	SB 185+73	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.45	S15	SB 185+65	S15	SB 185+73	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.51	S2	SB 188+81	S2	SB 188+89	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.52	S4	SB 189+22	S4	SB 189+31	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0
903.52	S4	SB 189+22	S4	SB 189+31	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.52	S5	SB 189+43	S5	SB 189+51	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.52	S5	SB 189+43	S6	SB 189+67	C	24.0	12.0	288.0	32.0	288.0	7.0	10.8	1.1								
903.53	S5	SB 189+55	S6	SB 189+67	L	12.0	12.0	144.0	16.0	144.0				22			4.4				
903.40	N2	NB 183+05	N2	NB 183+13	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.41	N3	NB 183+25	N3	NB 183+33	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.45	N14	NB 185+38	N15	NB 185+60	A	22.0	3.0	66.0	7.3	66.0				4			2.0	3.0		3.0	
903.46	N15	NB 185+76	N17	NB 186+08	L	30.0	12.0	360.0	40.0	360.0				11			11.1	12.0		12.0	
903.46	N15	NB 185+76	N17	NB 186+08	C	30.0	12.0	360.0	40.0	360.0	8.8	13.5	1.4								
903.46	N15	NB 185+76	N17	NB 186+08	R	30.0	12.0	360.0	40.0	360.0				11			11.1	12.0	43.0	12.0	43.0
903.51	N1	NB 188+55	N1	NB 188+65	C	8.0	12.0	96.0	10.7	96.0				11			3.0				
903.51	N1	NB 188+55	N1	NB 188+65	R	8.0	12.0	96.0	10.7	96.0				11			3.0		8.0		8.0
903.52	N5	NB 189+41	N5	NB 189+49	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.52	N5	NB 189+41	N5	NB 189+49	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0

SEE PLAN ON DWG. CPR-21

ALTERED ON:	AFFIXED ON:	ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS
	5/10/2024	304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
		404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
		404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF
		407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF
		502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE(PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
		502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA			
		502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR TABLE	DRAWING NUMBER: CPR-18



CHECKED BY: J.KLEINMANN

DRAFTED BY: N.HOY

CHECKED BY: J.KLEINMANN

DESIGNED BY: P.AMICONE

DESIGN SUPERVISOR: D.MARSH

FULL DEPTH REPAIR																					
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS	
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)
903.54	S8	SB 190+16	S9	SB 190+25	C	9.0	12.0	108.0	12.0	108.0				22			3.3				
903.55	S11	SB 190+76	S12	SB 191+00	C	24.0	12.0	288.0	32.0	288.0	7.0	10.8	1.1								
903.55	S11	SB 190+76	S12	SB 190+91	L	15.0	12.0	180.0	20.0	180.0				22			5.6				
903.56	S15	SB 191+43	S15	SB 191+51	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.60	S26	SB 193+64	S26	SB 193+72	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0
903.60	S26	SB 193+64	S26	SB 193+72	C	8.0	12.0	96.0	10.7	96.0				22	3		3.0				
903.60	S26	SB 193+64	S26	SB 193+72	L	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.61	S27	SB 193+83	S28	SB 194+07	R	24.0	12.0	288.0	32.0	288.0				22		12.0	8.9				
903.61	S27	SB 193+83	S27	SB 193+91	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.61	S28	SB 194+14	S1	SB 194+28	C	14.0	12.0	168.0	18.7	168.0	4.1	6.3	0.7								
903.61	S28	SB 194+14	S1	SB 194+28	L	14.0	12.0	168.0	18.7	168.0				22			5.2				
903.53	N7	NB 189+81	N7	NB 189+89	L	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.53	N7	NB 189+81	N7	NB 189+89	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.35	N8	NB 180+04	N9	NB 190+18	L	14.0	12.0	168.0	18.7	168.0				22			5.2				
903.54	N8	NB 190+04	N9	NB 190+18	C	14.0	12.0	168.0	18.7	168.0	4.1	6.3	0.7								
903.54	N8	NB 190+04	N9	NB 190+18	R	14.0	12.0	168.0	18.7	168.0				22			5.2		14.0		14.0
903.54	N9	NB 190+31	N10	NB 190+39	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.55	N11	NB 190+61	N11	NB 190+69	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.55	N12	NB 190+94	N14	NB 191+16	C	22.0	12.0	264.0	29.3	264.0	6.4	9.9	1.0								
903.57	N17	NB 191+81	N17	NB 191+89	L	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.57	N17	NB 191+81	N17	NB 191+89	C	8.0	12.0	96.0	10.7	96.0				22	3		3.0				
903.57	N17	NB 191+81	N17	NB 191+89	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0		8.0
903.59	N23	NB 193+01	N23	NB 193+09	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.60	N1	NB 193+41	N1	NB 193+49	C	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.61	N3	NB 193+81	N3	NB 193+89	R	8.0	12.0	96.0	10.7	96.0				22			3.0				
903.62	N7	NB 194+57	N7	NB 194+64	C	14.5	12.0	174.0	19.3	174.0	4.2	6.5	0.7								
903.62	N7	NB 194+57	N7	NB 194+64	R	14.5	12.0	174.0	19.3	174.0				22		12	5.4		23.0		23.0

SEE PLAN ON DWG. CPR-22

ALTERED ON:	AFFIXED ON: 5/10/2024	ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS		
SIGNATURE:	SIGNATURE:	304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF		
STAMP:	STAMP:	404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY		
		404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF		
		407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF		
		502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF		
		502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF		
		502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA					
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CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

FULL DEPTH REPAIR																							
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS										REMARKS				
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)		ITEM 502.38010018 (LF)	ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)	
903.68	S4	SB 198+63	S4	SB 198+71	L	8.0	12.0	96.0	10.7	96.0				22			3.0						
903.69	S5	SB 198+85	S5	SB 198+93	L	8.0	12.0	96.0	10.7	96.0				22			3.0						
903.70	S7	SB 199+25	S7	SB 199+33	R	8.0	12.0	96.0	10.7	96.0				22			3.0						
903.70	S1	SB 199+45	S1	SB 199+53	C	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0			8.0	
903.70	S1	SB 199+45	S1	SB 199+53	L	8.0	12.0	96.0	10.7	96.0				22			3.0						
903.70	S2	SB 199+65	S2	SB 199+73	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0			8.0	
903.70	S2	SB 199+65	S2	SB 199+73	C	8.0	12.0	96.0	10.7	96.0				22			3.0						
903.71	S3	SB 199+87	S4	SB 200+00	R	13.0	12.0	156.0	17.3	156.0				22			4.8						
903.72	S7	SB 200+65	S7	SB 200+73	R	8.0	12.0	96.0	10.7	96.0				22			3.0						
903.73	S9	SB 201+05	S9	SB 201+13	R	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0			8.0	
903.73	S9	SB 201+05	S9	SB 201+13	C	8.0	12.0	96.0	10.7	96.0				22			3.0						
903.73	S10	SB 201+25	S10	SB 201+33	R	8.0	12.0	96.0	10.7	96.0				22			3.0						
903.73	S10	SB 201+25	S10	SB 201+33	L	8.0	12.0	96.0	10.7	96.0				22			3.0						
903.74	S11	20145.46	S12	SB 201+73	R	28.0	12.0	336.0	37.3	336.0				22			10.4	12.0	28.0	12.0		28.0	
903.74	S11	SB 201+45	S12	SB 201+73	C	28.0	12.0	336.0	37.3	336.0	8.2	12.6	1.3										
903.74	S13	SB 201+86	S14	SB 201+94	C	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0			8.0	
903.74	S13	SB 201+86	S14	SB 201+94	L	8.0	12.0	96.0	10.7	96.0				22			3.0						
903.68	N1	NB 198+69	N2	NB 198+78	R	8.5	12.0	102.0	11.3	102.0				11			3.1		10.0			10.0	
903.69	N3	NB 199+04	N5	NB 199+18	A	14.0	3.0	42.0	4.7	42.0				2			1.3						
903.73	N14	NB 201+23	N14	NB 201+31	R	8.0	12.0	96.0	10.7	96.0				22			3.0						
903.74	N17	NB 201+83	N17	NB 201+97	C	14.0	12.0	168.0	18.7	168.0	4.1	6.3	0.7										
903.74	N17	NB 201+83	N17	NB 201+91	R	8.0	12.0	96.0	10.7	96.0				22			3.0						
903.74	N17	NB 201+83	N17	NB 201+91	A	8.0	12.0	96.0	10.7	96.0				22			3.0		8.0			8.0	

SEE PLAN ON DWG. CPR-23

ALTERED ON:	AFFIXED ON: 5/10/2024	ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS								
SIGNATURE: STAMP:		304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF								
		404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY								
		404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF								
		407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF								
		502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF								
		502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA											
		502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF								
<p>IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.</p>		<p>REVISIONS</p> <table border="1"> <tr> <th>DATE</th> <th>DESCRIPTION</th> <th>BY</th> <th>SYM.</th> </tr> <tr> <td> </td> <td> </td> <td> </td> <td> </td> </tr> </table>		DATE	DESCRIPTION	BY	SYM.							<p>TITLE OF PROJECT I-190 HIGHWAY REHABILITATION</p> <p>LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2</p>	<p>CONTRACT NUMBER: TAN 24-12</p> <p>DATE: MAY 2024</p> <p>DRAWING NUMBER: CPRT-20</p>
DATE	DESCRIPTION	BY	SYM.												

CHECKED BY: J.KLEINMANN

DRAFTED BY: N.HOY


CHECKED BY: J.KLEINMANN

DESIGNED BY: P.AMICONE

DESIGN SUPERVISOR: D.MARSH

FULL DEPTH REPAIR																						
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS											REMARKS		
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)		ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)
903.80	S2	SB 204+96	S3	SB 205+13	C	16.0	12.0	192.0	21.3	192.0	4.7	7.2	0.7									
903.81	S3	SB 205+21	S3	SB 205+27	L	8.0	12.0	96.0	10.7	96.0				22				3.0				
903.81	S3	SB 205+24	S4	SB 205+32	R	8.0	12.0	96.0	10.7	96.0				22				3.0				
903.82	S5	SB 205+63	S6	SB 205+74	L	11.0	12.0	132.0	14.7	132.0				22				4.1				
903.82	S6	SB 205+74	S11	SB 206+76	A	104.0	9.5	988.0	109.8	988.0				17				30.5	57.0	56.0	57.0	56.0
903.82	S7	SB 205+89	S7	SB 205+97	R	8.0	12.0	96.0	10.7	96.0				22				3.0				
903.82	S8	SB 206+08	S8	SB 206+16	L	8.0	12.0	96.0	10.7	96.0				22				3.0				
903.83	S9	SB 206+29	S11	SB 206+76	R	48.0	12.0	576.0	64.0	576.0				22	14			17.8	36.0		36.0	
903.83	S9	SB 206+29	S11	SB 206+63	C	34.0	12.0	408.0	45.3	408.0	9.9	15.3	1.6									
903.84	S12	SB 206+91	S13	SB 207+02	C	11.0	12.0	132.0	14.7	132.0				22				4.1				
903.85	S14	SB 207+35	S15	SB 207+45	A	10.0	12.0	120.0	13.3	120.0				22				3.7				
903.86	S17	SB 207+93	S18	SB 208+17	L	23.0	12.0	276.0	30.7	276.0				22		12		8.5				
903.86	S18	SB 208+12	S20	SB 208+24	A	40.0	12.0	480.0	53.3	480.0				22				14.8	24.0	12.0	24.0	12.0
903.86	S18	SB 208+12	S19	SB 208+24	R	12.0	12.0	144.0	16.0	144.0				22				4.4				
903.86	S18	SB 208+19	S19	SB 208+27	C	8.0	12.0	96.0	10.7	96.0				22				3.0				
903.87	S21	SB 208+69	S22	SB 208+92	A	23.0	12.0	276.0	30.7	276.0				22	5			8.5	12.0	15.0	12.0	15.0
903.87	S21	SB 208+69	S22	SB 208+84	R	15.0	12.0	180.0	20.0	180.0				22				5.6				
903.90	S23	SB 209+09	S3	SB 209+67	A	60.0	12.0	720.0	80.0	720.0				22		12		22.2	36.0	13.0	36.0	13.0
903.90	S23	SB 209+09	S1	SB 209+09	R	13.0	12.0	156.0	17.3	156.0				22				4.8				
903.90	S1	SB 209+36	S2	SB 209+44	R	8.0	12.0	96.0	10.7	96.0				22				3.0				
903.91	S2	SB 209+55	S3	SB 209+63	R	8.0	12.0	96.0	10.7	96.0				22				3.0				
903.92	S6	SB 210+24	S6	SB 210+31	A	8.0	12.0	96.0	10.7	96.0				22				3.0				
903.92	S6	SB 210+27	S7	SB 210+27	L	12.0	12.0	144.0	16.0	144.0				22				4.4				
903.92	S6	SB 210+37	S7	SB 210+47	R	10.0	12.0	120.0	13.3	120.0				22				3.7				
903.89	N24	NB 209+45	N25	NB 209+54	R	9.0	12.0	108.0	12.0	108.0				22	4			3.3				
903.89	N26	NB 209+85	N27	NB 209+93	C	8.0	12.0	96.0	10.7	96.0				22				3.0				
903.89	N26	NB 209+85	N27	NB 209+93	R	8.0	12.0	96.0	10.7	96.0				22				3.0	8.0		8.0	
903.90	N27	NB 210+05	N1	NB 210+13	C	8.0	12.0	96.0	10.7	96.0				22				3.0				

SEE PLAN ON DWG. CPR-24

ALTERED ON:	AFFIXED ON: 5/10/2024	ITEM	DESCRIPTION	UNITS	ITEM	DESCRIPTION	UNITS
SIGNATURE:	SIGNATURE: 	304.00010018	FINE GRADING OF EXISTING SUBBASE	SF	502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
STAMP:		404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON	502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
		404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON	502.37010018	TRANSVERSE JOINTS	LF
		407.01040009	NON-TRACKING TACK COAT	GAL	502.38010018	LONGITUDINAL JOINTS	LF
		502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY	502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE(PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
		502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA			
		502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA	502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR TABLE	DRAWING NUMBER: CPR-21

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. AMICONE

DESIGN SUPERVISOR: D. MARSH

ITEM	DESCRIPTION	UNITS
304.00010018	FINE GRADING OF EXISTING SUBBASE	SF
404.1959	19 F9 BINDER COURSE WMA, 50 SERIES COMPACTION	TON
404.3779	37.5 F9 BASE COURSE WMA, 70 SERIES COMPACTION	TON
407.01040009	NON-TRACKING TACK COAT	GAL
502.31010018	FULL-DEPTH PORTLAND CEMENT CONCRETE (PCC) LIFT-OUT	SY
502.32050025	DRILL AND ANCHOR DOWELS FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA
502.33010018	DRILL AND ANCHOR LONGITUDINAL JOINT TIES FOR FULL-DEPTH PORTLAND CEMENT CONCRETE PAVEMENT REPAIRS	EA
502.34010018	TRANSVERSE HINGE JOINTS IN PORTLAND CEMENT CONCRETE (PCC) PAVEMENT	LF
502.36010018	PORTLAND CEMENT CONCRETE (PCC) PLACEMENT FOR FULL-DEPTH PAVEMENT REPAIRS	CY
502.37010018	TRANSVERSE JOINTS	LF
502.38010018	LONGITUDINAL JOINTS	LF
502.90010018	CLEAN AND FILL CRACKS AND JOINTS IN PORTLAND CEMENT CONCRETE(PCC) PAVEMENT, ASTM D 6690 TYPE IV	LF
502.9310	SEALING LONGITUDINAL JOINTS - HIGHWAY JOINT SEALER	LF

SEE PLAN ON DWG. CPR-25

FULL DEPTH REPAIR																						REMARKS
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS													
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)	ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)	
903.93	S9	SB 210+85	S10	SB 211+09	R	24.0	12.0	288.0	32.0	288.0				22		12	8.9					
903.93	S9	SB 210+85	S10	SB 211+11	C	26.0	12.0	312.0	34.7	312.0	7.6	11.7	1.2									
903.94	S12	SB 211+43	S15	SB 212+16	A	74.0	12.0	888.0	98.7	888.0				22				27.4	60.0	33.0	60.0	33.0
903.94	S12	SB 211+43	S14	SB 211+75	R	33.0	12.0	396.0	44.0	396.0				22				12.2	24.0			
903.95	S14	SB 211+87	S15	SB 211+95	C	8.0	12.0	96.0	10.7	96.0				22				3.0				
903.95	S15	SB 212+12	S16	SB 212+25	R	13.0	12.0	156.0	17.3	156.0				22				4.8				
903.96	S17	SB 212+41	S18	SB 212+55	R	14.0	12.0	168.0	18.7	168.0				22				5.2				
903.97	S19	SB 212+86	S20	SB 212+97	R	11.0	12.0	132.0	14.7	132.0				22				4.1				
903.97	S19	SB 212+89	S20	SB 212+97	C	8.0	12.0	96.0	10.7	96.0				22				3.0				
903.98	S22	SB 213+46	S23	SB 213+59	R	13.0	12.0	156.0	17.3	156.0				22				4.8				
903.98	S22	SB 213+51	S23	SB 213+59	C	8.0	12.0	96.0	10.7	96.0				22				3.0				
903.99	S24	SB 213+93	S25	SB 214+01	L	8.0	12.0	96.0	10.7	96.0				22				3.0				
904.01	S2	SB 214+71	S3	SB 214+79	R	8.0	12.0	96.0	10.7	96.0				22				3.0				
904.01	S3	SB 214+90	S4	SB 215+00	R	10.0	12.0	120.0	13.3	120.0				22				3.7				
904.02	S6	SB 215+50	S7	SB 215+62	C	12.0	12.0	144.0	16.0	144.0	3.5	5.4	0.6									
904.02	S6	SB 215+50	S7	SB 215+62	R	12.0	12.0	144.0	16.0	144.0				22				4.4		12.0		12.0
904.05	S13	SB 216+89	S14	SB 217+04	R	15.0	12.0	180.0	20.0	180.0				22				5.6		15.0		15.0
904.05	S13	SB 216+89	S14	SB 217+04	C	15.0	12.0	180.0	20.0	180.0	4.4	6.7	0.7									
904.06	S15	SB 217+29	S16	SB 217+37	R	8.0	12.0	96.0	10.7	96.0				22				3.0				
904.06	S16	SB 217+55	S18	SB 217+75	C	20.0	12.0	240.0	26.7	240.0	5.8	9.0	0.9									
903.93	N9	NB 211+86	N12	NB 212+31	C	45.0	12.0	540.0	60.0	540.0	13.2	20.2	2.1									
903.94	N10	NB 212+01	N11	NB 212+16	R	15.0	12.0	180.0	20.0	180.0				22				5.6		15.0		15.0
903.95	N13	NB 212+65	N14	NB 212+72	L	8.0	12.0	96.0	10.7	96.0				22				3.0				
903.95	N13	NB 212+65	N14	NB 212+72	C	8.0	12.0	96.0	10.7	96.0				22				3.0		8.0		8.0
903.97	N19	NB 213+84	N20	NB 213+92	C	9.0	12.0	108.0	12.0	108.0				22				3.3				
903.97	N20	NB 214+05	N21	NB 214+14	R	9.0	12.0	108.0	12.0	108.0				22				3.3				
903.98	N22	NB 214+44	N23	NB 214+52	C	8.0	12.0	96.0	10.7	96.0				22				3.0				
904.02	N4	NB 215+44	N5	NB 215+52	C	8.0	12.0	96.0	10.7	96.0				22				3.0				
904.02	N6	NB 215+84	N7	NB 215+92	R	8.0	12.0	96.0	10.7	96.0				22				3.0				
904.03	N7	NB 216+02	N8	NB 216+21	C	19.0	12.0	228.0	25.3	228.0	5.6	8.5	0.9									
904.03	N8	NB 216+21	N9	NB 216+29	L	8.0	12.0	96.0	10.7	96.0				22				3.0				
904.03	N9	NB 216+43	N10	NB 216+53	R	10.0	12.0	120.0	13.3	120.0				22				3.7				
904.04	N10	NB 216+63	N11	NB 216+71	L	8.0	12.0	96.0	10.7	96.0				22				3.0				
904.04	N10	NB 216+65	N13	NB 216+89	A	25.0	12.0	300.0	33.3	300.0				22				9.3	12.0		12.0	
904.04	N13	NB 216+97	N14	NB 217+08	R	12.0	12.0	144.0	16.0	144.0				22				4.4				
904.05	N14	NB 217+25	N15	NB 217+32	R	8.0	12.0	96.0	10.7	96.0				22				3.0				
904.06	N16	NB 217+61	N17	NB 217+70	L	10.0	12.0	120.0	13.3	120.0				22				3.7				
904.06	N16	NB 217+61	N17	NB 217+70	R	10.0	12.0	120.0	13.3	120.0				22				3.7				

SEE PLAN ON DWG. CPR-26

FULL DEPTH REPAIR																						REMARKS
LOCATION						REPAIR DIMENSIONS			REPAIR ITEMS													
MILE MARKER	START SLAB	START STATION	END SLAB	END STATION	LANE	LENGTH (FT)	WIDTH (FT)	AREA (SF)	ITEM 502.31010018 (SY)	ITEM 304.00010018 (SF)	ITEM 404.1959 (TON)	ITEM 404.3779 (TON)	ITEM 407.01040009 (GAL)	ITEM 502.32050025 (EA)	ITEM 502.33010018 (EA)	ITEM 502.34010018 (LF)	ITEM 502.36010018 (CY)	ITEM 502.37010018 (LF)	ITEM 502.38010018 (LF)	ITEM 502.90010018 (LF)	ITEM 502.9310 (LF)	
904.07	S18	SB 217+84	S19	SB 217+92	R	8.0	12.0	96.0	10.7	96.0				22				3.0				

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

STATE OF NEW YORK  
JOSEPH W. KLEINMANN  
LICENSED PROFESSIONAL ENGINEER  
092577  
EXCELSIOR

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <small>©2019 New York State Thruway Authority</small>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONCRETE PAVEMENT REPAIR TABLE	DRAWING NUMBER: CPRT-22	



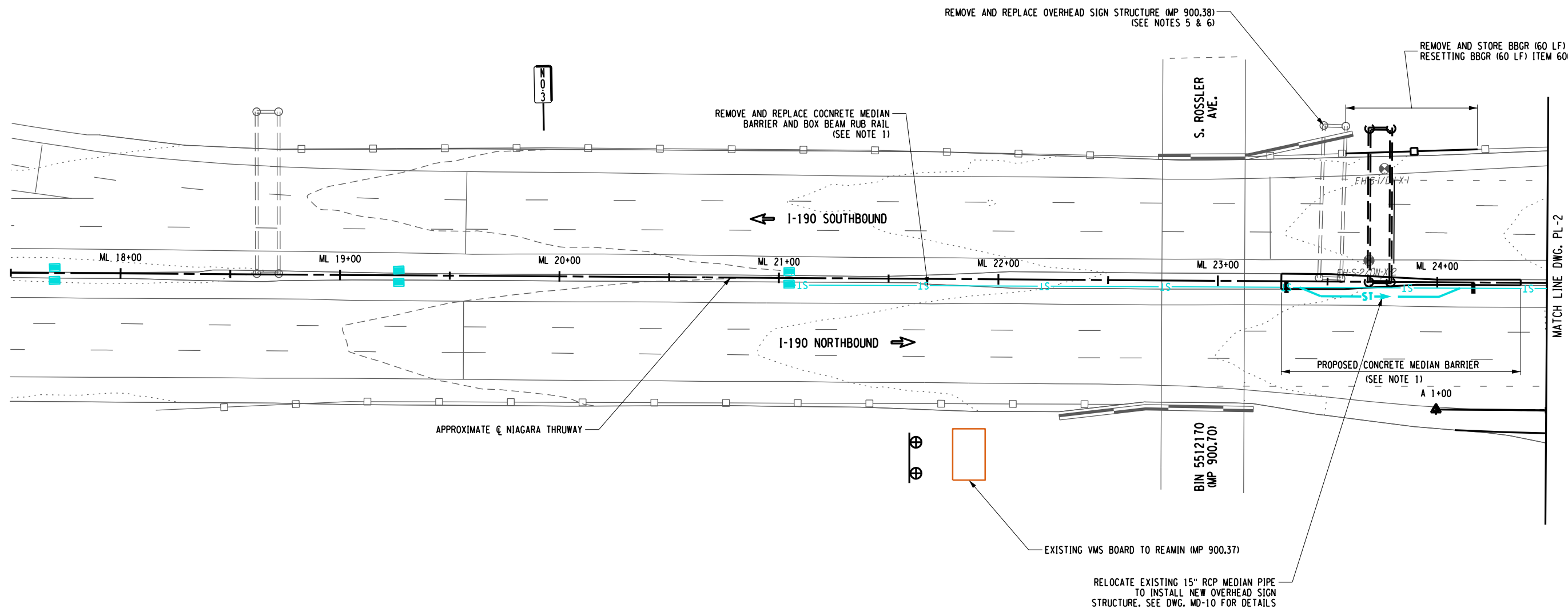
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

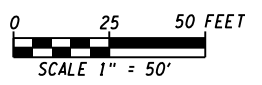
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



MATCH LINE DWG. PL-2

- GENERAL PLAN NOTES**
1. SEE DWGS. MT-15 TO MT-17 FOR GUIDERAIL REMOVAL AND INSTALLATION TABLES
  2. SEE SSP PLANS FOR LANE WIDTHS, PAVEMENT MARKINGS AND SIGN LOCATIONS.
  3. SEE DWG. MT-5 TO MT-14 FOR DRAINAGE TABLES.
  4. SEE DWG. MD-9 FOR DRAINAGE DETAILS.
- PL-1 NOTES**
5. SEE DWGS. SSP-1 FOR SIGN PANEL STRUCTURE LOCATIONS, SDS-4 FOR SIGN PANEL LAYOUT, AND OSP-01 TO OSP-02 FOR SIGN STRUCTURE DETAILS. SEE DWGS. OSP-01 AND OSD-14 FOR CONCRETE MEDIAN BARRIER LAYOUT AND DETAILS.
  6. SEE MT-18 FOR OVERHEAD SIGN PANEL STRUCTURE REMOVAL TABLE

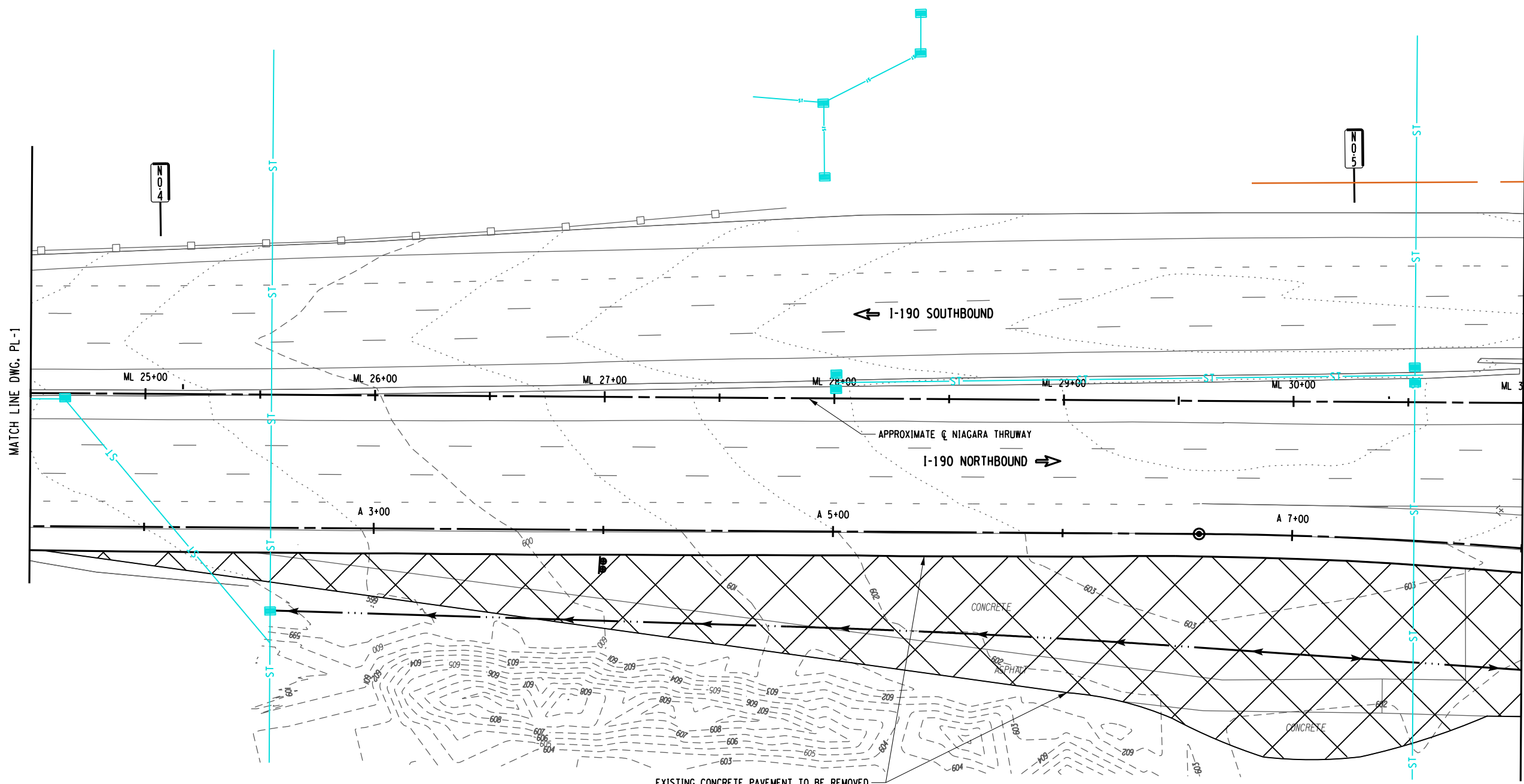


ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-1



CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ALTERED ON:

AFFIXED ON: 5/10/2024

SIGNATURE:  
STAMP:

SIGNATURE:  
STAMP:



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

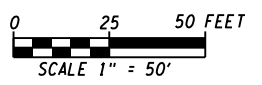
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
ROADWAY PLANS

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
PL-2



**LEGEND**

- PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION
- PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION
- PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY
- PROPOSED RAMP MILL AND OVERLAY
- PROPOSED RELIEF JOINT RECONSTRUCTION
- DS  
X-X PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

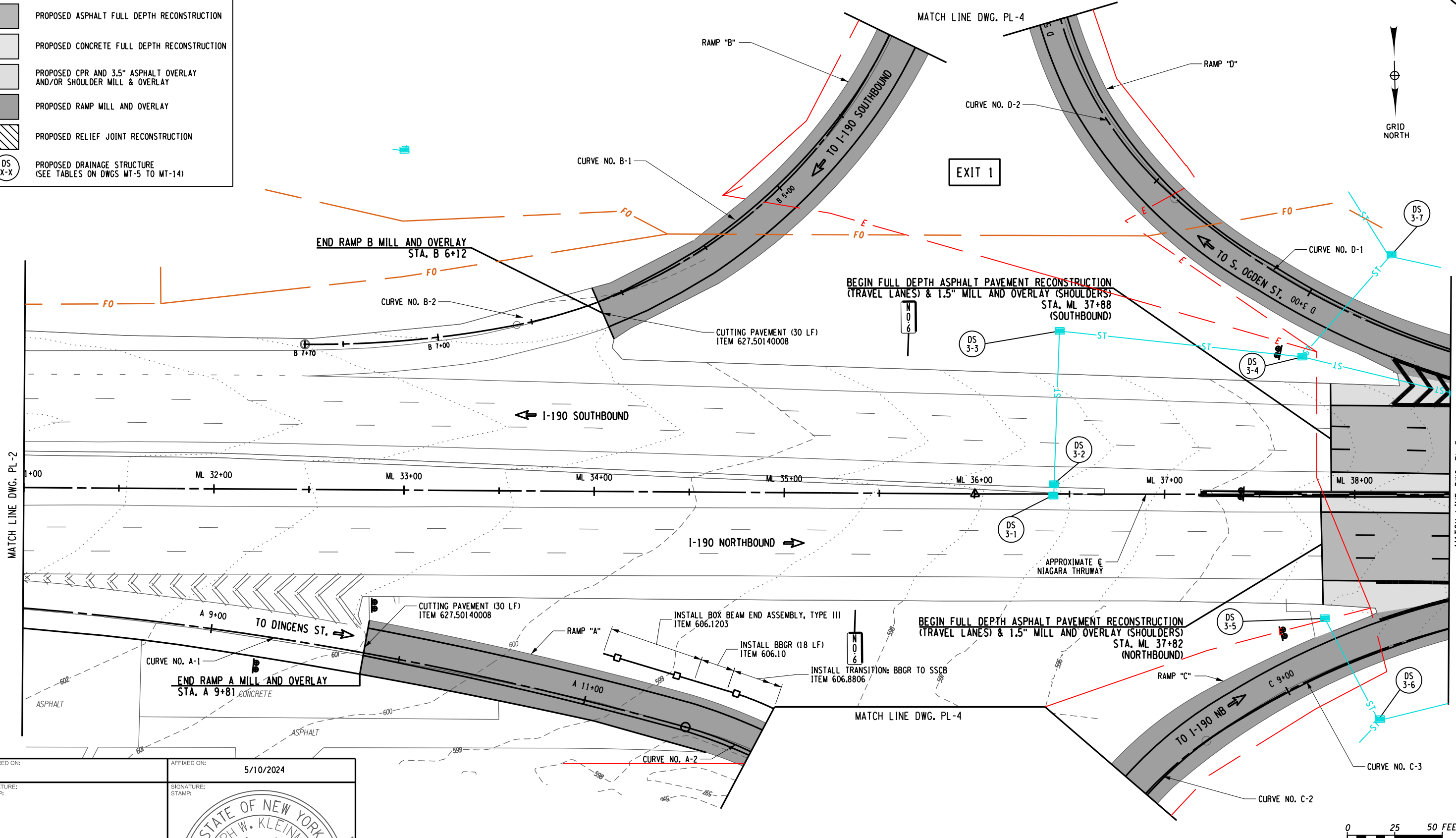
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



MATCH LINE DWG. PL-2

MATCH LINE DWG. PL-5

MATCH LINE DWG. PL-4

MATCH LINE DWG. PL-4

ALTERED ON: SIGNATURE: STAMP:	AFFIXED ON: 5/10/2024 SIGNATURE: STAMP:

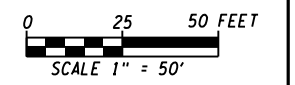
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority**

TITLE OF PROJECT	I-190 HIGHWAY REHABILITATION
LOCATION OF PROJECT	I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2
TITLE OF DRAWING	ROADWAY PLANS

CONTRACT NUMBER:	TAN 24-12
DATE:	MAY 2024
DRAWING NUMBER:	PL-3



CHECKED BY: J. KLEINMANN

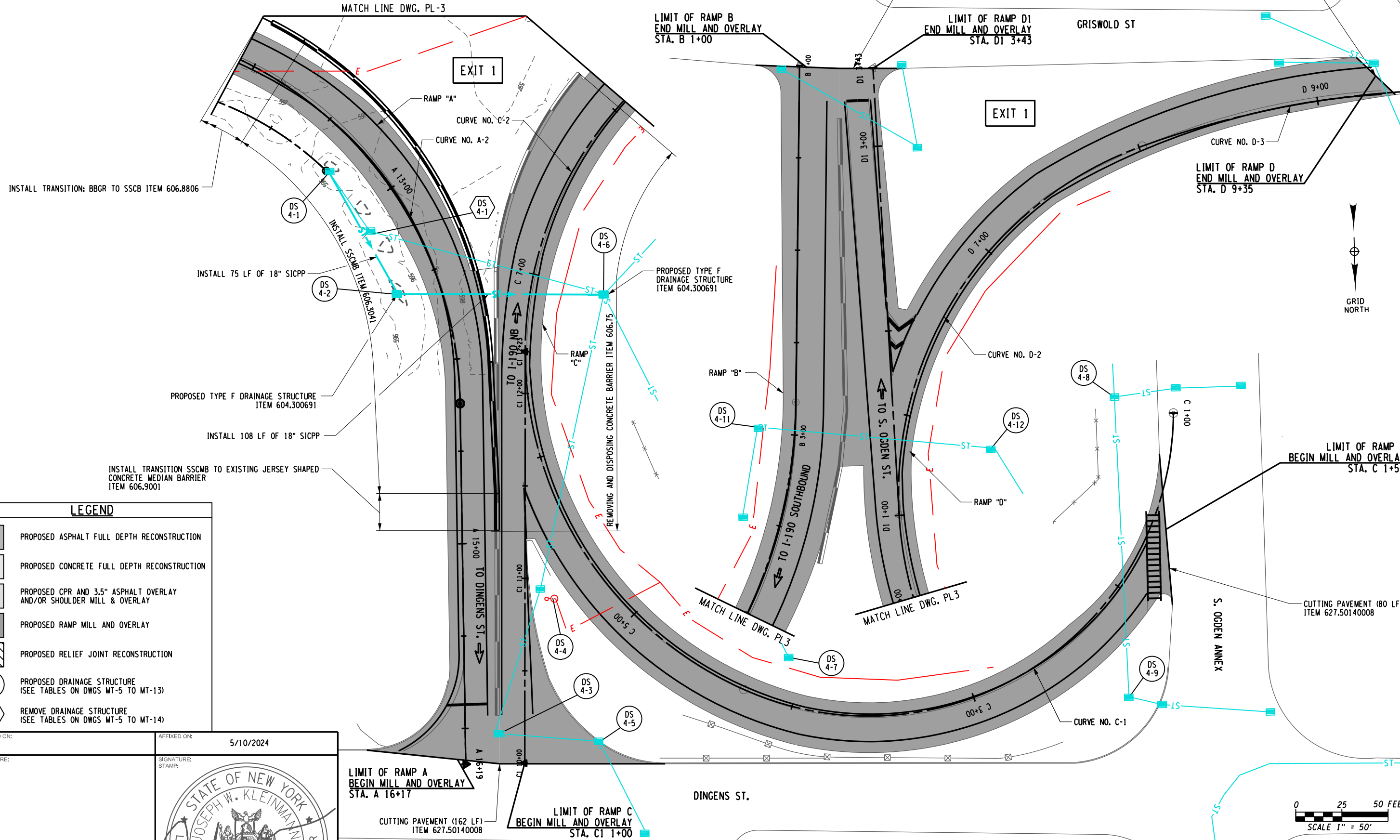
DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

128  
274



**LEGEND**

- PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION
- PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION
- PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY
- PROPOSED RAMP MILL AND OVERLAY
- PROPOSED RELIEF JOINT RECONSTRUCTION
- PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-13)
- REMOVE DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

ALTERED ON:	AFFIXED ON: 5/10/2024
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STATE OF NEW YORK  
JOSEPH W. KLEINMANN  
LICENSED PROFESSIONAL ENGINEER  
EXCELLENCE  
092577

**LIMIT OF RAMP A BEGIN MILL AND OVERLAY STA. A 16+17**

**LIMIT OF RAMP C BEGIN MILL AND OVERLAY STA. C1 1+00**

**LIMIT OF RAMP B END MILL AND OVERLAY STA. B 1+00**

**LIMIT OF RAMP D1 END MILL AND OVERLAY STA. D1 3+43**

**LIMIT OF RAMP D END MILL AND OVERLAY STA. D 9+35**

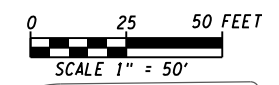
**LIMIT OF RAMP C BEGIN MILL AND OVERLAY STA. C 1+50**

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY  
Thruway Authority

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-4







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CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

END FULL DEPTH ASPHALT PAVEMENT RECONSTRUCTION (TRAVEL LANES) & 1.5" MILL AND OVERLAY (SHOULDER)  
BEGIN RAMP D MILL AND OVERLAY  
STA. D 1+86

END FULL DEPTH ASPHALT PAVEMENT RECONSTRUCTION (TRAVEL LANES)  
BEGIN PRE-CAST CONCRETE SLABS (TRAVEL LANES)  
STA. ML 39+75

END FULL DEPTH ASPHALT PAVEMENT RECONSTRUCTION (TRAVEL LANES) & 1.5" MILL AND OVERLAY (SHOULDER)  
BEGIN 3.5" ASPHALT OVERLAY (TRAVEL LANES & SHOULDER)  
STA. ML 43+60

REMOVE AND DISPOSING TURNED-DOWN TERMINAL, BBGR ITEM 606.7920

REMOVE AND DISPOSE BBGR (238 LF) ITEM 606.73

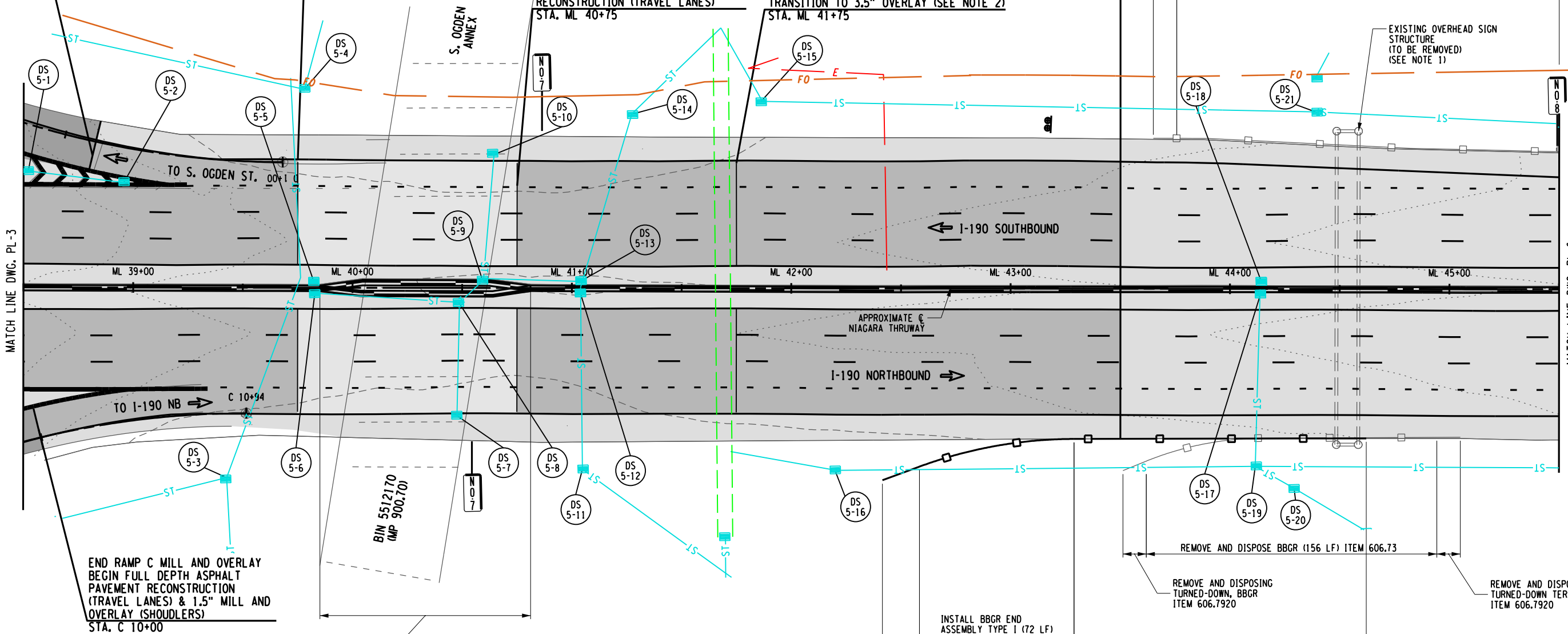
EXISTING OVERHEAD SIGN STRUCTURE (TO BE REMOVED) (SEE NOTE 1)

END PRE-CAST CONCRETE SLABS  
BEGIN FULL DEPTH ASPHALT PAVEMENT RECONSTRUCTION (TRAVEL LANES)  
STA. ML 40+75

BEGIN FULL DEPTH ASPHALT PAVEMENT TRANSITION TO 3.5" OVERLAY (SEE NOTE 2)  
STA. ML 41+75

MATCH LINE DWG. PL-3

MATCH LINE DWG. PL-6



END RAMP C MILL AND OVERLAY  
BEGIN FULL DEPTH ASPHALT PAVEMENT RECONSTRUCTION (TRAVEL LANES) & 1.5" MILL AND OVERLAY (SHOULDER)  
STA. C 10+00

REMOVE EXISTING BOX BEAM GUIDE RAIL ATTACHED TO THE MEDIAN CONCRETE BARRIER PIER PROTECTION. REPLACE WITH PROPOSED BOX BEAM GUIDE RAIL ATTACHED TO CONCRETE (198 LF BOTH DIRECTIONS) ITEM 606.1001--25

INSTALL BBGR END ASSEMBLY TYPE I (72 LF) ITEM 606.120102

REMOVE AND DISPOSING TURNED-DOWN, BBGR ITEM 606.7920

INSTALL BBGR (126 LF) ITEM 606.10 & BOX BEAM END PIECE ITEM 606.120101

REMOVE AND DISPOSING TURNED-DOWN TERMINAL, BBGR ITEM 606.7920

APPROXIMATE C NIAGARA THRUWAY

I-190 NORTHBOUND

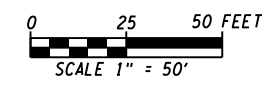
I-190 SOUTHBOUND

BIN 5512170 (MP 900.70)

**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

- NOTES:**
- SEE DWG. MT-18 FOR OVERHEAD SIGN STRUCTURE REMOVAL TABLE.
  - SEE DWG. MD-7 FOR FULL DEPTH PAVEMENT TRANSITION DETAILS AT OVERPASS STRUCTURES.



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2  
TITLE OF DRAWING  
ROADWAY PLANS

CONTRACT NUMBER:  
TAN 24-12  
DATE:  
MAY 2024  
DRAWING NUMBER:  
PL-5

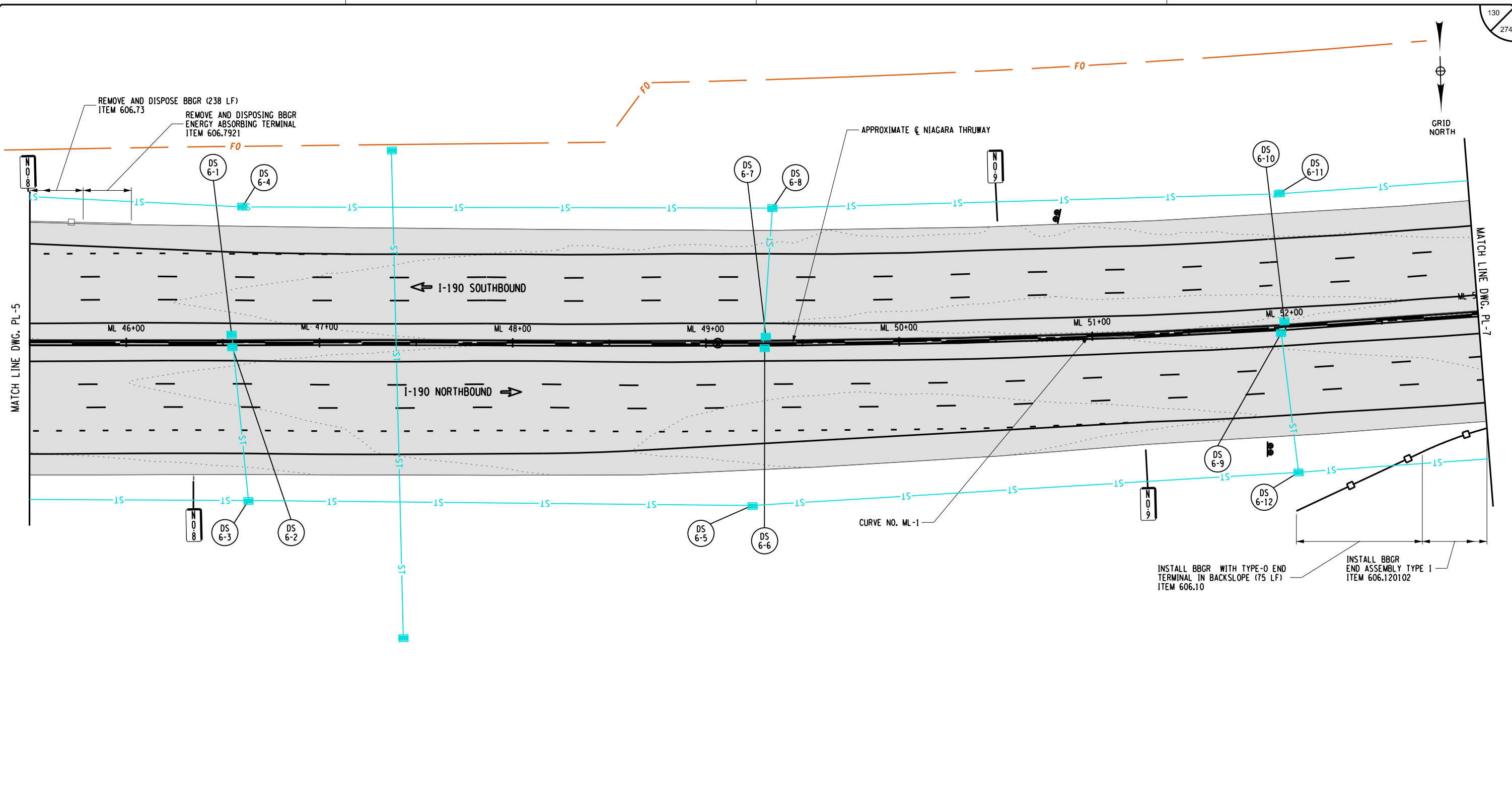
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



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STAMP:

AFFIXED ON:  
5/10/2024

SIGNATURE:  
STAMP:

**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

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REVISIONS

DATE	DESCRIPTION	BY	SYMBOL



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

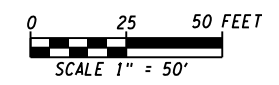
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
ROADWAY PLANS

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
PL-6



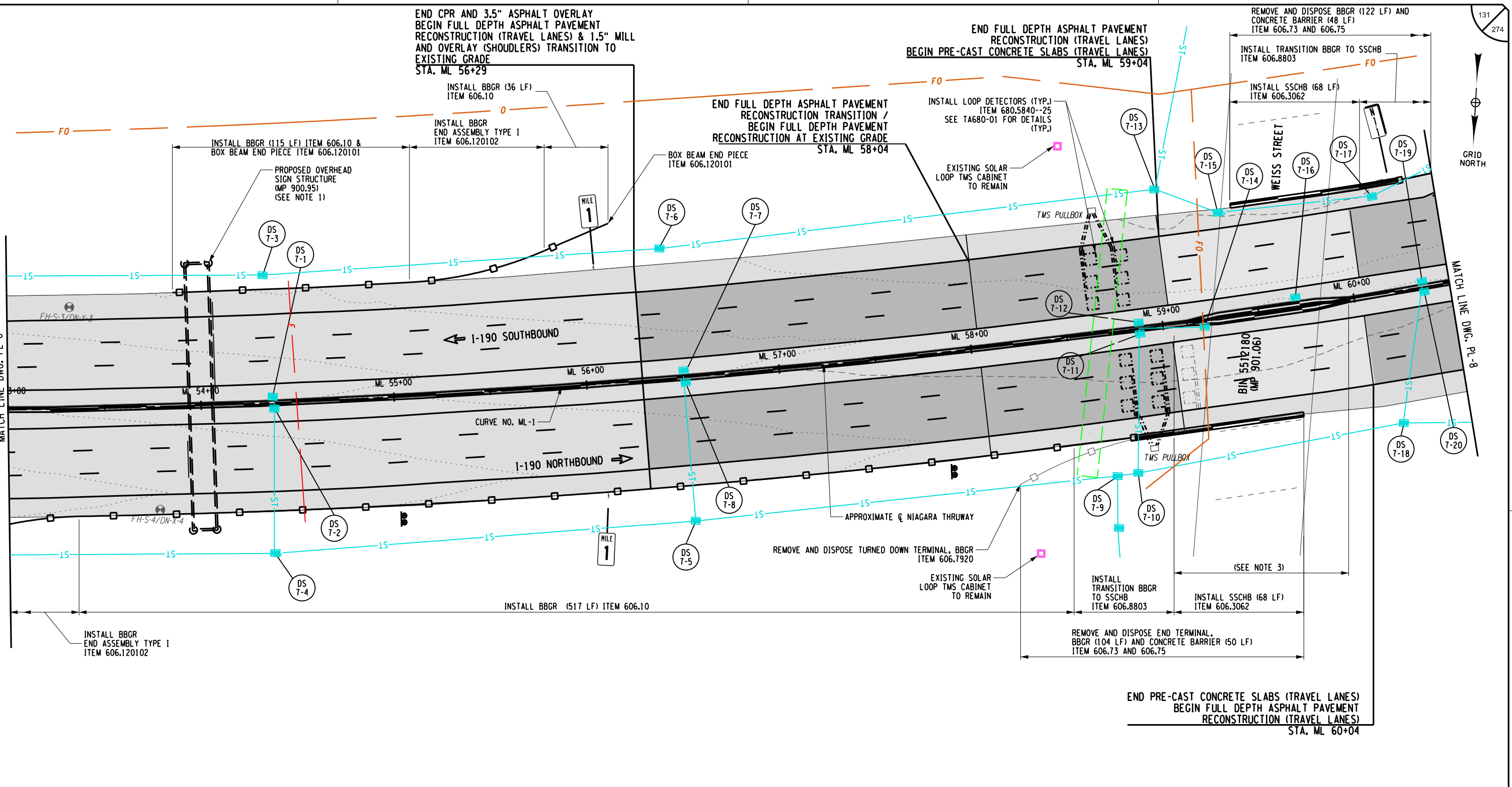
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



131  
274

GRID NORTH

MATCH LINE DWG. PL-8

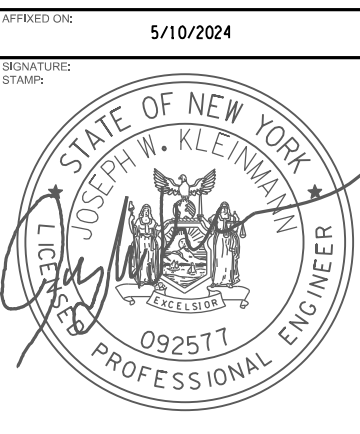
MATCH LINE DWG. PL-6

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AFFIXED ON: 5/10/2024

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**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

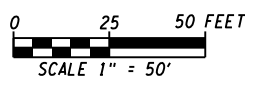
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- NOTES:**
- SEE DWGS SSP-3 FOR STRUCTURE LOCATION, SDS-4 FOR SIGN PANEL LAYOUT, AND OSP-3 TO OSP-4 FOR SIGN STRUCTURE DETAILS.
  - SEE DWG. MT-18 FOR OVERHEAD SIGN STRUCTURE REMOVAL TABLE.
  - REMOVE EXISTING BOX BEAM GUIDE RAIL ATTACHED TO THE MEDIAN CONCRETE BARRIER PIER PROTECTION. REPLACE WITH PROPOSED BOX BEAM GUIDE RAIL ATTACHED TO CONCRETE (98 LF) ITEM 606.1001--25
  - SEE DWG. MD-7 FOR FULL DEPTH TRANSITION DETAILS.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

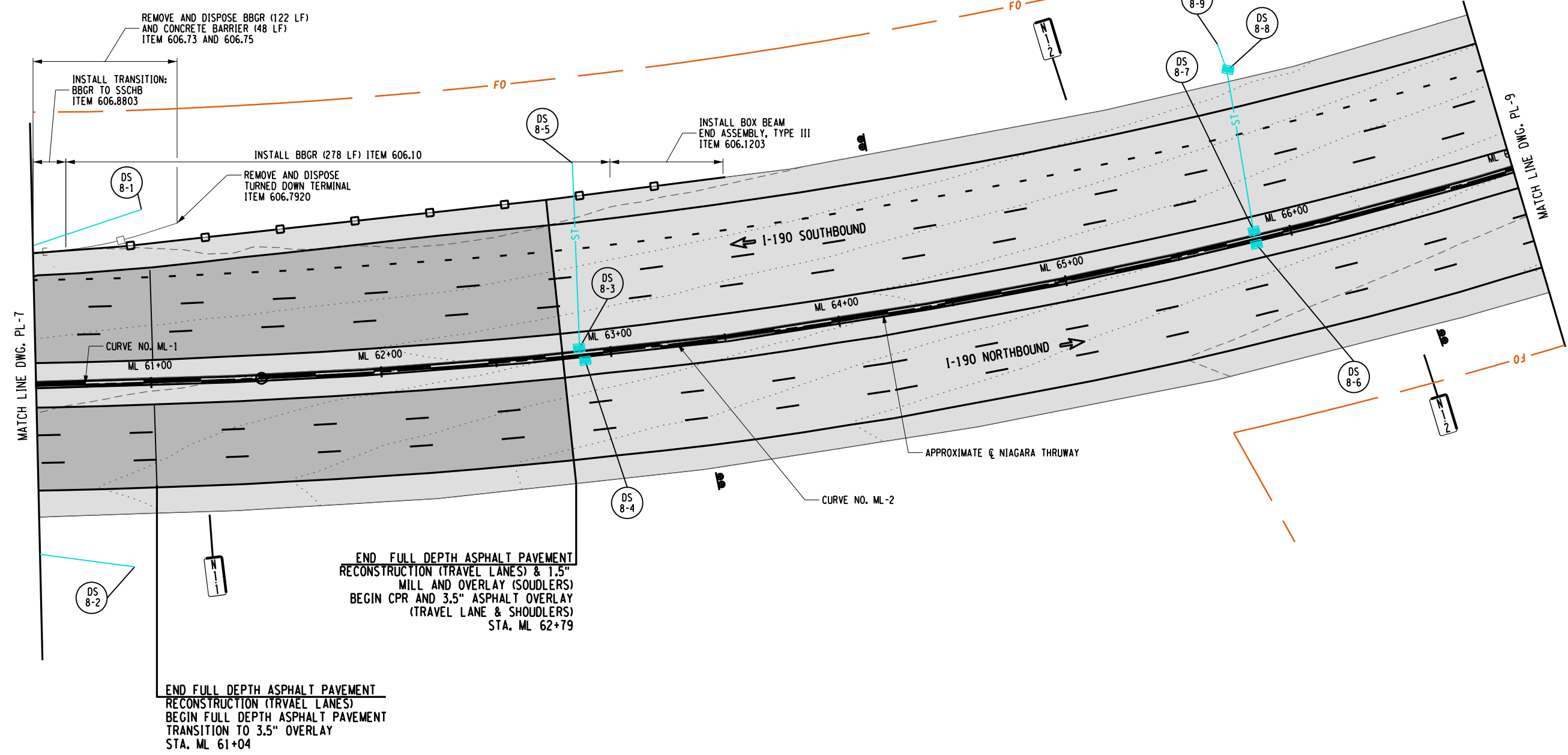


TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-7



J. KLEINMANN  
 N. HOY  
 J. KLEINMANN  
 P. THOMPSON  
 D. MARSH

CHECKED BY:  
 DRAFTED BY:  
 CHECKED BY:  
 DESIGNED BY:



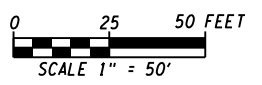
**END FULL DEPTH ASPHALT PAVEMENT  
 RECONSTRUCTION (TRAVEL LANES) & 1.5"  
 MILL AND OVERLAY (SHOULDERS)  
 BEGIN CPR AND 3.5" ASPHALT OVERLAY  
 (TRAVEL LANE & SHOULDERS)  
 STA. ML 62+79**

**END FULL DEPTH ASPHALT PAVEMENT  
 RECONSTRUCTION (TRAVEL LANES)  
 BEGIN FULL DEPTH ASPHALT PAVEMENT  
 TRANSITION TO 3.5" OVERLAY  
 STA. ML 61+04**

**LEGEND**

- PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION
- PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION
- PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY
- PROPOSED RAMP MILL AND OVERLAY
- PROPOSED RELIEF JOINT RECONSTRUCTION
- DS  
X-X PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

**NOTES:**  
 1. SEE DWG. MD-7 FOR FULL DEPTH TRANSITION DETAILS.



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 SIGNATURE:  
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AFFIXED ON: 5/10/2024

SIGNATURE: [Signature]

STAMP: [Professional Engineer Seal for Joseph W. Kleinmann, State of New York, License No. 092577]

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**TITLE OF PROJECT:** I-190 HIGHWAY REHABILITATION  
**LOCATION OF PROJECT:** I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
**TITLE OF DRAWING:** ROADWAY PLANS

**CONTRACT NUMBER:** TAN 24-12  
**DATE:** MAY 2024  
**DRAWING NUMBER:** PL-8

CHECKED BY: J. KLEINMANN

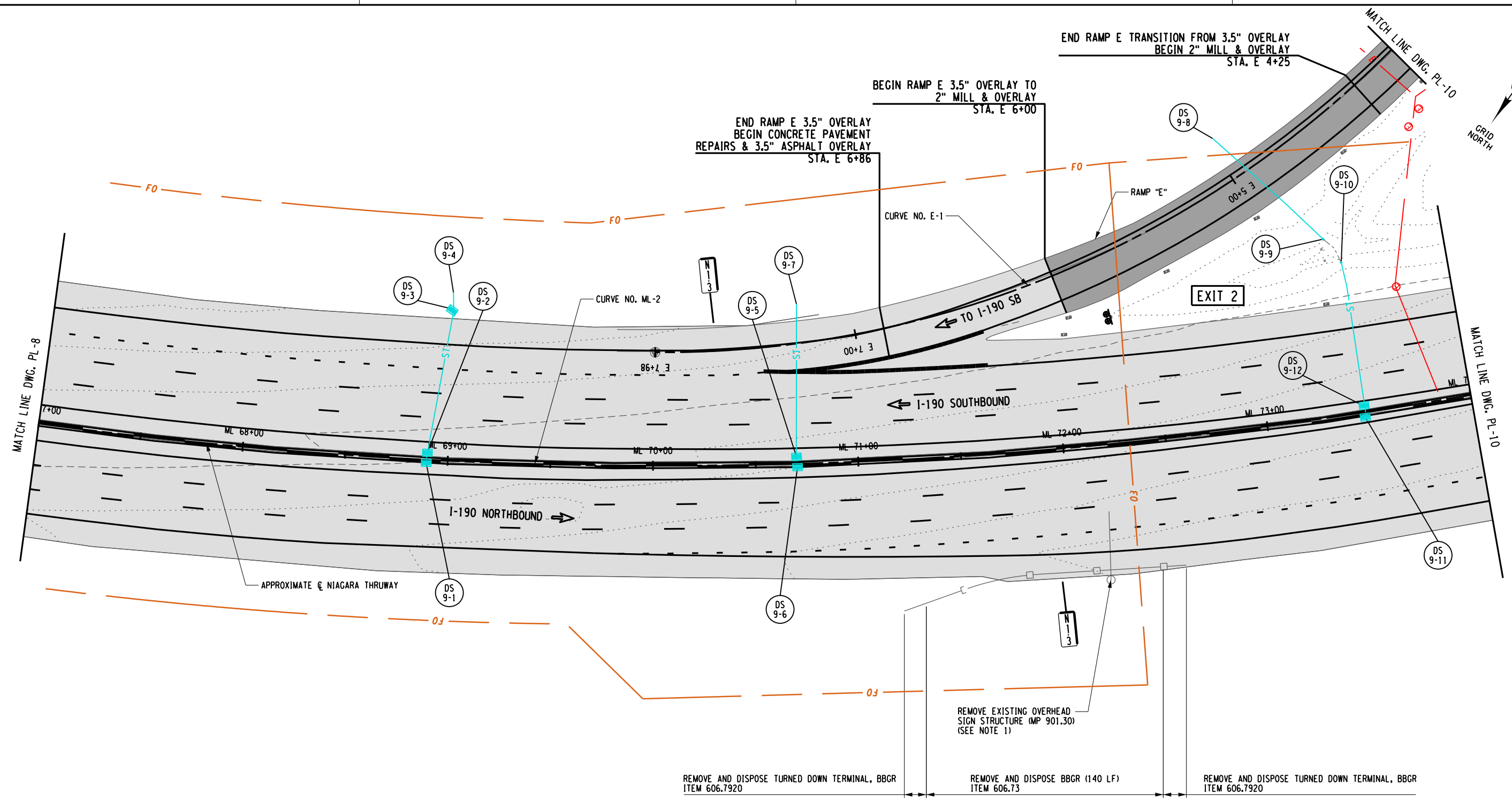
DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

133  
274



REMOVE AND DISPOSE TURNED DOWN TERMINAL, BBGR ITEM 606.7920

REMOVE AND DISPOSE BBGR (140 LF) ITEM 606.73

REMOVE AND DISPOSE TURNED DOWN TERMINAL, BBGR ITEM 606.7920

ALTERED ON:

SIGNATURE:  
STAMP:

AFFIXED ON: 5/10/2024

SIGNATURE:  
STAMP:

**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-3 TO MT-14)

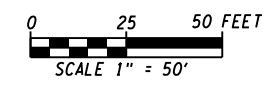
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**NOTES:**

1. SEE DWG. MT-18 FOR OVERHEAD SIGN STRUCTURE REMOVAL TABLE.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-9



- NOTES:**
- SEE DWGS SSP-5 FOR STRUCTURE LOCATION, SDS-4 FOR SIGN PANEL LAYOUT, AND OSP-05 TO OSP-06 FOR SIGN STRUCTURE DETAILS.
  - SEE DWG. MT-18 FOR OVERHEAD SIGN STRUCTURE REMOVAL TABLE. THE EXISTING TRANSMIT EQUIPMENT LOCATED ON THE OVERHEAD STRUCTURE SHALL BE CAREFULLY REMOVED & STORED FOR NYSTA PICK-UP.
  - EXISTING ITS EQUIPMENT MOUNTED ON THE EXISTING OVERHEAD SIGN STRUCTURE SHALL BE RELOCATED TO THE NEW SIGN STRUCTURE LOCATION (MP 901.41) SEE DETAILS ON DWG. MD-15.

**LEGEND**

- PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION
- PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION
- PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY
- PROPOSED RAMP MILL AND OVERLAY
- PROPOSED RELIEF JOINT RECONSTRUCTION
- PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

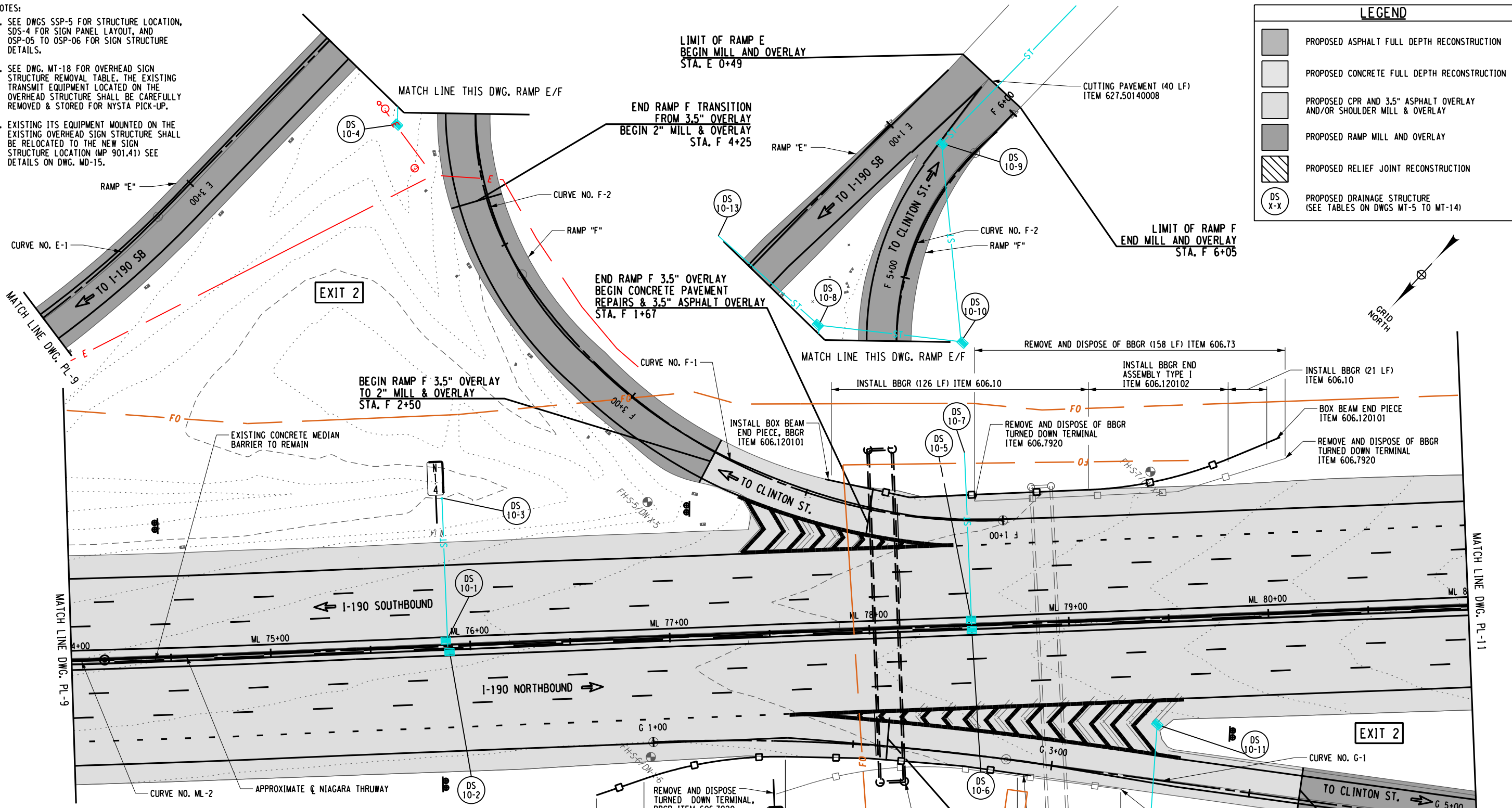
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

STATE OF NEW YORK  
JOSEPH W. KLEINMANN  
LICENSED PROFESSIONAL ENGINEER  
EXCELLENCE  
092577

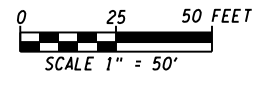
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY

Thruway Authority

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-10



CHECKED BY: J. KLEINMANN

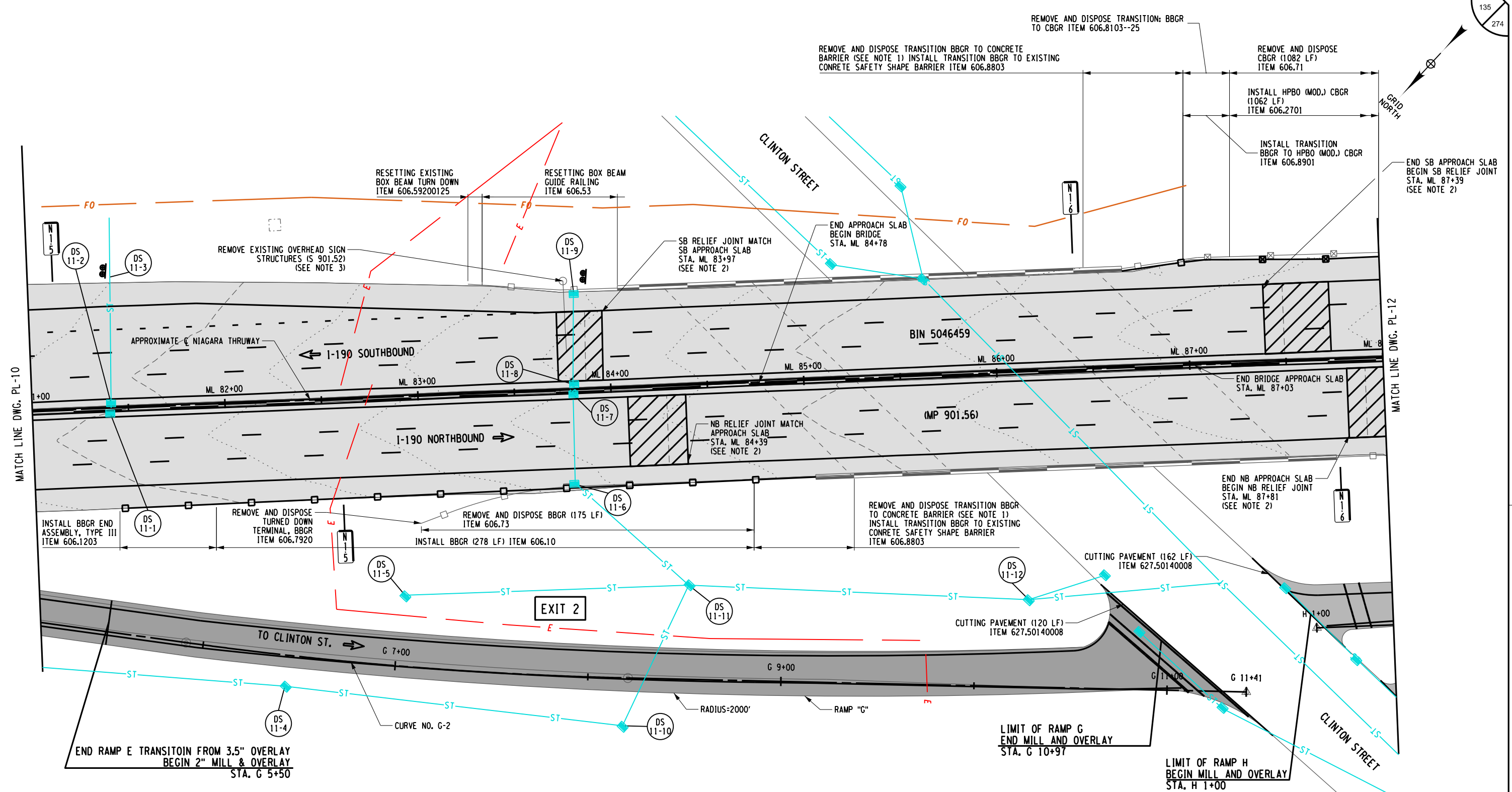
DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

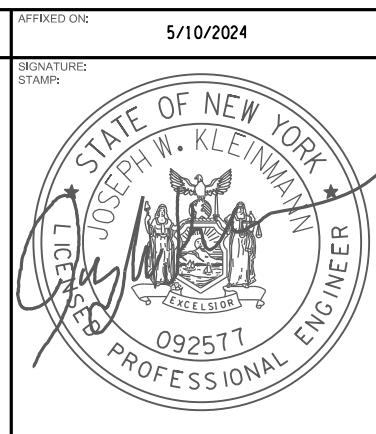
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

135  
274



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 AFFIXED ON: 5/10/2024  
 SIGNATURE: \_\_\_\_\_  
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**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

**NOTES:**

- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPLACED AT NO ADDITIONAL COST.
- SEE CPR DRAWING AND DWG. MD-10 OF LIMITS OF JOINT REPLACEMENT AND CONSTRUCTION DETAILS.
- SEE DWG. MT-18 FOR OVERHEAD SIGN STRUCTURE REMOVAL TABLE.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY** | **Thruway Authority**

**QMA**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-11

SCALE 1" = 50'

CHECKED BY: J. KLEINMANN

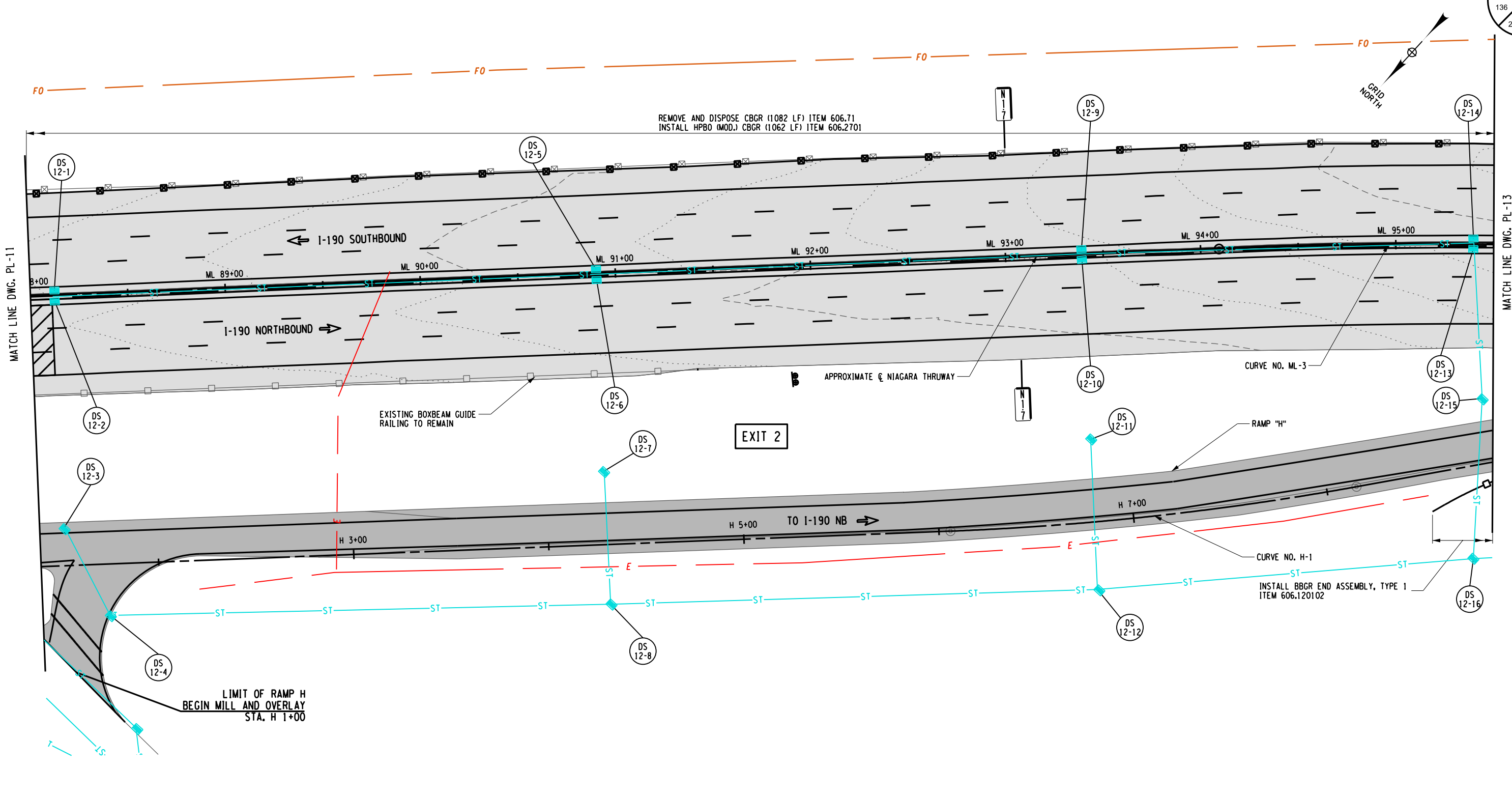
DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

136  
274



ALTERED ON:

SIGNATURE:  
STAMP:

AFFIXED ON: 5/10/2024

SIGNATURE:  
STAMP:

**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

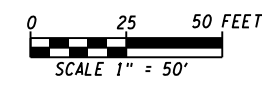
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
ROADWAY PLANS

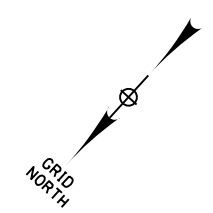
CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
PL-12







CHECKED BY: J. KLEINMANN  
 DRAFTED BY: N. HOY  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: P. THOMPSON  
 DESIGN SUPERVISOR: D. MARSH

END CPR AND 3.5" ASPHALT OVERLAY  
 BEGIN FULL DEPTH ASPHALT PAVEMENT (TRAVEL  
 LANES) & 1.5" MILL AND OVERLAY (SHOULDERS)  
 TRANSITION TO EXISTING GRADE  
 STA. ML 95+75

REMOVE AND DISPOSE  
 TRANSITION CBGR TO BBGR  
 ITEM 606.8103-25

REMOVE AND DISPOSE TRANSITION BBGR TO CONCRETE BRIDGE BARRIER (SEE NOTE 1)  
 INSTALL TRANSITION BBGR TO EXISTING CONCRETE SAFETY SHAPE BARRIER  
 ITEM 606.8803

INSTALL TRANSITION  
 HPB(MOD.) CBGR TO BBGR  
 ITEM 606.8901

REMOVE AND DISPOSE CBGR (1082 LF) ITEM 606.71  
 INSTALL HPB(MOD.) CBGR (1062 LF) ITEM 606.2701

END FULL WIDTH SB SHOULDER  
 MILL & OVERLAY MATCH EXISTING  
 CONCRETE PAVEMENT GRADE  
 STA. ML 98+33

END FULL DEPTH ASPHALT PAVEMENT  
 RECONSTRUCTION (TRAVEL LANES)  
 END SB RELIEF JOINT MATCH EXISTING  
 APPROACH SLAB/DECK  
 STA. ML 97+84

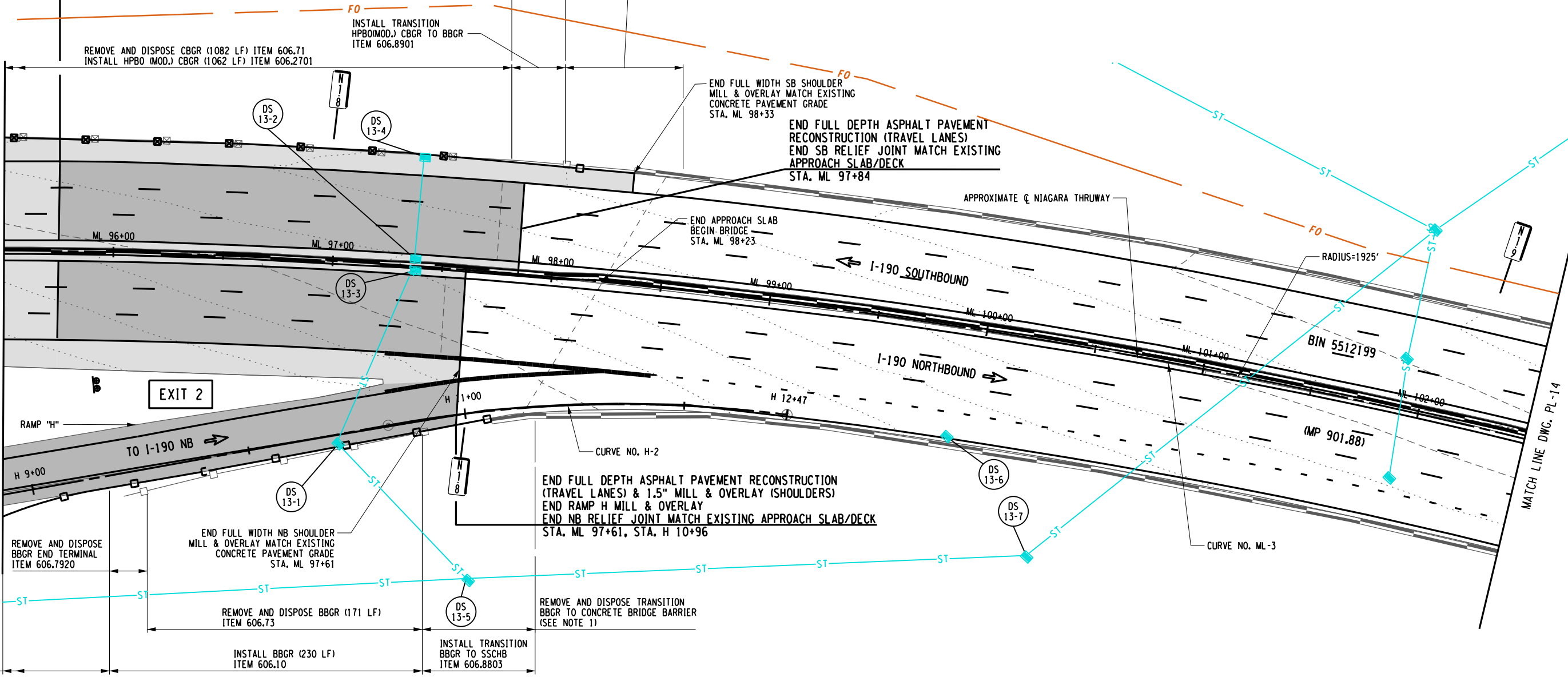
END APPROACH SLAB  
 BEGIN BRIDGE  
 STA. ML 98+23

APPROXIMATE Q NIAGARA THRUWAY

RADIUS=1925'

MATCH LINE DWG. PL-12

MATCH LINE DWG. PL-14



END FULL DEPTH ASPHALT PAVEMENT RECONSTRUCTION  
 (TRAVEL LANES) & 1.5" MILL & OVERLAY (SHOULDERS)  
 END RAMP H MILL & OVERLAY  
 END NB RELIEF JOINT MATCH EXISTING APPROACH SLAB/DECK  
 STA. ML 97+61, STA. H 10+96

REMOVE AND DISPOSE TRANSITION  
 BBGR TO CONCRETE BRIDGE BARRIER  
 (SEE NOTE 1)

REMOVE AND DISPOSE  
 BBGR END TERMINAL  
 ITEM 606.7920

END FULL WIDTH NB SHOULDER  
 MILL & OVERLAY MATCH EXISTING  
 CONCRETE PAVEMENT GRADE  
 STA. ML 97+61

REMOVE AND DISPOSE BBGR (171 LF)  
 ITEM 606.73

INSTALL BBGR (230 LF)  
 ITEM 606.10

INSTALL TRANSITION  
 BBGR TO SSCHB  
 ITEM 606.8803

ALTERED ON:

AFFIXED ON: 5/10/2024

SIGNATURE:  
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SIGNATURE:  
 STAMP:



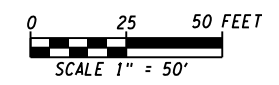
**LEGEND**

- PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION
- PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION
- PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY
- PROPOSED RAMP MILL AND OVERLAY
- PROPOSED RELIEF JOINT RECONSTRUCTION
- DS  
X-X PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-3 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

**NOTES:**

1. EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
2. SEE DWG. MD-7 FOR FULL DEPTH TRANSITION DETAILS.



REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-13

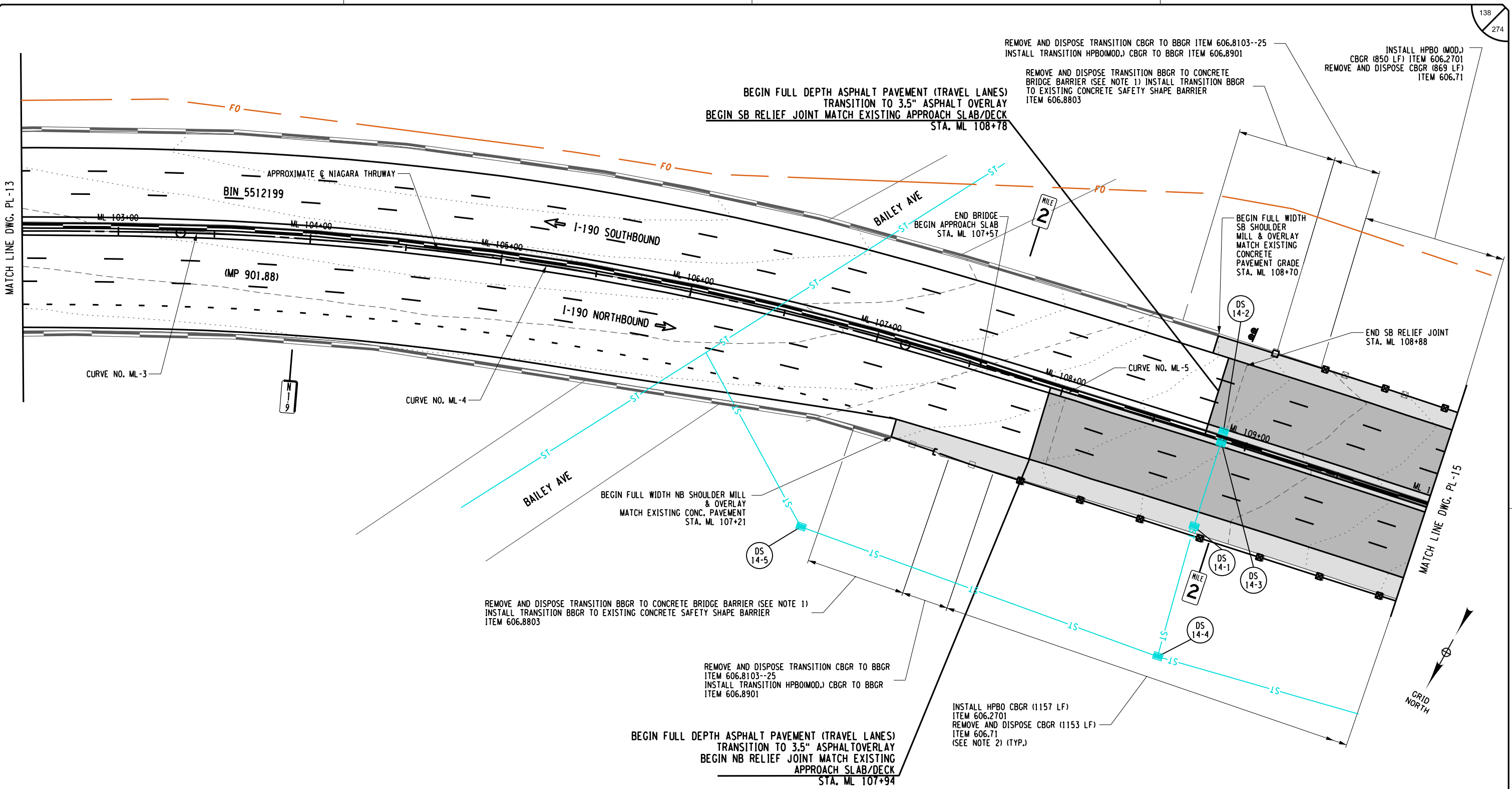
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

STATE OF NEW YORK  
JOSEPH W. KLEINMANN  
LICENSED PROFESSIONAL ENGINEER  
EXCELLENCE  
092577

**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

**NOTES:**

- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
- FACE OF RAIL TO BE FLUSH WITH THE BOTTOM EDGE OF THE EXISTING CURB.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY** | **Thruway Authority**

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 TITLE OF DRAWING: ROADWAY PLANS

CONTRACT NUMBER: TAN 24-12  
 DATE: MAY 2024  
 DRAWING NUMBER: PL-14

SCALE 1" = 50'



CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

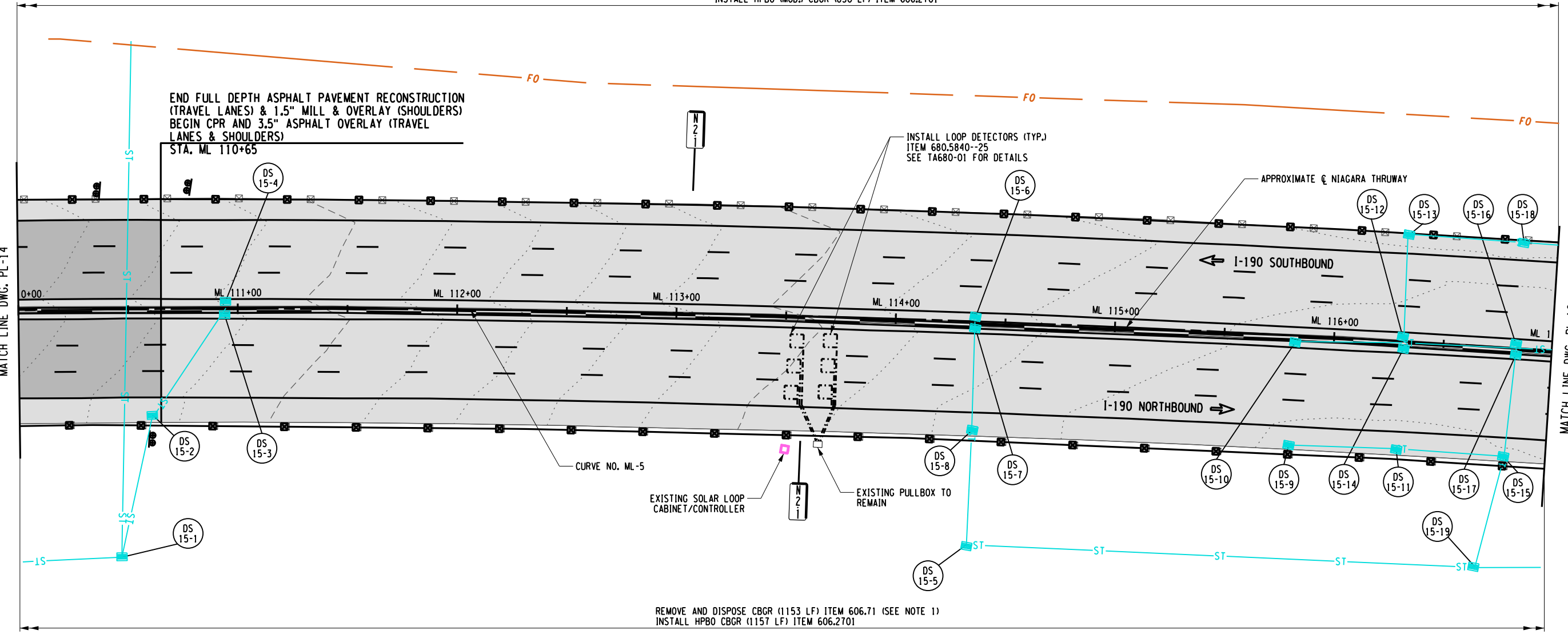
DESIGN SUPERVISOR: D. MARSH

REMOVE AND DISPOSE CBGR (869 LF) ITEM 606.71  
INSTALL HPBO (MOD.) CBGR (850 LF) ITEM 606.2701

END FULL DEPTH ASPHALT PAVEMENT RECONSTRUCTION  
(TRAVEL LANES) & 1.5" MILL & OVERLAY (SHOULDERS)  
BEGIN CPR AND 3.5" ASPHALT OVERLAY (TRAVEL  
LANES & SHOULDERS)  
STA. ML 110+65

INSTALL LOOP DETECTORS (TYP.)  
ITEM 680.5840-25  
SEE TA680-01 FOR DETAILS

APPROXIMATE  $\bar{C}$  NIAGARA THRUWAY



REMOVE AND DISPOSE CBGR (1153 LF) ITEM 606.71 (SEE NOTE 1)  
INSTALL HPBO CBGR (1157 LF) ITEM 606.2701

ALTERED ON:  
SIGNATURE:  
STAMP:

AFFIXED ON:  
5/10/2024  
SIGNATURE:  
STAMP:

**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

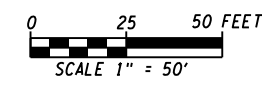
**NOTES:**  
1. FACE OF RAIL TO BE FLUSH WITH THE BOTTOM OF THE EXISTING CURB.  
2. SEE DWG. MD-7 FOR FULL DEPTH TRANSITION DETAILS.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2  
TITLE OF DRAWING  
ROADWAY PLANS

CONTRACT NUMBER:  
TAN 24-12  
DATE:  
MAY 2024  
DRAWING NUMBER:  
PL-15



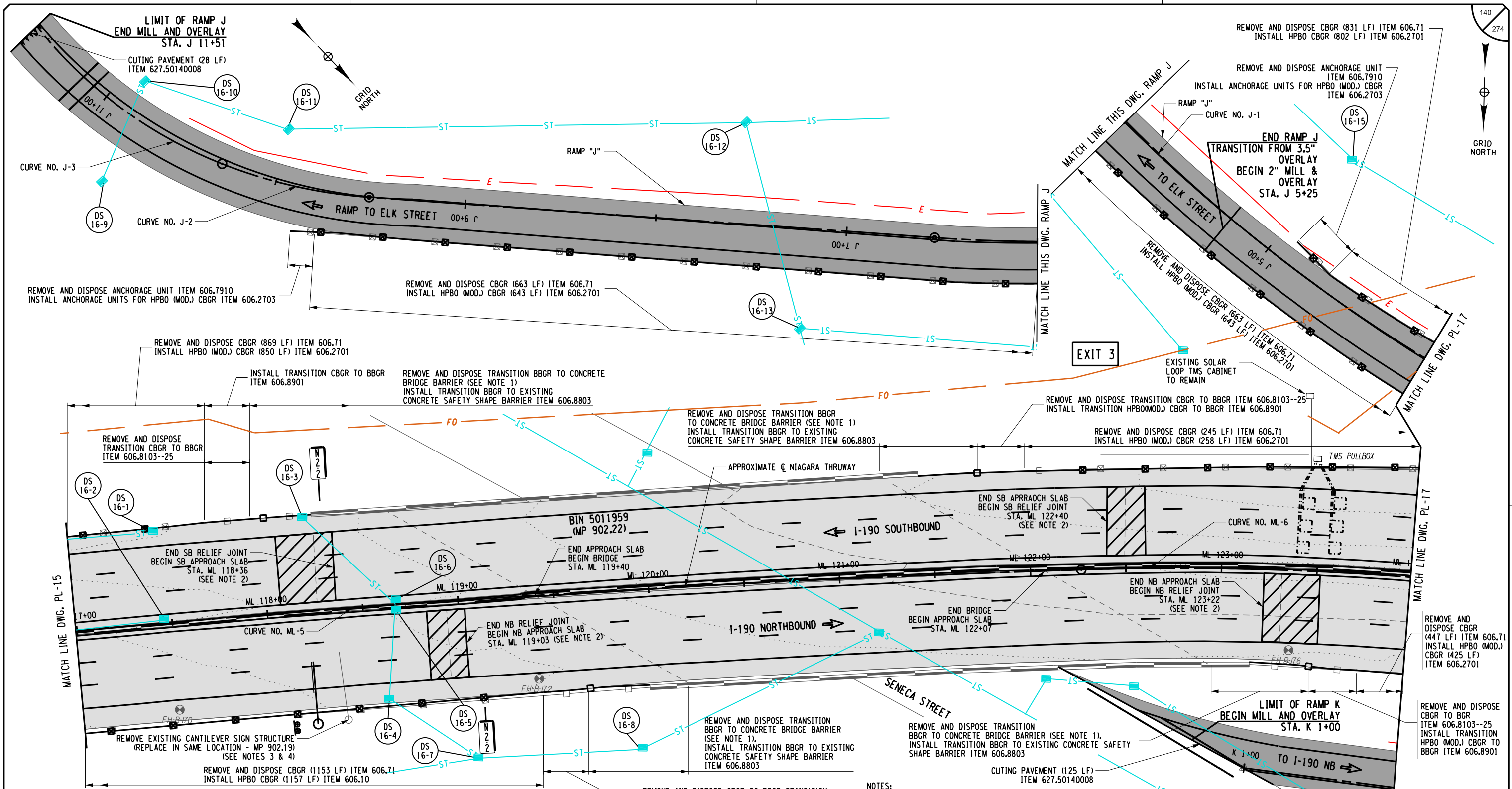
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

**NOTES:**

- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
- SEE CPR DRAWINGS AND DWG. MD-10 OF LIMITS OF JOINT REPLACEMENT AND CONSTRUCTION DETAILS.
- SEE DWGS SSP-8 FOR STRUCTURE LOCATION, SDS-4 FOR SIGN PANEL LAYOUT, AND OSP-07 TO OSP-08 FOR SIGN STRUCTURE DETAILS.
- SEE DWG. MT-18 FOR OVERHEAD SIGN STRUCTURE REMOVAL TABLE.

REVISIONS

DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY** | **Thruway Authority**

**QMA**

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 TITLE OF DRAWING: ROADWAY PLANS

CONTRACT NUMBER: TAN 24-12  
 DATE: MAY 2024  
 DRAWING NUMBER: PL-16

SCALE 1" = 50'



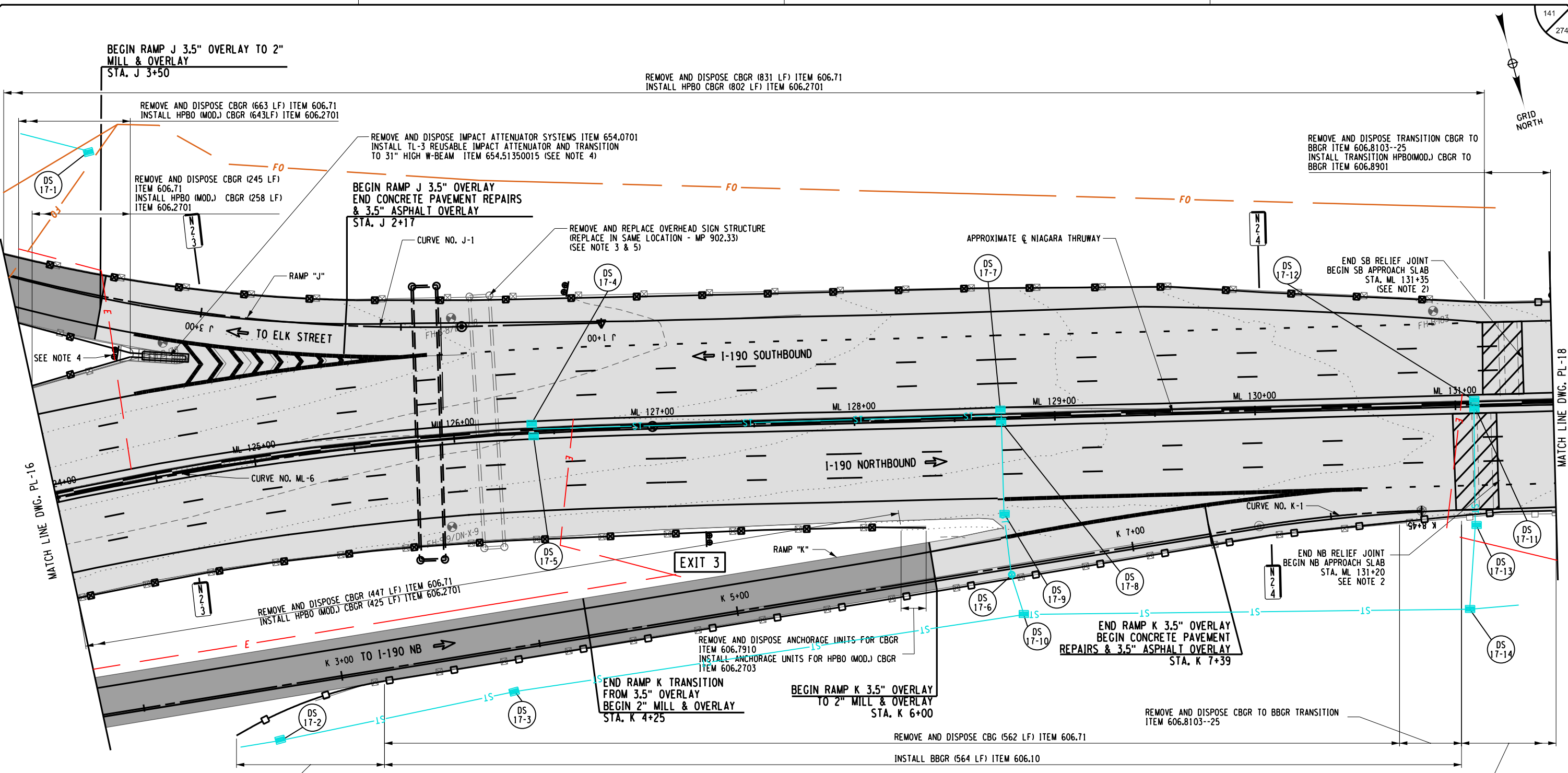
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

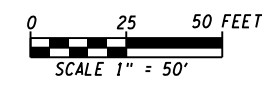
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



**NOTES:**

- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
- SEE CPR DRAWINGS AND DWG. MD-10 OF LIMITS OF JOINT REPLACEMENT AND CONSTRUCTION DETAILS.
- SEE DWGS SSP-8 FOR STRUCTURE LOCATION, SDS-4 FOR SIGN PANEL LAYOUT, AND OSP-09 TO OSP-10 FOR SIGN STRUCTURE DETAILS.
- ALL CONCRETE, REINFORCING, ANCHORING, ETC. ASSOCIATED WITH THE REMOVAL OF THE ATTENUATOR ANCHOR CONCRETE BLOCK SHALL BE PAID FOR UNDER ITEM 203.02.
- SEE DWG. MT-18 FOR OVERHEAD SIGN STRUCTURE REMOVAL TABLE.



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

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**LEGEND**

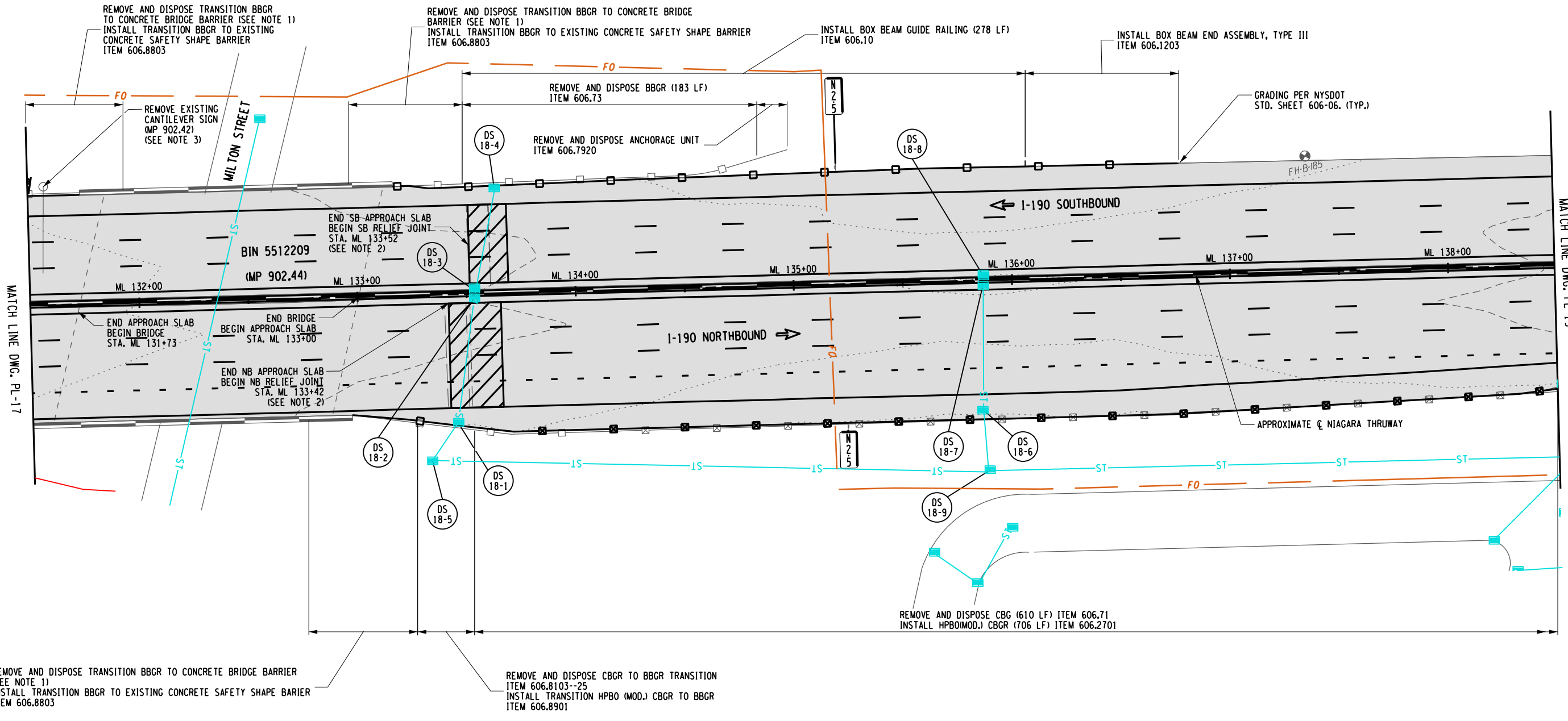
	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

**REVISIONS**

DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-17



CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

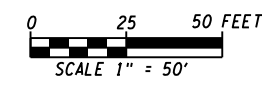
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**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

- NOTES:**
- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED, THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
  - SEE CPR DRAWINGS AND DWG. MD-10 OF LIMITS OF JOINT REPLACEMENT AND CONSTRUCTION DETAILS.
  - SEE DWG. MT-18 FOR OVERHEAD SIGN STRUCTURE REMOVAL TABLE.



REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-18



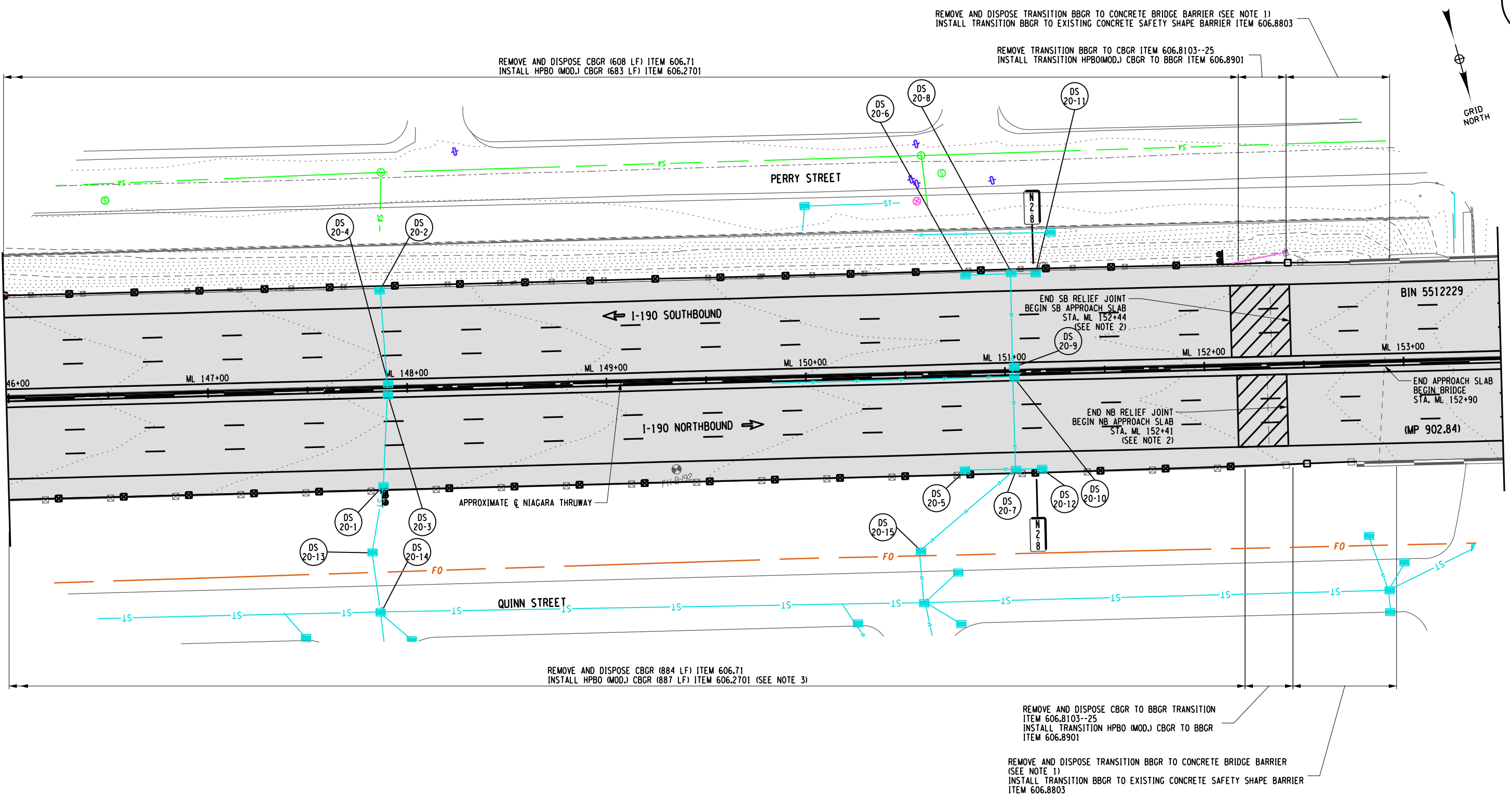
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



MATCH LINE DWG. PL-19

MATCH LINE DWG. PL-21

ALTERED ON:

SIGNATURE:  
STAMP:

AFFIXED ON:  
5/10/2024

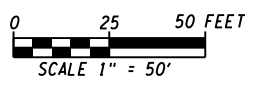
SIGNATURE:  
STAMP:

**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

- NOTES:**
- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
  - SEE CPR DRAWINGS AND DWG. MD-10 OF LIMITS OF JOINT REPLACEMENT AND CONSTRUCTION DETAILS.
  - FACE OF RAIL TO BE FLUSH WITH THE BOTTOM EDGE OF THE EXISTING CURB.



REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
ROADWAY PLANS

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
PL-20



CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

END FULL DEPTH ASPHALT PAVEMENT RECONSTRUCTION (TRAVEL LANES) & 1.5" MILL & OVERLAY (SHOULDERS)  
END SB RELIEF JOINT MATCH EXISTING APPROACH SLAB/DECK  
STA. ML 157+86

END SB APPROACH SLAB  
BEGIN SB RELIEF JOINT  
STA. ML 154+54  
(SEE NOTE 2)  
END BRIDGE  
BEGIN APPROACH SLAB  
STA. ML 154+20

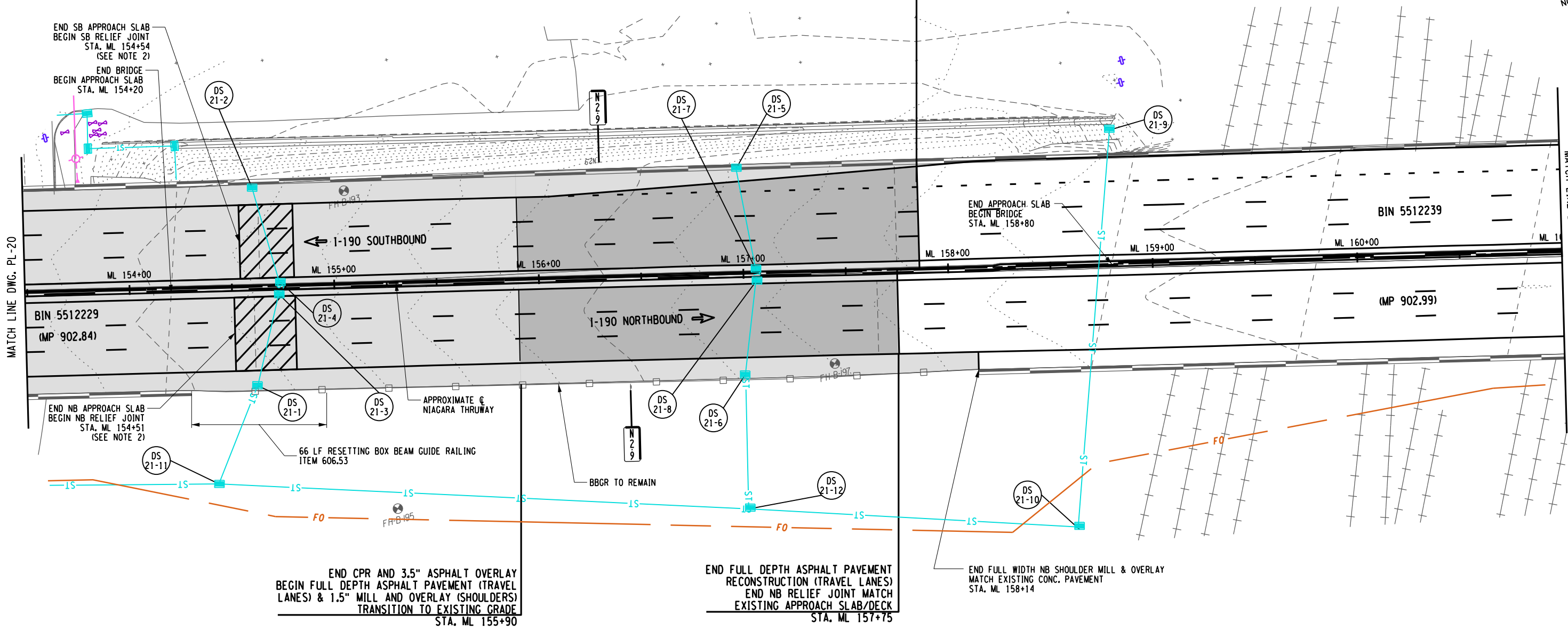
END APPROACH SLAB  
BEGIN BRIDGE  
STA. ML 158+80

END NB APPROACH SLAB  
BEGIN NB RELIEF JOINT  
STA. ML 154+51  
(SEE NOTE 2)

END CPR AND 3.5" ASPHALT OVERLAY  
BEGIN FULL DEPTH ASPHALT PAVEMENT (TRAVEL LANES) & 1.5" MILL AND OVERLAY (SHOULDERS)  
TRANSITION TO EXISTING GRADE  
STA. ML 155+90

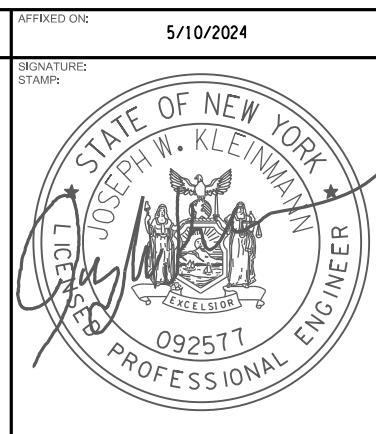
END FULL DEPTH ASPHALT PAVEMENT RECONSTRUCTION (TRAVEL LANES)  
END NB RELIEF JOINT MATCH EXISTING APPROACH SLAB/DECK  
STA. ML 157+75

END FULL WIDTH NB SHOULDER MILL & OVERLAY  
MATCH EXISTING CONC. PAVEMENT  
STA. ML 158+14



ALTERED ON:

SIGNATURE:  
STAMP:



**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

**NOTES:**

- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.880.3. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
- SEE CPR DRAWINGS AND DWG. MD-10 OF LIMITS OF JOINT REPLACEMENT AND CONSTRUCTION DETAILS.

REVISIONS

DATE	DESCRIPTION	BY	SYM.

0 25 50 FEET  
SCALE 1" = 50'

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-21

LEGEND

- PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION
- PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION
- PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY
- PROPOSED RAMP MILL AND OVERLAY
- PROPOSED RELIEF JOINT RECONSTRUCTION
- PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

NOTES:

- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
- SEE CPR DRAWINGS AND DWG. MD-10 OF LIMITS OF JOINT REPLACEMENT AND CONSTRUCTION DETAILS.
- SEE DWGS SSP-12 FOR STRUCTURE LOCATION, SDS-4 AND SDS-5 FOR SIGN PANEL LAYOUT, AND OSP-13 TO OSP-14 FOR SIGN STRUCTURE DETAILS.
- SEE DWG. MT-18 FOR OVERHEAD SIGN PANEL STRUCTURE REMOVAL TABLE.

BEGIN FULL DEPTH ASPHALT PAVEMENT (TRAVEL LANES) TRANSITION TO 3.5" ASPHALT OVERLAY  
 BEGIN SB RELIEF JOINT MATCH EXISTING APPROACH SLAB/DECK  
 STA. ML 164+37

END MILL AND OVERLAY  
 BEGIN CONCRETE PAVEMENT REPAIRS  
 RAMP - L  
 STA. L 2+80

REMOVE AND DISPOSE TRANSITION CONCRETE BRIDGE BARRIER TO BBGR (SEE NOTE 1)  
 INSTALL TRANSITION BBGR TO EXISTING CONCRETE SAFETY SHAPE BARRIER  
 ITEM 606.8803

BEGIN FULL WIDTH SB MILL & OVERLAY SHOULDER MATCH EXISTING CONCRETE PAVEMENT STA. ML 163+33

INSTALL BBGR (SHOP BENT 208.0' RADIUS) (76 LF) ITEM 606.100002

INSTALL BBGR (67 LF) ITEM 606.10

REMOVE AND DISPOSE BBGR (146 LF) ITEM 606.73

INSTALL BBGR (SHOP BENT 134.0' RADIUS) (144 LF) ITEM 606.100002

REMOVE AND DISPOSE TRANSITION BBGR TO CBGR  
 ITEM 606.8103--25

REMOVE AND DISPOSE CBGR (551 LF) ITEM 606.71

END RAMP E TRANSITION FROM 3.5" OVERLAY BEGIN 2" MILL & OVERLAY  
 STA. L1 6+44

INSTALL BBGR (186 LF) ITEM 606.10

BEGIN RAMP M 3.5" OVERLAY TO 2" MILL & OVERLAY  
 STA. M 2+50

END CONCRETE PAVEMENT REPAIRS & 3.5" ASPHALT OVERLAY BEGIN RAMP M 3.5" OVERLAY  
 STA. M 1+16

INSTALL BBGR (SHOP BENT 130.0' RADIUS) (96 LF) ITEM 606.100002

INSTALL BBGR (SHOP BENT 183.0' RADIUS) (128 LF) ITEM 606.100002

INSTALL BBGR (20 LF) ITEM 606.10

REMOVE AND DISPOSE TRANSITION BBGR TO BBGR  
 ITEM 606.8103--25

REMOVE AND DISPOSE TRANSITION BBGR TO CONCRETE BRIDGE BARRIER (SEE NOTE 1)  
 INSTALL TRANSITION BBGR TO SSCSB  
 ITEM 606.8803

EXIT 4

I-190 SOUTHBOUND

I-190 NORTHBOUND

BEGIN FULL WIDTH NB MILL & OVERLAY SHOULDER MATCH EXISTING CONCRETE PAVEMENT GRADE  
 STA. ML 164+17

END FULL DEPTH ASPHALT PAVEMENT RECONSTRUCTION (TRAVEL LANES) & 1.5" MILL & OVERLAY (SHOULDERS) BEGIN CPR AND 3.5" ASPHALT OVERLAY (TRAVEL LANES & SHOULDERS)  
 STA. ML 166+20

REMOVE AND REPLACE OVERHEAD SIGN STRUCTURE (REPLACE SIGN STRUCTURE IN SAME LOCATION) (MP 903.13) (SEE NOTES 3 & 4)

REMOVE AND DISPOSE BBGR (245 LF) ITEM 606.73  
 INSTALL BBGR (245 LF) ITEM 606.10

INSTALL TRANSITION BBGR TO SSCSB  
 ITEM 606.8803

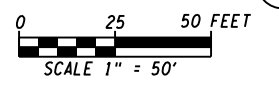
BEGIN FULL DEPTH ASPHALT PAVEMENT (TRAVEL LANES) TRANSITION TO 3.5" ASPHALT OVERLAY  
 BEGIN NB RELIEF JOINT MATCH EXISTING APPROACH SLAB/DECK  
 STA. ML 164+36

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-22



CHECKED BY: J. KLEINMANN  
 DRAFTED BY: N. HOY  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: P. THOMPSON  
 DESIGN SUPERVISOR: D. MARSH

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

STATE OF NEW YORK  
 JOSEPH W. KLEINMANN  
 LICENSED PROFESSIONAL ENGINEER  
 092577

MATCH LINE DWG. PL-21

MATCH LINE DWG. PL-24

BIN 5512239

(MP 902.99)

MILE 3

MILE 3

146  
274

GRID NORTH

MILE 3

MILE 3

DS 22-13

DS 22-11

DS 22-12

DS 22-10

DS 22-10

DS 22-10

DS 22-6

DS 22-5

DS 22-5

DS 22-8

DS 22-7

DS 22-4

DS 22-2

DS 22-3

DS 22-1

DS 22-1

DS 22-1

DS 22-1

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DS 22-1



**LEGEND**

[Grey Box]	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION	[Diagonal Lines Box]	PROPOSED RELIEF JOINT RECONSTRUCTION
[Light Grey Box]	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION	[Circle with X]	PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)
[Dark Grey Box]	PROPOSED RAMP MILL AND OVERLAY		
[Light Grey Box]	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		

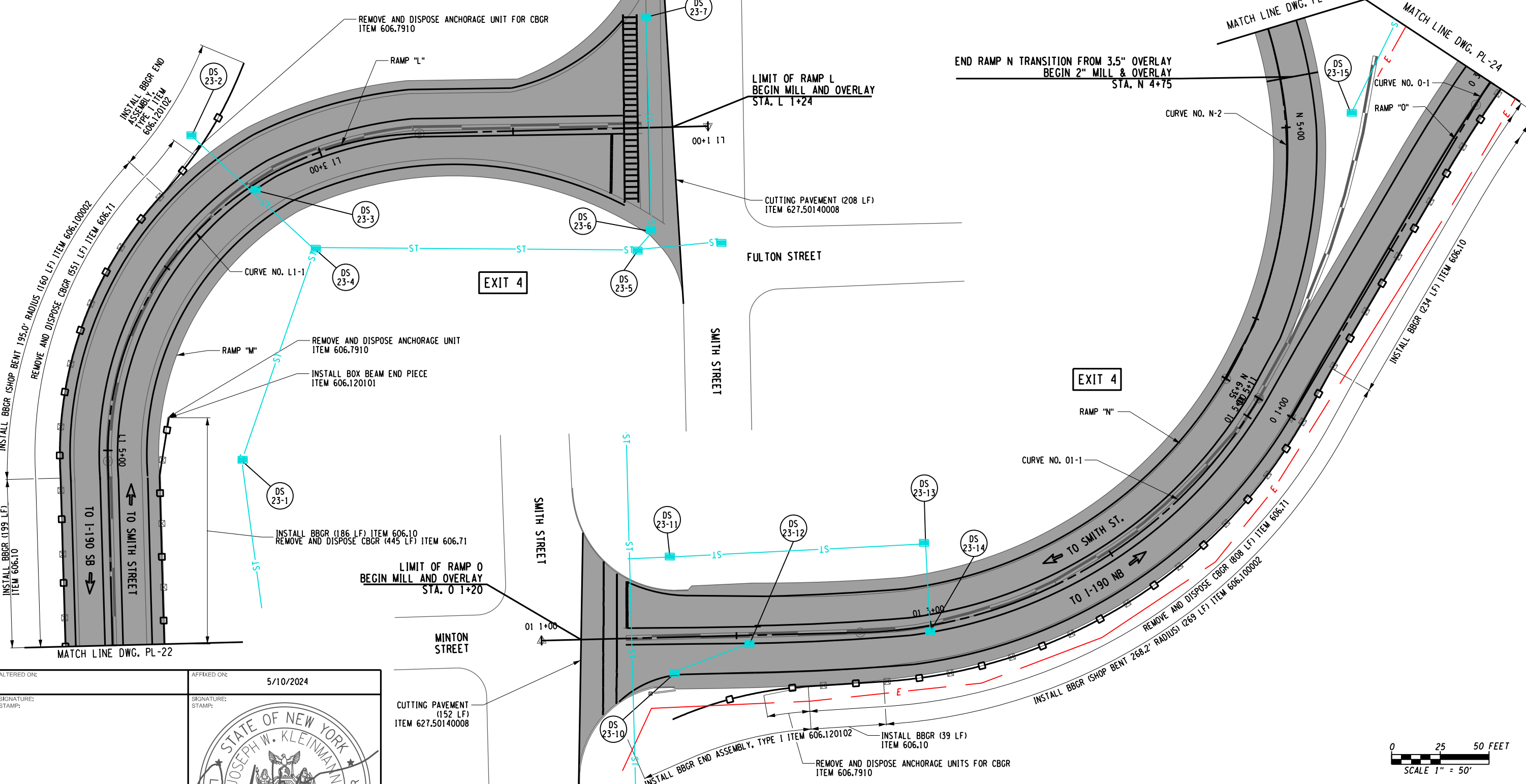
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



ALTERED ON: AFFIXED ON: 5/10/2024

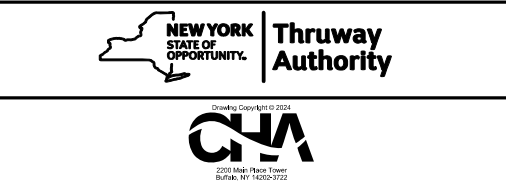
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CUTTING PAVEMENT (152 LF) ITEM 627.50140008

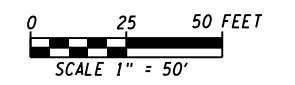
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 TITLE OF DRAWING: ROADWAY PLANS

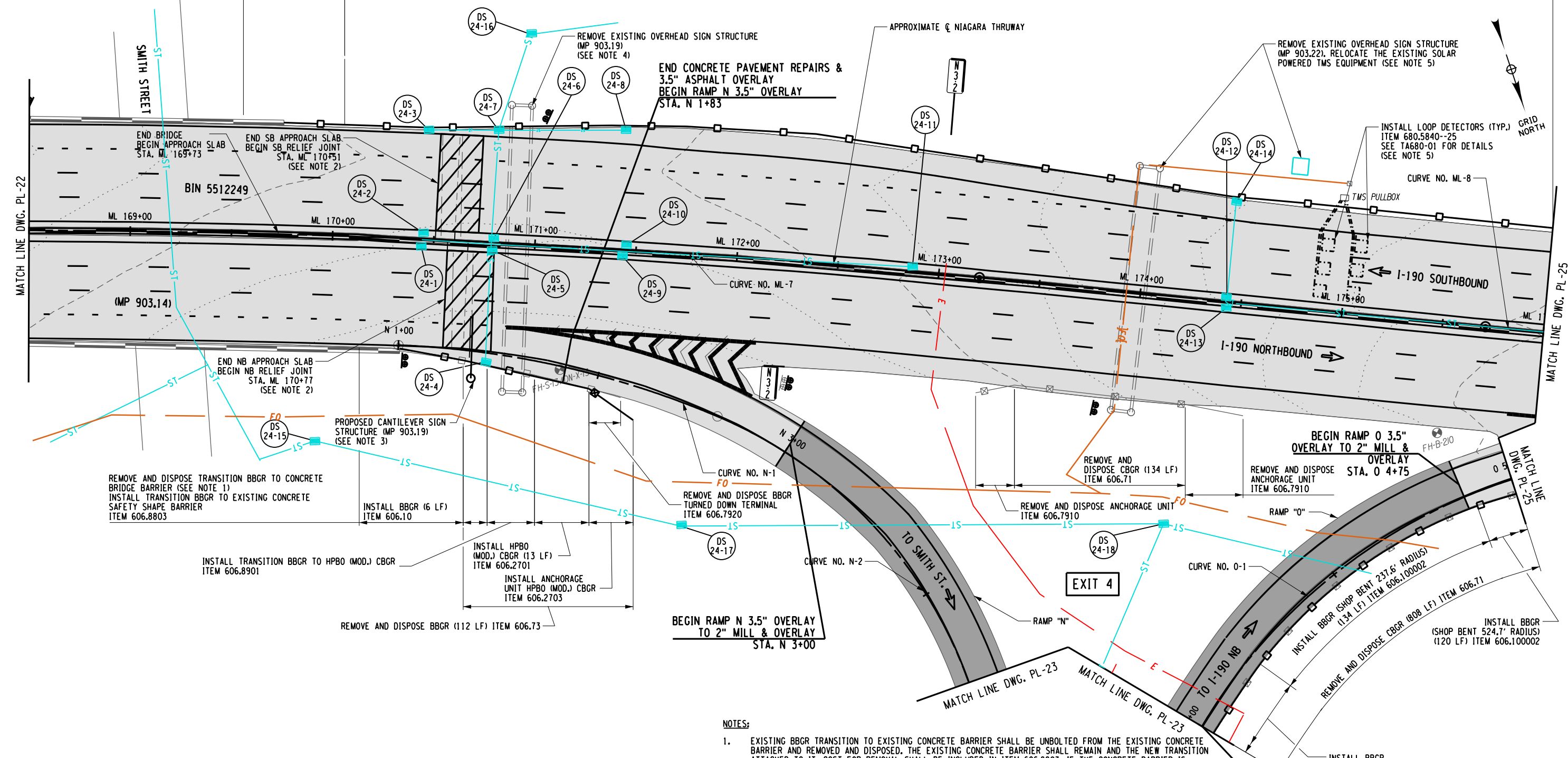
CONTRACT NUMBER: TAN 24-12  
 DATE: MAY 2024  
 DRAWING NUMBER: PL-23



REMOVE AND DISPOSE TRANSITION BBGR TO CONCRETE  
BRIDGE BARRIER (SEE NOTE 1) INSTALL TRANSITION  
BBGR TO EXISTING CONCRETE SAFETY SHAPE BARRIER  
ITEM 606.8803

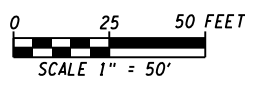
REMOVE AND DISPOSE BBGR (798 LF) ITEM 606.73  
INSTALL BBGR (802 LF) ITEM 606.10

CHECKED BY: J. KLEINMANN  
 DRAFTED BY: N. HOY  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: P. THOMPSON  
 DESIGN SUPERVISOR: D. MARSH



**NOTES:**

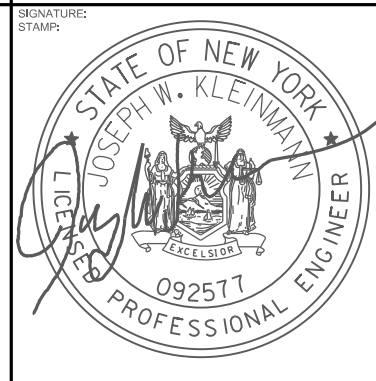
- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
- SEE CPR DRAWINGS AND DWG. MD-10 OF LIMITS OF JOINT REPLACEMENT AND CONSTRUCTION DETAILS.
- SEE DWGS SSP-12 FOR STRUCTURE LOCATION, SDS-4 AND SDS-5 FOR SIGN PANEL LAYOUT, AND OSP-15 TO OSP-16 FOR SIGN STRUCTURE DETAILS.
- SEE DWG. MT-18 FOR OVERHEAD SIGN STRUCTURE REMOVAL TABLE.
- THE EXISTING SOLAR POWERED TMS EQUIPMENT ATTACHED TO THE EXISTING OVERHEAD SIGN POLES SHALL BE RELOCATED ON A NEW POLE, SEE DETAILS ON DWG. MD-14. THE EXISTING TRANSMIT EQUIPMENT LOCATED ON THE OVERHEAD STRUCTURE SHALL BE CAREFULLY REMOVED & STORED FOR NYSTA PICK-UP.



ALTERED ON:

AFFIXED ON:  
5/10/2024

SIGNATURE:  
STAMP:



LEGEND	
	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION
	PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY
	PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)
	PROPOSED RAMP MILL AND OVERLAY

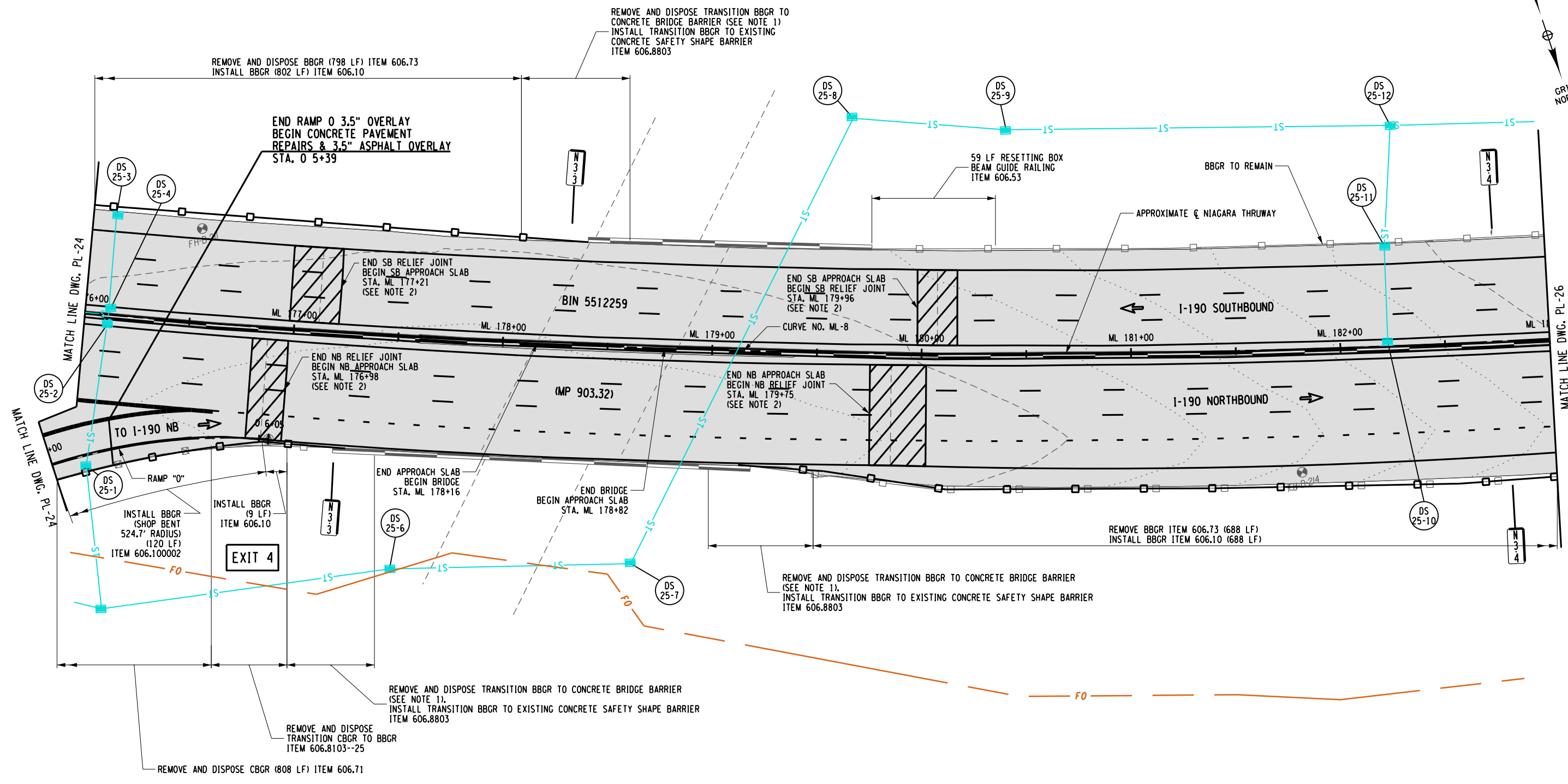
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-24

DESIGNED BY: P THOMPSON  
CHECKED BY: J KLEINMANN  
DRAFTED BY: N HOY  
CHECKED BY: J KLEINMANN



ALTERED ON:

SIGNATURE:  
STAMP:

AFFIXED ON:  
5/10/2024

SIGNATURE:  
STAMP:

**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

**NOTES:**

- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
- SEE CPR DRAWINGS AND DWG. MD-10 OF LIMITS OF JOINT REPLACEMENT AND CONSTRUCTION DETAILS.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY | Thruway Authority**

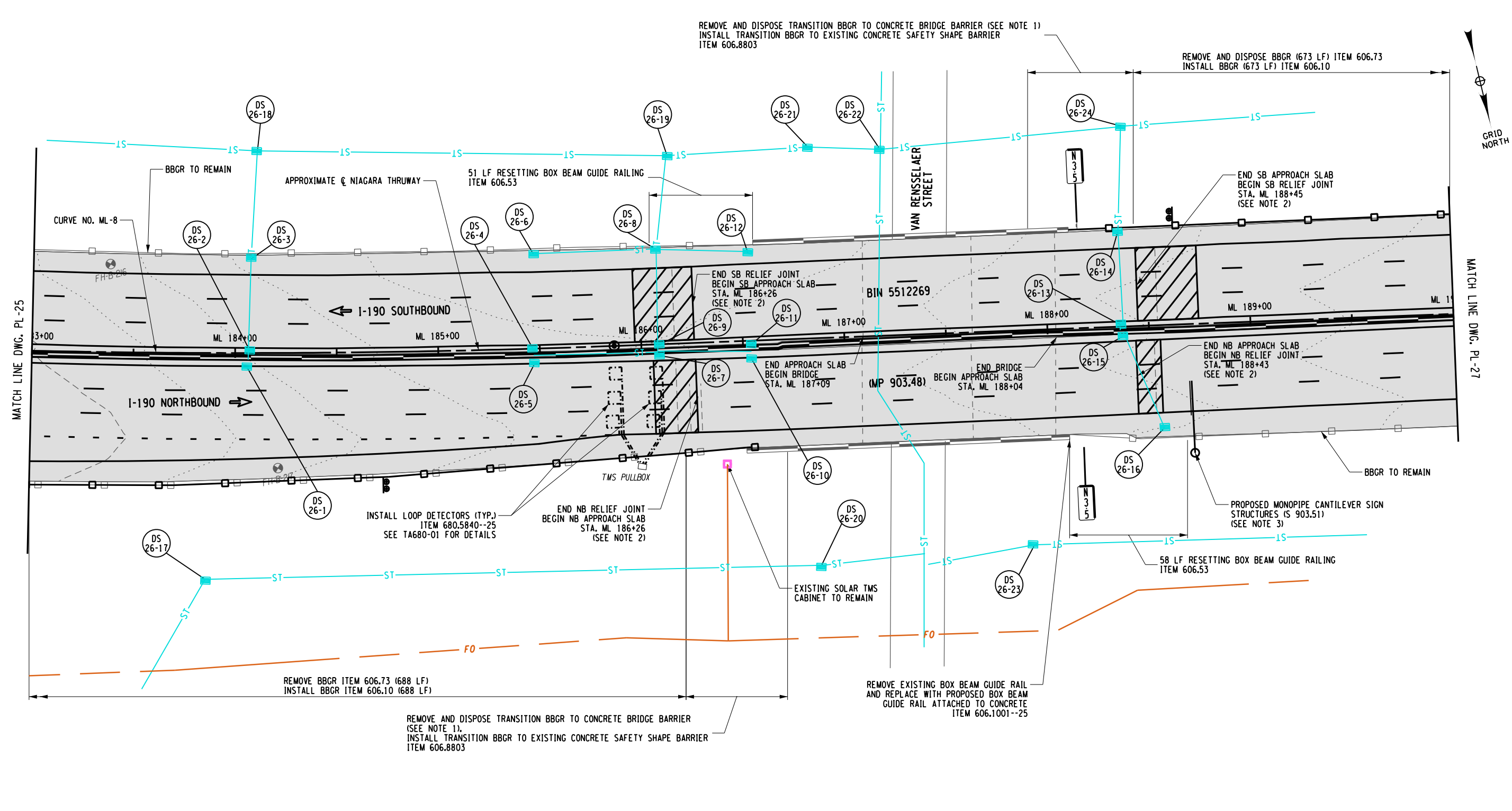
**GA**

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 TITLE OF DRAWING: ROADWAY PLANS

CONTRACT NUMBER: TAN 24-12  
 DATE: MAY 2024  
 DRAWING NUMBER: PL-25

SCALE 1" = 50'

CHECKED BY: J. KLEINMANN  
DRAFTED BY: N. HOY  
CHECKED BY: J. KLEINMANN  
DESIGNED BY: P. THOMPSON  
DESIGN SUPERVISOR: D. MARSH



ALTERED ON:

SIGNATURE:  
STAMP:

AFFIXED ON:  
5/10/2024

SIGNATURE:  
STAMP:

**LEGEND**

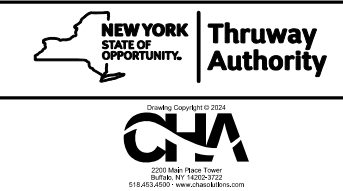
	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

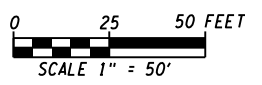
**NOTES:**

- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
- SEE CPR DRAWINGS AND DWG. MD-10 OF LIMITS OF JOINT REPLACEMENT AND CONSTRUCTION DETAILS.
- SEE DWGS SSP-14 FOR STRUCTURE LOCATION, SDS-5 FOR SIGN PANEL LAYOUT, AND OSP-17 TO OSP-18 FOR SIGN STRUCTURE DETAILS.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-26



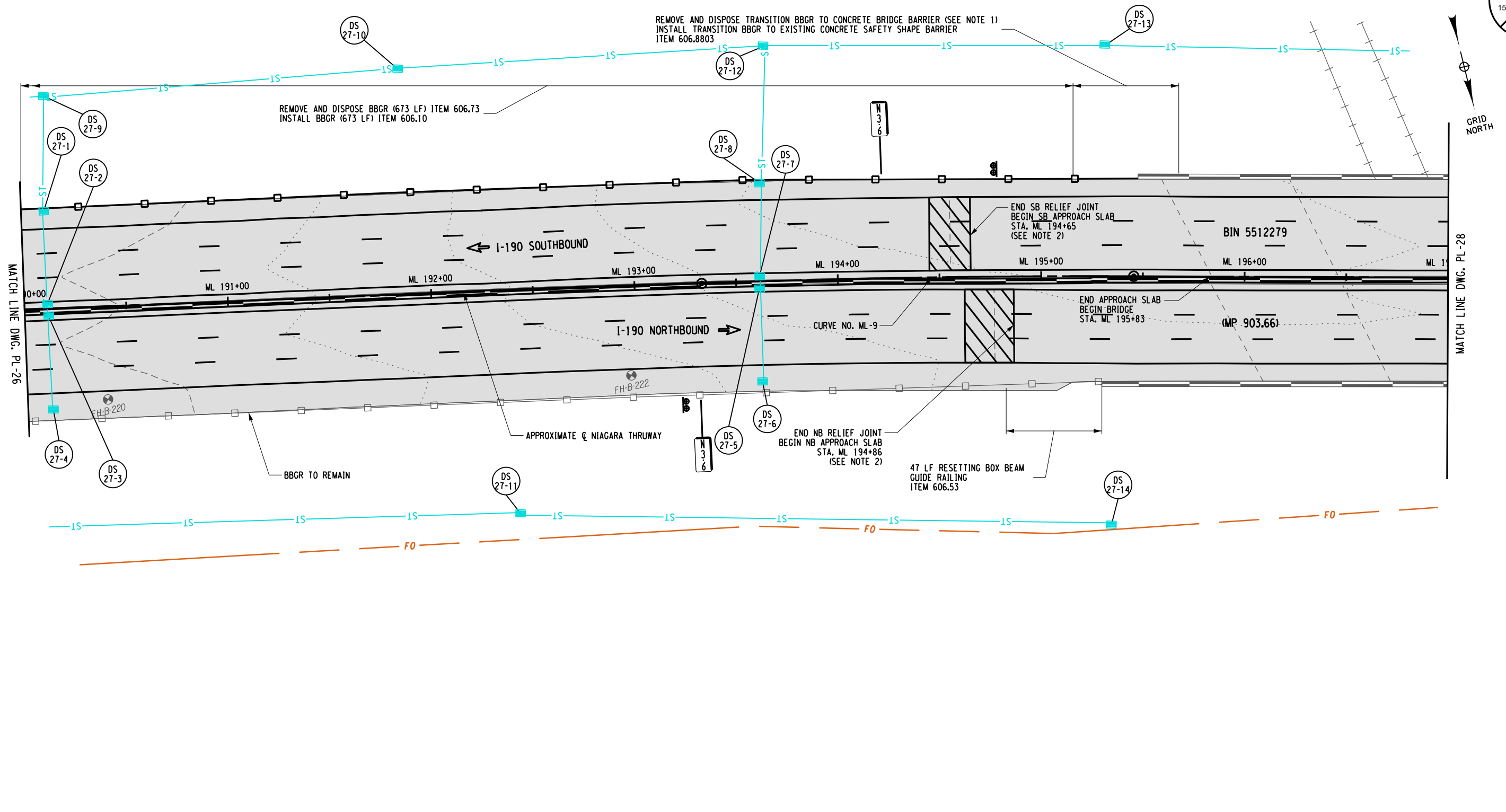
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



ALTERED ON: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_

AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_

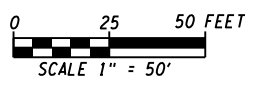
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**LEGEND**

PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION	PROPOSED RELIEF JOINT RECONSTRUCTION
PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION	PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)
PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY	
PROPOSED RAMP MILL AND OVERLAY	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

- NOTES:**
- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
  - SEE CPR DRAWINGS AND DWG. MD-10 OF LIMITS OF JOINT REPLACEMENT AND CONSTRUCTION DETAILS.



REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY Thruway Authority

TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
ROADWAY PLANS

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
PL-27

CHECKED BY: J.KLEINMANN

DRAFTED BY: N.HOY

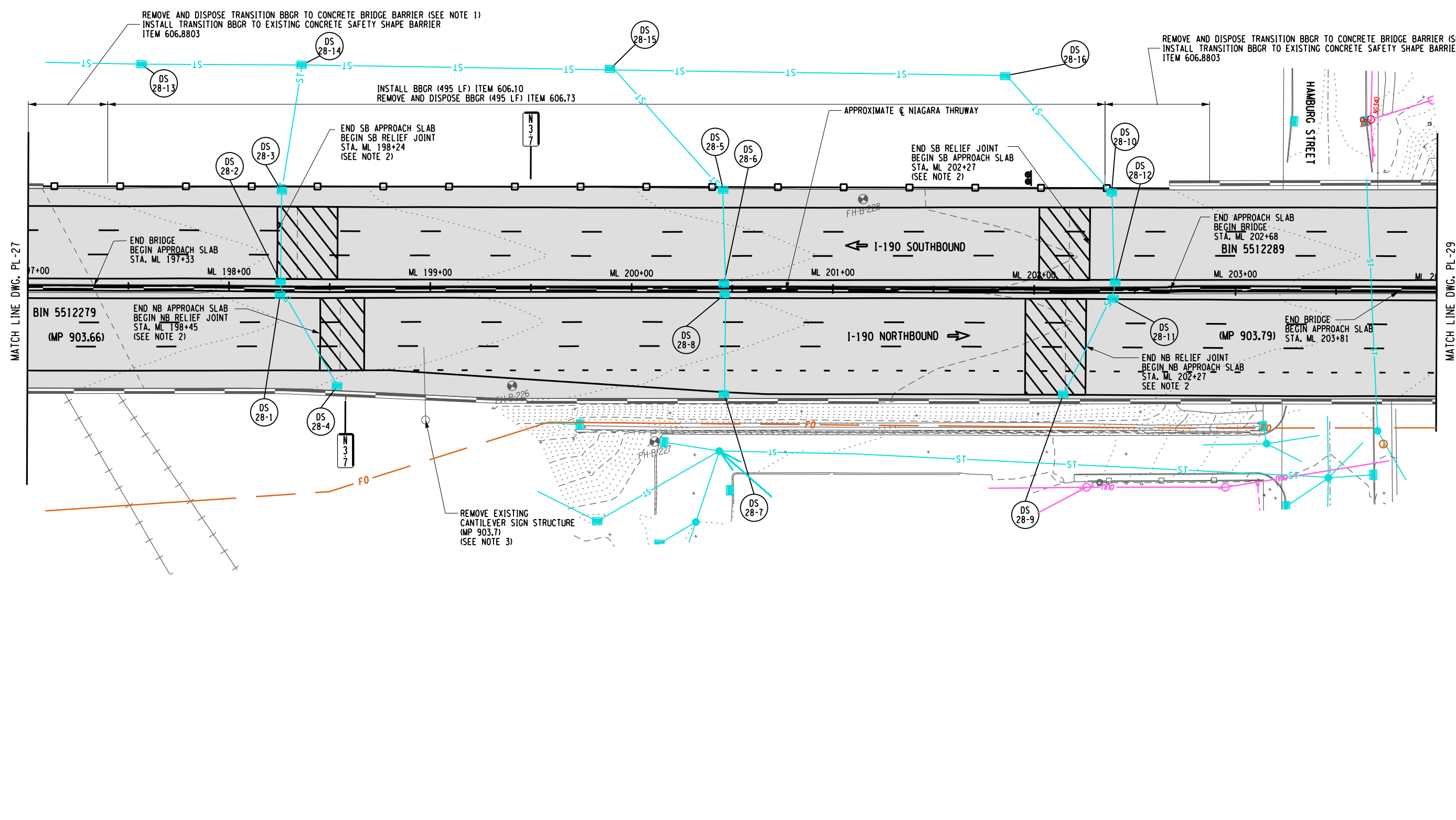
CHECKED BY: J.KLEINMANN

DESIGNED BY: P.THOMPSON

DESIGN SUPERVISOR: D.MARSH

152  
274

GRID NORTH



ALTERED ON:

SIGNATURE:  
STAMP:

AFFIXED ON:  
5/10/2024

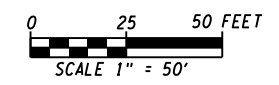
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**LEGEND**

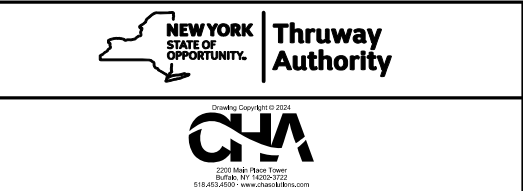
	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

- NOTES:**
- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
  - SEE CPR DRAWINGS AND DWG. MD-10 OF LIMITS OF JOINT REPLACEMENT AND CONSTRUCTION DETAILS.
  - SEE DWG. MT-18 FOR OVERHEAD SIGN STRUCTURE REMOVAL TABLE.



REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
ROADWAY PLANS

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
PL-28



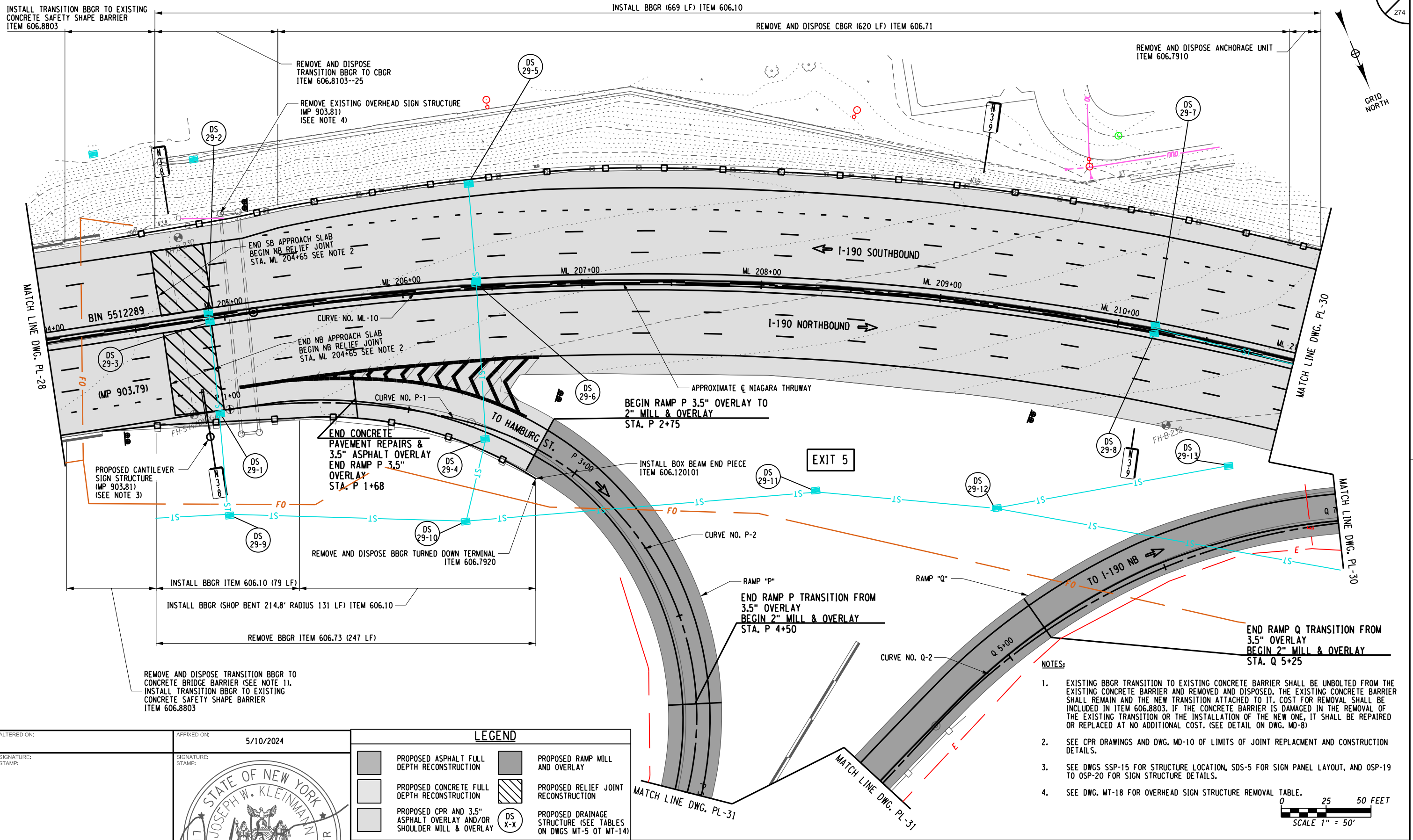
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



**NOTES:**

- EXISTING BBGR TRANSITION TO EXISTING CONCRETE BARRIER SHALL BE UNBOLTED FROM THE EXISTING CONCRETE BARRIER AND REMOVED AND DISPOSED. THE EXISTING CONCRETE BARRIER SHALL REMAIN AND THE NEW TRANSITION ATTACHED TO IT. COST FOR REMOVAL SHALL BE INCLUDED IN ITEM 606.8803. IF THE CONCRETE BARRIER IS DAMAGED IN THE REMOVAL OF THE EXISTING TRANSITION OR THE INSTALLATION OF THE NEW ONE, IT SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST. (SEE DETAIL ON DWG. MD-8)
- SEE CPR DRAWINGS AND DWG. MD-10 OF LIMITS OF JOINT REPLACEMENT AND CONSTRUCTION DETAILS.
- SEE DWGS SSP-15 FOR STRUCTURE LOCATION, SDS-5 FOR SIGN PANEL LAYOUT, AND OSP-19 TO OSP-20 FOR SIGN STRUCTURE DETAILS.
- SEE DWG. MT-18 FOR OVERHEAD SIGN STRUCTURE REMOVAL TABLE.

SCALE 1" = 50'

ALTERED ON: \_\_\_\_\_

AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_

**STATE OF NEW YORK**  
**JOSEPH W. KLEINMANN**  
 LICENSED PROFESSIONAL ENGINEER  
 092577

**LEGEND**

- PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION
- PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION
- PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY
- PROPOSED RAMP MILL AND OVERLAY
- PROPOSED RELIEF JOINT RECONSTRUCTION
- PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 OR MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

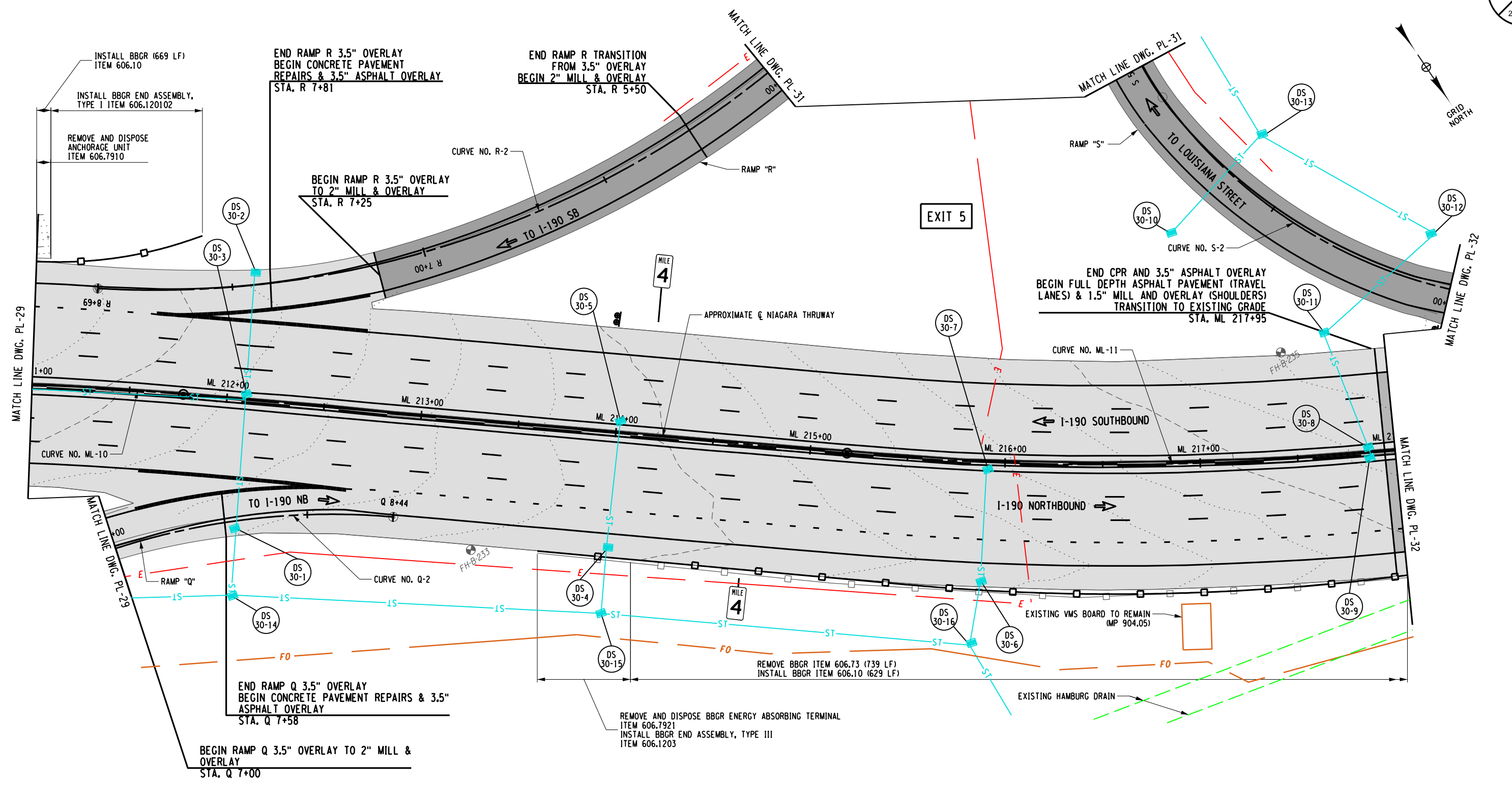
**NEW YORK STATE OF OPPORTUNITY**

**Thruway Authority**

**QMA**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-29

CHECKED BY: J. KLEINMANN  
DRAFTED BY: N. HOY  
DESIGNED BY: P. THOMPSON  
DESIGN SUPERVISOR: D. MARSH



ALTERED ON:

AFFIXED ON: 5/10/2024

SIGNATURE:  
STAMP:

**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

0 25 50 FEET  
SCALE 1" = 50'

<p><b>NEW YORK STATE OF OPPORTUNITY</b> <b>Thruway Authority</b></p>	<p>TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION</p> <p>LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2</p> <p>TITLE OF DRAWING: ROADWAY PLANS</p> <p>CONTRACT NUMBER: TAN 24-12</p> <p>DATE: MAY 2024</p> <p>DRAWING NUMBER: PL-30</p>
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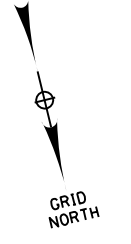
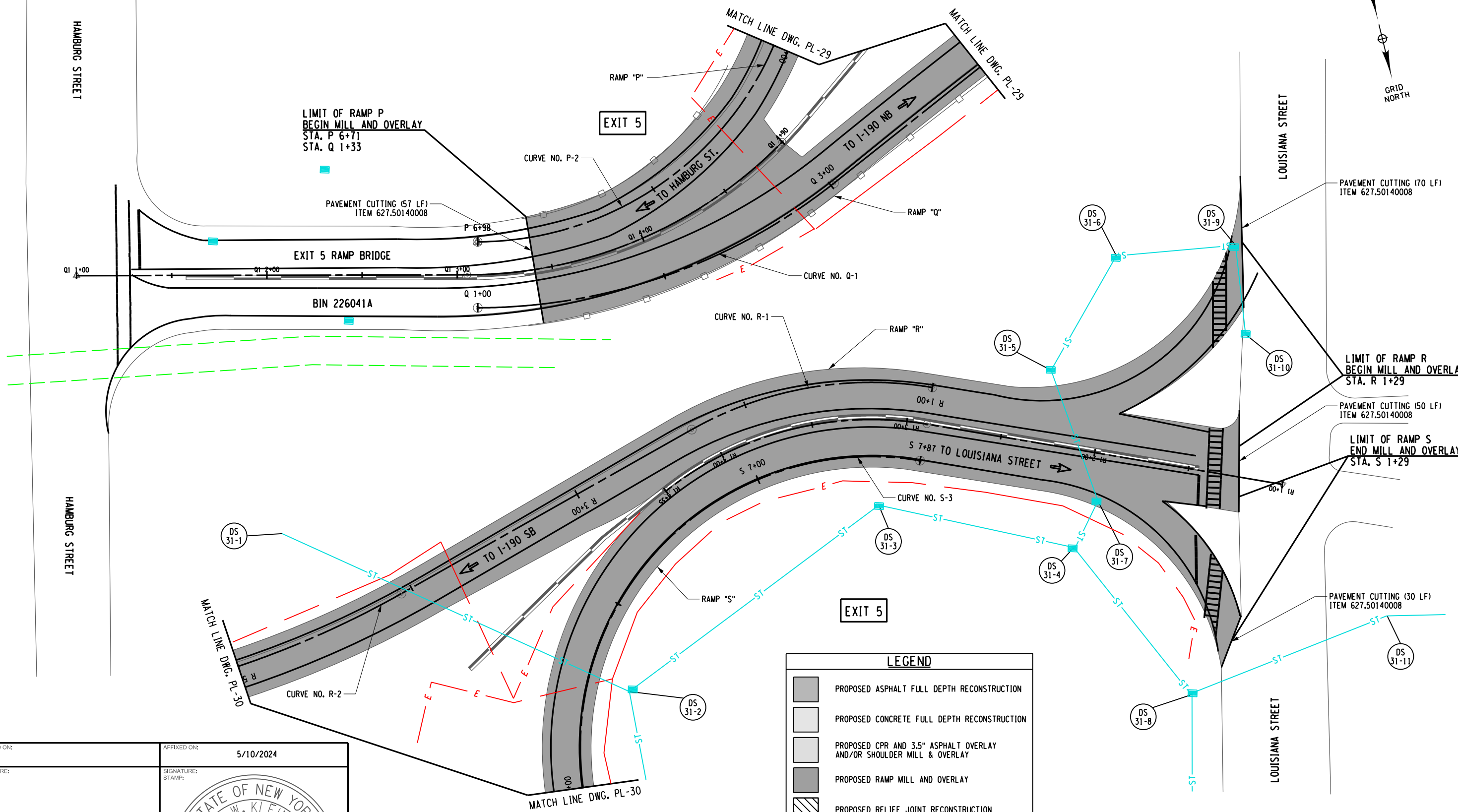
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



LIMIT OF RAMP P  
BEGIN MILL AND OVERLAY  
STA. P 6+71  
STA. Q 1+33

PAVEMENT CUTTING (57 LF)  
ITEM 627.50140008

EXIT 5 RAMP BRIDGE

BIN 226041A

EXIT 5

PAVEMENT CUTTING (70 LF)  
ITEM 627.50140008

LIMIT OF RAMP R  
BEGIN MILL AND OVERLAY  
STA. R 1+29

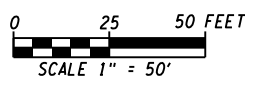
PAVEMENT CUTTING (50 LF)  
ITEM 627.50140008

LIMIT OF RAMP S  
END MILL AND OVERLAY  
STA. S 1+29

PAVEMENT CUTTING (30 LF)  
ITEM 627.50140008

**LEGEND**

- PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION
- PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION
- PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY
- PROPOSED RAMP MILL AND OVERLAY
- PROPOSED RELIEF JOINT RECONSTRUCTION
- DS  
X-X PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)



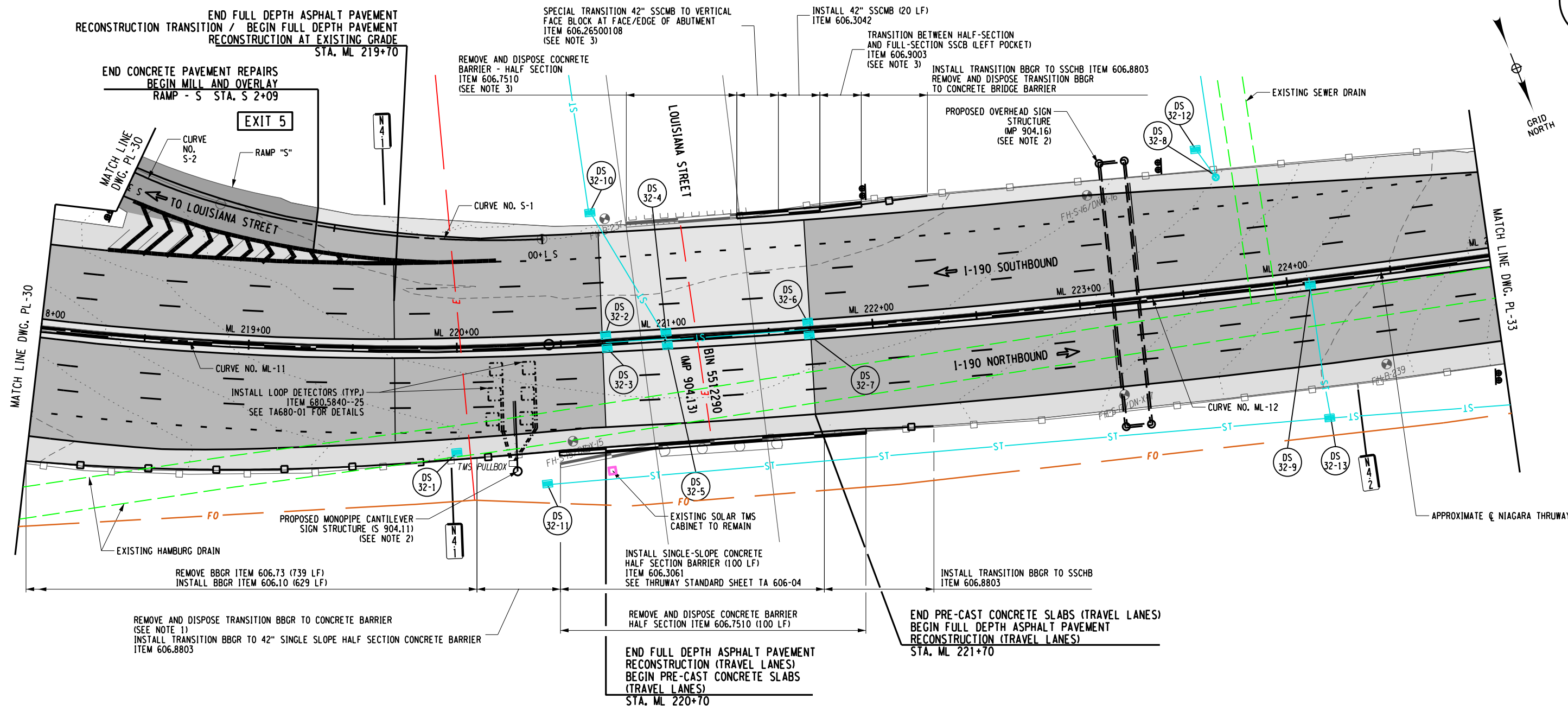
ALTERED ON: _____ SIGNATURE: _____ STAMP: _____	AFFIXED ON: <b>5/10/2024</b> SIGNATURE: _____ STAMP: _____

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING ROADWAY PLANS	DRAWING NUMBER: PL-31

CHECKED BY: J. KLEINMANN  
 DRAFTED BY: N. HOY  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: P. THOMPSON  
 DESIGN SUPERVISOR: D. MARSH



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

**LEGEND**

PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION	PROPOSED RAMP MILL AND OVERLAY
PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION	PROPOSED RELIEF JOINT RECONSTRUCTION
PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY	PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

**NOTES:**

- COST TO REMOVE BBGR TO HALF SECTION CONCRETE BARRIER INCLUDED IN ITEM 606.73 FOR THE RAILING AND ITEM 606.7501 FOR THE CONCRETE BARRIER.
- SEE DWGS SSP-16 FOR STRUCTURE LOCATION, SDS-4 AND SDS-5 FOR SIGN PANEL LAYOUT, AND OSP-21 TO OSP-24 FOR SIGN STRUCTURE DETAILS.
- SEE MD-10 FOR CONCRETE BARRIER REMOVAL AND SPECIAL TRANSITION DETAIL.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>	<b>TITLE OF PROJECT</b> I-190 HIGHWAY REHABILITATION	<b>CONTRACT NUMBER:</b> TAN 24-12
	<b>LOCATION OF PROJECT</b> I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	<b>DATE:</b> MAY 2024
	<b>TITLE OF DRAWING</b> ROADWAY PLANS	<b>DRAWING NUMBER:</b> PL-32

0 25 50 FEET  
SCALE 1" = 50'

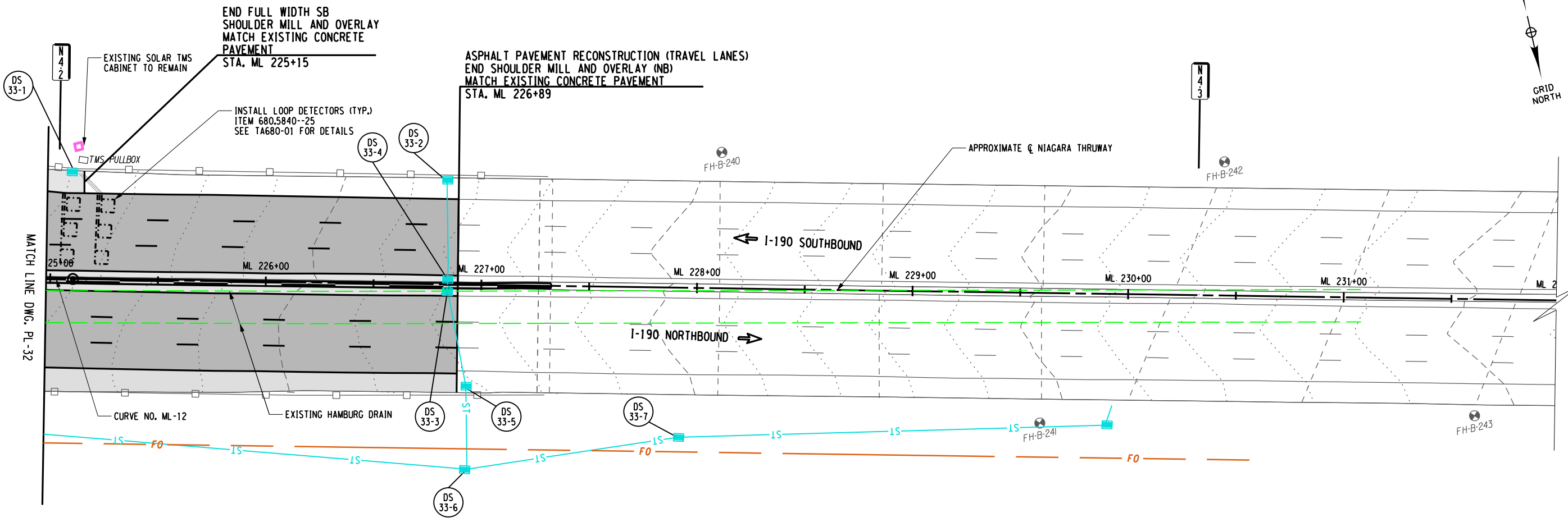
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



ALTERED ON:  
SIGNATURE:  
STAMP:

AFFIXED ON:  
5/10/2024  
SIGNATURE:  
STAMP:

**LEGEND**

	PROPOSED ASPHALT FULL DEPTH RECONSTRUCTION		PROPOSED RAMP MILL AND OVERLAY
	PROPOSED CONCRETE FULL DEPTH RECONSTRUCTION		PROPOSED RELIEF JOINT RECONSTRUCTION
	PROPOSED CPR AND 3.5" ASPHALT OVERLAY AND/OR SHOULDER MILL & OVERLAY		PROPOSED DRAINAGE STRUCTURE (SEE TABLES ON DWGS MT-5 TO MT-14)

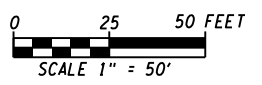
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2  
TITLE OF DRAWING  
ROADWAY PLANS

CONTRACT NUMBER:  
TAN 24-12  
DATE:  
MAY 2024  
DRAWING NUMBER:  
PL-33



GENERAL ELECTRICAL NOTES:

1. ALL ELECTRICAL WIRING CONDUCTORS SHALL BE NEW, 98% COPPER AND COLOR CODED WITH TYPE "THWN" INSULATION FOR 600 V RATED CONDUCTORS. WIRE SHALL BE INSTALLED IN 2" DIAMETER CONDUIT UNLESS OTHERWISE SPECIFIED. BELOW GRADE CONDUIT, SHALL BE RIGID PLASTIC CONDUIT; CONDUIT UNDER ROADWAYS SHALL BE GALVANIZED STEEL PLASTIC COATED 2" CONDUIT. CONDUIT SHALL BE BURIED A MINIMUM OF 24" BELOW GRADE UNLESS OTHERWISE SPECIFIED. PAYMENT WILL BE MADE UNDER ITEMS 206.03, 670.2602, 670.2306, 670.7004, 670.7501, & 680.53.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING THE EXTENT OF WORK REQUIRED AT THE EXISTING SERVICE DISTRIBUTION AREAS. SOME OF WHICH MAY NOT BE SHOWN IN THE CONTRACT DOCUMENTS. PRIOR TO SUBMITTING HIS BID AND PREPARE HIS BID ACCORDINGLY. PAYMENT FOR THE WORK AT THE SERVICE DISTRIBUTION AREAS SHALL BE PAID FOR UNDER ITEM 670.40230008.
3. ALL WORK SHALL BE IN ACCORDANCE WITH THE RULES AND REGULATIONS OF THE 2023 NATIONAL ELECTRIC CODE, NATIONAL SAFETY CODE, OSHA, NATIONAL GRID, NEW YORK STATE THRUWAY AUTHORITY AND ALL OTHER AGENCIES WHICH MAY HAVE JURISDICTION OVER WORK. THE ENGINEER SHALL RESOLVE ALL CONFLICTING REQUIREMENTS. ALL NEW AND TEMPORARY LIGHTING SHALL BE SUBJECT TO AN UNDERWRITERS INSPECTION AND APPROVAL.
4. THE LOCATIONS OF EXISTING SERVICE POINTS, LIGHTING, CONDUIT AND PULL BOXES SHOWN ON THE PLANS ARE APPROXIMATE ONLY. EXACT LOCATIONS SHALL BE FIELD VERIFIED BY NYSTA.
5. CONDUIT CONNECTED TO PULL BOXES SHALL BE SLOPED TO DRAIN THEM. NO DRAINS SHALL BE INSTALLED IN THE CONDUIT AND ALL CONDUIT RUNS SHALL BE WATERTIGHT AND KEPT FREE OF WATER BY THE CONTRACTOR AT ALL TIMES.
6. THE ENDS OF ALL CONDUIT RUNS SHALL BE PROVIDED WITH CAPPED BUSHINGS DURING CONSTRUCTION TO AVOID FILLING WITH DEBRIS; ETC.
7. ALL PROPOSED LIGHTING EQUIPMENT SHALL BE BONDED AND GROUNDED TO THE SYSTEM GROUNDING WIRE PLUS A DRIVEN GROUNDING ROD INSTALLED AT EACH LIGHT FIXTURE LOCATION EXCEPT WHEN ON A BRIDGE STRUCTURE IN WHICH CASE THE SUPPLEMENTARY GROUNDING ROD SHALL BE OMITTED.
8. ALL AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO ITS ORIGINAL CONDITION OR BETTER USING MATERIALS AND METHODS APPROVED BY THE ENGINEER. TURF AREAS SHALL BE RESTORED BY TOPSOILING (4") AND SEEDING. THE COST OF ALL RESTORATION WORK SHALL BE INCLUDED IN THE VARIOUS CONTRACT ITEMS.
9. ALL FIELD CUT CONDUIT ENDS SHALL BE REAMED TO PREVENT WIRE DAMAGE. CONDUIT ENDS TERMINATING IN PULL BOXES OR POLES SHALL BE FURNISHED WITH BUSHINGS. NO CONDUIT BENDS SHALL BE MADE WITH RADIUS SMALLER THAN RECOMMENDED BY THE MANUFACTURER. ALL BENDS SHALL BE MADE WITH TOOLS AND METHODS RECOMMENDED BY THE MANUFACTURER. EXPOSED CONDUIT SHALL BE SUPPORTED AT LEAST EVERY THREE FEET.
10. SPLICES THAT MAY BECOME NECESSARY IN CIRCUIT WORK AT POLE BASES AND PULLBOXES SHALL BE MADE WITH APPROVED SOLDERLESS CONNECTORS. UNDERGROUND SPLICES SHALL BE MADE WITH WATERPROOF SPLICE KITS -- EPOXY TYPE AS MANUFACTURED BY 3M OR APPROVED EQUAL.
11. THE COST OF ALL WIRING FROM THE POLE BASE TO THE LUMINAIRE, INCLUDING SPLICING, FUSES AND OTHER LABOR AND MATERIAL, SHALL BE INCLUDED IN THE PRICE BID FOR RESPECTIVE WIRE ITEMS.
12. THE CONTRACTOR SHALL MAINTAIN THE NEW LIGHTING SYSTEM UNTIL FINAL ACCEPTANCE OF THE CONTRACT. PAYMENT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE VARIOUS LIGHTING ITEMS.
13. CONDUITS INSTALLED IN BRIDGE RAILING CONCRETE SECTIONS SHALL TERMINATE A MINIMUM OF 2 FT BEYOND THE END OF THE BRIDGE RAILING OUTSIDE OF THE PAVED SURFACE AND A MINIMUM OF 2 FT UNDER THE GROUND SURFACE.
14. HOT DIPPED GALVANIZED, MALLEABLE PIPE STRAPS AND SPACERS SHALL BE USED TO ATTACH CONDUIT TO BRIDGE STRUCTURES. GALVANIZED STEEL CONDUIT HANGERS OR PIPE CLAMPS WILL NOT BE ALLOWED. PIPE STRAPS OF THE PROPER SIZE SHALL BE INSTALLED 4 FT CENTER TO CENTER ALONG THE CONDUIT. WHEN FASTENING PIPE STRAPS TO CONCRETE, A 3/8 IN. BY 2 1/2 IN. GALVANIZED STEEL LAG SCREW, WITH AN APPROVED SLEEVE, SHALL BE USED; HOWEVER, OTHER APPROVED EXPANSION ANCHORS MAY BE USED. THE PIPE STRAP AND SPACER SHALL BE BOLTED TO THE STEEL BEAMS IF APPLICABLE.  
  
EXPANSION FITTINGS SHALL BE INSTALLED WHERE CONDUIT CROSSES AN EXPANSION JOINT IN THE STRUCTURE. WHERE IT IS DEEMED INADVISABLE TO INSTALL EXPANSION FITTINGS IN CLOSELY CONFINED AREAS, THE INSTALLATION OF APPROVED FLEXIBLE TUBING MAY BE ALLOWED. SUCH EXPANSION JOINTS OR TUBING SHALL BE THE SAME SIZE AS THE CONDUIT.
15. CONTRACTOR SHALL REMOVE AND DISPOSE OF THE EXISTING LIGHTING WIRE FROM THE PREVIOUS MEDIAN LIGHTING UNDER ITEM 670.94120004. THE EXISTING ANCHOR BOLTS PROTRUDING FROM THE BARRIER SHALL BE CUT AND THE EXISTING CONDUITS CAPPED. COST INCLUDED IN ITEM 670.94120004.
16. CONTRACTOR SHALL SUPPLY AS-BUILT LOCATIONS OF ALL PULLBOXES, LIGHT POLES, AND LIGHTING EQUIPMENT. COST INCLUDED IN THE PRICE BID FOR ITEM 625.01.
17. CONTRACTOR SHALL INSTALL POLE ID TAGS ON ALL NEW MAINLINE AND EXISTING RAMP LIGHT POLES. THE ID TAGS SHALL BE REFLECTIVE STICKERS, 3 INCHES HIGH, BLACK NUMBERS WITH A WHITE BACKGROUND, PLACED 10 FT ABOVE THE BASE OF THE LIGHT POLE AND VISIBLE FROM THE SHOULDER IN THE DIRECTION OF TRAVEL. NUMBERING OF THE LIGHT POLES SHALL START FROM THE POLES NEAR EXIT 1 AND BE NUMBERED SEQUENTIALLY AS FOLLOWS: I-190 NB SHALL BE EVEN NUMBERS STARTING AT 02. I-190 SB SHALL BE ODD NUMBERS STARTING AT 01. CONTRACTOR SHALL SUBMIT A SAMPLE TO THE AUTHORITY FOR APPROVAL PRIOR TO ORDERING THE POLE ID TAG STICKERS. COST TO FURNISH AND INSTALL THE POLE ID TAGS IS INCLUDED IN THE PRICE BID FOR ITEM 670.1540.

SERVICE POINT PANELBOARD SCHEDULE

SERVICE POINT (SEE PLANS)	CIRCUIT NUMBER	100 AMP, 1 PHASE, 3 WIRE, 120/240 VOLTS			MINIMUM BRANCH CIRCUIT WIRE SIZE	
		BREAKERS				LUMINAIRE NUMBER SERVED OR REMARKS
		FRAME	POLES	TRIP		
"OG" AT SOUTH ODGEN STREET	OG-1		2	20	13, 15, 17, 19, 21, 23, 25, 26, 28, 30, 32, 34, 36, 38, 40, W30	*6
	OG-2		2	20	14, 16, 18, 20, 22, 24, 27, 29, 31, 33, 35, W31	*6
	OG-3		1	20	1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12	*12
	OG-4				CONVENIENCE OUTLET (EXISTING)	
			2	100	MAIN DISCONNECT (EXISTING) 240/120V 1PH 100A MCB PANEL	*2
"CL" AT CLINTON STREET	CL-1		2	20	37, 39, 41, 42, 43, 44, 45, 46, 47, 48	*6
	CL-2		2	20	CONVENIENCE OUTLET (EXISTING)	*6
			2	100	MAIN DISCONNECT (EXISTING) 240/120V 1PH 100A MCB PANEL	*2
"KE" AT MILTON STREET	KE-1		2	20	49, 50, 51, 52, 54, 55, 56, 58, 59	*6
	KE-2		2	20	57, 60, 61, 62, 63	*6
	KE-3		2	20	64, 65, 66, 68	*6
	KE-4		2	20	67, 69, 70, 72, 73, 74, 75, 76, 77, 78, 79	*6
	KE-5		1	20	CONVENIENCE OUTLET (EXISTING)	*12
			2	100	MAIN DISCONNECT (EXISTING) 240/120V 1PH 100A MCB PANEL	*2
"SM" AT SMITH STREET	SM-1		2	20	80, 81, 82, 83, 84, 85, 86, 88	*6
	SM-2		2	20	87, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100, 101	*6
	SM-3		1	20	CONVENIENCE OUTLET (EXISTING)	*12
		2	100	MAIN DISCONNECT (EXISTING) 240/120V 1PH 100A MCB PANEL	*1/0	
"LO" AT LOUISIANA STREET	LO-1		2	20	102, 103, 104, 105, 109, 113, 115	*6
	LO-2		2	20	106, 107, 108, 110, 111, 112, 114, 116	*6
	LO-3		1	20	CONVENIENCE OUTLET (EXISTING)	*12
		2	100	MAIN DISCONNECT (EXISTING) 240/120V 1PH 100A MCB PANEL	*1/0	

CHECKED BY: J. KLEINMANN

DRAFTED BY: T. HANSEN

CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

JOSEPH W. KLEINMANN  
LICENSED PROFESSIONAL ENGINEER  
EXCELSIOR  
092577

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE. THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS				NEW YORK STATE OF OPPORTUNITY Thruway Authority 2024 Mark Year Tower GMA ©2024 GMA. All Rights Reserved. 518.465.4600 • www.thruwayauthority.com	TITLE OF PROJECT	CONTRACT NUMBER:
DATE	DESCRIPTION	BY	SYM.		I-190 HIGHWAY REHABILITATION	TAN 24-12
					LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
					TITLE OF DRAWING	DRAWING NUMBER:
					LIGHTING DETAILS	LD-1

CHECKED BY: J. KLEINMANN

DRAFTED BY: T. HANSEN

CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH

PROPOSED LIGHTING SCHEDULE

Table with columns: POLE NUMBER, STATION, OFFSET, 670.0107 EA, 670.1540 EA, 670.1718 EA, 670.1720 EA, 670.19 EA, 670.5075--25 EA, TYPE 3, TYPE 4, SERVICE POINT, CIRCUIT. Rows LP-1 to LP-55.

PROPOSED LIGHTING SCHEDULE - CONTINUED

Table with columns: POLE NUMBER, STATION, OFFSET, 670.0107 EA, 670.1540 EA, 670.1718 EA, 670.1720 EA, 670.19 EA, 670.5075--25 EA, TYPE 3, TYPE 4, SERVICE POINT, CIRCUIT. Rows LP-56 to LP-116 and MISCELLANEOUS.

PROPOSED PULL BOX SCHEDULE ITEM 670.3001

Table with columns: PULL BOX NUMBER, STATION, OFFSET, NO. OF BOXES ITEM 670.3001. Rows PB-1 to PB-46.

PROPOSED PULL BOX SCHEDULE - CONTINUED ITEM 670.3001

Table with columns: PULL BOX NUMBER, STATION, OFFSET, NO. OF BOXES ITEM 670.3001. Rows PB-47 to PB-75 and TOTAL.

NOTE:

1. THIS PROJECT WAS DESIGNED USING A 187 WATT LED LUMINAIRE WITH TYPE 3 AND 4 IES DISTRIBUTIONS.

ALTERED ON: [Blank] AFFIXED ON: 5/10/2024. SIGNATURE: [Blank] STAMP: [Professional Engineer Seal for Joseph W. Kleinmann, License No. 092577].

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Table with columns: DATE, DESCRIPTION, BY, SYM. Header: REVISIONS.

NEW YORK STATE OF OPPORTUNITY logo and Thruway Authority logo.

Table with columns: TITLE OF PROJECT, LOCATION OF PROJECT, TITLE OF DRAWING, CONTRACT NUMBER, DATE, DRAWING NUMBER. Values: I-190 HIGHWAY REHABILITATION, I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2, LIGHTING DETAILS, TAN 24-12, MAY 2024, LD-2.

Table with columns: ITEM, DESCRIPTION, UNITS. Rows: 670.0107 FOUNDATION FOR LIGHT STANDARDS, 7 FEET LONG EA; 670.1540 ALUMINUM LIGHT STANDARDS FOR SINGLE DAVIT ARM, 40 FEET MOUNTING HEIGHT EA; 670.1718 ALUMINUM DAVIT ARM, 18 FEET LONG EA; 670.1720 ALUMINUM DAVIT ARM, 20 FEET LONG EA; 670.19 BREAKAWAY TRANSFORMER BASE (ALUMINUM) EA; 670.3001 PULLBOXES LESS THAN 5 CUBIC FEET, INSIDE VOLUME (LIGHTING) EA; 670.5075-25 LUMINAIRE - LED TYPE - HIGHWAY EA.

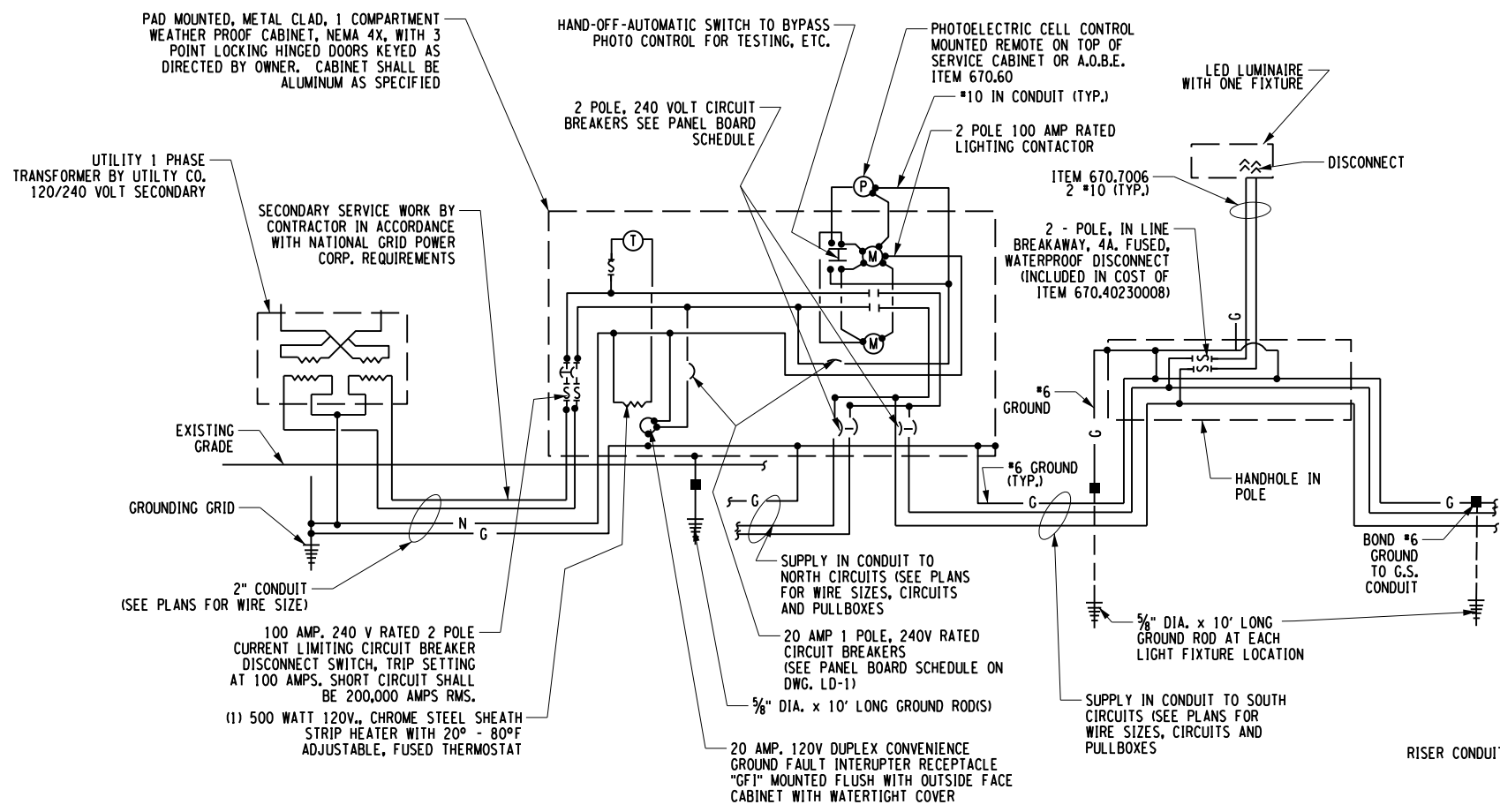
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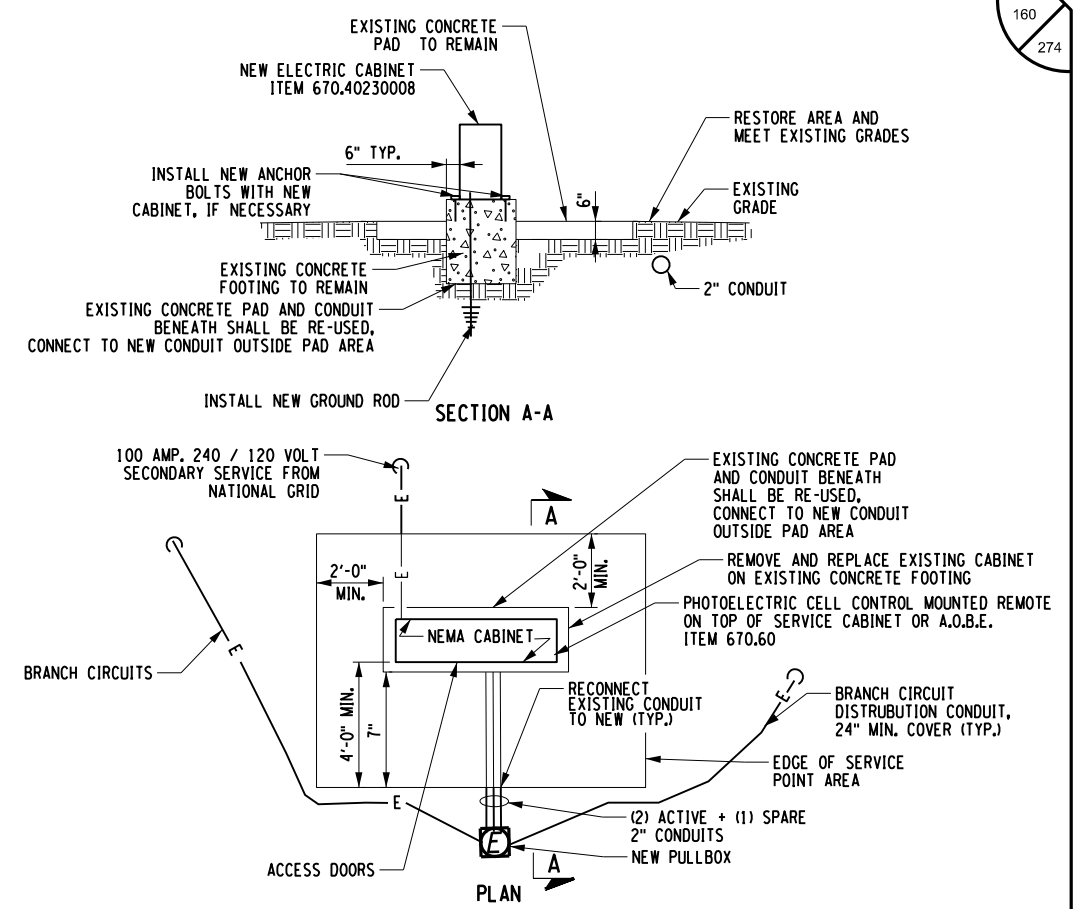
CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH

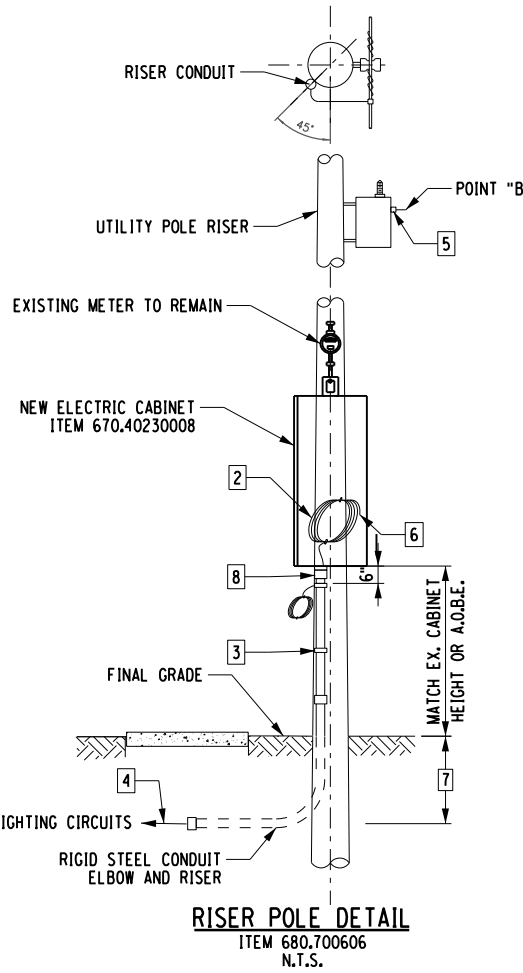


TYPICAL CIRCUIT BOARD SCHEMATIC  
N.T.S.



TYPICAL DETAIL - ELECTRICAL SERVICE POINT  
N.T.S.

- NOTES:
- GROUND MOUNTED POLES NOT SHOWN.
  - ALL WIRE SHALL BE "THWN" INSULATED AND SHALL BE IN RIGID PLASTIC 2" CONDUIT EXCEPT ABOVE GRADE WIRE SHALL BE IN GALVANIZED STEEL PLASTIC COATED 2" CONDUIT.
  - SPLICING SHALL BE DONE IN THE PULLBOX. NO SEPARATE FUSING WILL BE REQUIRED. SEE PLANS AND PULLBOX SCHEDULE ON DWG. LD-2.
  - ATTACH CONDUIT TO PIER AND ABUTMENT WITH GALV. STEEL 2 HOLE HEAVY DUTY CLAMPS 3" O.C. SECURE CLAMPS TO STRUCTURE USING CONCRETE EXPANSION ANCHORS MEETING THE REQUIREMENTS OF N.Y.S.D.O.T. STANDARD SPECIFICATION 567-2.01. THE COST SHALL BE INCLUDED IN THE PRICE BID FOR CONDUIT.
  - PULLBOXES SHALL BE IN ACCORDANCE WITH NYS DOT STANDARD SHEETS 680-02 AND 680-04. PULLBOX COVERS SHALL HAVE THE WORDS "NYSTA LIGHTING" CAST INTO COVERS.
  - COST TO CONNECT THE PROPOSED CIRCUIT TO THE PROPOSED POWER/SERVICE CABINET AND ANY ASSOCIATED WORK NOT IDENTIFIED WITH OTHER ITEMS SHALL BE PAID FOR UNDER ITEM 670.40230008.



RISER POLE DETAIL  
ITEM 680.700606  
N.T.S.

- CODED NOTES
- UTILITY COMPANY'S SECONDARY RACK-POINT "A".
  - SERVICE LATERAL CABLE TO BE FURNISHED AND INSTALLED BY CUSTOMER. IT MUST BE LONG ENOUGH TO EXTEND 3 FT. (900 MM) ABOVE POINT "A" OR "B" WITHOUT A SPLICE.
  - PIPE STRAPS, INSTALL AT NOT MORE THAN 30" (750 MM) INTERVALS.
  - SERVICE LATERAL TO LIGHTING CIRCUITS.
  - UTILITY COMPANY'S TRANSFORMER SECONDARY TERMINALS -- POINT "B".
  - SEAL CABLE ENDS TO PREVENT ENTRANCE OF MOISTURE DURING CONSTRUCTION.
  - THE BURIAL DEPTH SHALL BE 30" MINIMUM.
  - INSULATING BUSHING

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING LIGHTING DETAILS	DRAWING NUMBER: LD-3



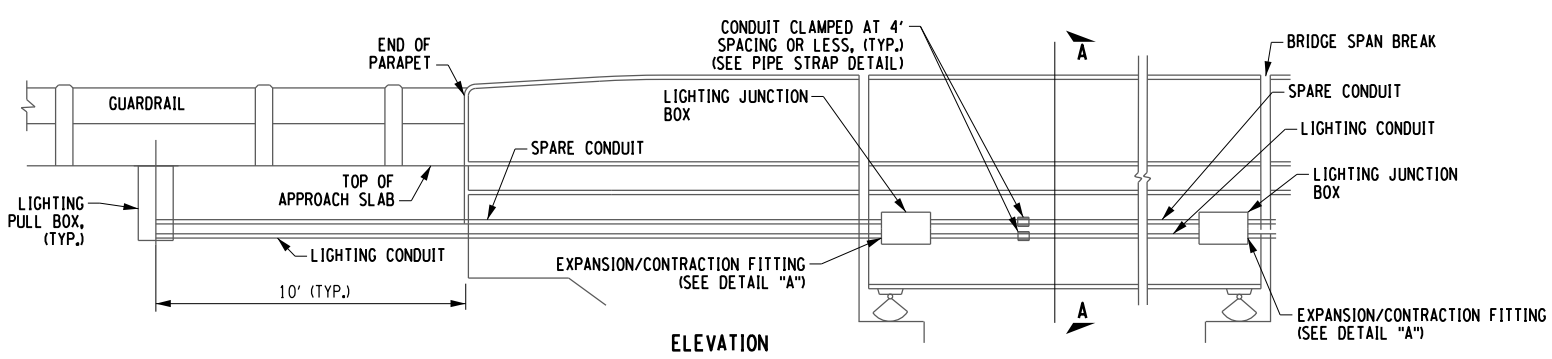
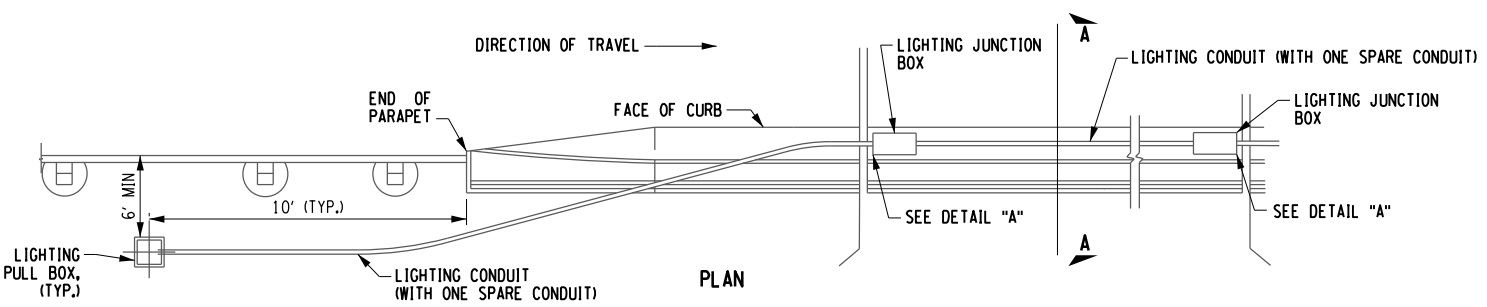
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DRAFTED BY: T. HANSEN

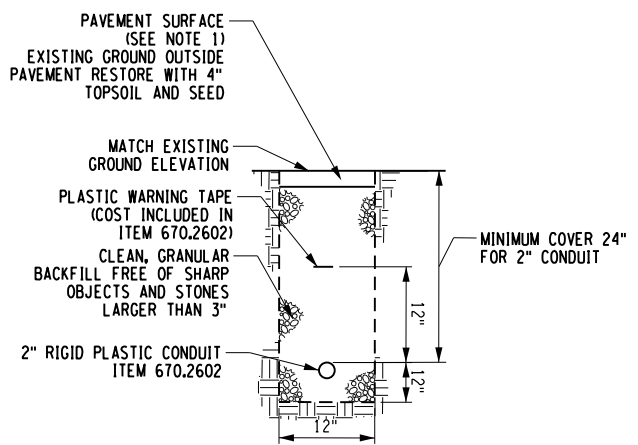
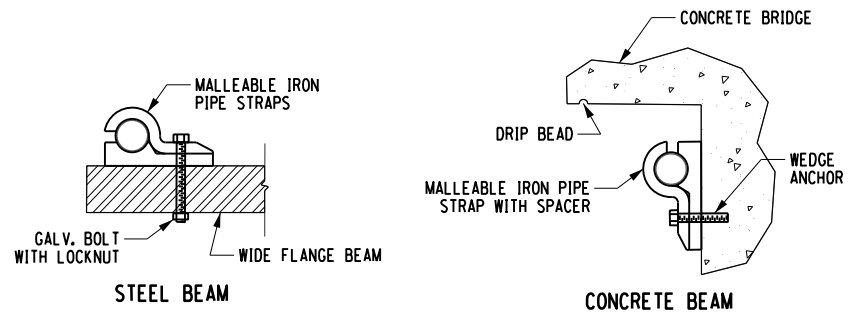
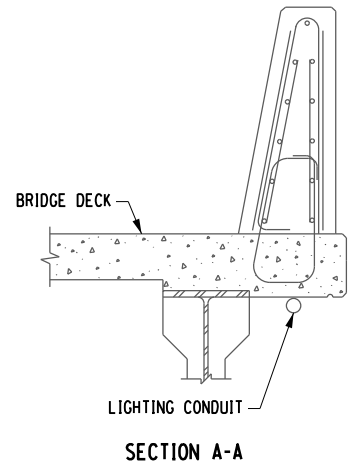
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DESIGNED BY: T. ANDREWS

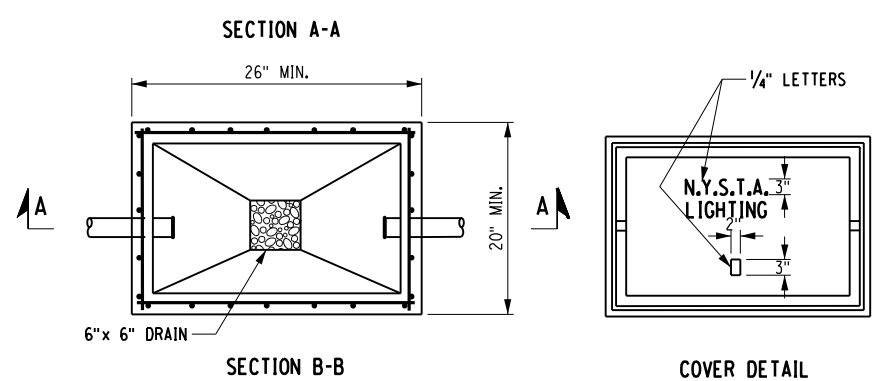
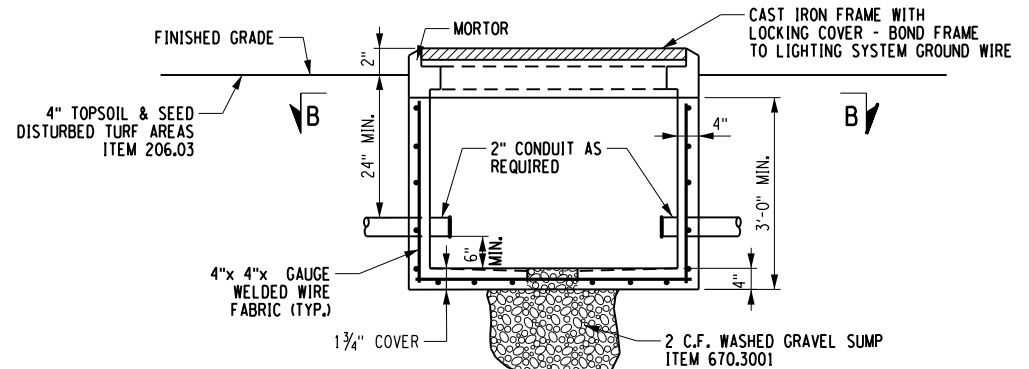
DESIGN SUPERVISOR: D. MARSH



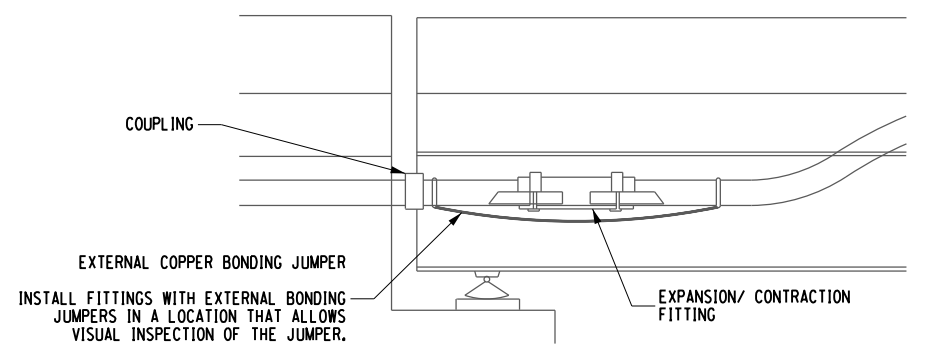
**CONDUIT PLACEMENT**  
N.T.S.



**NOTE:**  
1. WHEN LIGHTING CONDUIT IS INSTALLED USING TRENCHLESS METHODS BENEATH ROADWAY PAVEMENTS, CONDUIT SHALL BE GALVANIZED STEEL PLASTIC COATED CONDUIT, ITEM 670.2306.



**TYPICAL PULLBOX DETAIL**  
ITEM 670.3001  
N.T.S.



**CONDUIT EXPANSION/CONTRACTION FITTING**

EXPANSION/CONTRACTION FITTINGS FOR CONDUIT ON STRUCTURE SHALL BE TYPE OZ AXDX OR CROUSE HINDS XJGD. EACH FITTING SHALL HAVE AN EXTERNAL BONDING JUMPER.

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NEW YORK STATE OF OPPORTUNITY

Thruway Authority

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TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING LIGHTING DETAILS	DRAWING NUMBER: LD-4

CHECKED BY: J. KLEINMANN

DRAFTED BY: T. HANSEN

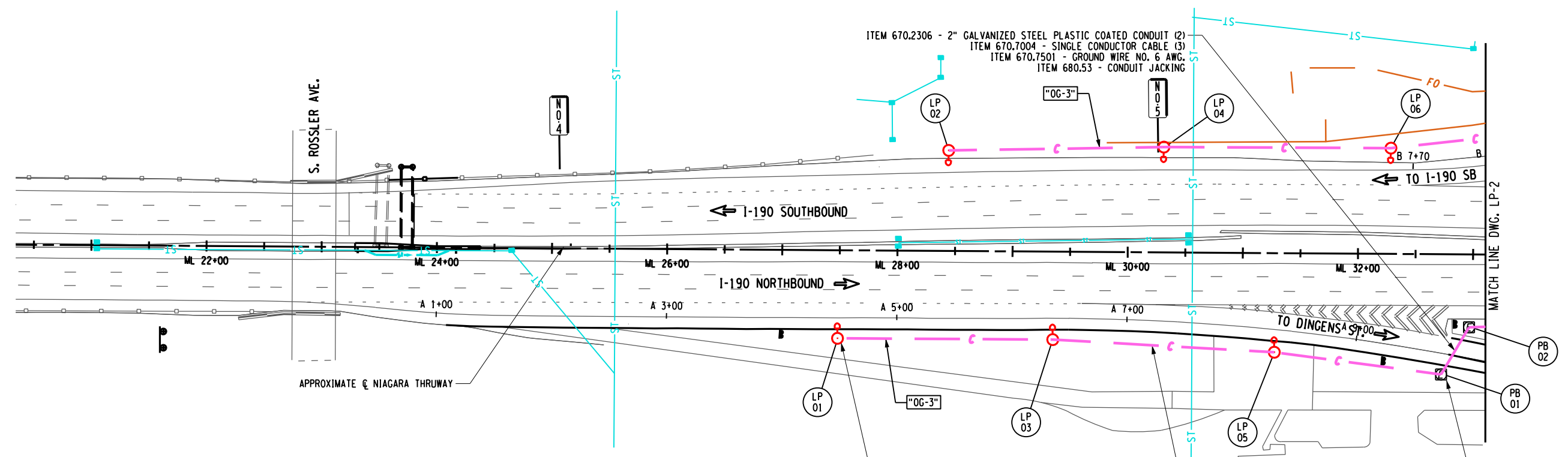
CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH

162  
274

GRID NORTH



LIGHTING LEGEND	
	EXISTING SERVICE UTILITY POLE FEED
	EXISTING LIGHT POLE
	EXISTING BELOW DECK LIGHT
	EXISTING FLUSH MEDIAN PULLBOX
	EXISTING GROUND PULLBOX
	EXISTING SERVICE PANEL
	EXISTING UNDERGROUND LIGHTING CONDUIT
	PROPOSED LIGHT POLE NUMBER
	PROPOSED PULLBOX NUMBER (SEE DWG. LD-2 FOR APPROXIMATE LOCATION)
	PROPOSED NEW ALUMINUM LIGHT STANDARD
	PROPOSED FLUSH-MOUNTED MEDIAN PULLBOX REHABILITATION WORK
	PROPOSED GROUND PULLBOX ITEM 670.3001
	PROPOSED CURCUIT NUMBER
	PROPOSED 2" RIGID PLASTIC CONDUITS (TRENCHED) (1 PRIMARY CONDUIT WITH 1 SPARE CONDUIT) ITEM 670.2602 UNLESS OTHERWISE NOTED
	PROPOSED CONDUITS AND WIRE CABLES ACROSS BRIDGES AND VIADUCTS (1 PRIMARY CONDUIT WITH 1 SPARE CONDUIT)

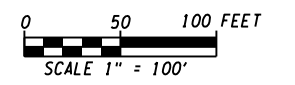
ITEM 670.2306 - 2" GALVANIZED STEEL PLASTIC COATED CONDUIT (2)  
 ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
 ITEM 670.7501 - GROUND WIRE NO. 6 AWG.  
 ITEM 680.53 - CONDUIT JACKING

ITEM 670.0107 - LIGHT FOUNDATION  
 ITEM 670.1540 - ALUMINUM LIGHT STANDARD  
 ITEM 670.17XX - ALUMINUM DAVIT ARM  
 (TYP.) SEE NOTE 4

ITEM 206.03 - CONDUIT EXCAVATION  
 ITEM 670.2602 - 2" RIGID PLASTIC CONDUIT (2)  
 ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
 ITEM 670.7501 - GROUND WIRE NO. 6 AWG.  
 (TYP.)

ITEM 670.3001 - PULLBOX (TYP.)

- NOTES:
- SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.
  - ALL CONDUIT CROSSING BENEATH RAMP AND MAINLINE SHALL BE PLACED BY TRENCHLESS METHODS (ITEM 680.53). (OPEN CUTTING WILL NOT BE ALLOWED) AND ALL ROADWAY PAVEMENT CROSSINGS SHALL CONSIST OF 2" GALVANIZED STEEL (PLASTIC COATED CONDUIT (ITEM 670.2306).
  - THE CONTRACTOR SHALL INTERCEPT THE PROPOSED CONDUITS WITH THE EXISTING CONDUITS OR PULL BOXES AT THE EXISTING SERVICE POINT. UNUSED EXISTING CONDUIT AND CABLING LEADING AWAY FROM THE SERVICE POINT SHALL BE REMOVED. THE COST OF THE INTERCEPT SHALL BE INCLUDED IN THE COST OF THE CONDUIT, 670.2602.
  - SEE LD-2 FOR POLE LOCATIONS AND ITEM NUMBERS.



ALTERED ON:	AFFIXED ON: 5/10/2024
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JOSEPH W. KLEINMANN  
 LICENSED PROFESSIONAL ENGINEER  
 092577

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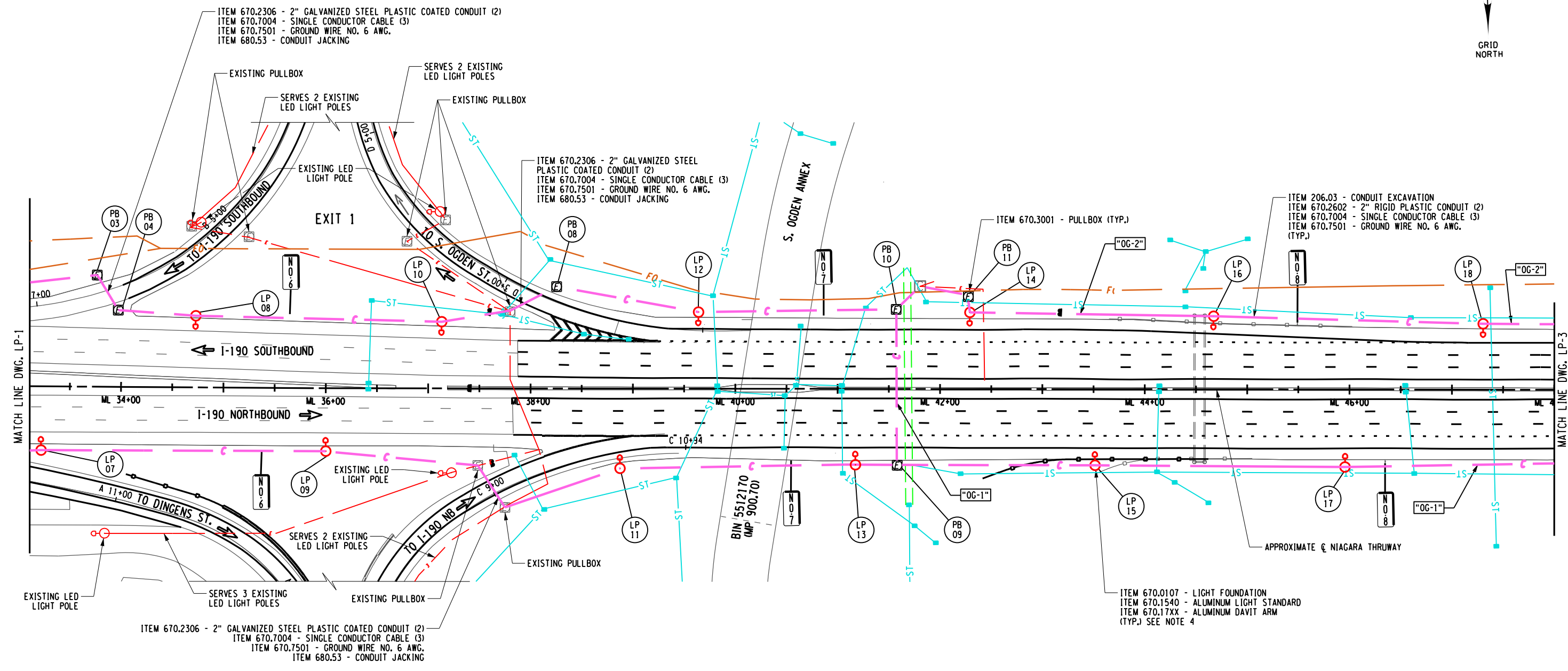
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DATE	DESCRIPTION	BY	SYM.

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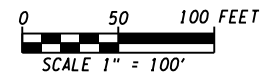
TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING LIGHTING PLANS	DRAWING NUMBER: LP-1



CHECKED BY: J. KLEINMANN  
 DRAFTED BY: T. HANSEN  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: T. ANDREWS



- NOTES:**
- SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.
  - ALL CONDUIT CROSSING BENEATH RAMPS AND MAINLINE SHALL BE PLACED BY TRENCHLESS METHODS (ITEM 680.53). (OPEN CUTTING WILL NOT BE ALLOWED) AND ALL ROADWAY PAVEMENT CROSSINGS SHALL CONSIST OF 2" GALVANIZED STEEL (PLASTIC COATED CONDUIT (ITEM 670.2306).
  - THE CONTRACTOR SHALL INTERCEPT THE PROPOSED CONDUITS WITH THE EXISTING CONDUITS OR PULL BOXES AT THE EXISTING SERVICE POINT. UNUSED EXISTING CONDUIT AND CABLING LEADING AWAY FROM THE SERVICE POINT SHALL BE REMOVED. THE COST OF THE INTERCEPT SHALL BE INCLUDED IN THE COST OF THE CONDUIT, 670.2602.
  - SEE LD-2 FOR POLE LOCATIONS AND ITEM NUMBERS.



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STATE OF NEW YORK  
 JOSEPH W. KLEINMANN  
 LICENSED PROFESSIONAL ENGINEER  
 092577

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING LIGHTING PLANS	DRAWING NUMBER: LP-2

GRID NORTH

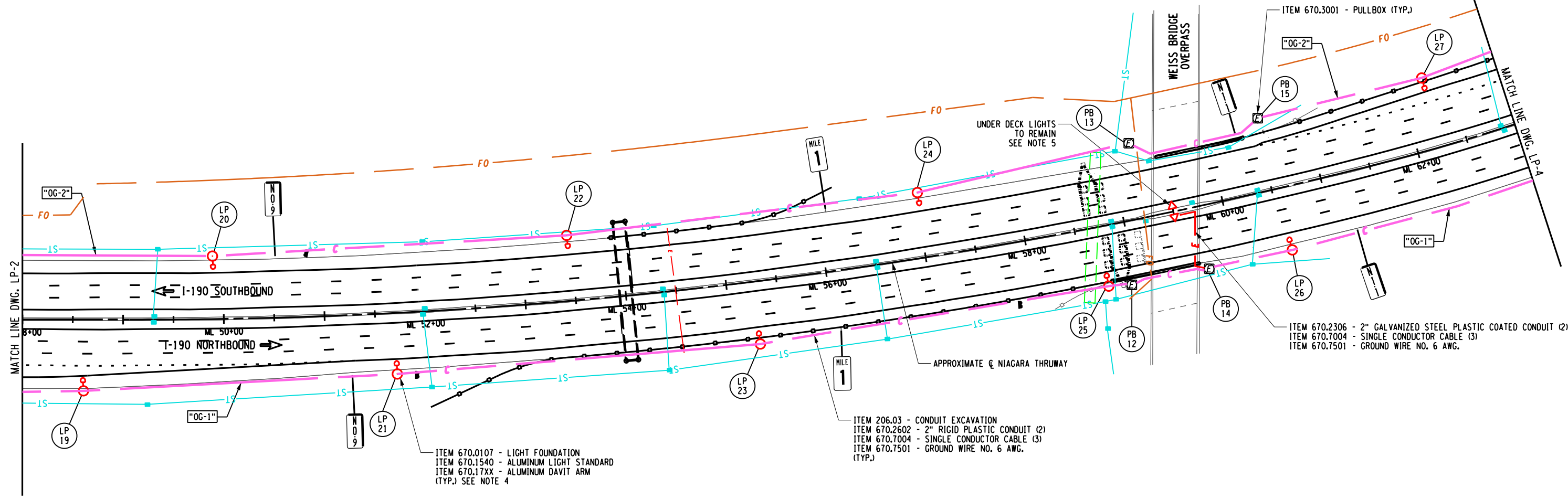
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DRAFTED BY: T. HANSEN

CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH



ITEM 670.0107 - LIGHT FOUNDATION  
 ITEM 670.1540 - ALUMINUM LIGHT STANDARD  
 ITEM 670.17XX - ALUMINUM DAVIT ARM  
 (TYP.) SEE NOTE 4

ITEM 206.03 - CONDUIT EXCAVATION  
 ITEM 670.2602 - 2" RIGID PLASTIC CONDUIT (2)  
 ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
 ITEM 670.7501 - GROUND WIRE NO. 6 AWG.  
 (TYP.)

ITEM 670.3001 - PULLBOX (TYP.)  
 ITEM 670.2306 - 2" GALVANIZED STEEL PLASTIC COATED CONDUIT (2)  
 ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
 ITEM 670.7501 - GROUND WIRE NO. 6 AWG.

- NOTES:
- SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.
  - ALL CONDUIT CROSSING BENEATH RAMPS AND MAINLINE SHALL BE PLACED BY TRENCHLESS METHODS (ITEM 680.53). (OPEN CUTTING WILL NOT BE ALLOWED) AND ALL ROADWAY PAVEMENT CROSSINGS SHALL CONSIST OF 2" GALVANIZED STEEL (PLASTIC COATED CONDUIT (ITEM 670.2306).
  - THE CONTRACTOR SHALL INTERCEPT THE PROPOSED CONDUITS WITH THE EXISTING CONDUITS OR PULL BOXES AT THE EXISTING SERVICE POINT, UNUSED EXISTING CONDUIT AND CABLING LEADING AWAY FROM THE SERVICE POINT SHALL BE REMOVED. THE COST OF THE INTERCEPT SHALL BE INCLUDED IN THE COST OF THE CONDUIT, 670.2602.
  - SEE LD-2 FOR POLE LOCATIONS AND ITEM NUMBERS.
  - THE EXISTING UNDERPASS LUMINAIRES ON THE WEISS BRIDGE OVERPASS SHALL BE CONNECTED TO THE "OG" CIRCUIT AS SHOWN ON THE PLANS.



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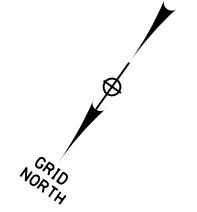
JOSEPH W. KLEINMANN  
 LICENSED PROFESSIONAL ENGINEER  
 092577

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING LIGHTING PLANS	DRAWING NUMBER: LP-3



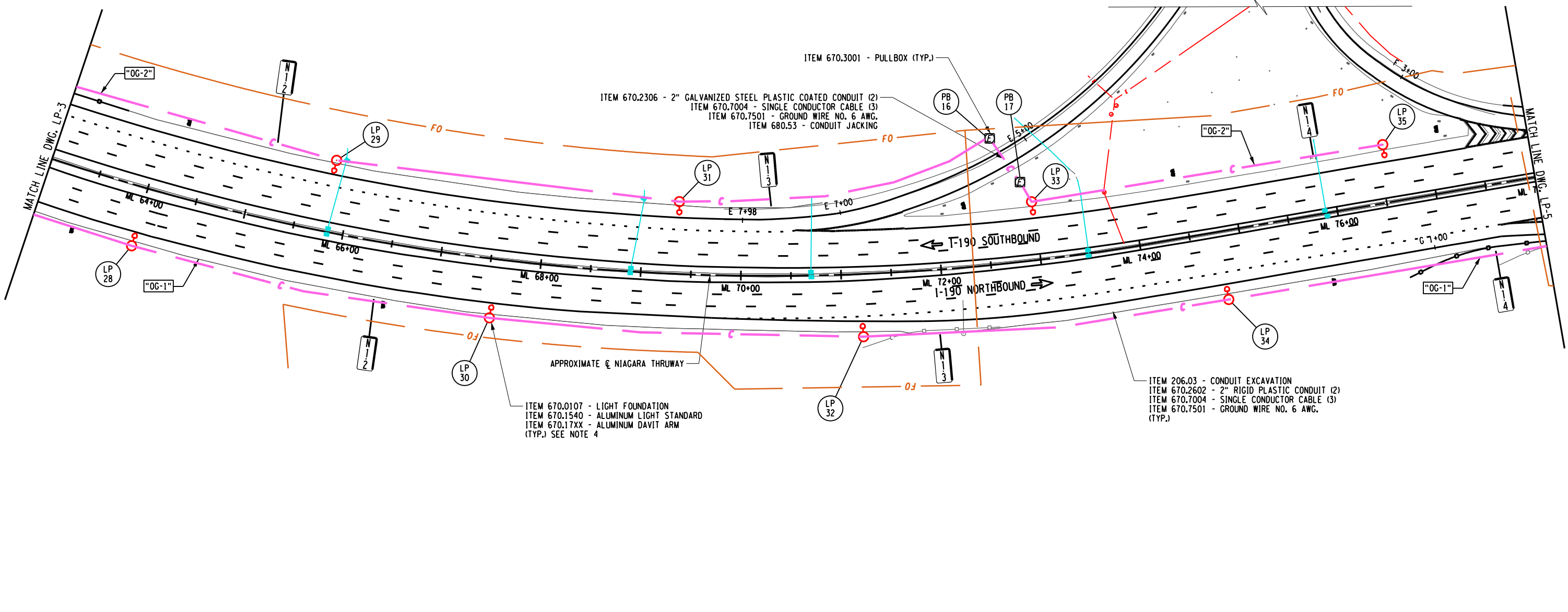
CHECKED BY: J. KLEINMANN

DRAFTED BY: T. HANSEN

CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH



ITEM 670.3001 - PULLBOX (TYP.)  
 ITEM 670.2306 - 2" GALVANIZED STEEL PLASTIC COATED CONDUIT (2)  
 ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
 ITEM 670.7501 - GROUND WIRE NO. 6 AWG.  
 ITEM 680.53 - CONDUIT JACKING

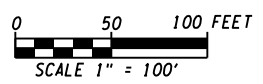
ITEM 670.0107 - LIGHT FOUNDATION  
 ITEM 670.1540 - ALUMINUM LIGHT STANDARD  
 ITEM 670.17XX - ALUMINUM DAVIT ARM  
 (TYP.) SEE NOTE 4


ITEM 206.03 - CONDUIT EXCAVATION  
 ITEM 670.2602 - 2" RIGID PLASTIC CONDUIT (2)  
 ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
 ITEM 670.7501 - GROUND WIRE NO. 6 AWG.  
 (TYP.)

**NOTES:**

1. SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.
2. ALL CONDUIT CROSSING BENEATH RAMPS AND MAINLINE SHALL BE PLACED BY TRENCHLESS METHODS (ITEM 680.53). OPEN CUTTING WILL NOT BE ALLOWED) AND ALL ROADWAY PAVEMENT CROSSINGS SHALL CONSIST OF 2" GALVANIZED STEEL (PLASTIC COATED CONDUIT (ITEM 670.2306).
3. THE CONTRACTOR SHALL INTERCEPT THE PROPOSED CONDUITS WITH THE EXISTING CONDUITS OR PULL BOXES AT THE EXISTING SERVICE POINT. UNUSED EXISTING CONDUIT AND CABLING LEADING AWAY FROM THE SERVICE POINT SHALL BE REMOVED. THE COST OF THE INTERCEPT SHALL BE INCLUDED IN THE COST OF THE CONDUIT, 670.2602.
4. SEE LD-2 FOR POLE LOCATIONS AND ITEM NUMBERS.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

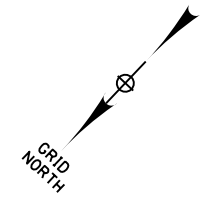


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REVISIONS			
DATE	DESCRIPTION	BY	SYM.




TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING LIGHTING PLANS	DRAWING NUMBER: LP-4



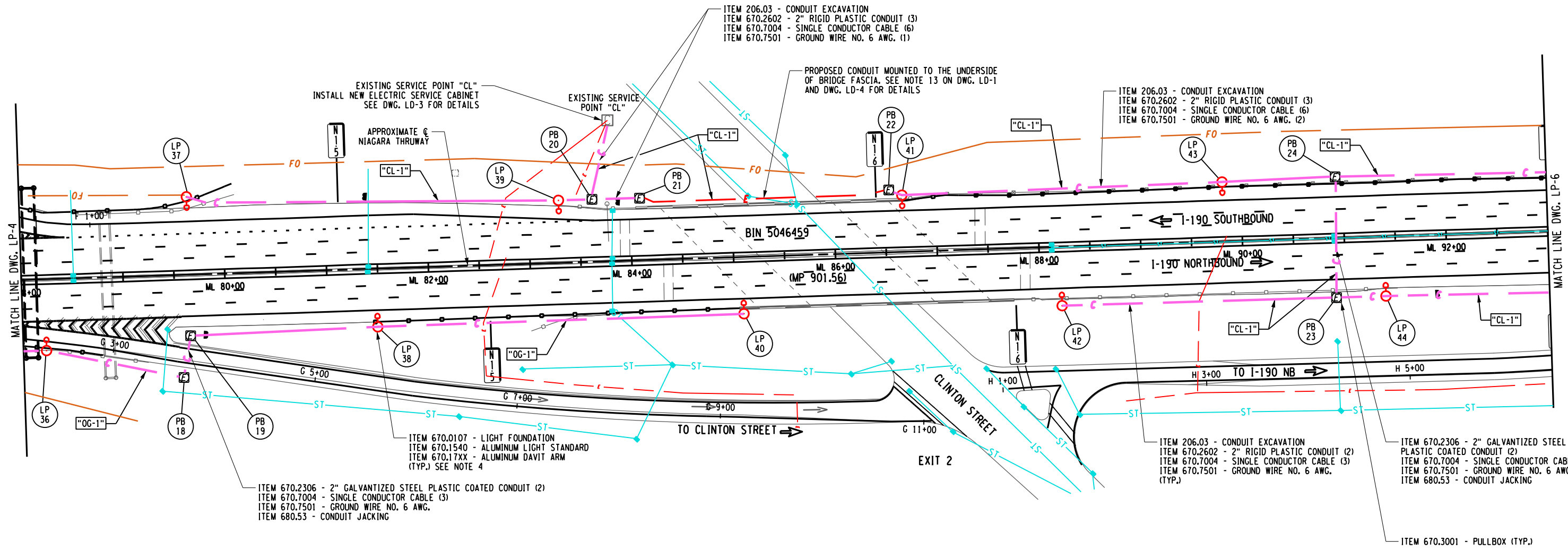
CHECKED BY: J. KLEINMANN

DRAFTED BY: T. HANSEN

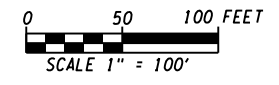
CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH



- NOTES:
1. SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.
  2. ALL CONDUIT CROSSING BENEATH RAMPS AND MAINLINE SHALL BE PLACED BY TRENCHLESS METHODS (ITEM 680.53). (OPEN CUTTING WILL NOT BE ALLOWED) AND ALL ROADWAY PAVEMENT CROSSINGS SHALL CONSIST OF 2" GALVANIZED STEEL (PLASTIC COATED CONDUIT (ITEM 670.2306).
  3. THE CONTRACTOR SHALL INTERCEPT THE PROPOSED CONDUITS WITH THE EXISTING CONDUITS OR PULL BOXES AT THE EXISTING SERVICE POINT. UNUSED EXISTING CONDUIT AND CABLING LEADING AWAY FROM THE SERVICE POINT SHALL BE REMOVED. THE COST OF THE INTERCEPT SHALL BE INCLUDED IN THE COST OF THE CONDUIT, 670.2602.
  4. SEE LD-2 FOR POLE LOCATIONS AND ITEM NUMBERS.



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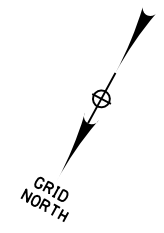
JOSEPH W. KLEINMANN  
LICENSED PROFESSIONAL ENGINEER  
092577

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING LIGHTING PLANS	DRAWING NUMBER: LP-5



CHECKED BY: J. KLEINMANN

DRAFTED BY: T. HANSEN

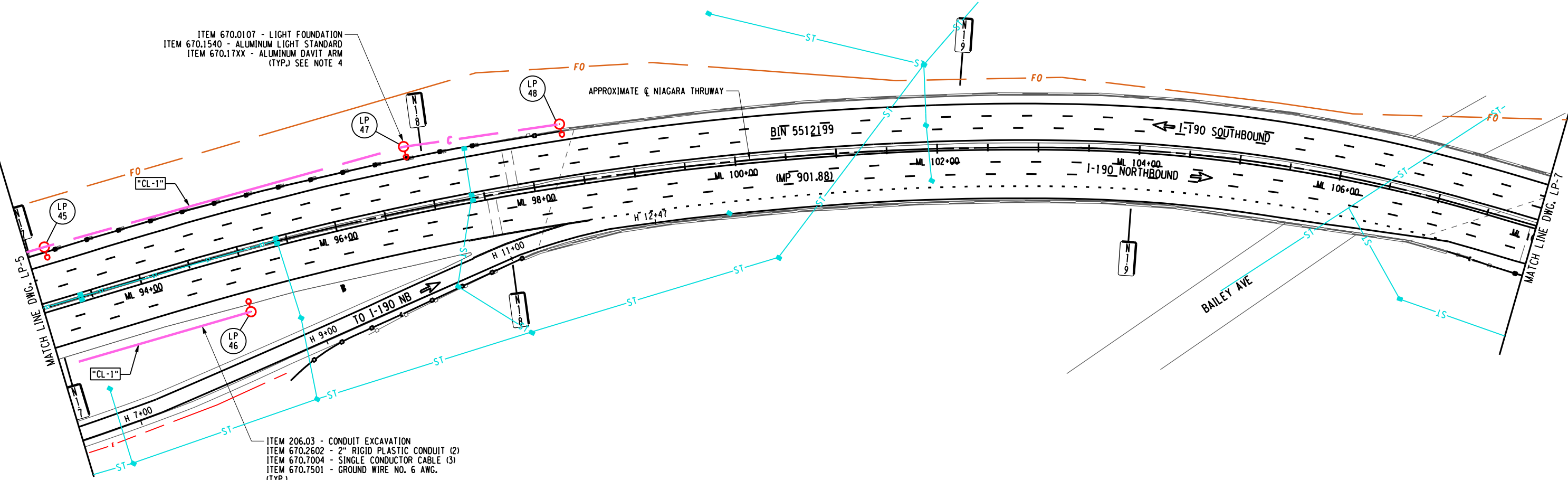
CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH

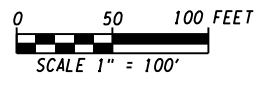
ITEM 670.0107 - LIGHT FOUNDATION  
ITEM 670.1540 - ALUMINUM LIGHT STANDARD  
ITEM 670.17XX - ALUMINUM DAVIT ARM  
(TYP.) SEE NOTE 4

ITEM 206.03 - CONDUIT EXCAVATION  
ITEM 670.2602 - 2" RIGID PLASTIC CONDUIT (2)  
ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
ITEM 670.7501 - GROUND WIRE NO. 6 AWG.  
(TYP.)



NOTES:

1. SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.
2. ALL CONDUIT CROSSING BENEATH RAMPS AND MAINLINE SHALL BE PLACED BY TRENCHLESS METHODS (ITEM 680.53). (OPEN CUTTING WILL NOT BE ALLOWED) AND ALL ROADWAY PAVEMENT CROSSINGS SHALL CONSIST OF 2" GALVANIZED STEEL (PLASTIC COATED CONDUIT (ITEM 670.2306).
3. THE CONTRACTOR SHALL INTERCEPT THE PROPOSED CONDUITS WITH THE EXISTING CONDUITS OR PULL BOXES AT THE EXISTING SERVICE POINT. UNUSED EXISTING CONDUIT AND CABLING LEADING AWAY FROM THE SERVICE POINT SHALL BE REMOVED. THE COST OF THE INTERCEPT SHALL BE INCLUDED IN THE COST OF THE CONDUIT, 670.2602.
4. SEE LD-2 FOR POLE LOCATIONS AND ITEM NUMBERS.



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING LIGHTING PLANS	DRAWING NUMBER: LP-6



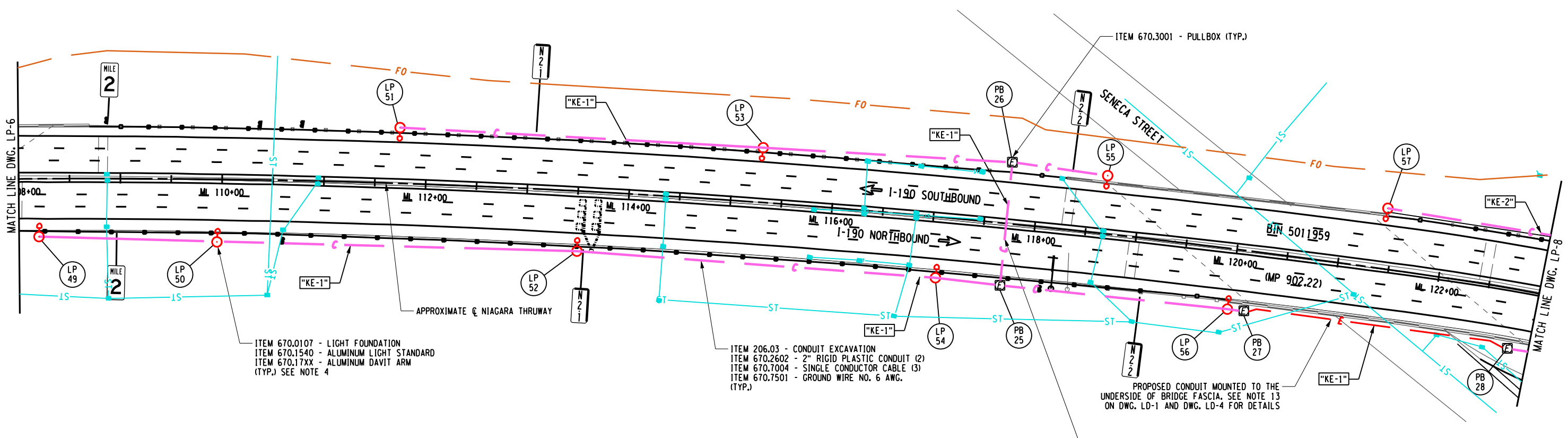
CHECKED BY: J. KLEINMANN

DRAFTED BY: T. HANSEN

CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH



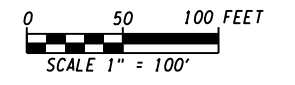
ITEM 670.0107 - LIGHT FOUNDATION  
 ITEM 670.1540 - ALUMINUM LIGHT STANDARD  
 ITEM 670.17XX - ALUMINUM DAVIT ARM  
 (TYP.) SEE NOTE 4

ITEM 206.03 - CONDUIT EXCAVATION  
 ITEM 670.2602 - 2" RIGID PLASTIC CONDUIT (2)  
 ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
 ITEM 670.7501 - GROUND WIRE NO. 6 AWG.  
 (TYP.)

PROPOSED CONDUIT MOUNTED TO THE  
 UNDERSIDE OF BRIDGE FASCIA. SEE NOTE 13  
 ON DWG. LD-1 AND DWG. LD-4 FOR DETAILS

ITEM 670.2306 - 2" GALVANIZED STEEL PLASTIC COATED CONDUIT (2)  
 ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
 ITEM 670.7501 - GROUND WIRE NO. 6 AWG.  
 ITEM 680.53 - CONDUIT JACKING

- NOTES:
- SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.
  - ALL CONDUIT CROSSING BENEATH RAMPS AND MAINLINE SHALL BE PLACED BY TRENCHLESS METHODS (ITEM 680.53). OPEN CUTTING WILL NOT BE ALLOWED) AND ALL ROADWAY PAVEMENT CROSSINGS SHALL CONSIST OF 2" GALVANIZED STEEL (PLASTIC COATED CONDUIT (ITEM 670.2306).
  - THE CONTRACTOR SHALL INTERCEPT THE PROPOSED CONDUITS WITH THE EXISTING CONDUITS OR PULL BOXES AT THE EXISTING SERVICE POINT. UNUSED EXISTING CONDUIT AND CABLING LEADING AWAY FROM THE SERVICE POINT SHALL BE REMOVED. THE COST OF THE INTERCEPT SHALL BE INCLUDED IN THE COST OF THE CONDUIT, 670.2602.
  - SEE LD-2 FOR POLE LOCATIONS AND ITEM NUMBERS.



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING LIGHTING PLANS	DRAWING NUMBER: LP-7





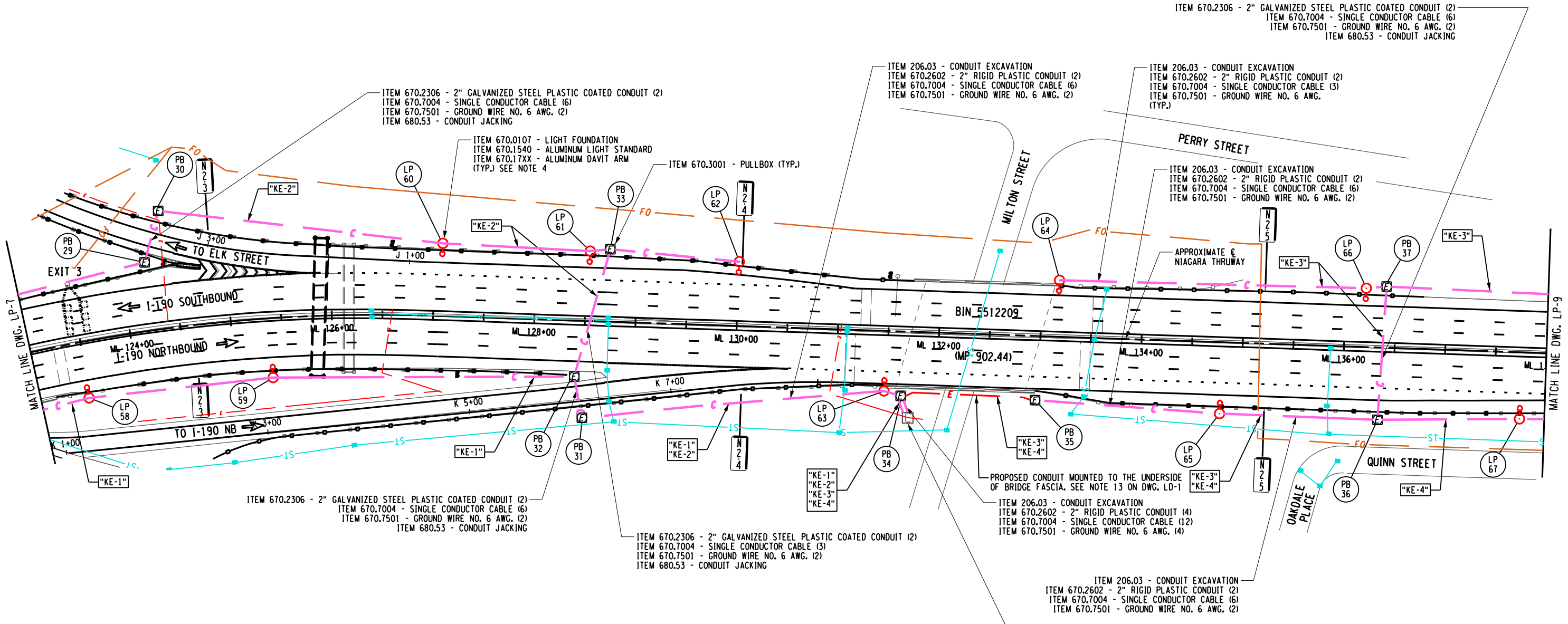
CHECKED BY: J. KLEINMANN

DRAFTED BY: T. HANSEN

CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH



ITEM 670.2306 - 2" GALVANIZED STEEL PLASTIC COATED CONDUIT (2)  
ITEM 670.7004 - SINGLE CONDUCTOR CABLE (6)  
ITEM 670.7501 - GROUND WIRE NO. 6 AWG. (2)  
ITEM 680.53 - CONDUIT JACKING

ITEM 670.2306 - 2" GALVANIZED STEEL PLASTIC COATED CONDUIT (2)  
ITEM 670.7004 - SINGLE CONDUCTOR CABLE (6)  
ITEM 670.7501 - GROUND WIRE NO. 6 AWG. (2)  
ITEM 680.53 - CONDUIT JACKING

ITEM 206.03 - CONDUIT EXCAVATION  
ITEM 670.2602 - 2" RIGID PLASTIC CONDUIT (2)  
ITEM 670.7004 - SINGLE CONDUCTOR CABLE (6)  
ITEM 670.7501 - GROUND WIRE NO. 6 AWG. (2)

ITEM 206.03 - CONDUIT EXCAVATION  
ITEM 670.2602 - 2" RIGID PLASTIC CONDUIT (2)  
ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
ITEM 670.7501 - GROUND WIRE NO. 6 AWG. (TYP.)

ITEM 206.03 - CONDUIT EXCAVATION  
ITEM 670.2602 - 2" RIGID PLASTIC CONDUIT (2)  
ITEM 670.7004 - SINGLE CONDUCTOR CABLE (6)  
ITEM 670.7501 - GROUND WIRE NO. 6 AWG. (2)

ITEM 206.03 - CONDUIT EXCAVATION  
ITEM 670.2602 - 2" RIGID PLASTIC CONDUIT (4)  
ITEM 670.7004 - SINGLE CONDUCTOR CABLE (12)  
ITEM 670.7501 - GROUND WIRE NO. 6 AWG. (4)

ITEM 206.03 - CONDUIT EXCAVATION  
ITEM 670.2602 - 2" RIGID PLASTIC CONDUIT (2)  
ITEM 670.7004 - SINGLE CONDUCTOR CABLE (6)  
ITEM 670.7501 - GROUND WIRE NO. 6 AWG. (2)

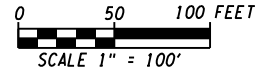
NOTES:  
1. SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.

2. ALL CONDUIT CROSSING BENEATH RAMP AND MAINLINE SHALL BE PLACED BY TRENCHLESS METHODS (ITEM 680.53). (OPEN CUTTING WILL NOT BE ALLOWED) AND ALL ROADWAY PAVEMENT CROSSINGS SHALL CONSIST OF 2" GALVANIZED STEEL (PLASTIC COATED CONDUIT (ITEM 670.2306).

3. THE CONTRACTOR SHALL INTERCEPT THE PROPOSED CONDUITS WITH THE EXISTING CONDUITS OR PULL BOXES AT THE EXISTING SERVICE POINT. UNUSED EXISTING CONDUIT AND CABLING LEADING AWAY FROM THE SERVICE POINT SHALL BE REMOVED. THE COST OF THE INTERCEPT SHALL BE INCLUDED IN THE COST OF THE CONDUIT, 670.2602.

4. SEE LD-2 FOR POLE LOCATIONS AND ITEM NUMBERS.

EXISTING SERVICE LOCATION "KE"  
INSTALL NEW SERVICE CABINET  
SEE DWG. LD-3 FOR DETAILS



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING LIGHTING PLANS	DRAWING NUMBER: LP-8

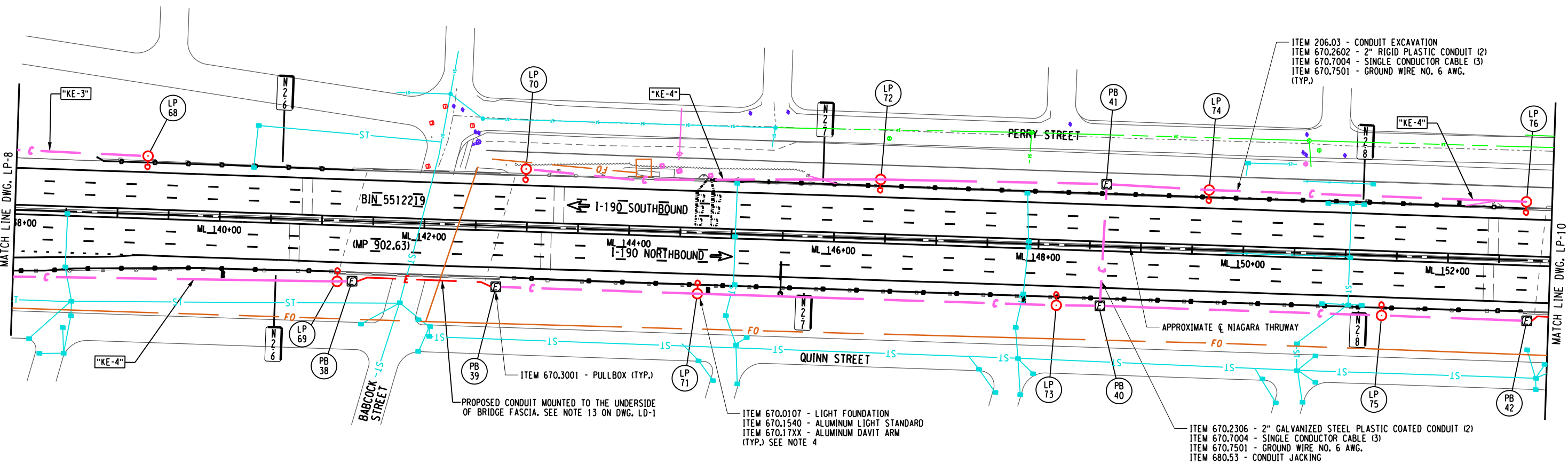
CHECKED BY: J. KLEINMANN

DRAFTED BY: T. HANSEN

CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH



- NOTES:
1. SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.
  2. ALL CONDUIT CROSSING BENEATH RAMPS AND MAINLINE SHALL BE PLACED BY TRENCHLESS METHODS (ITEM 680.53). OPEN CUTTING WILL NOT BE ALLOWED) AND ALL ROADWAY PAVEMENT CROSSINGS SHALL CONSIST OF 2" GALVANIZED STEEL (PLASTIC COATED CONDUIT (ITEM 670.2306).
  3. THE CONTRACTOR SHALL INTERCEPT THE PROPOSED CONDUITS WITH THE EXISTING CONDUITS OR PULL BOXES AT THE EXISTING SERVICE POINT. UNUSED EXISTING CONDUIT AND CABLING LEADING AWAY FROM THE SERVICE POINT SHALL BE REMOVED. THE COST OF THE INTERCEPT SHALL BE INCLUDED IN THE COST OF THE CONDUIT, 670.2602.
  4. SEE LD-2 FOR POLE LOCATIONS AND ITEM NUMBERS.

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

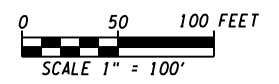
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
LIGHTING PLANS

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
LP-9



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

GRID NORTH

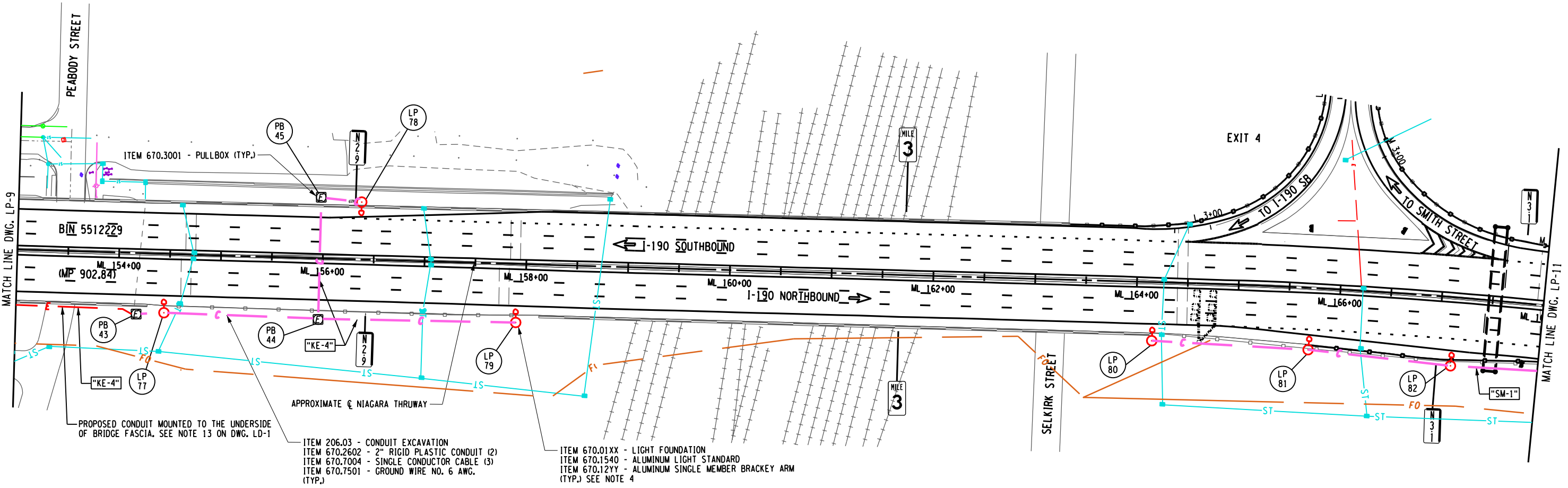
CHECKED BY: J. KLEINMANN

DRAFTED BY: T. HANSEN

CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH



- ITEM 206.03 - CONDUIT EXCAVATION
- ITEM 670.2602 - 2" RIGID PLASTIC CONDUIT (2)
- ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)
- ITEM 670.7501 - GROUND WIRE NO. 6 AWG. (TYP.)
- ITEM 670.01XX - LIGHT FOUNDATION
- ITEM 670.1540 - ALUMINUM LIGHT STANDARD
- ITEM 670.12YY - ALUMINUM SINGLE MEMBER BRACKET ARM (TYP.) SEE NOTE 4

NOTES:

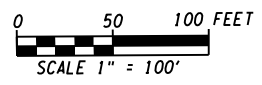
1. SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.
2. ALL CONDUIT CROSSING BENEATH RAMPS AND MAINLINE SHALL BE PLACED BY TRENCHLESS METHODS (ITEM 680.53). (OPEN CUTTING WILL NOT BE ALLOWED) AND ALL ROADWAY PAVEMENT CROSSINGS SHALL CONSIST OF 2" GALVANIZED STEEL (PLASTIC COATED CONDUIT (ITEM 670.2306).
3. THE CONTRACTOR SHALL INTERCEPT THE PROPOSED CONDUITS WITH THE EXISTING CONDUITS OR PULL BOXES AT THE EXISTING SERVICE POINT. UNUSED EXISTING CONDUIT AND CABLING LEADING AWAY FROM THE SERVICE POINT SHALL BE REMOVED. THE COST OF THE INTERCEPT SHALL BE INCLUDED IN THE COST OF THE CONDUIT, 670.2602.
4. SEE LD-2 FOR POLE LOCATIONS AND ITEM NUMBERS.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING LIGHTING PLANS	DRAWING NUMBER: LP-10



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 



CHECKED BY: J. KLEINMANN

DRAFTED BY: T. HANSEN

CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH

ITEM 670.2306 - 2" GALVANIZED STEEL PLASTIC COATED CONDUIT (2)  
ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
ITEM 670.7501 - GROUND WIRE NO. 6 AWG.  
ITEM 680.53 - CONDUIT JACKING

ITEM 670.2306 - 2" GALVANIZED STEEL PLASTIC COATED CONDUIT (2)  
ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
ITEM 670.7501 - GROUND WIRE NO. 6 AWG.  
ITEM 680.53 - CONDUIT JACKING

ITEM 206.03 - CONDUIT EXCAVATION  
ITEM 670.2602 - 2" RIGID PLASTIC CONDUIT (2)  
ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
ITEM 670.7501 - GROUND WIRE NO. 6 AWG.  
(TYP.)

ITEM 670.0107 - LIGHT FOUNDATION  
ITEM 670.1540 - ALUMINUM LIGHT STANDARD  
ITEM 670.17XX - ALUMINUM DAVIT ARM  
(TYP.) SEE NOTE 4

ITEM 670.7004 - SINGLE CONDUCTOR CABLE (6)  
ITEM 670.7501 - GROUND WIRE NO. 6 AWG.(2)

EXISTING SERVICE POINT "SM"  
INSTALL NEW ELECTRIC SERVICE CABINET  
SEE DWG. LD-3 FOR DETAILS

ITEM 670.2306 - 2" GALVANIZED STEEL PLASTIC COATED CONDUIT (2)  
ITEM 670.7004 - SINGLE CONDUCTOR CABLE (3)  
ITEM 670.7501 - GROUND WIRE NO. 6 AWG.  
ITEM 680.53 - CONDUIT JACKING

PROPOSED CONDUIT MOUNTED TO  
UNDERSIDE OF BRIDGE FASCIA.  
SEE NOTE 13 ON DWG. LD-1

PROPOSED CONDUIT MOUNTED TO UNDERSIDE OF  
BRIDGE FASCIA. SEE NOTE 13 ON DWG. LD-1

NOTES:

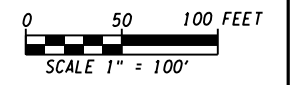
1. SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.
2. ALL CONDUIT CROSSING BENEATH RAMPS AND MAINLINE SHALL BE PLACED BY TRENCHLESS METHODS (ITEM 680.53). OPEN CUTTING WILL NOT BE ALLOWED) AND ALL ROADWAY PAVEMENT CROSSINGS SHALL CONSIST OF 2" GALVANIZED STEEL (PLASTIC COATED CONDUIT (ITEM 670.2306).
3. THE CONTRACTOR SHALL INTERCEPT THE PROPOSED CONDUITS WITH THE EXISTING CONDUITS OR PULL BOXES AT THE EXISTING SERVICE POINT. UNUSED EXISTING CONDUIT AND CABLING LEADING AWAY FROM THE SERVICE POINT SHALL BE REMOVED. THE COST OF THE INTERCEPT SHALL BE INCLUDED IN THE COST OF THE CONDUIT, 670.2602.
4. SEE LD-2 FOR POLE LOCATIONS AND ITEM NUMBERS.

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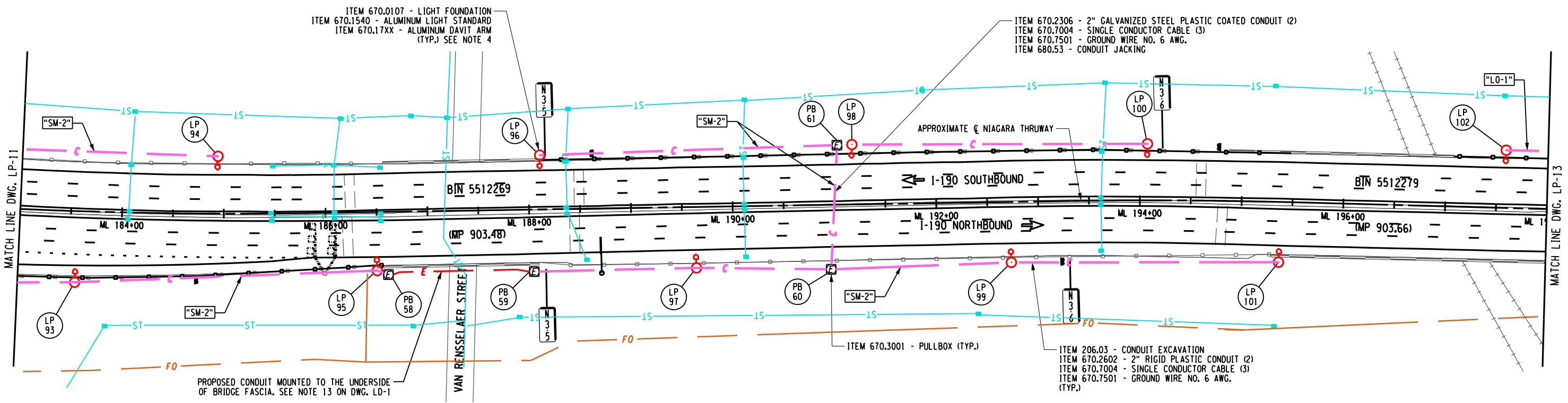
REVISIONS			
DATE	DESCRIPTION	BY	SYM.



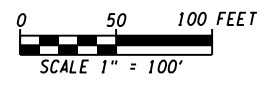
TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING LIGHTING PLANS	DRAWING NUMBER: LP-11



CHECKED BY: J. KLEINMANN  
DRAFTED BY: T. HANSEN  
DESIGNED BY: T. ANDREWS  
DESIGN SUPERVISOR: D. MARSH



- NOTES:
- SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.
  - ALL CONDUIT CROSSING BENEATH RAMPS AND MAINLINE SHALL BE PLACED BY TRENCHLESS METHODS (ITEM 680.53). (OPEN CUTTING WILL NOT BE ALLOWED) AND ALL ROADWAY PAVEMENT CROSSINGS SHALL CONSIST OF 2" GALVANIZED STEEL (PLASTIC COATED CONDUIT (ITEM 670.2306).
  - THE CONTRACTOR SHALL INTERCEPT THE PROPOSED CONDUITS WITH THE EXISTING CONDUITS OR PULL BOXES AT THE EXISTING SERVICE POINT. UNUSED EXISTING CONDUIT AND CABLING LEADING AWAY FROM THE SERVICE POINT SHALL BE REMOVED. THE COST OF THE INTERCEPT SHALL BE INCLUDED IN THE COST OF THE CONDUIT, 670.2602.
  - SEE LD-2 FOR POLE LOCATIONS AND ITEM NUMBERS.



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING LIGHTING PLANS	DRAWING NUMBER: LP-12



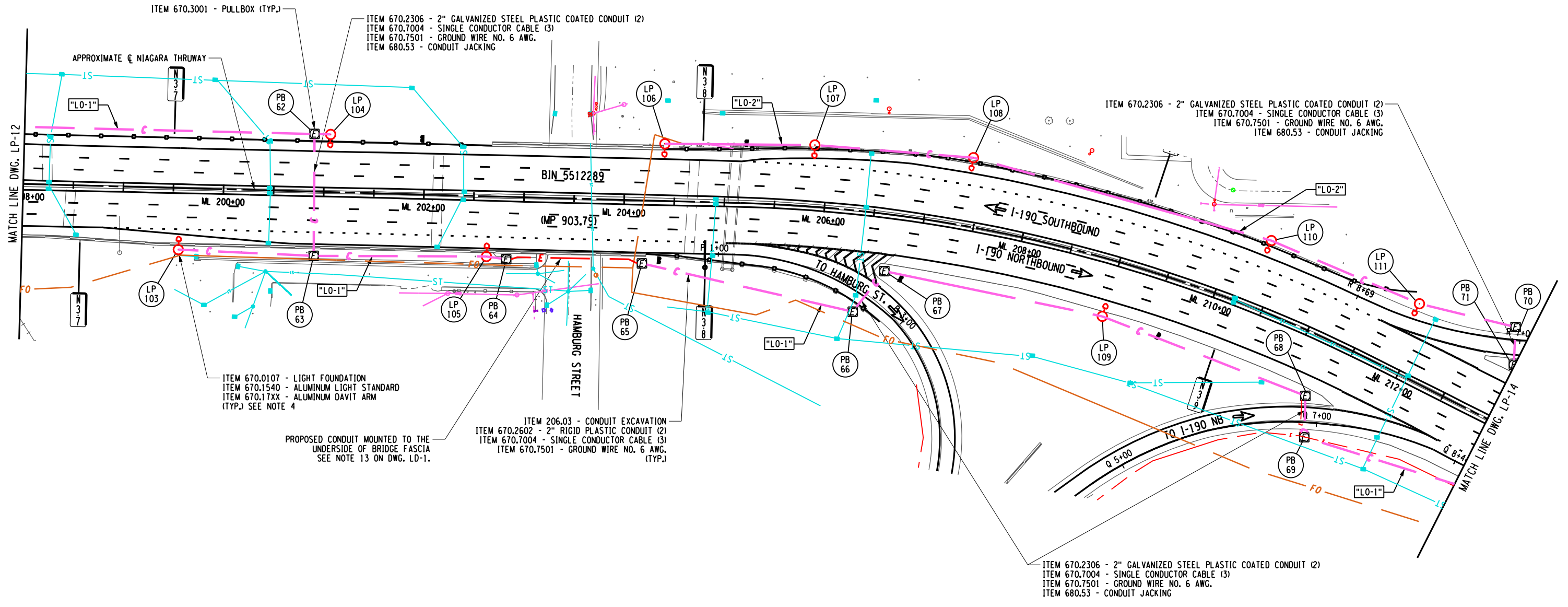
CHECKED BY: J. KLEINMANN

DRAFTED BY: T. HANSEN

CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH



NOTES:

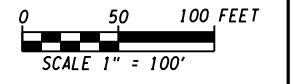
1. SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.
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DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING LIGHTING PLANS	DRAWING NUMBER: LP-13



ALTERED ON:	AFFIXED ON: 5/10/2024
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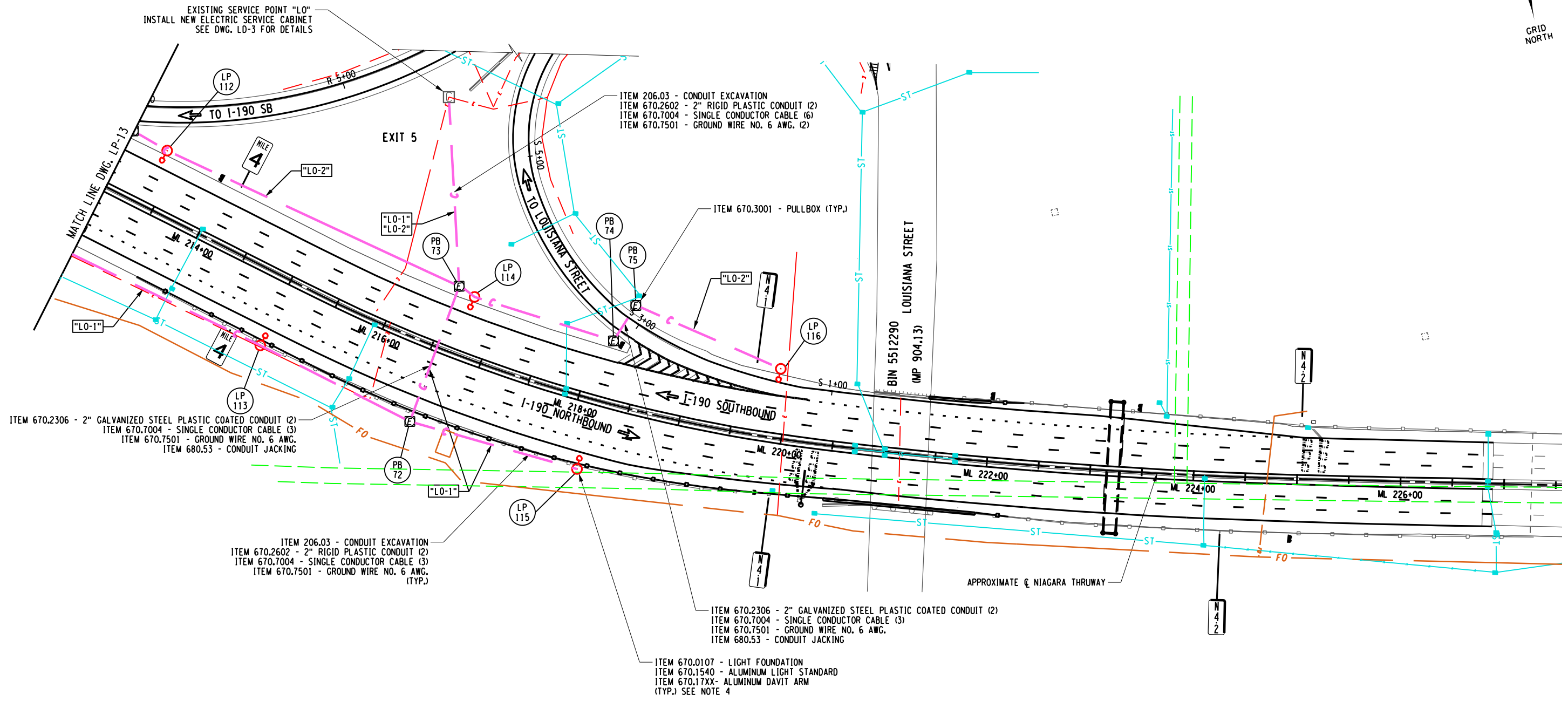
CHECKED BY: J. KLEINMANN

DRAFTED BY: T. HANSEN

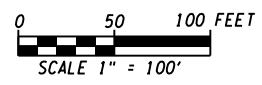
CHECKED BY: J. KLEINMANN

DESIGNED BY: T. ANDREWS

DESIGN SUPERVISOR: D. MARSH



- NOTES:
1. SEE ADDITIONAL GENERAL LIGHTING NOTES ON DWG. LD-1.
  2. ALL CONDUIT CROSSING BENEATH RAMPS AND MAINLINE SHALL BE PLACED BY TRENCHLESS METHODS (ITEM 680.53). (OPEN CUTTING WILL NOT BE ALLOWED) AND ALL ROADWAY PAVEMENT CROSSINGS SHALL CONSIST OF 2" GALVANIZED STEEL (PLASTIC COATED CONDUIT (ITEM 670.2306).
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  4. SEE LD-2 FOR POLE LOCATIONS AND ITEM NUMBERS.



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REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING LIGHTING PLANS	DRAWING NUMBER: LP-14

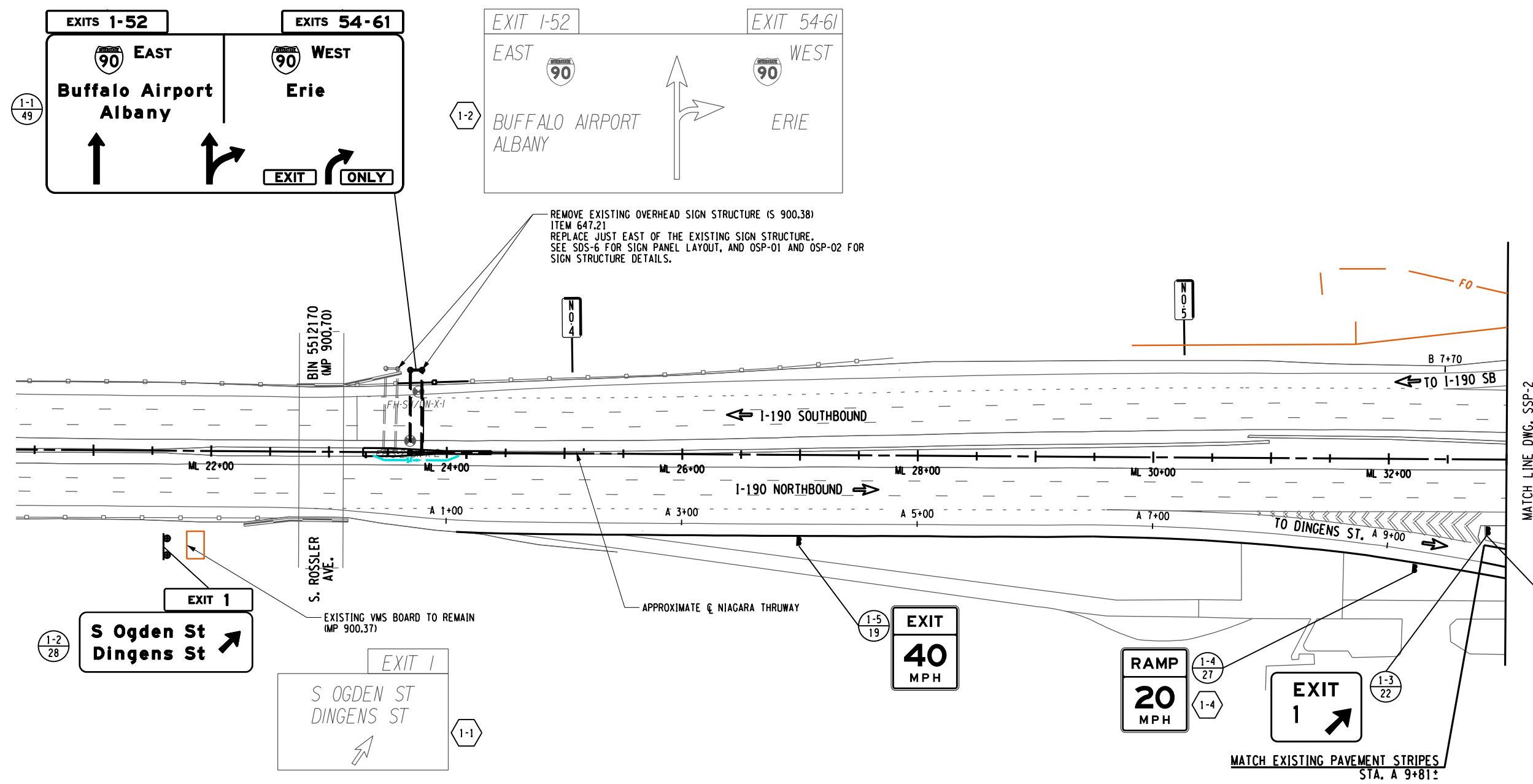
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



REMOVE EXISTING OVERHEAD SIGN STRUCTURE (S 900.38)  
ITEM 647.21  
REPLACE JUST EAST OF THE EXISTING SIGN STRUCTURE.  
SEE SDS-6 FOR SIGN PANEL LAYOUT, AND OSP-01 AND OSP-02 FOR  
SIGN STRUCTURE DETAILS.

EXISTING VMS BOARD TO REMAIN  
(MP 900.37)

APPROXIMATE  $\bar{c}$  NIAGARA THRUWAY

MATCH EXISTING PAVEMENT STRIPES  
STA. A 9+81.5

MATCH LINE DWG. SSP-2

**SIGN LEGENDS**

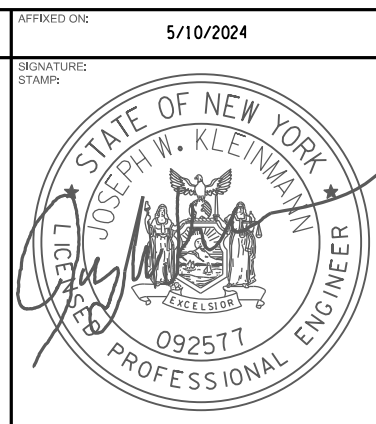
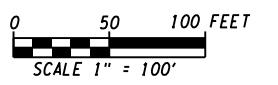
	NEW SIGN		EXISTING SIGN
	SIGN TO BE REMOVED		EXISTING GUIDE SIGN
	SIGN TO BE RELOCATED		SINGLE POST PROPOSED SIGN
	SIGN TO BE REMAIN		SINGLE POST BACK TO BACK PROPOSED SIGN
X	LOCATION NUMBER		DOUBLE POST PROPOSED SIGN
Y	TEXT NUMBER		PROPOSED GUIDE SIGN

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING SIGNING AND STRIPING PLANS	DRAWING NUMBER: SSP-1



ALTERED ON: \_\_\_\_\_  
AFFIXED ON: 5/10/2024  
SIGNATURE: \_\_\_\_\_  
STAMP: \_\_\_\_\_



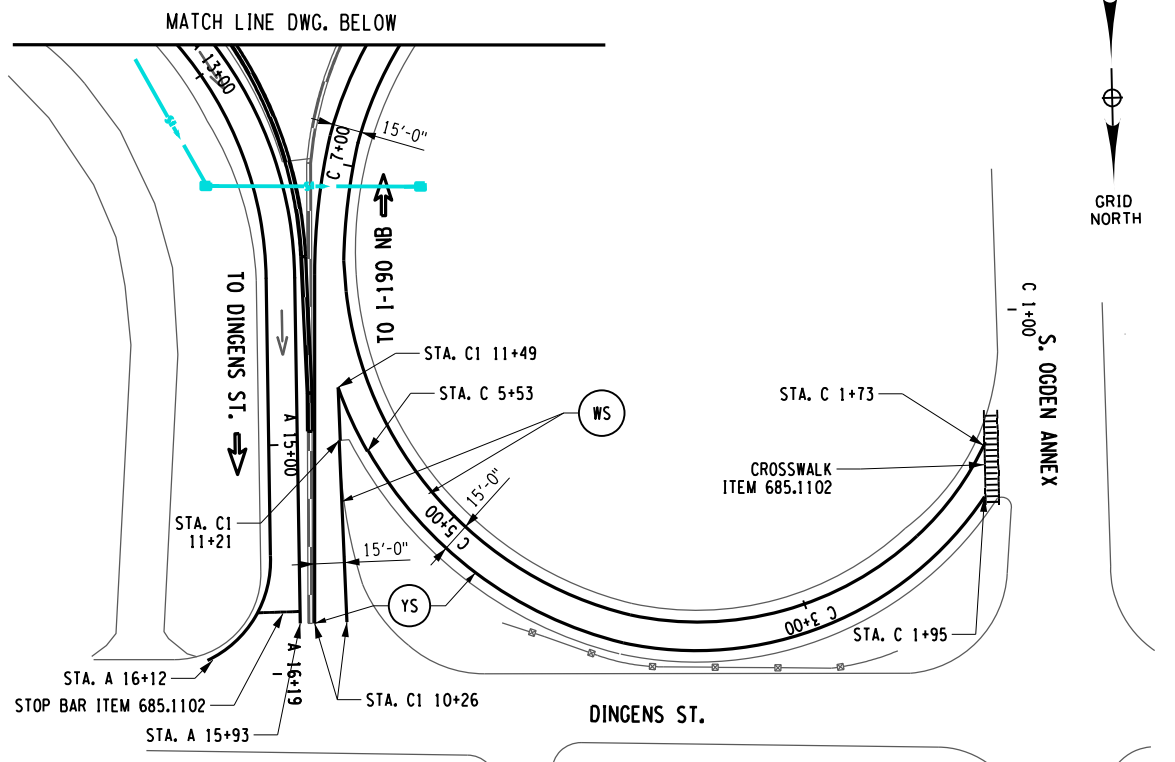
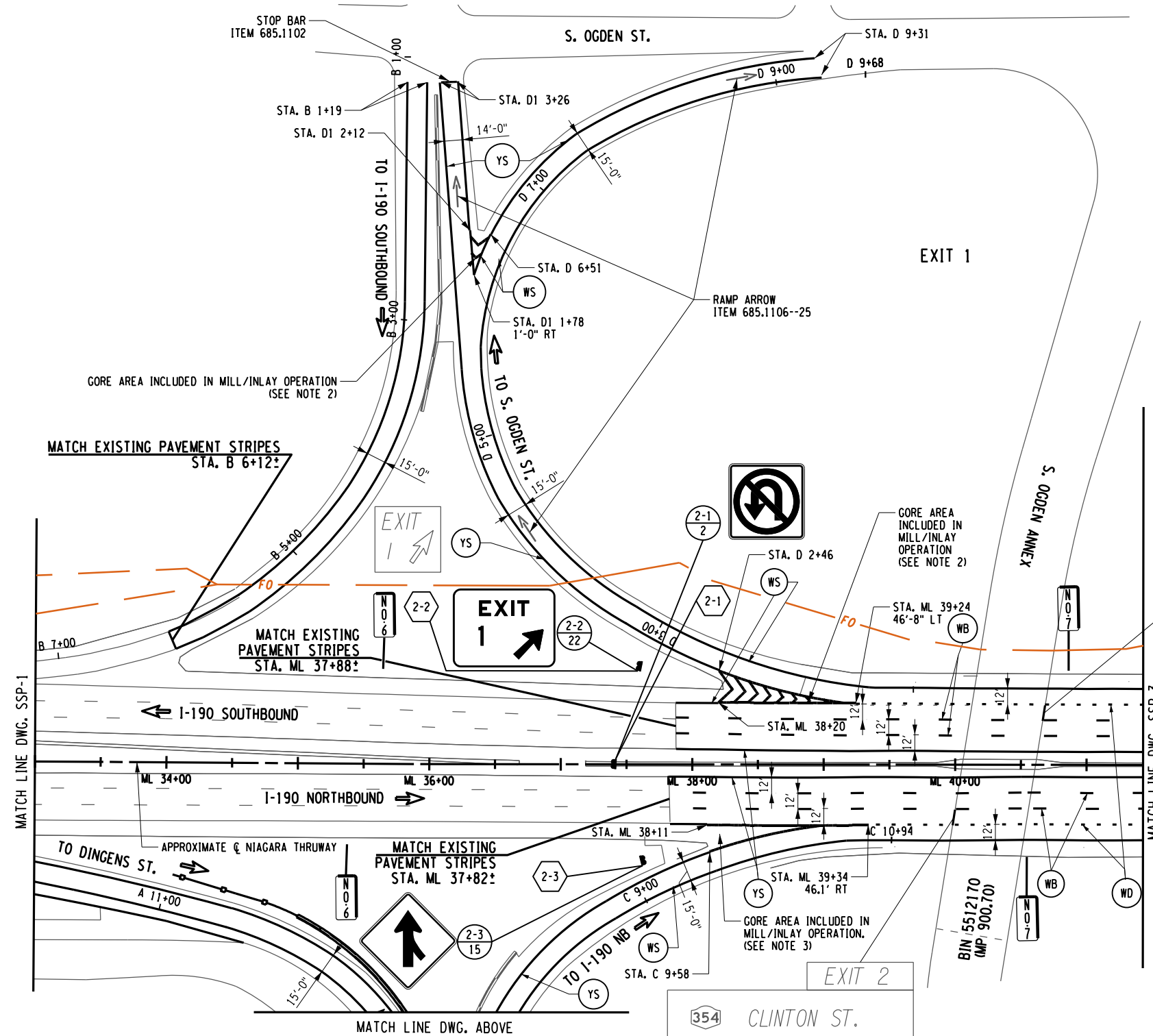
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



**SIGNING AND PAVEMENT MARKING PLAN NOTES:**

- WS** INSTALL 6" WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES (SOLID)

**WB** INSTALL 6" WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES (BROKEN)

**WD** INSTALL 6" WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES (DOTTED)

**YS** INSTALL 6" YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES (SOLID)
- STRIPING ITEMS ARE AS FOLLOWS:

**MAINLINE I-190:**

685.1707--25 - 6" WHITE HIGHLY REFL. TRIPLE DROP EPOXY PAVEMENT STRIPES

685.1708--25 - 6" YELLOW HIGHLY REFL. TRIPLE DROP EPOXY PAVEMENT STRIPES

635.04030225 - RECESS DIAMOND GRINDING FOR INLAID PAVEMENT MARKINGS

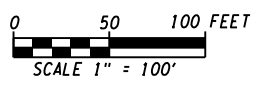
**RAMPS/INTERCHANGES 1-5:**

685.1106--25 - 6" WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES

685.1206--25 - 6" YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES

**NOTES:**

- SEE DWG. SSP-1 FOR SIGN LEGEND.
- INSTALL 2' WIDE CHEVRON MARKINGS IN GORE AREA IN ACCORDANCE WITH NYSTA STANDARD SHEETS TA 685-01 & 685-02.
- SEE NYSTA STANDARD SHEETS TA 865-01 & 685-02 FOR STRIPING LAYOUT.



ALTERED ON:	AFFIXED ON: 5/10/2024
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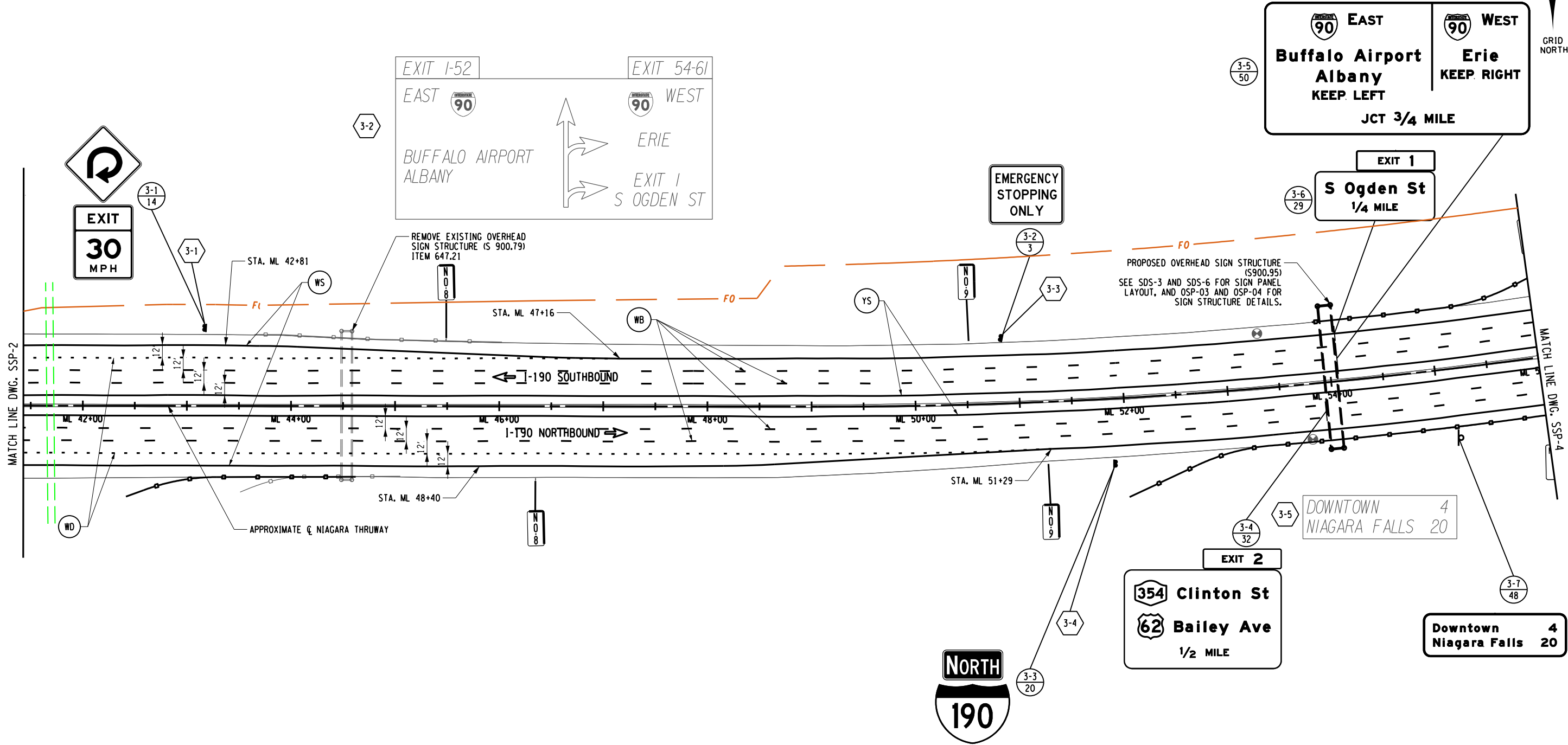
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	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING SIGNING AND STRIPING PLANS	DRAWING NUMBER: SSP-2



CHECKED BY: J. KLEINMANN  
DRAFTED BY: N. HOY  
CHECKED BY: J. KLEINMANN  
DESIGNED BY: P. THOMPSON  
DESIGN SUPERVISOR: D. MARSH



 <b>EAST</b> <b>Buffalo Airport</b> <b>Albany</b> KEEP LEFT JCT 3/4 MILE	 <b>WEST</b> <b>Erie</b> <b>KEEP RIGHT</b> JCT 3/4 MILE
---	---

<b>EXIT 1</b> <b>S Ogden St</b> 1/4 MILE
--

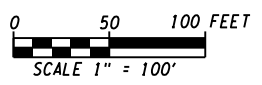
PROPOSED OVERHEAD SIGN STRUCTURE (S900.95)  
SEE SDS-3 AND SDS-6 FOR SIGN PANEL LAYOUT, AND OSP-03 AND OSP-04 FOR SIGN STRUCTURE DETAILS.

<b>EXIT 2</b>  <b>Clinton St</b>  <b>Bailey Ave</b> 1/2 MILE
---

<b>Downtown</b> <b>Niagara Falls</b> 4 20
--



NOTES:  
1. SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.

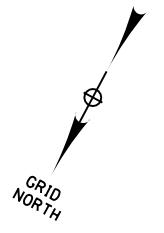


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 <b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
 <small>GMA 2024 Mid-Range Tower Suamico, WI 54220-3720 618.465.4897 - www.gma.com</small>	TITLE OF DRAWING SIGNING AND STRIPING PLANS	DRAWING NUMBER: SSP-3



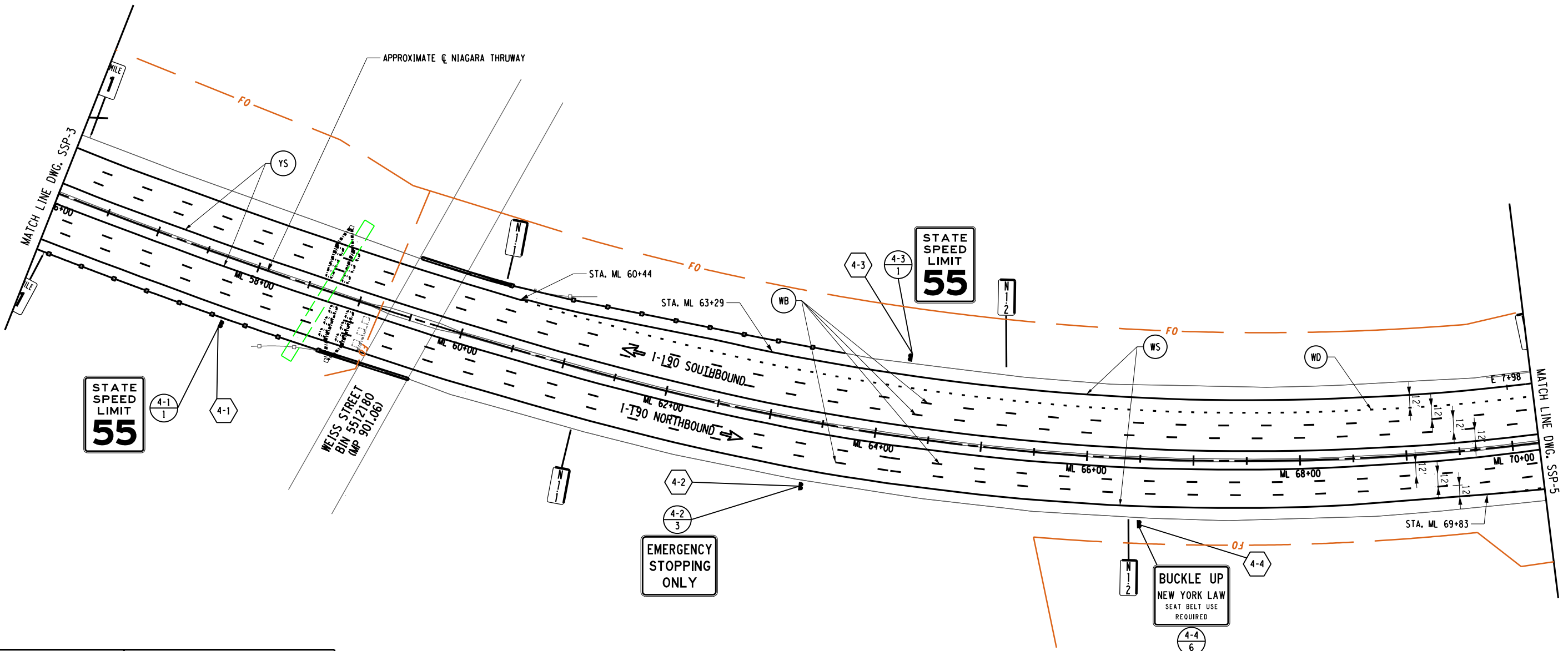
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



NOTES:  
1. SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.

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TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

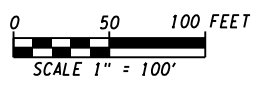
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
SIGNING AND STRIPING PLANS

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
SSP-4



CHECKED BY: J. KLEINMANN

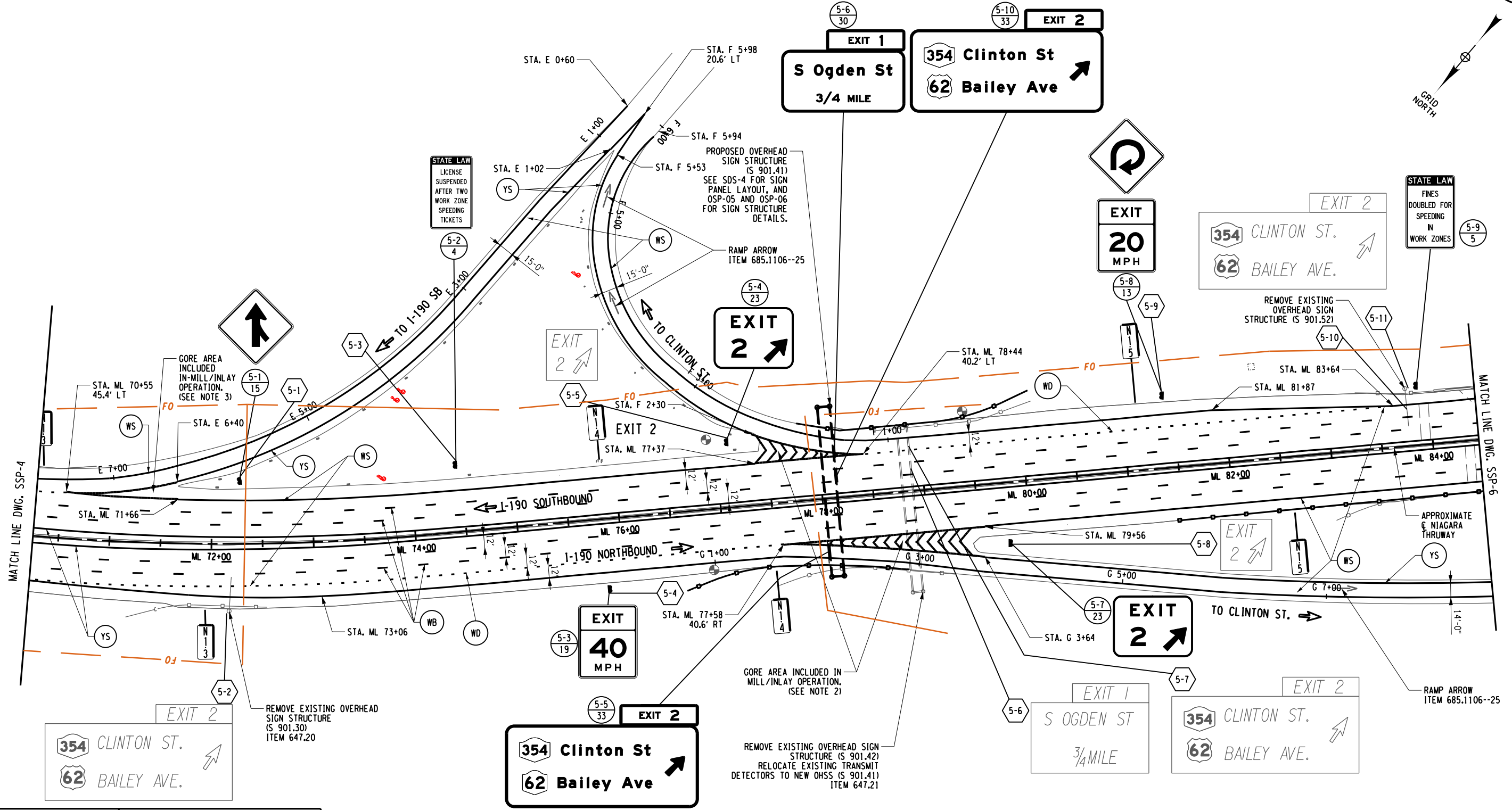
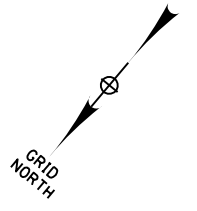
DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

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274



ALTERED ON:

SIGNATURE:  
STAMP:

AFFIXED ON:  
5/10/2024

SIGNATURE:  
STAMP:

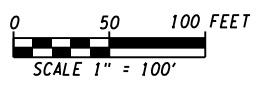
- NOTES:
1. SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.
  2. INSTALL 2' WIDE CHEVRON MARKINGS IN GORE AREA IN ACCORDANCE WITH NYSTA STANDARD SHEETS TA 685-01 & 685-02.
  3. SEE NYSTA STANDARD SHEETS TA 685-01 & 685-02 FOR STRIPING LAYOUT.

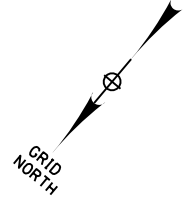
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TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING SIGNING AND STRIPING PLANS	DRAWING NUMBER: SSP-5





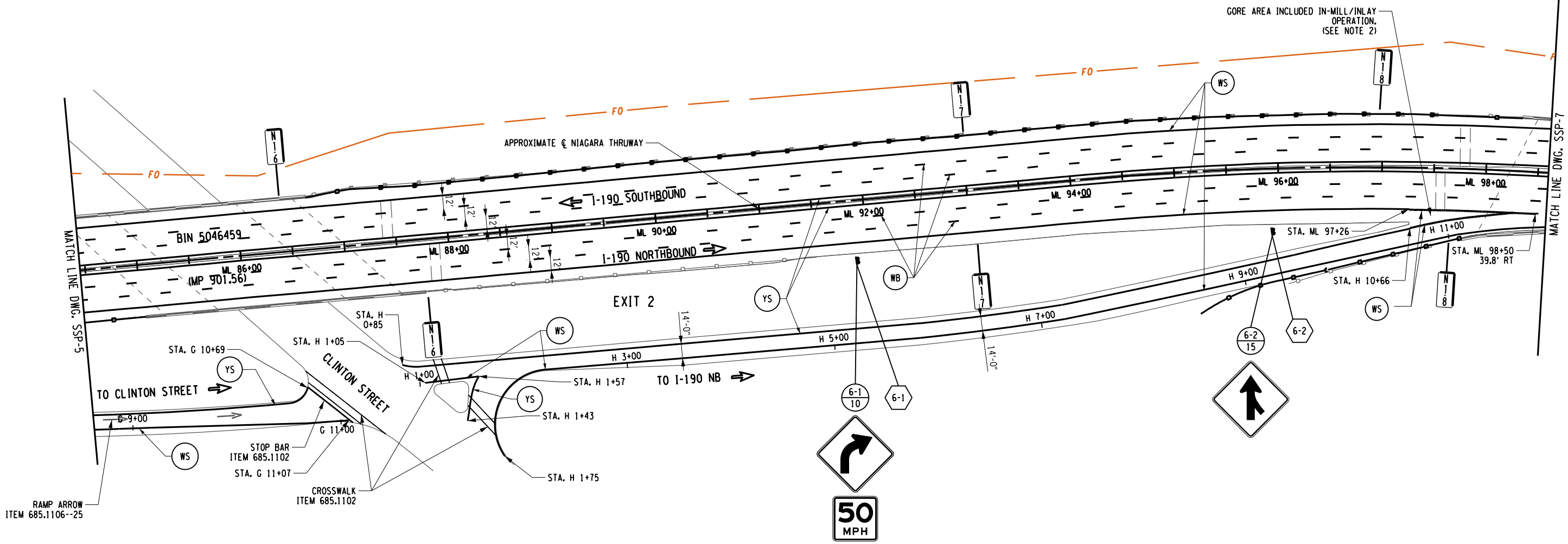
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



ALTERED ON:  
SIGNATURE:  
STAMP:

AFFIXED ON:  
5/10/2024  
SIGNATURE:  
STAMP:

- NOTES:
1. SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.
  2. SEE NYSTA STANDARD SHEETS TA 685-01 & 685-02 FOR STRIPING LAYOUT.

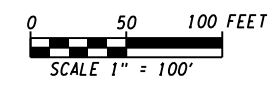
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DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2  
TITLE OF DRAWING  
SIGNING AND STRIPING PLANS

CONTRACT NUMBER:  
TAN 24-12  
DATE:  
MAY 2024  
DRAWING NUMBER:  
SSP-6





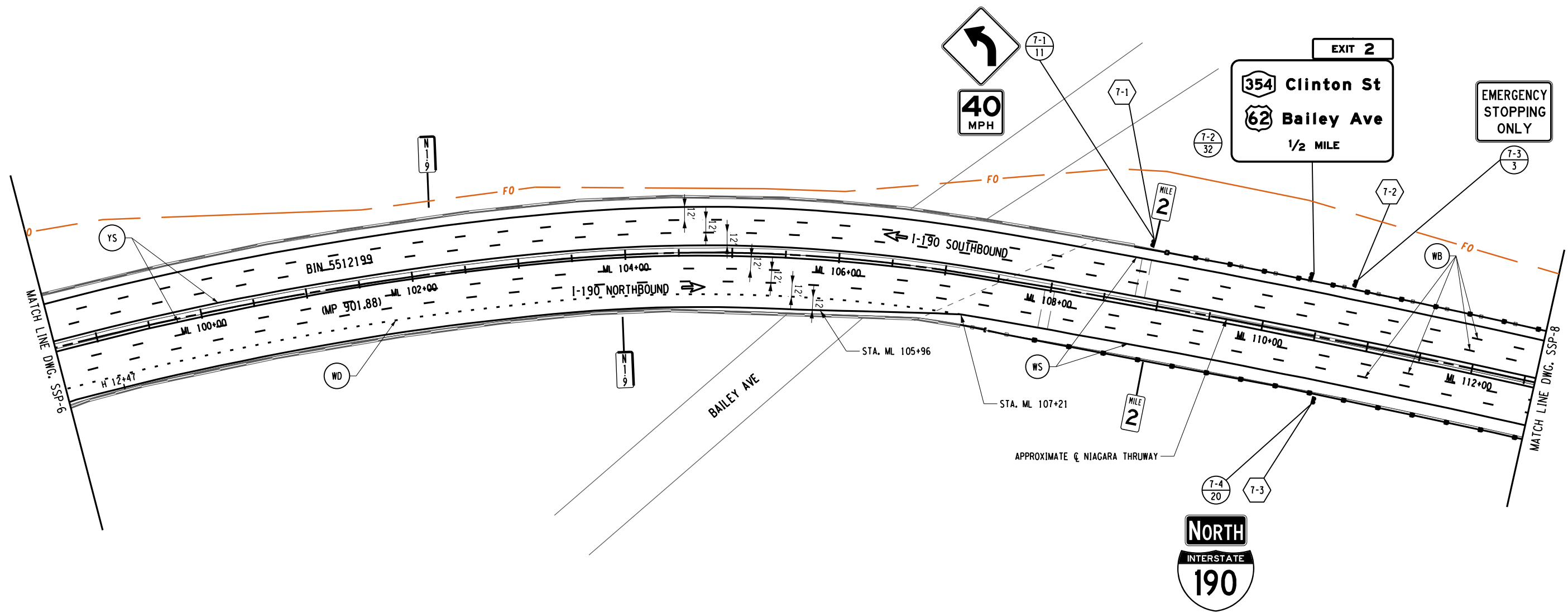
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY


CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



ALTERED ON:  
SIGNATURE:  
STAMP:

AFFIXED ON:  
5/10/2024  
SIGNATURE:  
STAMP:  


NOTES:  
1. SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.

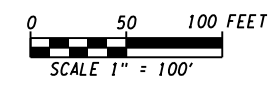
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2  
TITLE OF DRAWING  
SIGNING AND STRIPING PLANS

CONTRACT NUMBER:  
TAN 24-12  
DATE:  
MAY 2024  
DRAWING NUMBER:  
SSP-7



CHECKED BY: J. KLEINMANN

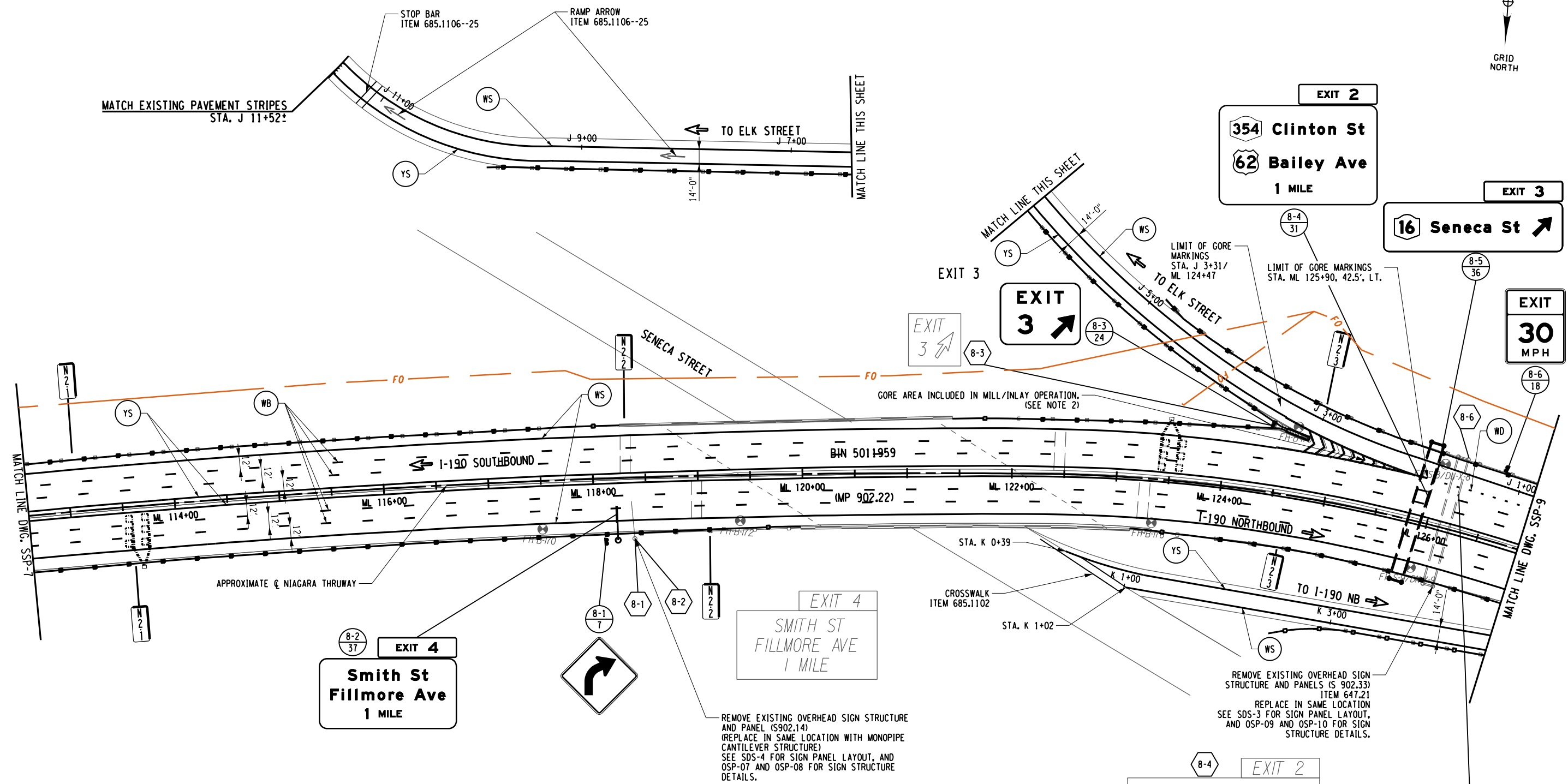
DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

183  
274



- NOTES:
- SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.
  - INSTALL 2' WIDE CHEVRON MARKINGS IN CORE AREA IN ACCORDANCE WITH NYSTA STANDARD SHEETS TA 685-01 & 685-02.

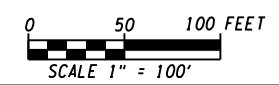
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING SIGNING AND STRIPING PLANS	DRAWING NUMBER: SSP-8

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 





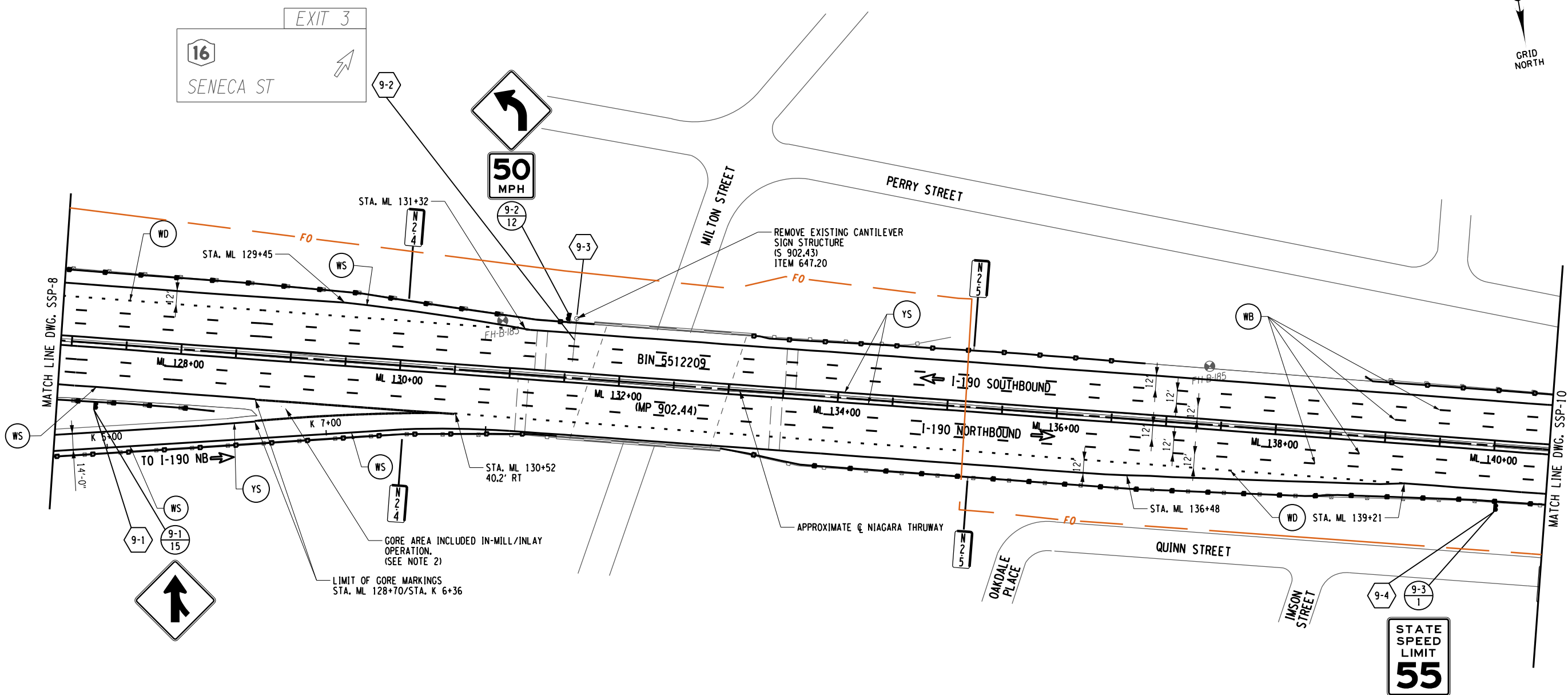
CHECKED BY: J. KLEINMANN


DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

- NOTES:
- SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.
  - SEE NYSTA STANDARD SHEETS TA 685-01 & 685-02 FOR STRIPING LAYOUT.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL




TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

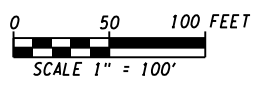
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
SIGNING AND STRIPING PLANS

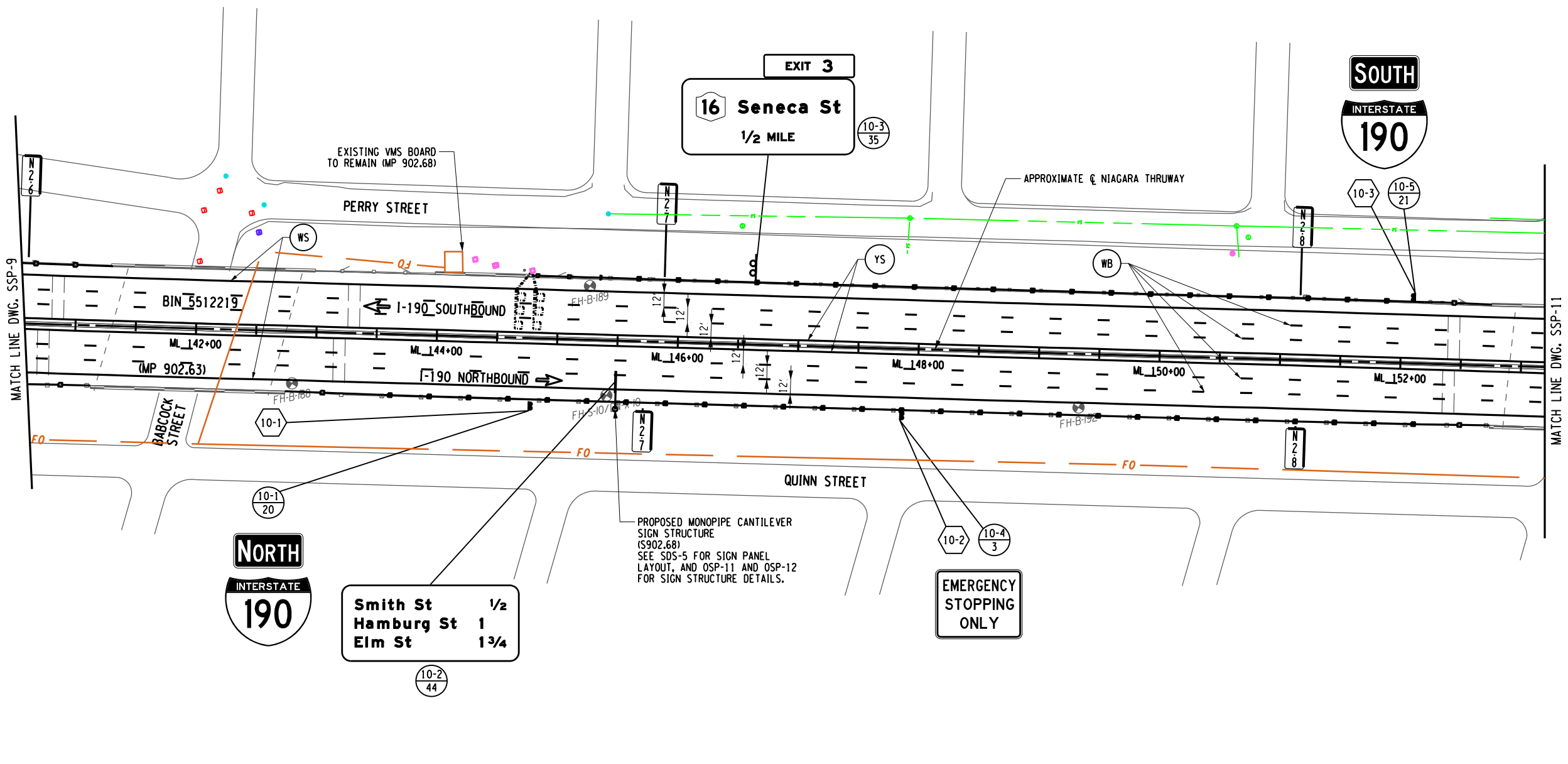
CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
SSP-9







CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

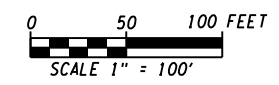
NOTES:  
1. SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.

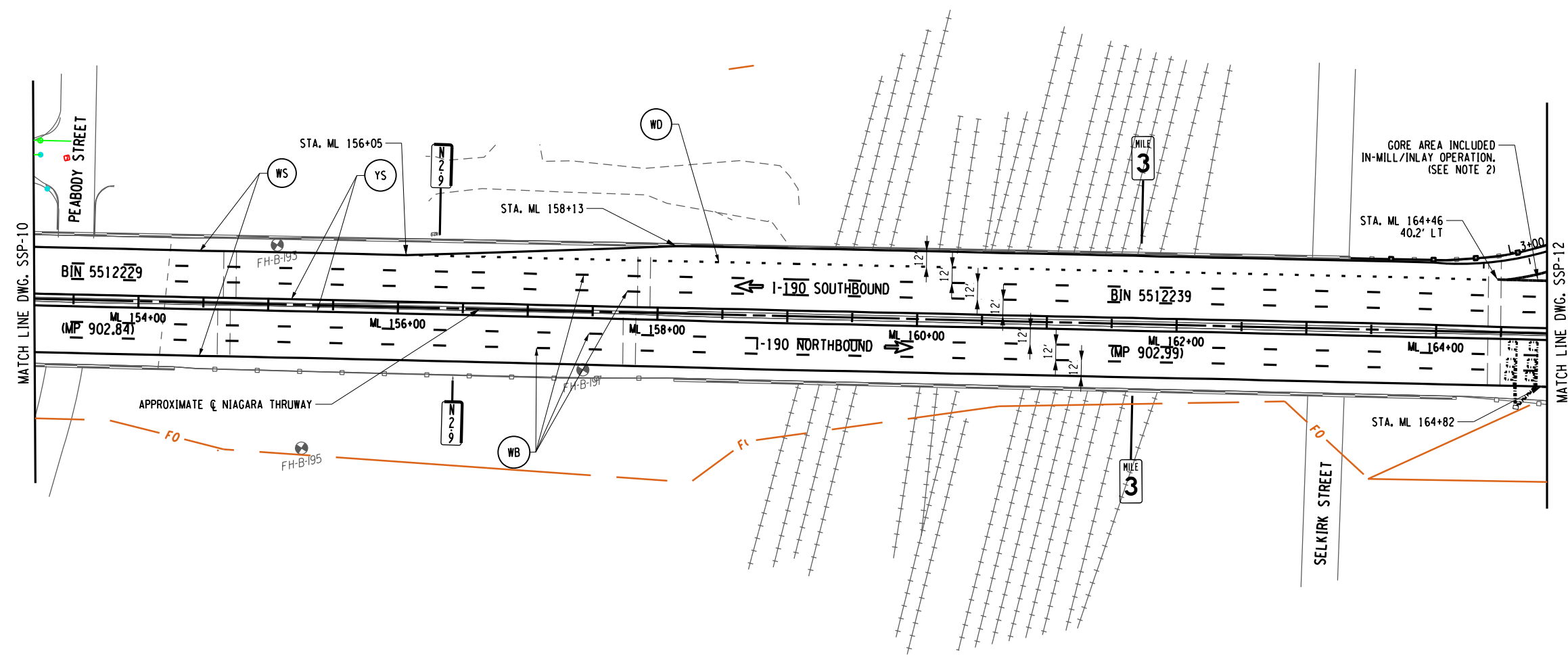
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

2025 Mid-Range Tower  
Suamico, WI 54223-3723  
618.463.4897 www.gma.com

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING SIGNING AND STRIPING PLANS	DRAWING NUMBER: SSP-10





CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_  
STAMP: \_\_\_\_\_



- NOTES:**
- SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.
  - SEE NYSTA STANDARD SHEETS TA 685-01 & 685-02 FOR STRIPING LAYOUT.

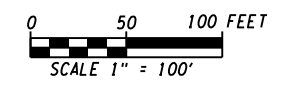
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL



TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 TITLE OF DRAWING: SIGNING AND STRIPING PLANS

CONTRACT NUMBER: TAN 24-12  
 DATE: MAY 2024  
 DRAWING NUMBER: SSP-11



CHECKED BY: J. KLEINMANN

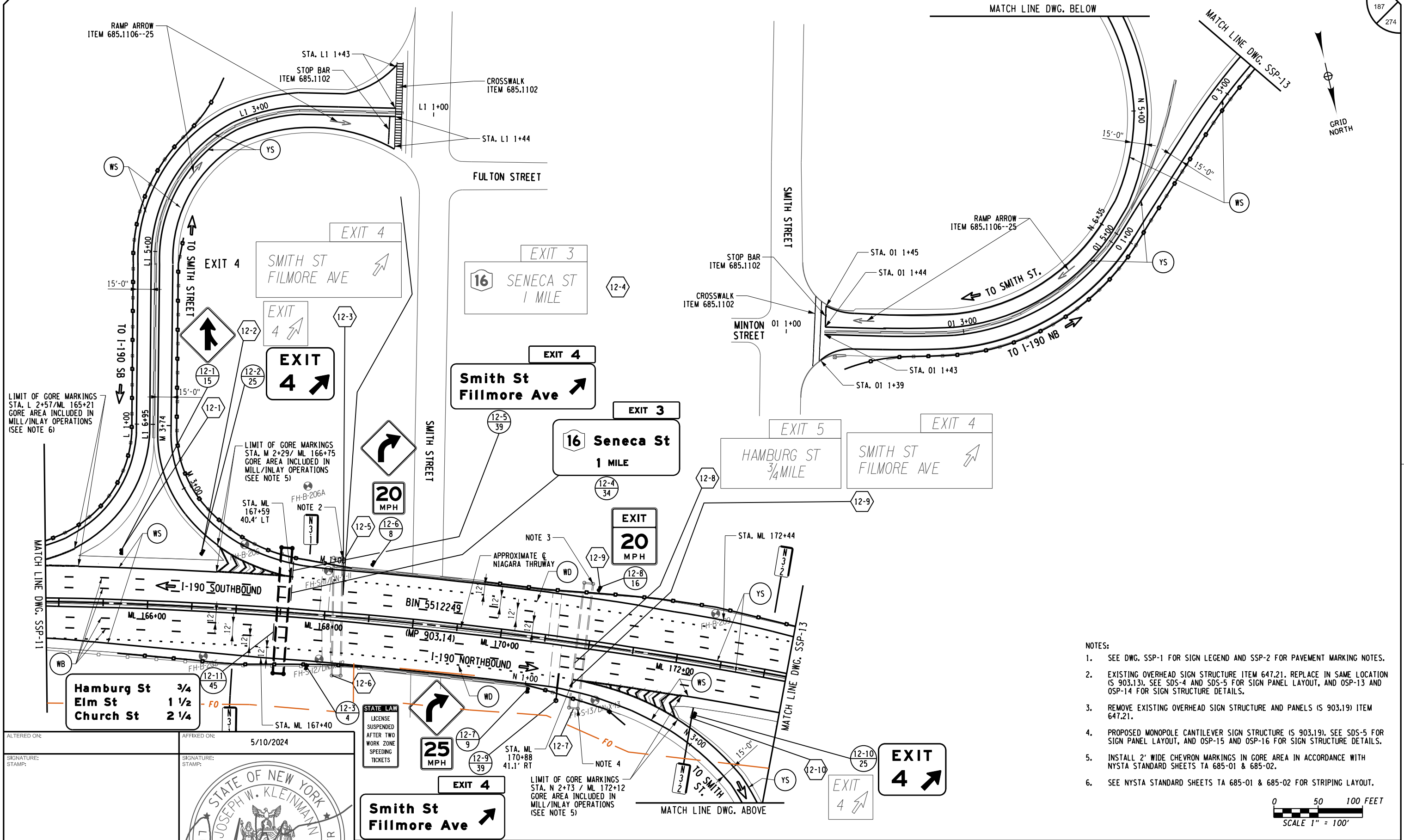
DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

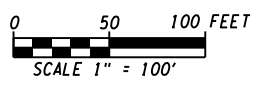
DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

187  
274



- NOTES:
1. SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.
  2. EXISTING OVERHEAD SIGN STRUCTURE ITEM 647.21. REPLACE IN SAME LOCATION (S 903.13), SEE SDS-4 AND SDS-5 FOR SIGN PANEL LAYOUT, AND OSP-13 AND OSP-14 FOR SIGN STRUCTURE DETAILS.
  3. REMOVE EXISTING OVERHEAD SIGN STRUCTURE AND PANELS (S 903.19) ITEM 647.21.
  4. PROPOSED MONOPOLE CANTILEVER SIGN STRUCTURE (S 903.19). SEE SDS-5 FOR SIGN PANEL LAYOUT, AND OSP-15 AND OSP-16 FOR SIGN STRUCTURE DETAILS.
  5. INSTALL 2" WIDE CHEVRON MARKINGS IN GORE AREA IN ACCORDANCE WITH NYSTA STANDARD SHEETS TA 685-01 & 685-02.
  6. SEE NYSTA STANDARD SHEETS TA 685-01 & 685-02 FOR STRIPING LAYOUT.



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

STATE LAW  
LICENSE  
SUSPENDED  
AFTER TWO  
WORK ZONE  
SPEEDING  
TICKETS

**EXIT 4**  
**Smith St  
Fillmore Ave**

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 TITLE OF DRAWING: SIGNING AND STRIPING PLANS

CONTRACT NUMBER: TAN 24-12  
 DATE: MAY 2024  
 DRAWING NUMBER: SSP-12



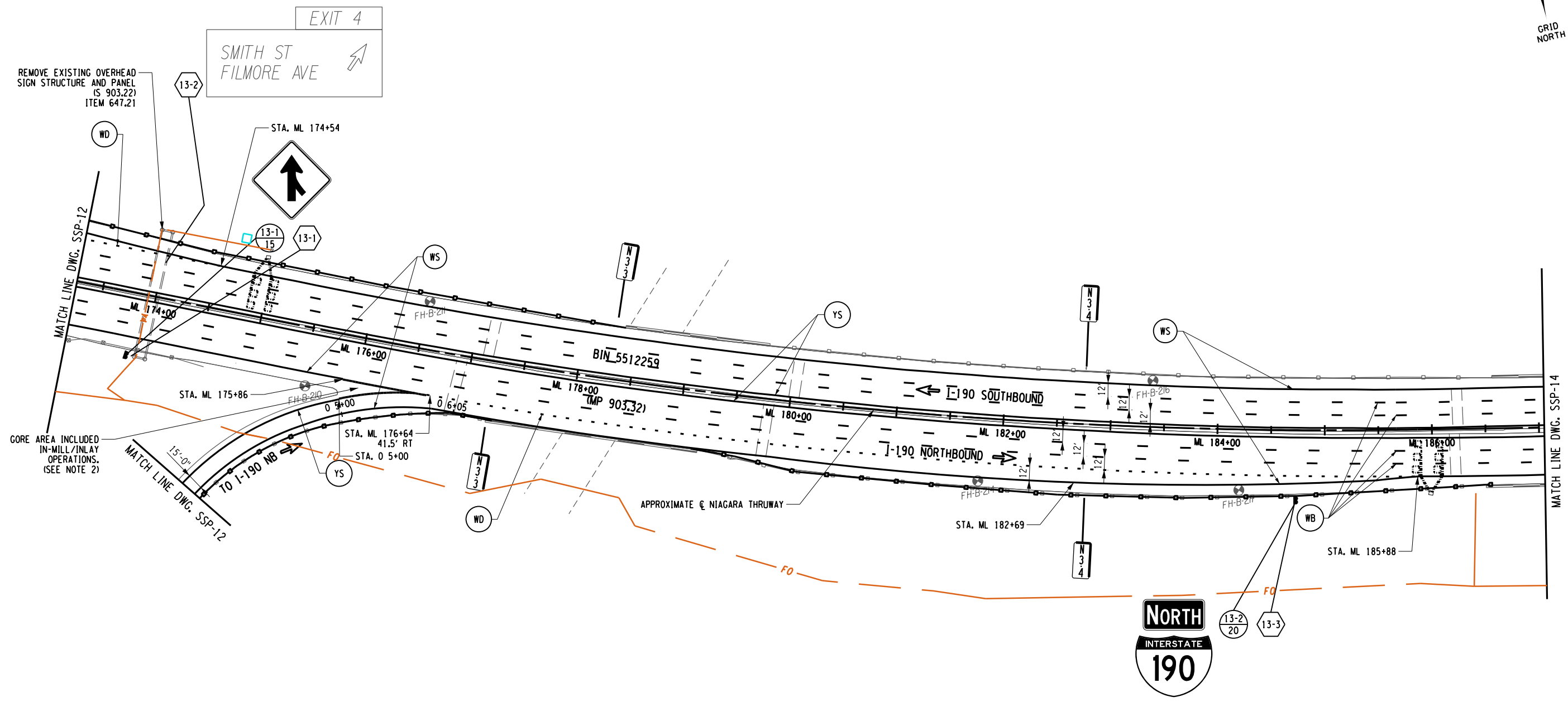
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



ALTERED ON:  
SIGNATURE:  
STAMP:

AFFIXED ON: 5/10/2024  
SIGNATURE:  
STAMP:

- NOTES:
1. SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.
  2. SEE NYSTA STANDARD SHEETS TA 685-01 & 685-02 STRIPING LAYOUT.

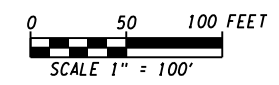
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 TITLE OF DRAWING: SIGNING AND STRIPING PLANS

CONTRACT NUMBER: TAN 24-12  
 DATE: MAY 2024  
 DRAWING NUMBER: SSP-13





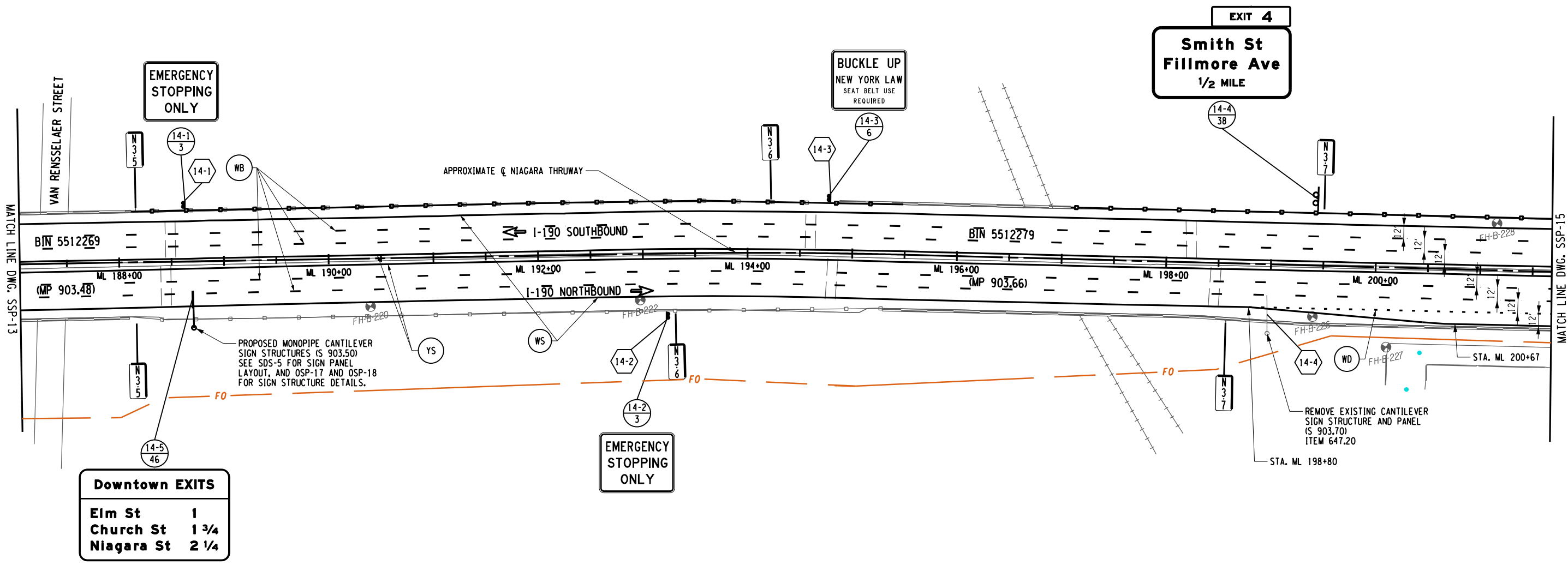
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



Downtown EXITS	
Elm St	1
Church St	1 3/4
Niagara St	2 1/4

PROPOSED MONOPIPE CANTILEVER SIGN STRUCTURES (S 903.50) SEE SDS-5 FOR SIGN PANEL LAYOUT, AND OSP-17 AND OSP-18 FOR SIGN STRUCTURE DETAILS.

REMOVE EXISTING CANTILEVER SIGN STRUCTURE AND PANEL (S 903.70) ITEM 647.20

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

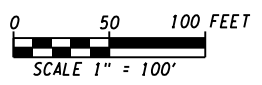
NOTES:  
1. SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING SIGNING AND STRIPING PLANS	DRAWING NUMBER: SSP-14



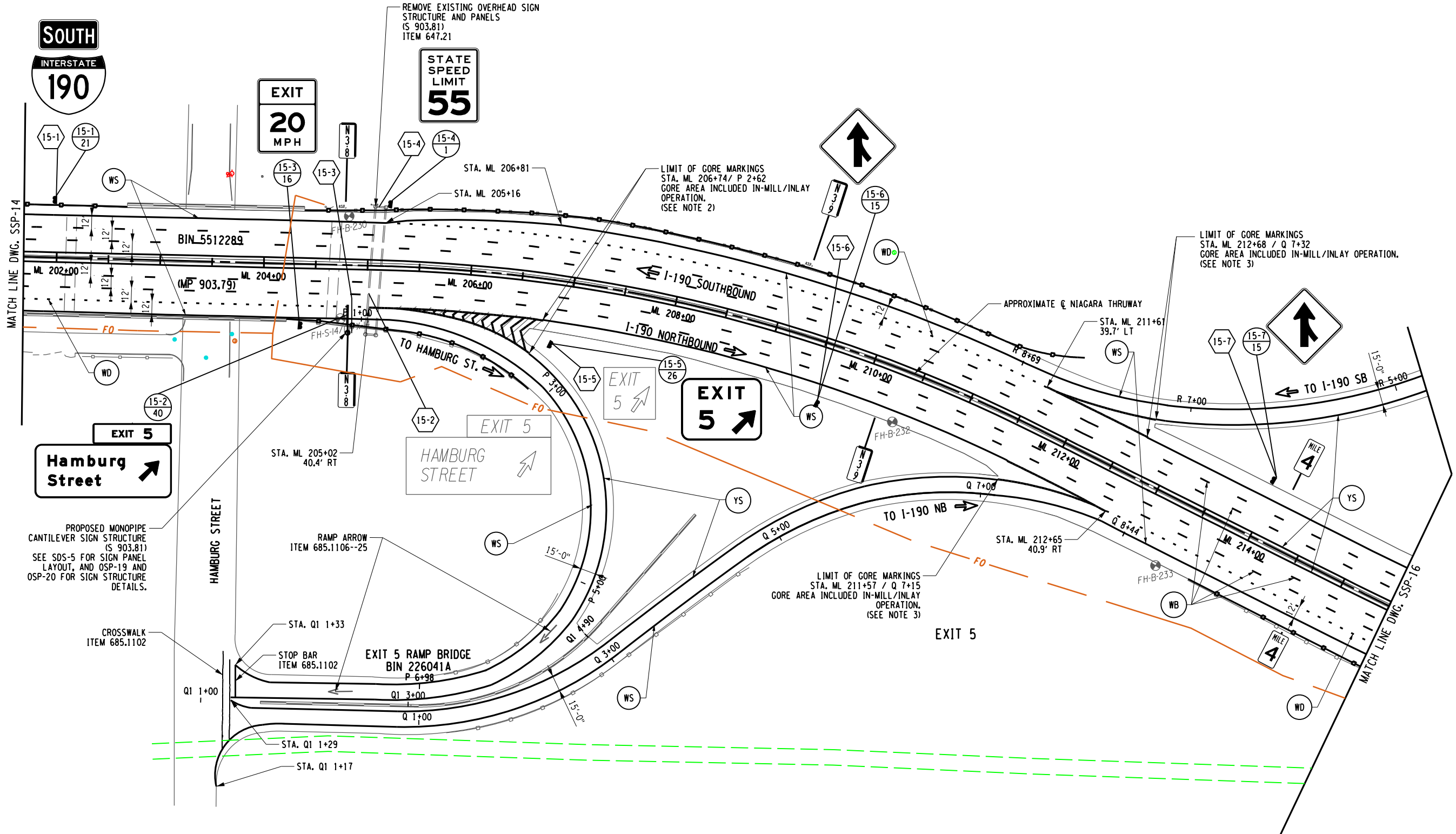
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



PROPOSED MONOPIPE CANTILEVER SIGN STRUCTURE (S 903.81) SEE SDS-5 FOR SIGN PANEL LAYOUT, AND OSP-19 AND OSP-20 FOR SIGN STRUCTURE DETAILS.

CROSSWALK ITEM 685.1102

STA. ML 205+02 40.4' RT

EXIT 5 RAMP BRIDGE BIN 226041A P-6+98

STA. Q1 1+33

STOP BAR ITEM 685.1102

STA. Q1 1+29

STA. Q1 1+17

HAMBURG STREET

EXIT 5

LIMIT OF GORE MARKINGS STA. ML 212+68 / Q 7+32 GORE AREA INCLUDED IN-MILL/INLAY OPERATION. (SEE NOTE 3)

LIMIT OF GORE MARKINGS STA. ML 211+57 / Q 7+15 GORE AREA INCLUDED IN-MILL/INLAY OPERATION. (SEE NOTE 3)

LIMIT OF GORE MARKINGS STA. ML 206+74 / P 2+62 GORE AREA INCLUDED IN-MILL/INLAY OPERATION. (SEE NOTE 2)

REMOVE EXISTING OVERHEAD SIGN STRUCTURE AND PANELS (S 903.81) ITEM 647.21

NOTES:

1. SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.
2. INSTALL 2' WIDE CHEVRON MARKINGS IN GORE AREA IN ACCORDANCE WITH NYSTA STANDARD SHEETS TA 685-01 & 865-02.
3. SEE NYSTA STANDARD SHEETS TA 685-01 & 685-02 FOR STRIPING LAYOUT.

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AFFIXED ON: 5/10/2024

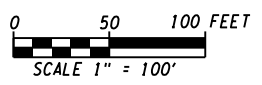
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SIGNATURE: STAMP:

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING SIGNING AND STRIPING PLANS	DRAWING NUMBER: SSP-15



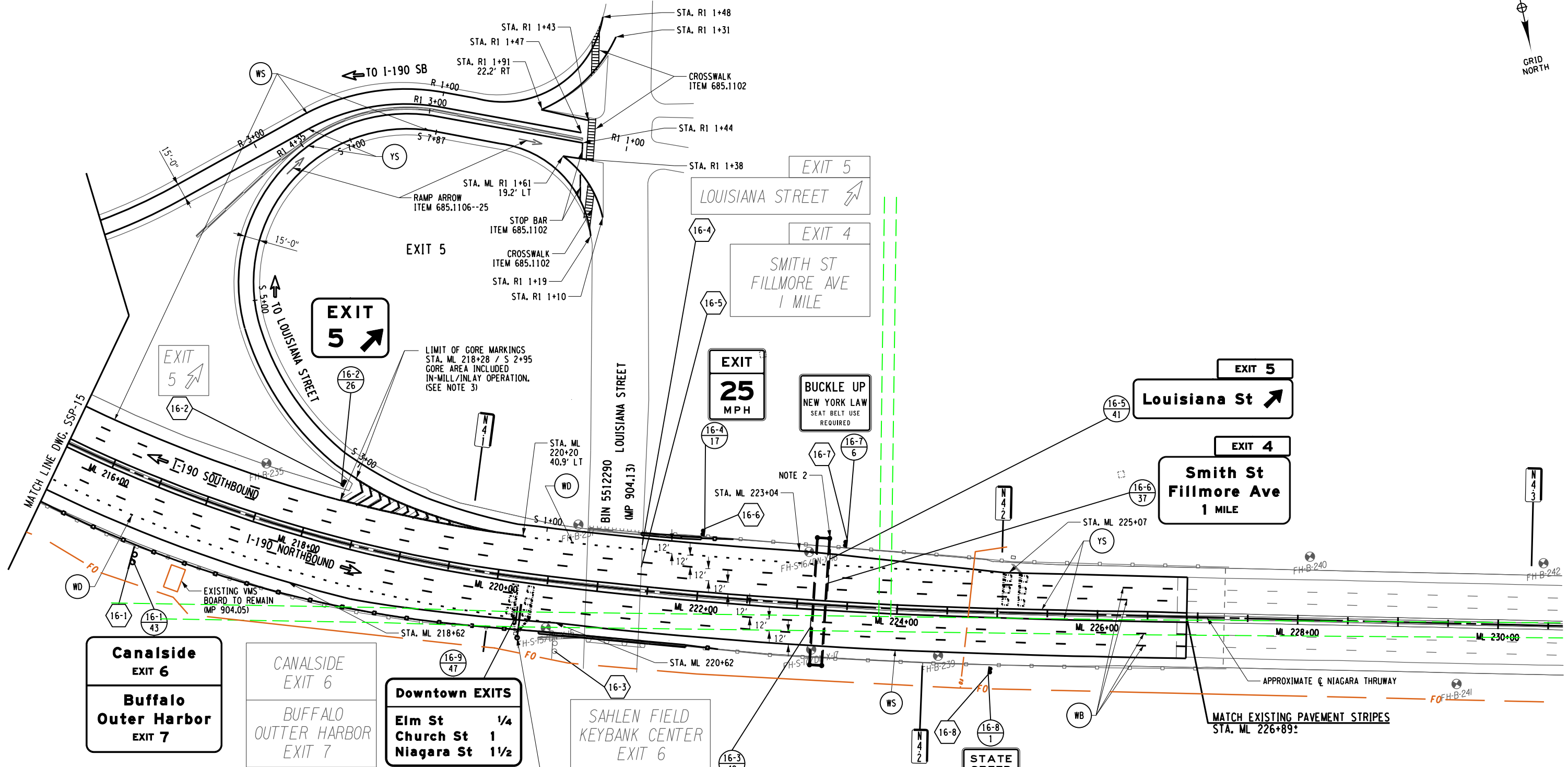
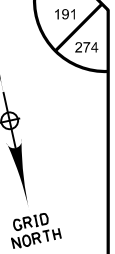
CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH



**Canalside**  
EXIT 6

**Buffalo Outer Harbor**  
EXIT 7

CANALSIDE  
EXIT 6

BUFFALO  
OUTTER HARBOR  
EXIT 7

**Downtown EXITS**

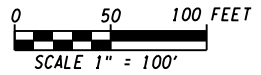
Elm St 1/4  
Church St 1  
Niagara St 1 1/2

SAHLEN FIELD  
KEYBANK CENTER  
EXIT 6

**Sahlen Field  
KeyBank Center**  
EXIT 6

PROPOSED MONOPIPE  
CANTILEVER SIGN STRUCTURE  
(S904.11)  
SEE SDS-5 FOR SIGN PANEL  
LAYOUT, AND OSP-21 AND  
OSP-22 FOR SIGN STRUCTURE  
DETAILS.

- NOTES:
- SEE DWG. SSP-1 FOR SIGN LEGEND AND SSP-2 FOR PAVEMENT MARKING NOTES.
  - PROPOSED OVERHEAD SIGN STRUCTURE (S904.16). SEE SDS-4 AND SDS-5 FOR SIGN PANEL LAYOUT, AND OSP-23 AND OSP-24 FOR SIGN STRUCTURE DETAILS.
  - INSTALL 2' WIDE CHEVRON MARKINGS IN GORE AREA IN ACCORDANCE WITH NYSTA STANDARD SHEETS TA 685-01 & 685-02.



ALTERED ON:

AFFIXED ON: 5/10/2024

SIGNATURE:  
STAMP:

SIGNATURE:  
STAMP:



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY**

**Thruway Authority**

TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
SIGNING AND STRIPING PLANS

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
SSP-16






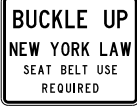

CHECKED BY: J. KLEINMANN




DRAFTED BY: J. PAGE

CHECKED BY: J. KLEINMANN

DESIGNED BY: J. PAGE


DESIGN SUPERVISOR: D. MARSH

TEXT NUMBER	DESIGNATION & COLOR (SEE NOTE 2)	LOCATION	TEXT	MOUNT	ITEM	PAYMENT FACTOR (SEE NOTES 3 & 4)	TOTAL QUANTITY (SEE NOTE 5)
1	NYR2-2	4-1 4-3 9-3 15-4 16-8 5	 48" X 60"	GROUND	645.5102	20.0 SF	100.0 SF
					645.81	2 EA	10 EA
2	R3-4	2-1 1	 36" X 36"	GROUND	645.5102	9.0 SF	9.0 SF
					645.81	2 EA	2 EA
3	R8-7	3-2 4-2 7-3 10-4 14-1 14-2 6	 48" X 36"	GROUND	645.5102	12.0 SF	72.0 SF
					645.81	2 EA	12 EA
4	NYR9-11	5-2 12-3 2	 48" X 84"	GROUND	645.5102	28.0 SF	56.0 SF
					645.81	2 EA	4 EA
5	NYR9-12	5-9 1	 48" X 72"	GROUND	645.5102	24.0 SF	24.0 SF
					645.81	2 EA	2 EA
6	R16-03PNY	4-4 14-3 16-7 3	 48" X 36"	GROUND	645.5102	12.0 SF	36.0 SF
					645.81	2 EA	6 EA
7	W1-2	8-1 1	 36" X 36"	GROUND	645.5102	9.0 SF	9.0 SF
					645.81	2 EA	2 EA

TEXT NUMBER	DESIGNATION & COLOR (SEE NOTE 2)	LOCATION	TEXT	MOUNT	ITEM	PAYMENT FACTOR (SEE NOTES 3 & 4)	TOTAL QUANTITY (SEE NOTE 5)
8	W1-2R	12-6 1	 48" X 48"	GROUND	645.5102	16.0 SF 6.3 SF	22.3 SF
					645.81	2 EA	2 EA
9	W1-2R	12-7 1	 48" X 48"	GROUND	645.5102	16.0 SF 6.3 SF	22.3 SF
					645.81	2 EA	2 EA
10	W1-2R	6-1 1	 48" X 48"	GROUND	645.5102	16.0 SF 6.3 SF	22.3 SF
					645.81	2 EA	2 EA

**SIGNING NOTES:**



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- THE TOTAL QUANTITY IS OBTAINED BY MULTIPLYING THE NUMBER OF LOCATIONS (SHOWN IN THE LOWER RIGHT CORNER OF THE LOCATIONS BLOCK) BY THE PAYMENT FACTOR.
- SEE SIGN REMOVAL TABLE ON DWG. MT-18.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

ITEM	DESCRIPTION	UNITS
645.5102	GROUND-MOUNTED SIGN PANELS LESS THAN OR EQUAL TO 32 SF, WITH Z-BARS	SF
645.5103	GROUND-MOUNTED SIGN PANELS GREATER THAN 32 SF, WITH Z-BARS	SF
645.61	OVERHEAD SIGN PANELS	SF
645.81	TYPE A SIGN POSTS	EA
645.830502	TYPE B SIGN POST, GALVANIZED, W10X19 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA
645.830702	TYPE B SIGN POST, GALVANIZED, W12X26 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA
645.830802	TYPE B SIGN POST, GALVANIZED, W14X34 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING SIGN DATA SHEET	DRAWING NUMBER: SDS-1









CHECKED BY: J. KLEINMANN












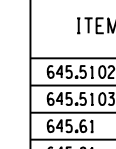
DRAFTED BY: J. PAGE

CHECKED BY: J. KLEINMANN

DESIGNED BY: J. PAGE

DESIGN SUPERVISOR: D. MARSH

TEXT NUMBER	DESIGNATION & COLOR (SEE NOTE 2)	LOCATION	TEXT	MOUNT	ITEM	PAYMENT FACTOR (SEE NOTES 3 & 4)	TOTAL QUANTITY (SEE NOTE 5)
11	W1-2L	7-1	 48" X 48"	GROUND	645.5102	16.0 SF 6.3 SF	22.3 SF
	W13-1P		 30" X 30"		645.81	2 EA	2 EA
12	W1-2L	9-2	 48" X 48"	GROUND	645.5102	16.0 SF 6.3 SF	22.3 SF
	W13-1P		 30" X 30"		645.81	2 EA	2 EA
13	W1-15R	5-8	 48" X 48"	GROUND	645.5102	16.0 SF 12.0 SF	28.0 SF
	W13-3		 36" X 48"		645.81	2 EA	2 EA


TEXT NUMBER	DESIGNATION & COLOR (SEE NOTE 2)	LOCATION	TEXT	MOUNT	ITEM	PAYMENT FACTOR (SEE NOTES 3 & 4)	TOTAL QUANTITY (SEE NOTE 5)
14	W1-15R	3-1	 48" X 48"	GROUND	645.5102	16.0 SF 12.0 SF	28.0 SF
	W13-3		 36" X 48"		645.81	2 EA	2 EA
15	W4-1	2-3 5-1 6-2 9-1 12-1 13-1 15-6 15-7	 48" X 48"	GROUND	645.5102	16.0 SF	128.0 SF
			 36" X 48"		645.81	2 EA	16 EA
16	W13-2	12-8 15-3	 36" X 48"	GROUND	645.5102	12.0 SF	24.0 SF
			 36" X 48"		645.81	2 EA	4 EA
17	W13-2	16-4	 36" X 48"	GROUND	645.5102	12.0 SF	12.0 SF
			 36" X 48"		645.81	2 EA	2 EA
18	W13-2	8-6	 36" X 48"	GROUND	645.5102	12.0 SF	12.0 SF
			 36" X 48"		645.81	2 EA	2 EA
19	W13-2	1-5 5-3	 36" X 48"	GROUND	645.5102	12.0 SF	24.0 SF
			 36" X 48"		645.81	2 EA	4 EA

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ITEM	DESCRIPTION	UNITS
645.5102	GROUND-MOUNTED SIGN PANELS LESS THAN OR EQUAL TO 32 SF, WITH Z-BARS	SF
645.5103	GROUND-MOUNTED SIGN PANELS GREATER THAN 32 SF, WITH Z-BARS	SF
645.61	OVERHEAD SIGN PANELS	SF
645.81	TYPE A SIGN POSTS	EA
645.830502	TYPE B SIGN POST, GALVANIZED, W10X19 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA
645.830702	TYPE B SIGN POST, GALVANIZED, W12X26 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA
645.830802	TYPE B SIGN POST, GALVANIZED, W14X34 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	CONTRACT NUMBER: TAN 24-12 DATE: MAY 2024 DRAWING NUMBER: SDS-2
	TITLE OF DRAWING SIGN DATA SHEET	








CHECKED BY: J. KLEINMANN



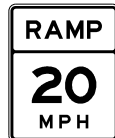
DRAFTED BY: J. PAGE

CHECKED BY: J. KLEINMANN

DESIGNED BY: J. PAGE

DESIGN SUPERVISOR: D. MARSH

TEXT NUMBER	DESIGNATION & COLOR (SEE NOTE 2)	LOCATION	TEXT	MOUNT	ITEM	PAYMENT FACTOR (SEE NOTES 3 & 4)	TOTAL QUANTITY (SEE NOTE 5)
20	M3-1	3-3 7-4 10-1 13-2	 36" X 18"	GROUND	645.5102	4.5 SF 11.3 SF	63.2 SF
	M1-1		 45" X 36"		645.81	2 EA	8 EA
21	M3-3	10-5 15-1	 36" X 18"	GROUND	645.5102	4.5 SF 11.3 SF	31.6 SF
	M1-1		 45" X 36"		645.81	2 EA	4 EA
22	E5-1a	1-3 2-2	 78" X 60"	GROUND	645.5103	32.5 SF	65.0 SF
					645.81	2 EA	4 EA
23	E5-1a	5-4 5-7	 78" X 60"	GROUND	645.5103	32.5 SF	65.0 SF
					645.81	2 EA	4 EA
24	E5-1a	8-3	 78" X 60"	GROUND	645.5103	32.5 SF	32.5 SF
					645.81	2 EA	2 EA


TEXT NUMBER	DESIGNATION & COLOR (SEE NOTE 2)	LOCATION	TEXT	MOUNT	ITEM	PAYMENT FACTOR (SEE NOTES 3 & 4)	TOTAL QUANTITY (SEE NOTE 5)
25	E5-1a	12-2 12-10	 78" X 60"	GROUND	645.5103	32.5 SF	65.0 SF
					645.81	2 EA	4 EA
26	E5-1a	15-5 16-2	 78" X 60"	GROUND	645.5103	32.5 SF	65.0 SF
					645.81	2 EA	4 EA
27	W13-3	1-4	 36" X 48"	GROUND	645.5102	12.0 SF	12.0 SF
					645.81	2 EA	2 EA

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645.5102	GROUND-MOUNTED SIGN PANELS LESS THAN OR EQUAL TO 32 SF, WITH Z-BARS	SF
645.5103	GROUND-MOUNTED SIGN PANELS GREATER THAN 32 SF, WITH Z-BARS	SF
645.61	OVERHEAD SIGN PANELS	SF
645.81	TYPE A SIGN POSTS	EA
645.830502	TYPE B SIGN POST, GALVANIZED, W10X19 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA
645.830702	TYPE B SIGN POST, GALVANIZED, W12X26 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA
645.830802	TYPE B SIGN POST, GALVANIZED, W14X34 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING SIGN DATA SHEET	DRAWING NUMBER: SDS-3

CHECKED BY: J. KLEINMANN  
 DRAFTED BY: J. PAGE  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: J. PAGE  
 DESIGN SUPERVISOR: D. MARSH

TEXT NUMBER	DESIGNATION & COLOR (SEE NOTE 2)	LOCATION	TEXT	MOUNT	ITEM	PAYMENT FACTOR (SEE NOTES 3 & 4)	TOTAL QUANTITY (SEE NOTE 5)
28	E1-5P SPECIAL (WHITE ON GREEN)	1-2	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 1</div> 114" X 30" <div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>S Ogden St</b>  <b>Dingens St</b> </div> 222" X 78"	GROUND	645.5103	144.1 SF	144.1 SF
			645.830502		2 EA	2 EA	
29	E1-5P SPECIAL (WHITE ON GREEN)	3-6	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 1</div> 114" X 30" <div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>S Ogden St</b>  <b>1/4 MILE</b> </div> 174" X 72"	OVERHEAD	645.61	110.8 SF	110.8 SF
			1				
30	E1-5P SPECIAL (WHITE ON GREEN)	5-6	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 1</div> 114" X 30" <div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>S Ogden St</b>  <b>3/4 MILE</b> </div> 174" X 72"	OVERHEAD	645.61	110.8 SF	110.8 SF
			1				
31	E1-5P SPECIAL (WHITE ON GREEN)	8-4	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 2</div> 114" X 30" <div style="border: 1px solid black; padding: 5px; display: inline-block;"> <div style="display: flex; align-items: center; gap: 5px;"> <span style="border: 1px solid black; padding: 2px;">354</span> <b>Clinton St</b> </div> <div style="display: flex; align-items: center; gap: 5px;"> <span style="border: 1px solid black; padding: 2px;">62</span> <b>Bailey Ave</b> </div> <b>1 MILE</b> </div> 228" X 144"	OVERHEAD	645.61	251.8 SF	251.8 SF
			1				
32	E1-5P SPECIAL (WHITE ON GREEN)	3-4, 7-2	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 2</div> 114" X 30" <div style="border: 1px solid black; padding: 5px; display: inline-block;"> <div style="display: flex; align-items: center; gap: 5px;"> <span style="border: 1px solid black; padding: 2px;">354</span> <b>Clinton St</b> </div> <div style="display: flex; align-items: center; gap: 5px;"> <span style="border: 1px solid black; padding: 2px;">62</span> <b>Bailey Ave</b> </div> <b>1/2 MILE</b> </div> 228" X 144"	1 GROUND & 1 OVERHEAD	645.5103 645.61	251.8 SF	503.6 SF
			2		645.830802	3 EA	3 EA

TEXT NUMBER	DESIGNATION & COLOR (SEE NOTE 2)	LOCATION	TEXT	MOUNT	ITEM	PAYMENT FACTOR (SEE NOTES 3 & 4)	TOTAL QUANTITY (SEE NOTE 5)
33	E1-5P SPECIAL (WHITE ON GREEN)	5-5 5-10	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 2</div> 114" X 30" <div style="border: 1px solid black; padding: 5px; display: inline-block;"> <div style="display: flex; align-items: center; gap: 5px;"> <span style="border: 1px solid black; padding: 2px;">354</span> <b>Clinton St</b> </div> <div style="display: flex; align-items: center; gap: 5px;"> <span style="border: 1px solid black; padding: 2px;">62</span> <b>Bailey Ave</b> </div> </div> 276" X 114"	2 OVERHEAD	645.61	242.3 SF	969.2 SF
			2				
34	E1-5P SPECIAL (WHITE ON GREEN)	12-4	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 3</div> 114" X 30" <div style="border: 1px solid black; padding: 5px; display: inline-block;"> <div style="border: 1px solid black; padding: 2px; display: inline-block;">16</div> <b>Seneca St</b>  <b>1 MILE</b> </div> 216" X 96"	OVERHEAD	645.61	167.8 SF	167.8 SF
			1				
35	E1-5P SPECIAL (WHITE ON GREEN)	10-3	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 3</div> 114" X 30" <div style="border: 1px solid black; padding: 5px; display: inline-block;"> <div style="border: 1px solid black; padding: 2px; display: inline-block;">16</div> <b>Seneca St</b>  <b>1/2 MILE</b> </div> 216" X 96"	GROUND	645.5103	167.8 SF	167.8 SF
			1			645.830702	3 EA
36	E1-5P SPECIAL (WHITE ON GREEN)	8-5	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 3</div> 114" X 30" <div style="border: 1px solid black; padding: 5px; display: inline-block;"> <div style="border: 1px solid black; padding: 2px; display: inline-block;">16</div> <b>Seneca St</b> </div> 258" X 72"	1 OVERHEAD	645.61	152.8 SF	305.6 SF
			1				
37	E1-5P SPECIAL (WHITE ON GREEN)	8-2, 16-6	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 4</div> 114" X 30" <div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>Smith St</b>  <b>Fillmore Ave</b>  <b>1 MILE</b> </div> 198" X 102"	OVERHEAD	645.61	164.1 SF	328.2 SF
			2				

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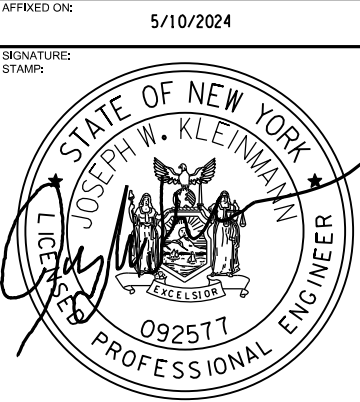
ITEM	DESCRIPTION	UNITS
645.5102	GROUND-MOUNTED SIGN PANELS LESS THAN OR EQUAL TO 32 SF, WITH Z-BARS	SF
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645.61	OVERHEAD SIGN PANELS	SF
645.81	TYPE A SIGN POSTS	EA
645.830502	TYPE B SIGN POST, GALVANIZED, W10X19 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA
645.830702	TYPE B SIGN POST, GALVANIZED, W12X26 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA
645.830802	TYPE B SIGN POST, GALVANIZED, W14X34 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING SIGN DATA SHEET	DRAWING NUMBER: SDS-4



CHECKED BY: J. KLEINMANN  
 DRAFTED BY: J. PAGE  
 CHECKED BY: J. KLEINMANN  
 DESIGNED BY: J. PAGE  
 DESIGN SUPERVISOR: D. MARSH

TEXT NUMBER	DESIGNATION & COLOR (SEE NOTE 2)	LOCATION	TEXT	MOUNT	ITEM	PAYMENT FACTOR (SEE NOTES 3 & 4)	TOTAL QUANTITY (SEE NOTE 5)
38	E1-5P SPECIAL (WHITE ON GREEN)	14-4 1	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 4</div> 114" X 30"	GROUND	645.5103	164.1 SF	164.1 SF
			<div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>Smith St Fillmore Ave</b>            1/2 MILE            198" X 102"         </div>		645.830802	2 EA	2 EA
39	E1-5P SPECIAL (WHITE ON GREEN)	12-5, 12-9 2	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 4</div> 114" X 30"	2 OVERHEAD	645.61	157.1 SF	471.3 SF
			<div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>Smith St Fillmore Ave</b> </div> 246" X 78"				
40	E1-5P SPECIAL (WHITE ON GREEN)	15-2 1	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 5</div> 114" X 30"	OVERHEAD	645.61	131.1 SF	131.1 SF
			<div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>Hamburg Street</b> </div> 198" X 78"				
41	E1-5P SPECIAL (WHITE ON GREEN)	16-5 1	<div style="border: 1px solid black; padding: 2px; display: inline-block;">EXIT 5</div> 114" X 30"	OVERHEAD	645.61	123.8 SF	123.8 SF
			<div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>Louisiana St</b> </div> 240" X 60"				
42	SPECIAL (WHITE ON GREEN)	16-3 1	<div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>Sahlen Field KeyBank Center</b>            EXIT 6            198" X 84"         </div>	OVERHEAD	645.61	115.5 SF	115.5 SF

TEXT NUMBER	DESIGNATION & COLOR (SEE NOTE 2)	LOCATION	TEXT	MOUNT	ITEM	PAYMENT FACTOR (SEE NOTES 3 & 4)	TOTAL QUANTITY (SEE NOTE 5)
43	SPECIAL (WHITE ON GREEN)	16-1 1	<div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>Canalside EXIT 6</b> </div>	GROUND	645.5103	154.0 SF	154.0 SF
			<div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>Buffalo Outer Harbor EXIT 7</b>            168" X 132"         </div>		645.830802	3 EA	3 EA
44	D2-3	10-2 1	<div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>Smith St 1/2 Hamburg St 1 Elm St 1 3/4</b>            222" X 96"         </div>	OVERHEAD	645.61	148.0 SF	148.0 SF
45	D2-3	12-11 1	<div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>Hamburg St 3/4 Elm St 1 1/2 Church St 2 1/4</b>            228" X 96"         </div>	OVERHEAD	645.61	152.0 SF	152.0 SF
46	SPECIAL (WHITE ON GREEN)	14-5 1	<div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>Downtown EXITS</b>  <b>Elm St 1 Church St 1 3/4 Niagara St 2 1/4</b>            216" X 132"         </div>	OVERHEAD	645.61	198.0 SF	198.0 SF
47	SPECIAL (WHITE ON GREEN)	16-9 1	<div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>Downtown EXITS</b>  <b>Elm St 1/4 Church St 1 Niagara St 1 1/2</b>            216" X 132"         </div>	OVERHEAD	645.61	198.0 SF	198.0 SF

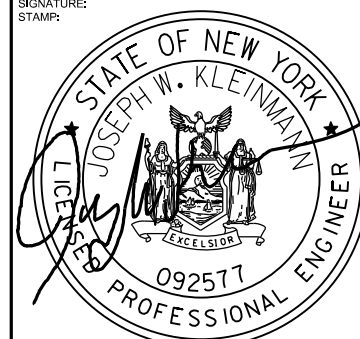
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- THE PAYMENT FACTOR FOR SIGNS ARE FROM THE APPLICABLE NYS DOT STANDARD SHEETS OR SIGN FACE LAYOUTS.
- THE PAYMENT FACTOR FOR POSTS IS THE NUMBER OF POSTS PROVIDED PER INSTALLATION.
- THE TOTAL QUANTITY IS OBTAINED BY MULTIPLYING THE NUMBER OF LOCATIONS (SHOWN IN THE LOWER RIGHT CORNER OF THE LOCATIONS BLOCK) BY THE PAYMENT FACTOR.
- SEE SIGN REMOVAL TABLE ON DWG. MT-18.


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ITEM	DESCRIPTION	UNITS
645.5102	GROUND-MOUNTED SIGN PANELS LESS THAN OR EQUAL TO 32 SF, WITH Z-BARS	SF
645.5103	GROUND-MOUNTED SIGN PANELS GREATER THAN 32 SF, WITH Z-BARS	SF
645.61	OVERHEAD SIGN PANELS	SF
645.81	TYPE A SIGN POSTS	EA
645.830502	TYPE B SIGN POST, GALVANIZED, W10X19 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA
645.830702	TYPE B SIGN POST, GALVANIZED, W12X26 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA
645.830802	TYPE B SIGN POST, GALVANIZED, W14X34 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA


ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:



REVISIONS			
DATE	DESCRIPTION	BY	SYM.



**Thruway Authority**



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING SIGN DATA SHEET	DRAWING NUMBER: SDS-5

CHECKED BY: J. KLEINMANN  
 DRAFTED BY:  
 CHECKED BY:  
 DESIGNED BY:

TEXT NUMBER	DESIGNATION & COLOR (SEE NOTE 2)	LOCATION	TEXT	MOUNT	ITEM	PAYMENT FACTOR (SEE NOTES 3 & 4)	TOTAL QUANTITY (SEE NOTE 5)
48	D2-2	3-7 1	<div style="border: 1px solid black; padding: 5px; display: inline-block;"> <b>Downtown Niagara Falls</b> </div> 138" X 42"	GROUND	645.5103	40.3 SF	40.3 SF
					645.830702	2 EA	2 EA
49	E1-5P E6-2a	1-1 1	<div style="display: flex; justify-content: space-around;"> <div style="border: 1px solid black; padding: 5px; text-align: center;"> <b>EXITS 1-52</b> 156" X 30"           </div> <div style="border: 1px solid black; padding: 5px; text-align: center;"> <b>EXITS 54-61</b> 156" X 30"           </div> </div> <div style="border: 1px solid black; padding: 10px; margin-top: 10px; text-align: center;"> <div style="display: flex; justify-content: space-between;"> <div style="text-align: center;">   <b>EAST</b>  <b>Buffalo Airport Albany</b>  </div> <div style="text-align: center;"> </div> <div style="text-align: center;">   <b>WEST</b>  <b>Erie</b>  </div> </div> <div style="margin-top: 10px; text-align: center;"> <div style="display: flex; justify-content: center; gap: 20px;"> <div style="border: 1px solid black; padding: 2px;">EXIT</div> <div style="border: 1px solid black; padding: 2px;">ONLY</div> </div> </div> </div> 456" X 204"	OVERHEAD	645.61	711.0 SF	711.0 SF
50	E6-2a	3-5 1	<div style="display: flex; justify-content: space-around;"> <div style="text-align: center;">   <b>EAST</b>  <b>Buffalo Airport Albany</b>  <b>KEEP LEFT</b> </div> <div style="text-align: center;">   <b>WEST</b>  <b>Erie</b>  <b>KEEP RIGHT</b> </div> </div> <p style="text-align: center; margin-top: 10px;"><b>JCT 3/4 MILE</b></p> 360" X 180"	OVERHEAD	645.61	450.0 SF	450.0 SF

**SIGNING NOTES:**

- SIGN LOCATIONS AS SHOWN ON PLANS ARE APPROXIMATE. THE CONTRACTOR SHALL RELOCATE EXISTING SIGNS AND INSTALL NEW SIGNS IN ACCORDANCE WITH THE MUTCD AND NYS SUPPLEMENT. THE ENGINEER SHOULD CONTACT THE REGIONAL TRAFFIC ENGINEERING AND SAFETY GROUP WITH QUESTIONS.
- THE COLOR IS ONLY SHOWN WHEN THERE IS AN OPTION THAT MUST BE SPECIFIED.
- THE PAYMENT FACTOR FOR SIGNS ARE FROM THE APPLICABLE NYS DOT STANDARD SHEETS OR SIGN FACE LAYOUTS.
- THE PAYMENT FACTOR FOR POSTS IS THE NUMBER OF POSTS PROVIDED PER INSTALLATION.
- THE TOTAL QUANTITY IS OBTAINED BY MULTIPLYING THE NUMBER OF LOCATIONS (SHOWN IN THE LOWER RIGHT CORNER OF THE LOCATIONS BLOCK) BY THE PAYMENT FACTOR.
- SEE SIGN REMOVAL TABLE ON DWG. MT-18.

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ITEM	DESCRIPTION	UNITS
645.5102	GROUND-MOUNTED SIGN PANELS LESS THAN OR EQUAL TO 32 SF, WITH Z-BARS	SF
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645.830802	TYPE B SIGN POST, GALVANIZED, W14X34 SECTION, BI-DIRECTIONAL BREAKAWAY BASE	EA

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING SIGN DATA SHEET	DRAWING NUMBER: SDS-6

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

TABLE OF PROPOSED EPOXY PAVEMENT MARKINGS

Table with 7 columns: LOCATION (FROM STA., TO STA.), COLOR AND TYPE OF LINE, WIDTH (IN), LENGTH (LF), ITEM 685.1707--25 (LF), ITEM 685.1708--25 (LF), ITEM 635.04030225 (LF). Includes sub-headers for MAINLINE SOUTHBOUND and SUBTOTAL.

TABLE OF PROPOSED EPOXY PAVEMENT MARKINGS

Table with 7 columns: LOCATION (FROM STA., TO STA.), COLOR AND TYPE OF LINE, WIDTH (IN), LENGTH (LF), ITEM 685.1707--25 (LF), ITEM 685.1708--25 (LF), ITEM 635.04030225 (LF). Includes sub-headers for MAINLINE NORTHBOUND and SHEET TOTAL.

TABLE OF PROPOSED EPOXY PAVEMENT MARKINGS (RAMP ARROWS)

Table with 5 columns: LOCATION, COLOR AND TYPE OF LINE, WIDTH (IN), LENGTH (LF), ITEM 685.1106--25 (WHITE). Includes sub-headers for INTERCHANGE 1 NB OFF-RAMP (RAMP A), INTERCHANGE 1 SB OFF-RAMP (RAMP D), INTERCHANGE 2 NB OFF-RAMP (RAMP G), INTERCHANGE 3 SB OFF-RAMP (RAMP J), INTERCHANGE 4 SB OFF-RAMP (RAMP M), INTERCHANGE 4 NB OFF-RAMP (RAMP N), INTERCHANGE 5 NB OFF-RAMP (RAMP P), INTERCHANGE 5 SB OFF-RAMP (RAMP S), and SHEET TOTAL.

TABLE OF PROPOSED EPOXY PAVEMENT MARKINGS (RAMPS)

Table with 6 columns: LOCATION (FROM STA., TO STA.), COLOR AND TYPE OF LINE, WIDTH (IN), LENGTH (LF), ITEM 685.1106--25 (WHITE), ITEM 685.1206--25 (YELLOW), ITEM 685.1102 (WHITE). Includes sub-headers for INTERCHANGE 1 NB OFF-RAMP (RAMP A), INTERCHANGE 1 SB ON-RAMP (RAMP B), INTERCHANGE 1 NB ON-RAMP (RAMP C), INTERCHANGE 1 SB OFF-RAMP (RAMP D), and SHEET TOTAL.

ALTERED ON: AFFIXED ON: 5/10/2024. SIGNATURE: STAMP: Professional Engineer seal for Joseph W. Kleinmann, License No. 092577.

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REVISIONS table with columns: DATE, DESCRIPTION, BY, SYM.

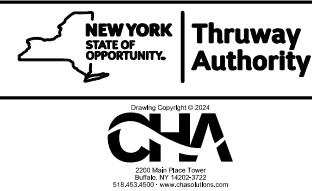


Table with 3 columns: ITEM, DESCRIPTION, UNITS. Lists items 685.1102, 685.1106--25, and 685.1206--25 with their descriptions and units.

Table with 2 columns: TITLE OF PROJECT, CONTRACT NUMBER. Includes location information for I-190 Niagara Thruway and drawing number PMT-1.

CHECKED BY: J. KLEINMANN

DRAFTED BY: N. HOY

CHECKED BY: J. KLEINMANN

DESIGNED BY: P. THOMPSON

DESIGN SUPERVISOR: D. MARSH

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TABLE OF PROPOSED EPOXY PAVEMENT MARKINGS (RAMPS)							
LOCATION		COLOR AND TYPE OF LINE	WIDTH (IN)	LENGTH (LF)	ITEM 685.1106--25 (WHITE)	ITEM 685.1206--25 (YELLOW)	ITEM 685.1102 (WHITE)
FROM STA.	TO STA.						
INTERCHANGE 2 SB ON-RAMP (RAMP E)							
E 0+60	E 6+40	SOLID YELLOW	6	600		600	
E 0+60	ML 60+44	SOLID WHITE	6	1672	1672		
E 6+40	ML 70+55	CHANNELIZING WHITE	12	98	195		
INTERCHANGE 2 SB OFF-RAMP (RAMP F)							
SB 83+64	F 5+94	SOLID WHITE	6	992	992		
ML 78+44	F 2+30	CHANNELIZING WHITE	12	106	211		
F 2+30	F 5+98	SOLID YELLOW	6	403		403	
ML 78+44	F 2+30	HATCHING	24	215	860		
INTERCHANGE 2 NB OFF-RAMP (RAMP G)							
ML 69+83	G 11+07	SOLID WHITE	6	1685	1685		
ML 77+58	G 3+64	CHANNELIZING WHITE	12	190	380		
G 3+64	G 10+69	SOLID YELLOW	6	692		692	
ML 77+58	G 3+64	HATCHING	24	264	1058		
-	-	STOP BAR	18	55			165
-	-	CROSSWALK-TYPE S	12	163			326
INTERCHANGE 2 NB ON-RAMP (RAMP H)							
H 0+85	H 10+66	SOLID YELLOW	6	979		979	
H 1+05	H 1+57	SOLID WHITE	6	52	52		
H 1+43	H 1+57	SOLID YELLOW	6	44		44	
H 1+75	ML 107+21	SOLID WHITE	6	1925	1925		
H 10+66	ML 98+50	CHANNELIZING WHITE	12	100	199		
-	-	CROSSWALK-TYPE S	12	129			258
INTERCHANGE 3 SB OFF-RAMP (RAMP J)							
ML 131+32	J 11+52	SOLID WHITE	6	1492	1492		
ML 125+90	J 3+31	CHANNELIZING WHITE	12	133	266		
ML 125+90	J 3+31	HATCHING	24	119	476		
J 3+31	J 11+52	SOLID YELLOW	6	836		836	
-	-	CROSSWALK-TYPE S	12	58			115
INTERCHANGE 3 NB ON-RAMP (RAMP K)							
K 1+02	ML 139+21	SOLID WHITE	6	1582	1582		
K 0+39	K 6+36	SOLID YELLOW	6	597		597	
K 6+36	ML 130+52	CHANNELIZING WHITE	12	167	334		
-	-	CROSSWALK-TYPE S	12	142			284
INTERCHANGE 4 SB ON-RAMP (RAMP L)							
L1 1+43	L1 6+95	SOLID WHITE	6	616	616		
L1 1+43	L1 6+95	SOLID YELLOW	6	571		571	
L 1+00	ML 156+05	SOLID WHITE	6	1056	1056		
L 1+00	ML 165+21	SOLID YELLOW	6	172		172	
ML 165+21	ML 164+46	CHANNELIZING WHITE	12	67	133		
INTERCHANGE 4 SB OFF-RAMP (RAMP M)							
ML 174+54	M 3+74	SOLID WHITE	6	930	930		
M 2+29	M 3+74	SOLID YELLOW	6	159		159	
ML 167+59	M 2+29	CHANNELIZING WHITE	12	84	167		
ML 167+59	M 2+29	HATCHING	24	103	410		
L1 6+95	L1 1+44	SOLID WHITE	6	556	556		
L1 6+95	L1 1+44	SOLID YELLOW	6	56		56	
-	-	STOP BAR	18	31			92
-	-	CROSSWALK-TYPE LS	12	41			370

TABLE OF PROPOSED EPOXY PAVEMENT MARKINGS (RAMPS)							
LOCATION		COLOR AND TYPE OF LINE	WIDTH (IN)	LENGTH (LF)	ITEM 685.1106--25 (WHITE)	ITEM 685.1206--25 (YELLOW)	ITEM 685.1102 (WHITE)
FROM STA.	TO STA.						
INTERCHANGE 4 NB OFF-RAMP (RAMP N)							
NB 164+82	N 6+35	SOLID WHITE	6	1083	1083		
ML 170+88	N 2+73	CHANNELIZING WHITE	12	118	236		
ML 170+88	N 2+73	HATCHING	24	133	532		
N 2+73	N 6+35	SOLID YELLOW	6	387		387	
O 5+11	O1 1+45	SOLID WHITE	6	347		347	
O 5+11	O1 1+44	SOLID YELLOW	6	367		367	
-	-	STOP BAR	18	24			71
INTERCHANGE 4 NB ON-RAMP (RAMP O)							
O1 1+39	O1 5+11	SOLID WHITE	6	399	399		
O1 1+43	O1 5+11	SOLID YELLOW	6	371		371	
O 1+00	ML 185+88	SOLID WHITE	6	1404	1404		
O 1+00	O 5+00	SOLID YELLOW	6	412		412	
O 5+00	ML 176+64	CHANNELIZING WHITE	12	71	142		
-	-	CROSSWALK-TYPE S	12	144			287
INTERCHANGE 5 NB OFF-RAMP (RAMP P)							
ML 198+80	P 6+98	SOLID WHITE	6	1221	1221		
ML 205+02	P 2+62	CHANNELIZING WHITE	12	137	274		
ML 205+02	P 2+62	HATCHING	24	184	738		
P 2+62	P 6+98	SOLID YELLOW	6	459		459	
Q1 3+11	Q1 1+33	SOLID WHITE	6	184	184		
Q1 3+11	Q1 1+29	SOLID YELLOW	6	182		182	
-	-	STOP BAR	18	31			94
INTERCHANGE 5 NB ON-RAMP (RAMP Q)							
Q1 1+17	Q1 3+11	SOLID WHITE	6	235	235		
Q1 1+29	Q1 3+11	SOLID YELLOW	6	183		183	
Q 1+00	Q 7+15	SOLID YELLOW	6	618		618	
Q 1+00	NB 220+63	SOLID WHITE	6	1558	1558		
Q 7+15	ML 212+65	CHANNELIZING WHITE	12	97	195		
-	-	CROSSWALK-TYPE S	12	173			346
INTERCHANGE 5 SB ON-RAMP (RAMP R)							
R1 1+47	R1 2+90	SOLID YELLOW	6	142		142	
R1 1+48	R1 2+90	SOLID WHITE	6	360	360		
R 1+00	ML 205+16	SOLID WHITE	6	1381	1381		
R 1+00	R 7+32	SOLID YELLOW	6	631		631	
R1 1+91	R1 1+31	SOLID WHITE	12	106	212		
R1 1+91	R1 1+43	SOLID WHITE	12	48	96		
R 7+32	ML 211+61	CHANNELIZING WHITE	12	99	198		
INTERCHANGE 5 SB OFF-RAMP (RAMP S)							
ML 225+07	S 7+87	SOLID WHITE	6	1138	1138		
ML 220+20	S 2+95	CHANNELIZING WHITE	12	149	298		
ML 220+20	S 2+95	HATCHING	24	171	683		
S 2+95	S 7+87	SOLID YELLOW	6	526		526	
R1 1+19	R1 2+90	SOLID WHITE	6	268	268		
R1 1+61	R1 1+38	SOLID WHITE	12	23	46		
R1 1+61	R1 1+10	SOLID WHITE	12	75	149		
R1 1+44	R1 2+90	SOLID YELLOW	6	153		153	
-	-	STOP BAR	18	41			123
-	-	CROSSWALK-TYPE LS	12	101			910
SHEET TOTAL					30625	9540	3441
SUB TOTAL					36801	12436	3973
GRAND TOTAL					39021	12436	3973

ITEM	DESCRIPTION	UNITS
ITEM 685.1102	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20 MIL	LF
ITEM 685.1106--25	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES 6" X 20 MILS	LF
ITEM 685.1206--25	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES 6" X 20 MILS	LF

ALTERED ON: \_\_\_\_\_

AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING PAVEMENT MARKING TABLE	DRAWING NUMBER: PMT-2

### GENERAL NOTES

- DESIGN SPECIFICATIONS: NEW YORK STATE DEPARTMENT OF TRANSPORTATION STANDARD DESIGN SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR OVERHEAD SIGN STRUCTURES, 2007 (FOR DESIGN PURPOSES, COMPRESSIVE STRENGTH OF CONCRETE FOR SUBSTRUCTURES AT 28 DAYS:  $f'_c = 3,000$  psi).
- CONSTRUCTION SPECIFICATIONS: NYS DOT STANDARD SPECIFICATIONS - CONSTRUCTION AND MATERIALS WITH ALL PROVISIONS IN EFFECT AS OF JANUARY 2024.
- DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS FOR WHICH NO SCALE IS SHOWN ARE DRAWN PROPORTIONALLY AND ARE FULLY DIMENSIONED.
- ALL SHOP DRAWINGS FOR THIS PROJECT SHALL BE PREPARED IN U.S. CUSTOMARY UNITS.
- NO KNOWN ASBESTOS CONTAINING MATERIALS ARE BELIEVED TO EXIST AND/OR THE WORK TO BE PERFORMED UNDER THIS CONTRACT DOES NOT REQUIRE THE DISTURBANCE, DESTRUCTION OR REMOVAL OF ANY OF THESE MATERIALS. IT IS THE EXPRESS INTENT OF THIS CONTRACT THAT THESE MATERIALS ARE NOT TO BE DISTURBED IN ANY WAY. SHOULD THE CONTRACTOR DISTURB OR ENCOUNTER ANY SUCH MATERIALS, THE CONTRACTOR SHALL IMMEDIATELY STOP WORK AND NOTIFY THE ENGINEER. THE CONTRACTOR SHALL OBTAIN WRITTEN PERMISSION FROM THE ENGINEER BEFORE PROCEEDING.

### STRUCTURAL STEEL NOTES

- ALL STRUCTURAL STEEL FOR STRUCTURAL SHAPES, PLATES, AND BARS SHALL CONFORM TO ASTM A709, GRADE 50.
- ANCHOR BOLTS SHALL CONFORM TO ASTM F1554, GRADE 55 (HOT-DIP GALVANIZED).
- HIGH STRENGTH BOLTS SHALL CONFORM TO ASTM F3125, GRADE A325 (TYPE 1, HOT-DIP GALVANIZED).
- THE COST OF CLEANING THE STEEL IN THE FABRICATION SHOP AND THE FIELD SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE SINGLE SPAN SIGN STRUCTURE ITEM.

### GALVANIZING NOTES

- ALL STEEL SHALL BE GALVANIZED IN ACCORDANCE WITH THE SINGLE SPAN SIGN STRUCTURE ITEM.
- ANCHOR BOLTS, NUTS, AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH ITEM 644.11- ANCHOR BOLTS.
- REINFORCING STEEL BARS SHALL BE GALVANIZED AND FABRICATED IN ACCORDANCE WITH ASTM A767, AND MEET THE REQUIREMENTS OF NYS DOT MATERIAL SPECIFICATION 709-11.
- HIGH STRENGTH BOLTS, NUTS, AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH THE SINGLE SPAN SIGN STRUCTURE ITEM.
- DRILLED HOLES SHALL BE CLEANED OF ALL EXCESS MATERIAL THAT PREVENTS THE PROPER INSTALLATION OF THE BOLTS.

### STEEL ERECTION NOTES

- THE CONTRACTOR SHALL PROVIDE FOR THE STABILITY OF STRUCTURAL STEEL DURING ALL PHASES OF ERECTION AND CONSTRUCTION, AS PROVIDED IN SECTION 2, SUBSECTION 204 OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL (SCM). THE MEANS AND METHODS USED BY THE CONTRACTOR SHALL BE DOCUMENTED ON THE ERECTION DRAWINGS AND INCLUDED IN THE SUPPORTING STABILITY CALCULATIONS. THE ERECTION DRAWINGS AND STABILITY CALCULATIONS SHALL BE SEALED BY A REGISTERED NEW YORK STATE PROFESSIONAL ENGINEER AND SUBMITTED TO THE ENGINEER IN ACCORDANCE WITH THE SCM.

### REMOVAL NOTE

- RECORD PLANS FOR THE EXISTING OVERHEAD SIGN STRUCTURES WILL BE SUPPLIED TO THE CONTRACTOR IF AVAILABLE.
- EXISTING FOUNDATIONS SHALL BE REMOVED TO A DEPTH OF 1'-0" BELOW EXISTING GRADE. RESTORATION OF THE AREA AROUND SIGNS TO BE REMOVED, WHERE NO OTHER WORK IS PROPOSED, SHALL BE INCLUDED IN THE PRICE BID FOR SIGN REMOVAL. THE AREA SHALL BE RESTORED SIMILAR TO THE SURROUNDING AREAS, AOE.
- THE CONTRACTOR SHALL NOT REMOVE THE EXISTING OVERHEAD SIGN STRUCTURES UNTIL THE PROPOSED STRUCTURES ARE INSTALLED TO THE SATISFACTION OF THE AUTHORITY.

### RECONSTRUCTION NOTES

- THE CONTRACTOR SHALL VERIFY DIMENSIONS NECESSARY FOR THE PROPER FIT OF STEEL PIECES PRIOR TO THE FABRICATION OF THE STEEL. THE COST OF FIELD VERIFYING DIMENSIONS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR SIGN STRUCTURE ITEMS.
- THE CONTRACTOR SHALL PERFORM ALL WORK WITH CARE SO THAT ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE AUTHORITY, WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN IN PLACE OR WHICH ARE TO REMAIN THE PROPERTY OF THE AUTHORITY, THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
- WHEN ITEMS IN THE CONTRACT REQUIRE MATERIALS TO BE REMOVED AND DISPOSED OF, THE COST OF SUPPLYING A DISPOSAL AREA AND TRANSPORTATION TO THAT AREA SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THOSE ITEMS.
- DURING REMOVAL OPERATIONS, THE CONTRACTOR SHALL NOT DROP WASTE CONCRETE, DEBRIS, AND OTHER MATERIAL TO THE AREA BELOW EXCEPT WHERE THE PLANS SPECIFICALLY PERMIT THE DROPPING OF MATERIAL. PLATFORMS, NETS, SCREENS OR OTHER PROTECTIVE DEVICES SHALL BE USED TO CATCH THE MATERIAL. IF ADEQUATE PROTECTIVE DEVICES ARE NOT BEING EMPLOYED, THE WORK SHALL BE STOPPED UNTIL ADEQUATE PROTECTION IS PROVIDED.

### TEST PITS

- WHERE SHOWN, OR WHERE DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL PERFORM A TEST PIT (ITEM 206.05) TO CONFIRM THE SUITABILITY OF THE PROPOSED SHAFT LOCATION WITH REGARD TO AVOIDING UTILITIES. THE TEST PITS SHALL BE EXCAVATED TO THE MINIMUM DEPTH NECESSARY TO REVEAL POTENTIAL UTILITY CONFLICTS AS IT IS DESIRED TO LEAVE THE SOIL AT THESE LOCATIONS AS UNDISTURBED AS POSSIBLE FOR THE SHAFT INSTALLATION. TEST PITS SHALL BE PERFORMED PRIOR TO SHOP DRAWING SUBMISSION AND PRIOR TO FABRICATION OF THE OVERHEAD SIGN STRUCTURES.
- THE CONTRACTOR SHALL ONLY BE PAID ONCE PER LOCATION SHOWN IN THE EVENT WHERE MULTIPLE ATTEMPTS ARE REQUIRED TO LOCATE THE UNDERGROUND FACILITY, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
- LOCATIONS SHOWN ARE APPROXIMATE. IT IS THE CONTRACTORS RESPONSIBILITY TO REQUEST STAKE-OUT AND TO VERIFY ALL UNDERGROUND FACILITIES WITHIN THE LIMITS OF THE PROPOSED WORK.
- MISCELLANEOUS TEST PITS SHALL BE COMPLETED, AS APPROVED BY THE ENGINEER, IN ORDER TO DETERMINE LOCATIONS OF EXISTING UNDERGROUND FACILITIES WHERE STAKE-OUT SHOWS A POTENTIAL CONFLICT WITH THE PROPOSED WORK.

TEST PIT LOCATION TABLE		
LOCATION	STRUCTURE TYPE	ITEM 206.05 (EACH)
MP 900.38	SPAN	2
MP 900.95	SPAN	4
MP 901.41	SPAN	3
MP 904.16	SPAN	2
SEE NOTE 4		4
TOTAL		15

DRAWING INDEX - OVERHEAD SIGN STRUCTURES		
SHEET NO.	DESCRIPTION	DRAWING NO.
200	OVERHEAD SIGN STRUCTURES - SPAN STRUCTURES - GENERAL NOTES	OSN-01
201	OVERHEAD SIGN STRUCTURES - CANTILEVER STRUCTURES - GENERAL NOTES	OSN-02
202	OVERHEAD SIGN STRUCTURES - LOCATION MAP	OSM-01
203	OVERHEAD SIGN STRUCTURES - GENERAL PLAN - LOCATION MP 900.38	OSP-01
204	OVERHEAD SIGN STRUCTURES - ELEVATION - LOCATION MP 900.38	OSP-02
205	OVERHEAD SIGN STRUCTURES - GENERAL PLAN - LOCATION MP 900.95	OSP-03
206	OVERHEAD SIGN STRUCTURES - ELEVATION - LOCATION MP 900.95	OSP-04
207	OVERHEAD SIGN STRUCTURES - GENERAL PLAN - LOCATION MP 901.41	OSP-05
208	OVERHEAD SIGN STRUCTURES - ELEVATION - LOCATION MP 901.41	OSP-06
209	OVERHEAD SIGN STRUCTURES - GENERAL PLAN - LOCATION MP 902.19	OSP-07
210	OVERHEAD SIGN STRUCTURES - ELEVATION - LOCATION MP 902.19	OSP-08
211	OVERHEAD SIGN STRUCTURES - GENERAL PLAN - LOCATION MP 902.33	OSP-09
212	OVERHEAD SIGN STRUCTURES - ELEVATION - LOCATION MP 902.33	OSP-10
213	OVERHEAD SIGN STRUCTURES - GENERAL PLAN - LOCATION MP 902.69	OSP-11
214	OVERHEAD SIGN STRUCTURES - ELEVATION - LOCATION MP 902.69	OSP-12
215	OVERHEAD SIGN STRUCTURES - GENERAL PLAN - LOCATION MP 903.13	OSP-13
216	OVERHEAD SIGN STRUCTURES - ELEVATION - LOCATION MP 903.13	OSP-14
217	OVERHEAD SIGN STRUCTURES - GENERAL PLAN - LOCATION MP 903.19	OSP-15
218	OVERHEAD SIGN STRUCTURES - ELEVATION - LOCATION MP 903.19	OSP-16
219	OVERHEAD SIGN STRUCTURES - GENERAL PLAN - LOCATION MP 903.51	OSP-17
220	OVERHEAD SIGN STRUCTURES - ELEVATION - LOCATION MP 903.51	OSP-18
221	OVERHEAD SIGN STRUCTURES - GENERAL PLAN - LOCATION MP 903.81	OSP-19
222	OVERHEAD SIGN STRUCTURES - ELEVATION - LOCATION MP 903.81	OSP-20
223	OVERHEAD SIGN STRUCTURES - GENERAL PLAN - LOCATION MP 904.11	OSP-21
224	OVERHEAD SIGN STRUCTURES - ELEVATION - LOCATION MP 904.11	OSP-22
225	OVERHEAD SIGN STRUCTURES - GENERAL PLAN - LOCATION MP 904.16	OSP-23
226	OVERHEAD SIGN STRUCTURES - ELEVATION - LOCATION MP 904.16	OSP-24
227	OVERHEAD SIGN STRUCTURES - SPAN STRUCTURES	OSD-01
228	OVERHEAD SIGN STRUCTURES - SPAN STRUCTURES - SELECTION TABLE	OSD-02
229	OVERHEAD SIGN STRUCTURES - SPAN STRUCTURES - FOUNDATION DETAILS	OSD-03
230	OVERHEAD SIGN STRUCTURES - SPAN STRUCTURES - POST DETAILS (SHEET 1 OF 2)	OSD-04
231	OVERHEAD SIGN STRUCTURES - SPAN STRUCTURES - POST DETAILS (SHEET 2 OF 2)	OSD-05
232	OVERHEAD SIGN STRUCTURES - SPAN STRUCTURES - TRUSS DETAILS (SHEET 1 OF 2)	OSD-06
233	OVERHEAD SIGN STRUCTURES - SPAN STRUCTURES - TRUSS DETAILS (SHEET 2 OF 2)	OSD-07
234	OVERHEAD SIGN STRUCTURES - SPAN STRUCTURES - BASE AND CAP DETAILS	OSD-08
235	OVERHEAD SIGN STRUCTURES - SPAN STRUCTURES - SIGN PANEL ATTACHMENT DETAILS	OSD-09
236	OVERHEAD SIGN STRUCTURES - CANTILEVER STRUCTURES - MISC. DETAILS (SHEET 1 OF 4)	OSD-10
237	OVERHEAD SIGN STRUCTURES - CANTILEVER STRUCTURES - MISC. DETAILS (SHEET 2 OF 4)	OSD-11
238	OVERHEAD SIGN STRUCTURES - CANTILEVER STRUCTURES - MISC. DETAILS (SHEET 3 OF 4)	OSD-12
239	OVERHEAD SIGN STRUCTURES - CANTILEVER STRUCTURES - MISC. DETAILS (SHEET 4 OF 4)	OSD-13
240	OVERHEAD SIGN STRUCTURES - BARRIER DETAILS - LOCATION MP 900.38 (SHEET 1 OF 3)	OSD-14
241	OVERHEAD SIGN STRUCTURES - BARRIER DETAILS - LOCATION MP 900.38 (SHEET 2 OF 3)	OSD-15
242	OVERHEAD SIGN STRUCTURES - BARRIER DETAILS - LOCATION MP 900.38 (SHEET 3 OF 3)	OSD-16
243	BRIDGE MOUNTED SIGN - REMOVAL DETAILS - BIN 5512290	OSD-17


CHECKED BY: M. BEDFORD

DRAFTED BY: S. LOFORTE

CHECKED BY: M. BEDFORD



DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING OVERHEAD SIGN STRUCTURES SPAN STRUCTURES GENERAL NOTES	DRAWING NUMBER: OSN-01



**GENERAL NOTES**

1. PROVIDE 3-INCH CONCRETE COVER ON REINFORCEMENT BARS, EXCEPT AS NOTED.
2. DO NOT WELD REINFORCING STEEL BARS.
3. RAKE-FINISH ALL HORIZONTAL CONSTRUCTION JOINTS, EXCEPT AS INDICATED.
4. VERIFY ALL DIMENSIONS AND GEOMETRY OF THE EXISTING STRUCTURES IN THE FIELD AS NECESSARY FOR PROPER FIT OF THE PROPOSED CONSTRUCTION.
5. CHAMFER EXPOSED CONCRETE EDGES 1 INCH BY 1 INCH.
6. ALL DIMENSIONS SHOWN ARE HORIZONTAL, EXCEPT AS NOTED.
7. DIMENSIONS ARE BASED ON A NORMAL TEMPERATURE OF 68 DEGREES F.
8. PIPE DIAMETERS SHOWN IN THE DESIGN TABLES ARE OUTSIDE DIAMETERS.
9. USE STANDARD SIZE HOLE. THE STANDARD HOLE DIAMETER FOR BOLTS SMALLER THAN 1" DIAMETER SHALL BE THE NOMINAL DIAMETER OF THE BOLT PLUS 1/16". FOR BOLTS 1" DIAMETER AND LARGER, THE WIDTH OF EACH STANDARD HOLE SHALL BE THE NOMINAL DIAMETER OF THE BOLT PLUS 1/8".
10. CLEAR DISTANCE BETWEEN BOLT HOLES OR BETWEEN THE BOLT HOLE AND THE END OF THE MEMBER IN THE DIRECTION OF THE APPLIED BEARING FORCE SHALL BE CHECKED.
11. PROVIDE ANCHOR BOLT HOLES 1/4" LARGER THAN BOLT DIAMETER.
12. PROVIDE A MINIMUM ANCHOR BOLT EMBEDMENT LENGTH OF 20 ANCHOR BOLT DIAMETERS.
13. PROVIDE DOUBLE NUTS AND WASHER FOR EACH ANCHOR BOLT.
14. STEEL MEMBER COMPONENTS REQUIRING CHARPY V-NOTCH TESTING ARE DESIGNATED ON THE PLANS BY (CVN), PROVIDE STEEL CONFORMING TO THE CVN REQUIREMENTS FOR ZONE 2, NON FRACTURE CRITICAL AS GIVEN IN THE AASHTO MATERIAL SPECIFICATIONS.

**MATERIAL**

1. PROVIDE MATERIALS AND WORKMANSHIP IN ACCORDANCE WITH THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION DESIGN SPECIFICATIONS FOR OVERHEAD STRUCTURES, STANDARD SPECIFICATIONS SECTION 644, THE NYS STEEL CONSTRUCTION MANUAL, THE AASHTO/AWS/D1.5M/D1.5:2002 BRIDGE WELDING CODE, AND THE CONTRACT SPECIAL PROVISIONS. USE ANSI/AWS/D1.1/D1.1M-2002 FOR WELDING NOT COVERED IN AASHTO/AWS/D1.5M/D1.5:2002.
2. PROVIDE STRUCTURAL STEEL CONFORMING TO ASTM A709, GRADE 36 DESIGNATION EXCEPT WHEN NOTED OTHERWISE.
3. PROVIDE WELDED OR SEAMLESS STEEL PIPE CONFORMING TO ASTM A53, GRADE B, TYPE E OR S, Fy = 35 ksi WITH CVN TESTING ZONE 2 FOR PIPE WITH WALL THICKNESS GREATER THAN 1/2".
4. PROVIDE HIGH-STRENGTH STEEL BOLTS CONFORMING TO ASTM A325. MECHANICALLY GALVANIZE ALL BOLTS (EXCEPT ANCHOR BOLTS), NUTS AND WASHERS. PROVIDE U-BOLTS CONFORMING TO ASTM A449. PROVIDE ANCHOR BOLTS CONFORMING TO ASTM F1554, GRADE 5S.
5. USE CLASS A CEMENT CONCRETE f'c = 3000 PSI IN PEDESTALS, FOOTINGS AND DRILLED SHAFTS.
6. ALL REINFORCING STEEL BARS SHALL BE ASTM A615 GRADE 60. BARS SHALL BE GALVANIZED AND FABRICATED IN ACCORDANCE WITH ASTM A767, AND MEET THE REQUIREMENTS OF NYS DOT MATERIAL SPECIFICATION 709-11.

**FABRICATION**

1. CONSTRUCT SIGN STRUCTURES TRUE TO DIMENSION, FREE FROM KINKS, TWISTS OR BENDS, AND UNIFORM IN APPEARANCE. ASSEMBLE COMPLETED SECTIONS IN THE SHOP AND CHECK FOR STRAIGHTNESS, ALIGNMENT, DIMENSION, AND THE FIRM SEATING OF THE SPLICE PLATES. CORRECT ANY VARIATIONS TO THE SATISFACTION OF THE ENGINEER.
2. FORM MASTS FOR SIGN STRUCTURES TO THE RADII SHOWN ON THE PLANS IN ACCORDANCE WITH THE TUBE AND PIPE ASSOCIATION INTERNATIONAL RECOMMENDED STANDARDS FOR INDUCTION BENDING OF PIPE AND TUBE (TPA-IBS-98).
3. AFFIX CLIPS, EYES, OR REMOVABLE BRACKETS TO ALL MASTS AND MAST ARMS, AS NECESSARY, TO SECURE THE SIGN STRUCTURE DURING SHIPPING AND FOR LIFTING AND MOVING DURING ERECTION. THIS IS TO PREVENT DAMAGE TO THE FINISHED GALVANIZED OR PAINTED SURFACES. REMOVE BRACKETS ON SIGN STRUCTURES AFTER ERECTION. INCLUDE DETAILS OF SUCH DEVICES ON THE SHOP DRAWINGS.
4. FABRICATE ALL SIGN STRUCTURES INTO THE LARGEST PRACTICAL SECTIONS PRIOR TO GALVANIZING. SUBMIT SPLICE LOCATIONS TO THE ENGINEER FOR APPROVAL. DO NOT COMMENCE FABRICATION UNTIL SUCH SPLICE LOCATIONS ARE APPROVED.
5. GRIND ALL AREAS TO BE WELDED TO BRIGHT METAL. BUTT WELD SPLICES ARE NOT PERMITTED, UNLESS SHOWN ON THE PLANS. COMPLETE ALL WELDING AND REQUIRED TESTING BEFORE ANY MATERIAL IS GALVANIZED. NON-DESTRUCTIVELY TEST ALL CIRCUMFERENTIAL AND STIFFENER WELDS USING THE METHODS AND PROCEDURES IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL SECTIONS 17 & 18 ULTRASONIC INSPECTION AND MAGNETIC PARTICLE INSPECTION, RESPECTIVELY. PROVIDE FULL PENETRATION GROOVE WELDS FOR ALL LONGITUDINAL WELDS WITHIN 6" OF A FULL PENETRATION CIRCUMFERENTIAL GROOVE WELD AND INSPECT AS SPECIFIED ABOVE. PROVIDE MAXIMUM WELD UNDERCUT OF 0.01".
6. HOT-DIP GALVANIZE ALL COMPONENTS (EXCEPT ALUMINUM AND NON-FERROUS INCIDENTALS) AFTER FABRICATION PER ASTM A123 OR ASTM A153, AS APPROPRIATE.

**CONSTRUCTION**

1. USE TEMPLATES TO ACCURATELY SET BASE PLATE ANCHOR BOLTS TO CORRECT ELEVATION AND ALIGNMENT. SECURELY BRACE ANCHOR BOLTS AGAINST DISPLACEMENT BEFORE DRILLED SHAFT CONCRETE IS PLACED AND DURING CONCRETE CURING.
2. ERECT SIGN STRUCTURE ONLY AFTER DRILLED SHAFT CONCRETE MEETS 7 DAY STRENGTH REQUIREMENTS.
3. TEMPORARILY SUPPORT MAST ARMS TO RELIEVE LOAD FROM THE SPLICES WHILE HIGH-STRENGTH BOLTS ARE BEING TIGHTENED IN ORDER TO FIRMLY SEAT THE CONNECTION PLATES.
4. PRIOR TO ERECTION, DEMONSTRATE TO THE ENGINEER BY PREASSEMBLY OR OTHER APPROVED METHOD THAT FRAME STRUCTURE LENGTH IN A NO-LOAD CONDITION MATCHES FIELD MEASURED DRILLED SHAFT SPACING WITHIN 1/2".

**NOTES TO FABRICATOR**

1. DESIGN COMPUTATIONS ARE REQUIRED FOR ANY PORTION OF A STRUCTURE FOR WHICH THE INFORMATION IS NOT TAKEN DIRECTLY FROM THE CONTRACT DRAWINGS OR THE DETAILS CONTAINED IN THIS STANDARD.
2. USE A MINIMUM OF 2" THICKNESS FOR ALL SPLICE PLATES TO PREVENT DISTORTION DURING WELDING.

**DESIGN**

1. SPECIFICATIONS: "AASHTO 4TH EDITION STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS" (2001 WITH INTERIMS THROUGH 2006) AND "AASHTO 17TH EDITION STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES" (2002).
2. WIND LOADS ARE BASED ON 90 MPH (3 SECOND GUST) BASIC WIND SPEED.
3. CANTILEVER DRILLED SHAFT FOUNDATIONS ARE BASED ON A MAXIMUM 1/2" LATERAL DEFLECTION AT TOP OF DRILLED SHAFT.
4. DESIGN TABLE MEMBER SIZES ARE ADEQUATE FOR FATIGUE CATEGORY 1.

**TEST PITS**

1. WHERE SHOWN, OR WHERE DIRECTED BY THE ENGINEER, THE CONTRACTOR SHALL PERFORM A TEST PIT (ITEM 206.05) TO CONFIRM THE SUITABILITY OF THE PROPOSED SHAFT LOCATION WITH REGARD TO AVOIDING UTILITIES. THE TEST PITS SHALL BE EXCAVATED TO THE MINIMUM DEPTH NECESSARY TO REVEAL POTENTIAL UTILITY CONFLICTS AS IT IS DESIRED TO LEAVE THE SOIL AT THESE LOCATIONS AS UNDISTURBED AS POSSIBLE FOR THE SHAFT INSTALLATION. TEST PITS SHALL BE PERFORMED PRIOR TO SHOP DRAWING SUBMISSION AND PRIOR TO FABRICATION OF THE OVERHEAD SIGN STRUCTURES.
2. THE CONTRACTOR SHALL ONLY BE PAID ONCE PER LOCATION SHOWN IN THE EVENT WHERE MULTIPLE ATTEMPTS ARE REQUIRED TO LOCATE THE UNDERGROUND FACILITY, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
3. LOCATIONS SHOWN ARE APPROXIMATE. IT IS THE CONTRACTORS RESPONSIBILITY TO REQUEST STAKE-OUT AND TO VERIFY ALL UNDERGROUND FACILITIES WITHIN THE LIMITS OF THE PROPOSED WORK.
4. MISCELLANEOUS TEST PITS SHALL BE COMPLETED, AS APPROVED BY THE ENGINEER, IN ORDER TO DETERMINE LOCATIONS OF EXISTING UNDERGROUND FACILITIES WHERE STAKE-OUT SHOWS A POTENTIAL CONFLICT WITH THE PROPOSED WORK.

LOCATION	STRUCTURE TYPE	ITEM 206.05 (EACH)
MP 903.81	CANTILEVER	1
MP 904.11	CANTILEVER	1
SEE NOTE 4		2
TOTAL		4


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DRAFTED BY: S. LOFORTE

CHECKED BY: M. BEDFORD



DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING OVERHEAD SIGN STRUCTURES CANTILEVER STRUCTURES GENERAL NOTES	DRAWING NUMBER: OSN-02

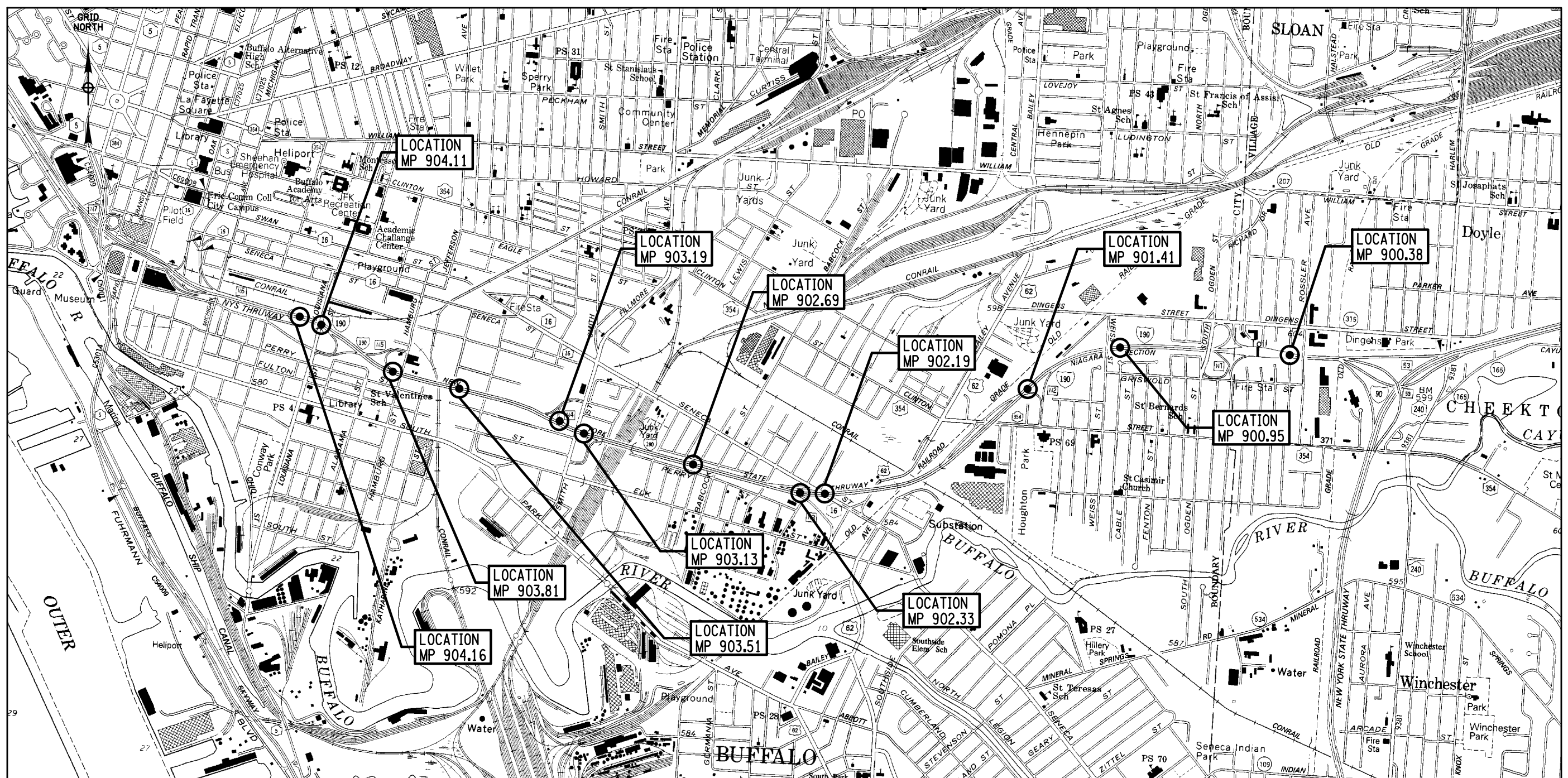
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DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



LOCATION MAP  
NOT TO SCALE

ALTERED ON: AFFIXED ON: 05/13/2024

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES LOCATION MAP	DRAWING NUMBER: OSM-01

**PROPOSED FOUNDATION C  
COORDINATES**

FOUNDATION NO.	COORDINATES
1	N = 1047944.891 E = 1091508.500
2	N = 1047945.146 E = 1091498.670
3	N = 1048014.868 E = 1091510.315
4	N = 1048015.122 E = 1091500.485



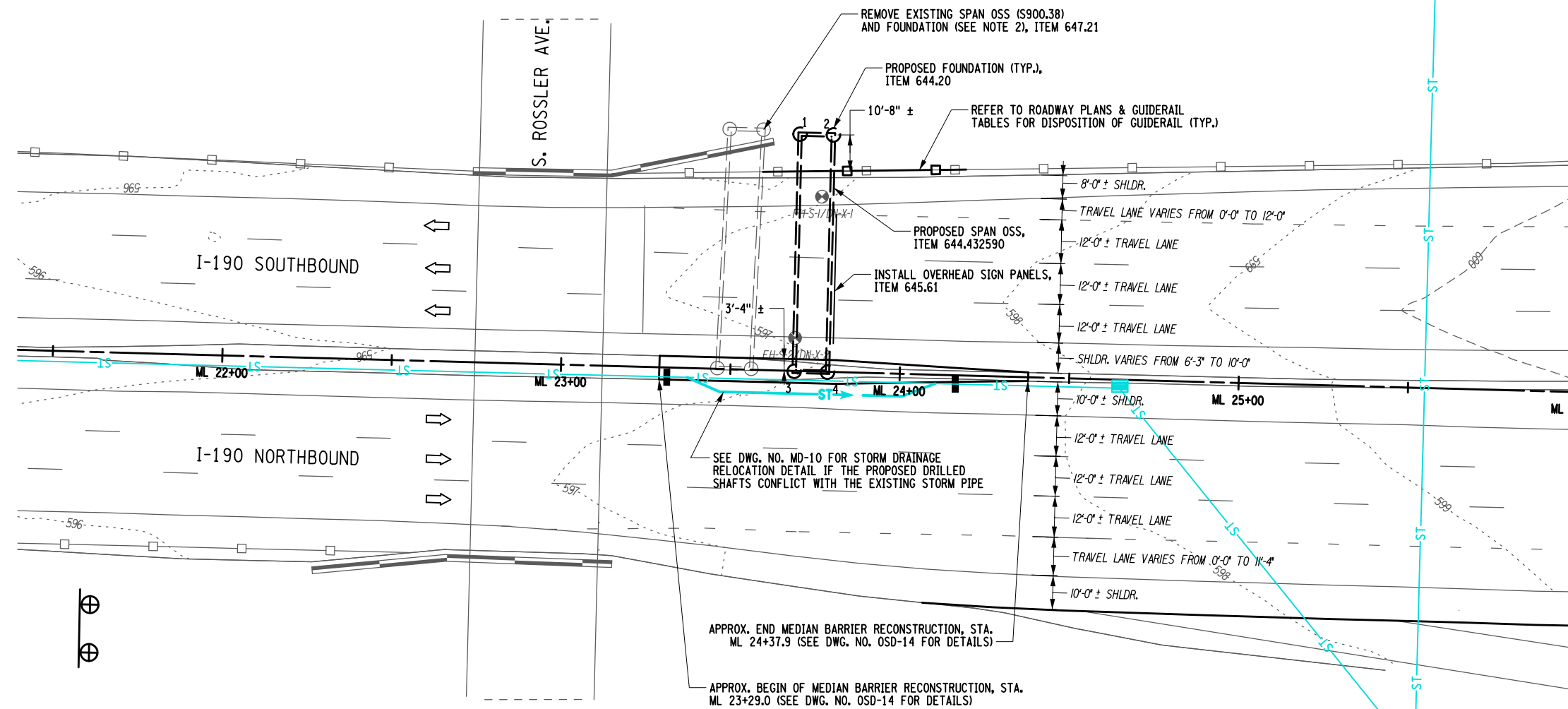
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DRAFTED BY: M. OZOG

CHECKED BY: S. LOFORTE

DESIGNED BY: M. OZOG

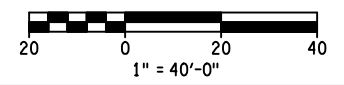
DESIGN SUPERVISOR: M. BEDFORD



**PLAN**  
**LOCATION MP 900.38**  
**1" = 40'-0"**

**NOTES:**

1. THE EXISTING SPAN OSS AND SIGN PANELS SHALL REMAIN IN PLACE AND FULLY FUNCTIONAL UNTIL THE REPLACEMENT OSS IS ERECTED, TESTED, AND ACCEPTED BY THE NYSTA.
2. UPON REMOVAL OF THE EXISTING SPAN OSS, THE EXISTING FOUNDATIONS SHALL BE REMOVED TO A DEPTH OF 1'-0" BELOW EXISTING GRADE AND THE SURFACE SHALL BE RESTORED TO MATCH THE SURROUNDING AREA.



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REVISIONS			
DATE	DESCRIPTION	BY	SYML



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES GENERAL PLAN LOCATION MP 900.38	DRAWING NUMBER: OSP-01

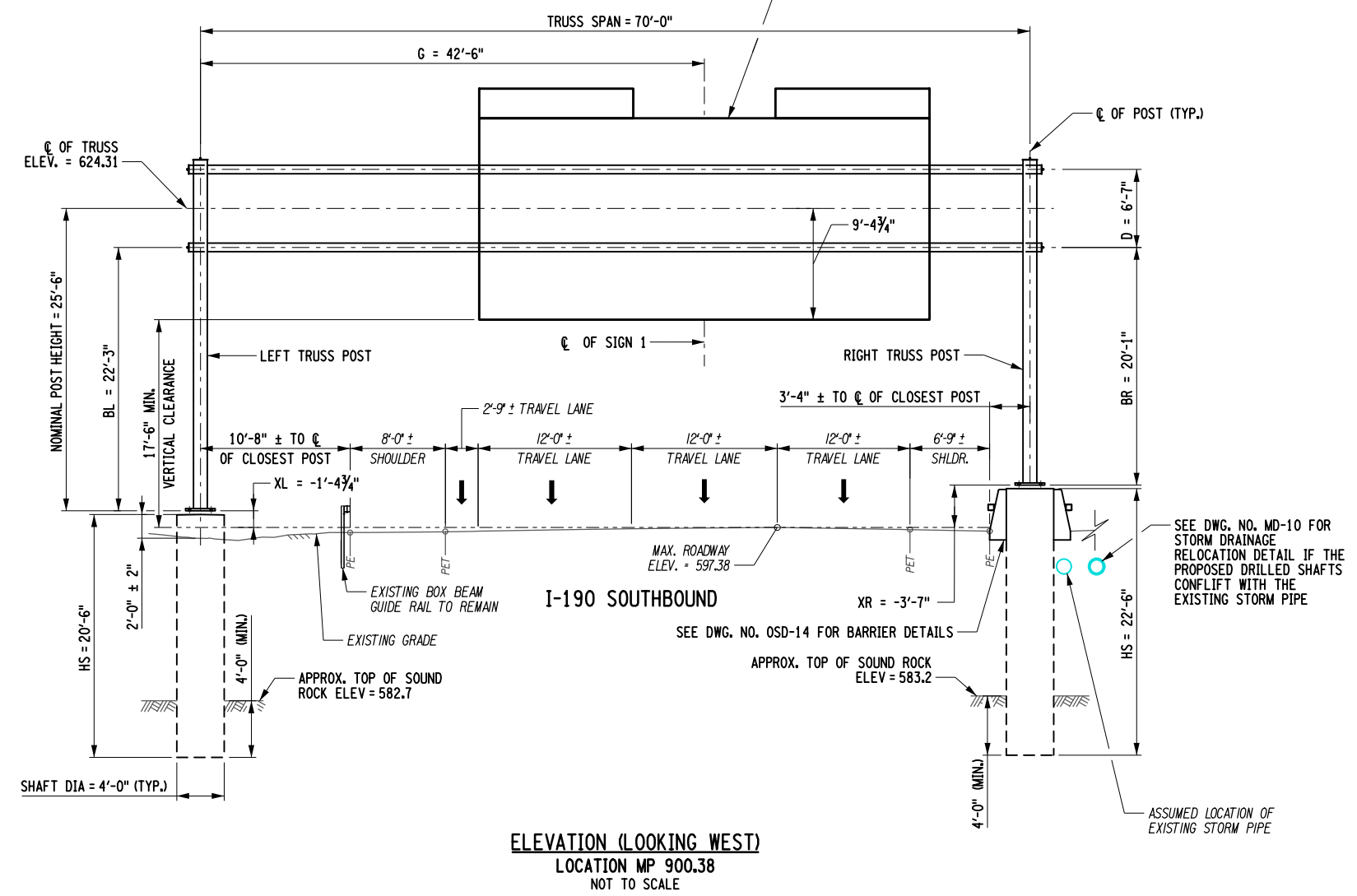
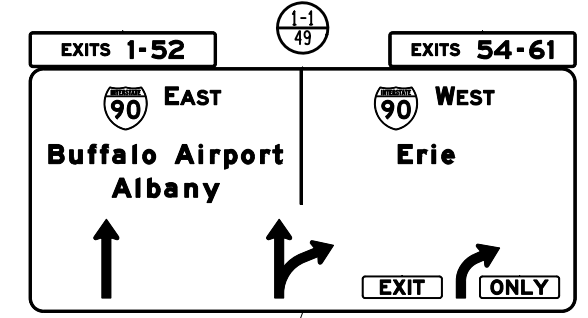
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CHECKED BY: Q. WANG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



SEE DWG. NO. MD-10 FOR STORM DRAINAGE RELOCATION DETAIL IF THE PROPOSED DRILLED SHAFTS CONFLICT WITH THE EXISTING STORM PIPE

ASSUMED LOCATION OF EXISTING STORM PIPE

ELEVATION (LOOKING WEST)  
LOCATION MP 900.38  
NOT TO SCALE

NOTES:

1. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO THE FABRICATION OF THE SIGN STRUCTURE.
2. SPAN TRUSS DIAGONALS, CROSS BRACING AND SPLICES NOT SHOWN FOR CLARITY.

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

NEW YORK STATE OF OPPORTUNITY  
**Thruway Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES ELEVATION LOCATION MP 900.38	DRAWING NUMBER: OSP-02

**PROPOSED FOUNDATION C COORDINATES**

FOUNDATION NO.	COORDINATES
1	N = 1047993.249 E = 1088492.008
2	N = 1047992.325 E = 1088479.877
3	N = 1048130.850 E = 1088481.532
4	N = 1048129.927 E = 1088469.400



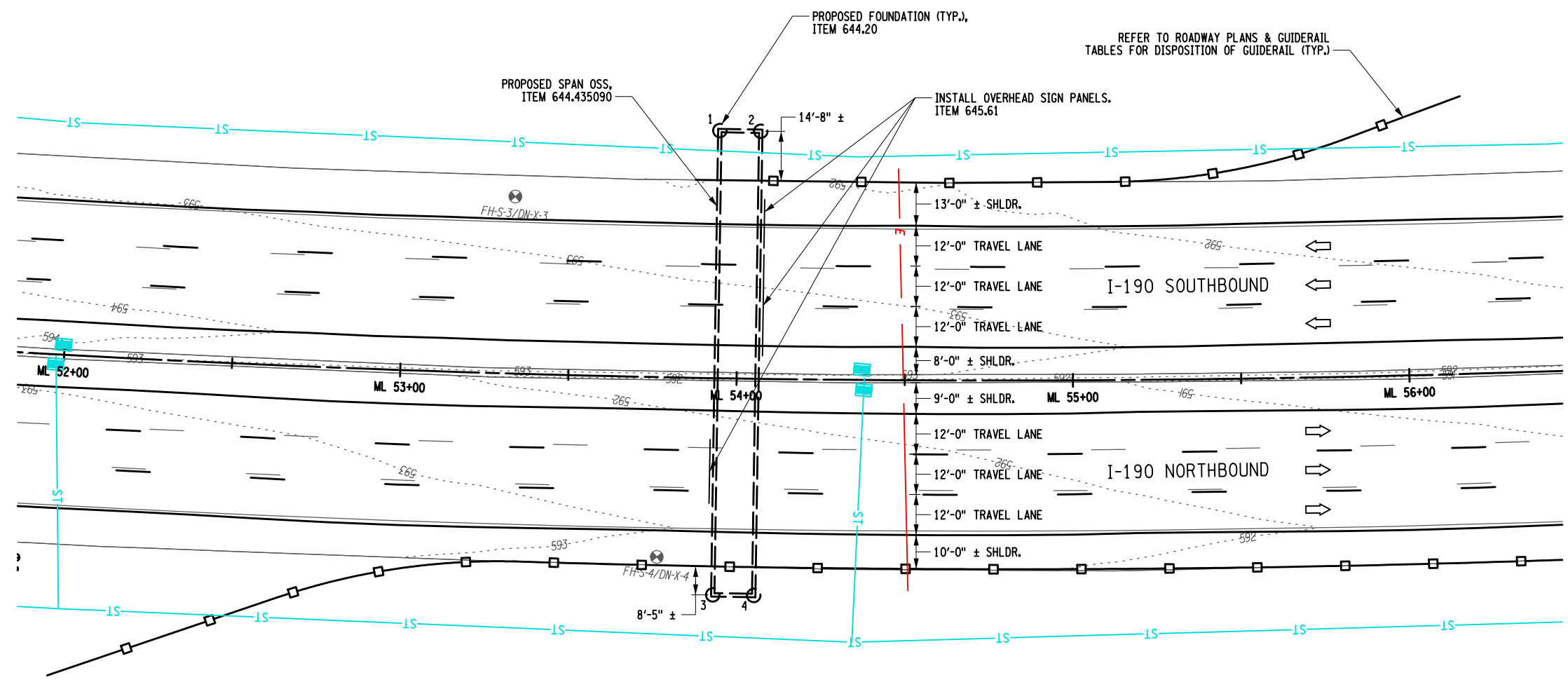
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CHECKED BY: M. OZOG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



**PLAN**  
LOCATION MP 900.95  
1" = 40'-0"

ALTERED ON:

SIGNATURE:  
STAMP:

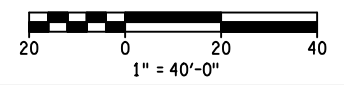
AFFIXED ON: **05/13/2024**

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REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES GENERAL PLAN LOCATION MP 900.95	DRAWING NUMBER: OSP-03



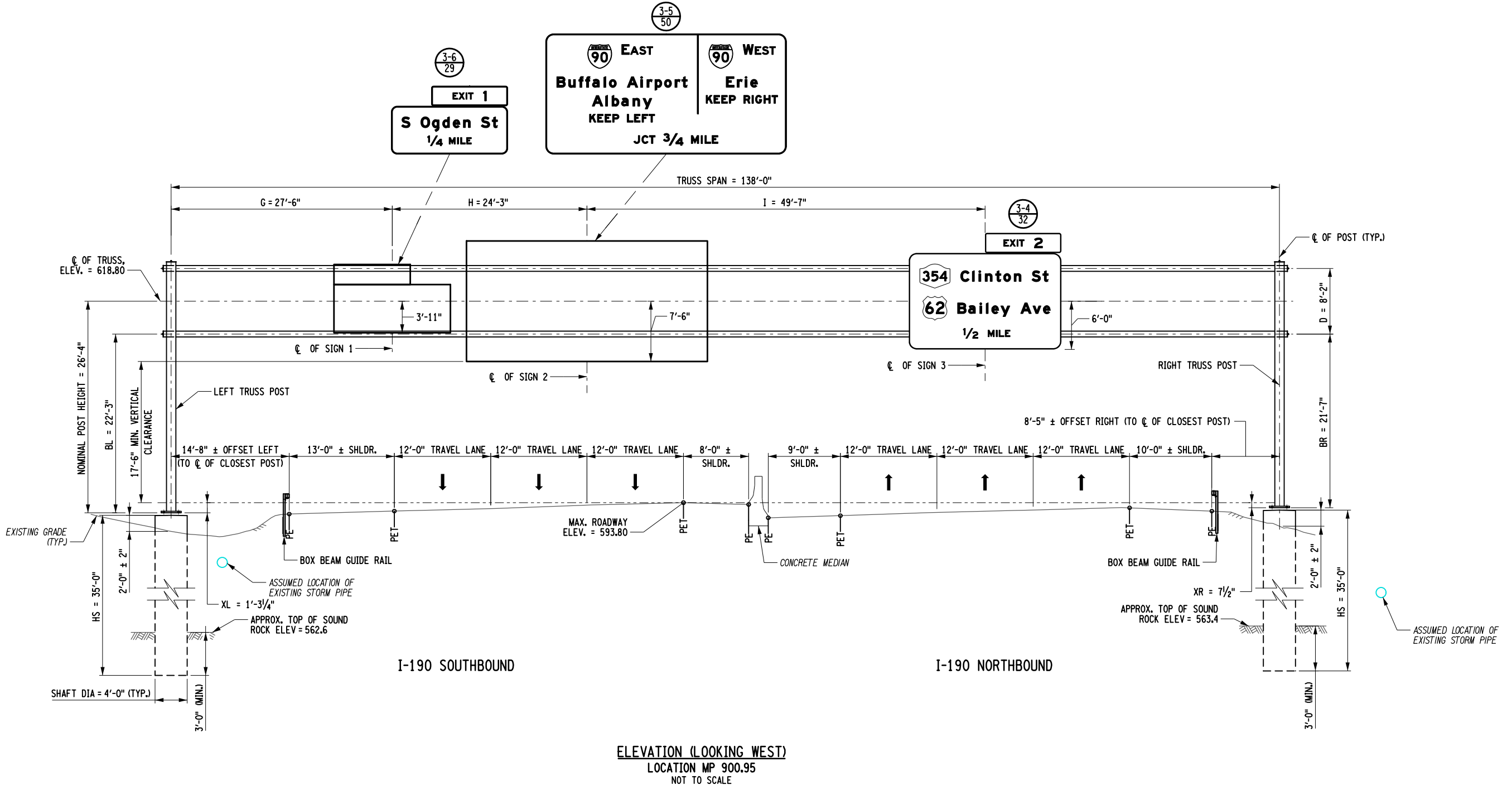
CHECKED BY: Q. WANG

DRAFTED BY: S. LOFORTE

CHECKED BY: Q. WANG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



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AFFIXED ON: **05/13/2024**

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REVISIONS			
DATE	DESCRIPTION	BY	SYL.

NEW YORK STATE OF OPPORTUNITY

Thruway Authority

PDG  
POPULI DESIGN GROUP

TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
OVERHEAD SIGN STRUCTURES  
ELEVATION  
LOCATION MP 900.95

CONTRACT NUMBER:  
TAN 24-12

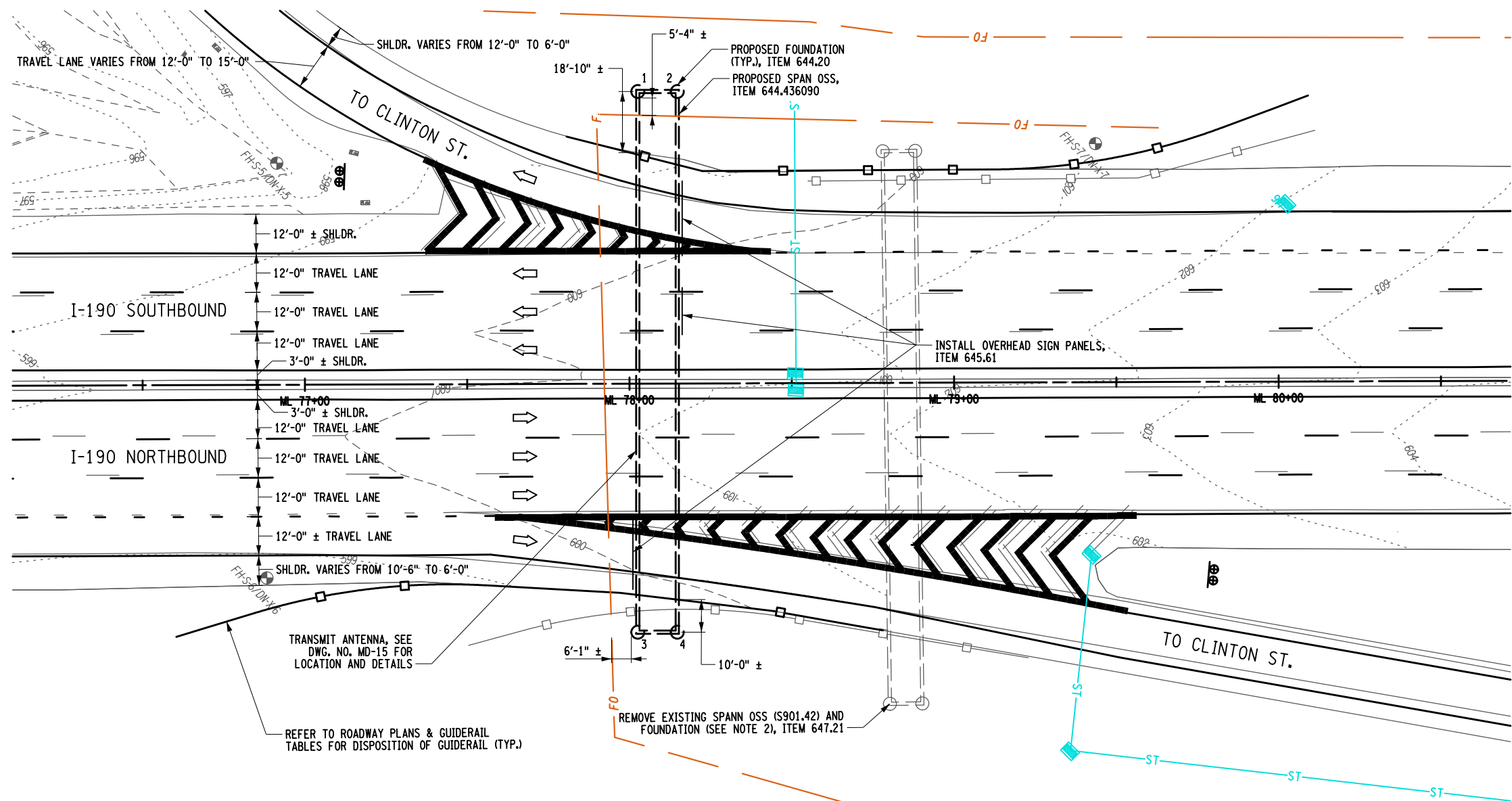
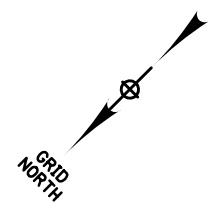
DATE:  
MAY 2024

DRAWING NUMBER:  
OSP-04

- NOTES:
1. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO THE FABRICATION OF THE SIGN STRUCTURE.
  2. SPAN TRUSS DIAGONALS, CROSS BRACING AND SPLICES NOT SHOWN FOR CLARITY.

**PROPOSED FOUNDATION COORDINATES**

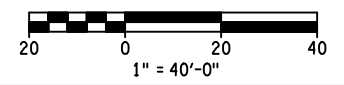
FOUNDATION NO.	COORDINATES
1	N = 1047009.259 E = 1086438.049
2	N = 1047000.665 E = 1086429.438
3	N = 1047127.109 E = 1086320.433
4	N = 1047118.514 E = 1086311.821



**PLAN**  
LOCATION MP 901.41  
1" = 40'-0"

**NOTES:**

- THE EXISTING SPAN OSS AND SIGN PANELS SHALL REMAIN IN PLACE AND FULLY FUNCTIONAL UNTIL THE REPLACEMENT OSS IS ERECTED, TESTED, AND ACCEPTED BY THE NYSTA.
- UPON REMOVAL OF THE EXISTING SPAN OSS, THE EXISTING FOUNDATION SHALL BE REMOVED TO A DEPTH OF 1'-0" BELOW EXISTING GRADE AND THE SURFACE SHALL BE RESTORED TO MATCH THE SURROUNDING AREA.



CHECKED BY: S. LOFORTE  
 DRAFTED BY: M. OZOG  
 CHECKED BY: S. LOFORTE  
 DESIGNED BY: M. OZOG  
 DESIGN SUPERVISOR: M. BEDFORD

ALTERED ON:  SIGNATURE: STAMP:	AFFIXED ON: <p style="text-align: center; color: blue; font-weight: bold;">05/13/2024</p> SIGNATURE: STAMP: <div style="text-align: center;"> </div>
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IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

**Thruway Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES GENERAL PLAN LOCATION 901.41	DRAWING NUMBER: OSP-05

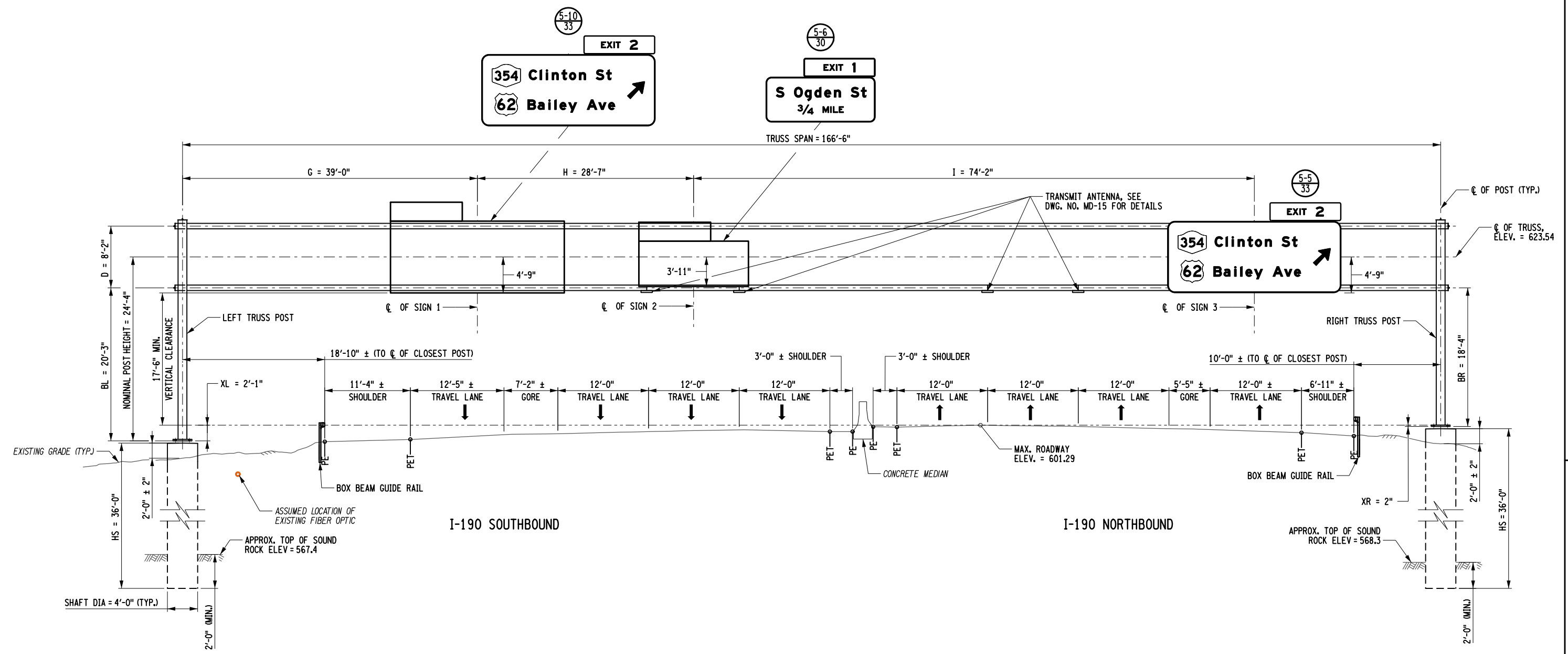
CHECKED BY: S. LOFORTE

DRAFTED BY: M. OZOG

CHECKED BY: S. LOFORTE

DESIGNED BY: M. OZOG

DESIGN SUPERVISOR: M. BEDFORD



**ELEVATION (LOOKING WEST)**  
LOCATION MP 901.41  
NOT TO SCALE

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYL

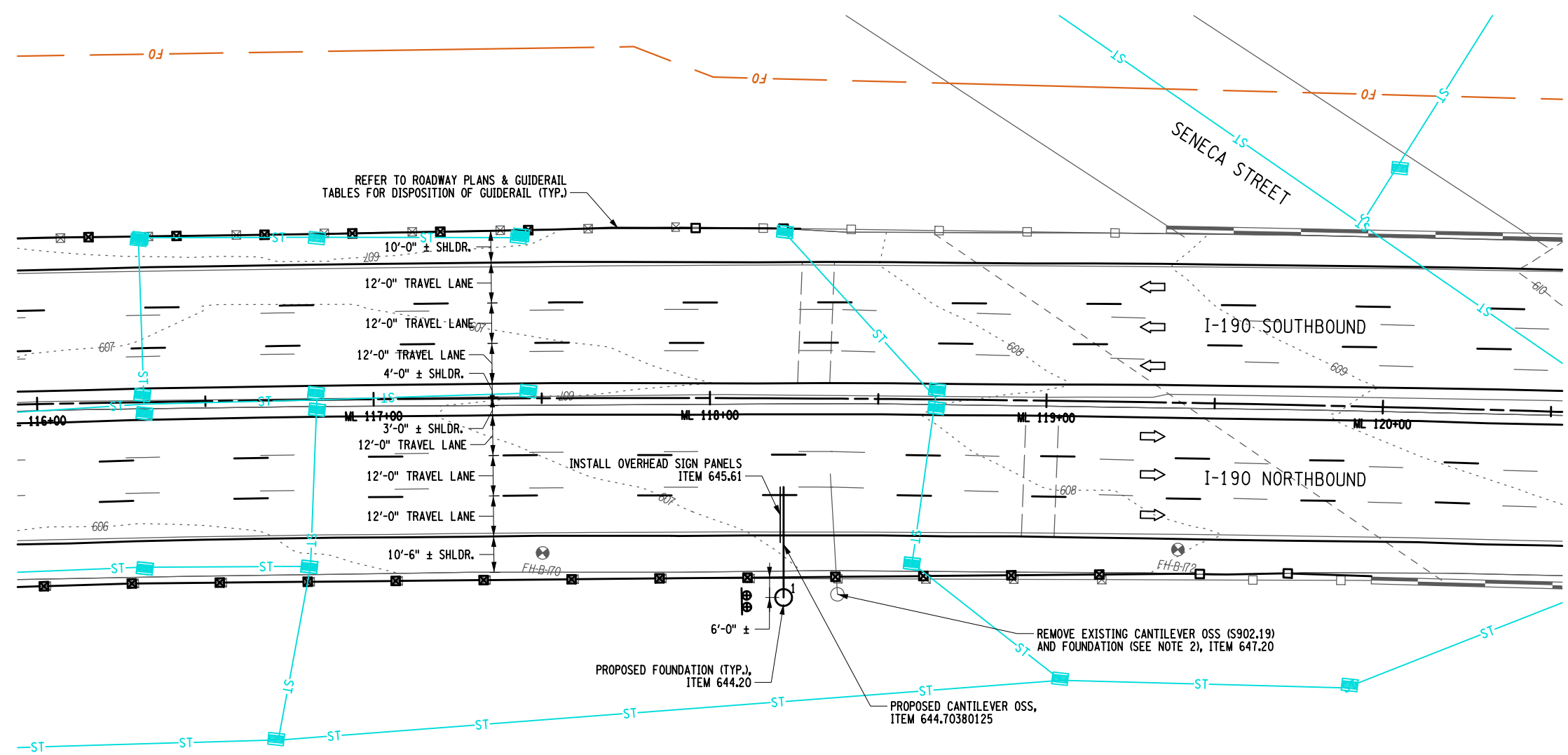


TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES ELEVATION LOCATION MP 901.41	DRAWING NUMBER: OSP-06

- NOTES:**
- THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO THE FABRICATION OF THE SIGN STRUCTURE.
  - SPAN TRUSS DIAGONALS, CROSS BRACING AND SPLICES NOT SHOWN FOR CLARITY.



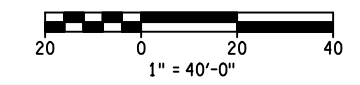
PROPOSED FOUNDATION C COORDINATES	
FOUNDATION NO.	COORDINATES
1	N = 1045135.026 E = 1083044.720



PLAN  
LOCATION MP 902.19  
1" = 40'-0"

NOTES:

1. THE EXISTING CANTILEVER OSS AND SIGN PANELS SHALL REMAIN IN PLACE AND FULLY FUNCTIONAL UNTIL THE REPLACEMENT OSS IS ERECTED, TESTED, AND ACCEPTED BY THE NYSTA.
2. UPON REMOVAL OF THE EXISTING CANTILEVER OSS, THE EXISTING FOUNDATIONS SHALL BE REMOVED TO A DEPTH OF 1'-0" BELOW EXISTING GRADE AND THE SURFACE SHALL BE RESTORED TO MATCH THE SURROUNDING AREA.



CHECKED BY: S. LOFORTE  
DRAFTED BY: M. OZOG  
CHECKED BY: S. LOFORTE  
DESIGNED BY: M. OZOG  
DESIGN SUPERVISOR: M. BEDFORD

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

NEW YORK STATE OF OPPORTUNITY

**Thruway Authority**

**PDG**  
POPPL DESIGN GROUP

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES GENERAL PLAN LOCATION MP 902.19	DRAWING NUMBER: OSP-07

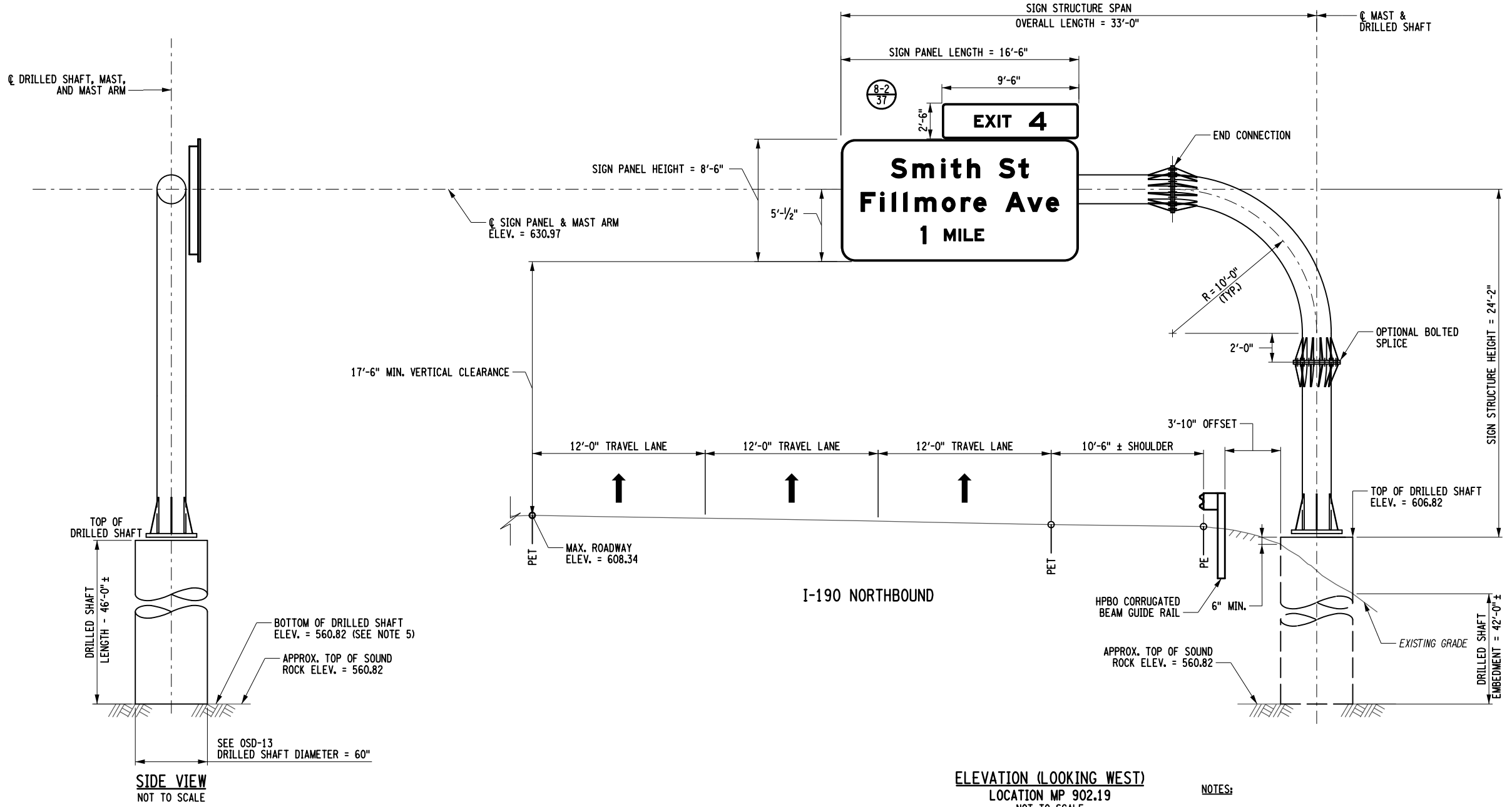
CHECKED BY: S. LOFORTE

DRAFTED BY: M. OZOG

CHECKED BY: S. LOFORTE

DESIGNED BY: M. OZOG

DESIGN SUPERVISOR: M. BEDFORD



**ELEVATION (LOOKING WEST)**  
LOCATION MP 902.19  
NOT TO SCALE

**NOTES:**

1. FABRICATOR HAS OPTION TO ADD OR ELIMINATE SPLICES.
2. PAYMENT FOR THE SIGN STRUCTURE SHALL BE MADE UNDER ITEM 644.70380125 - MONOPIPE OVERHEAD CANTILEVER SIGN STRUCTURE. PAYMENT FOR THE DRILLED SHAFT SHALL BE MADE UNDER ITEM 644.20 - DRILLED SHAFT FOR OVERHEAD STRUCTURE. ANCHOR BOLTS AND ASSOCIATED HARDWARE SHALL BE PAID FOR UNDER ITEM 644.11.
3. FOR CANTILEVER SIGN STRUCTURE DETAILS, SEE DWG. NO. OSD-10.
4. CONTRACTOR SHALL VERIFY ALL CONDITIONS AND DIMENSIONS PRIOR TO FABRICATION OF THE PROPOSED STRUCTURE.
5. BOTTOM OF DRILLED SHAFT TO BEAR ON TOP OF SOUND ROCK. SEE DWG. NO. OSD-13 FOR ADDITIONAL NOTES.

ALTERED ON:

AFFIXED ON: **05/13/2024**

SIGNATURE: STAMP:

SIGNATURE: STAMP:



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

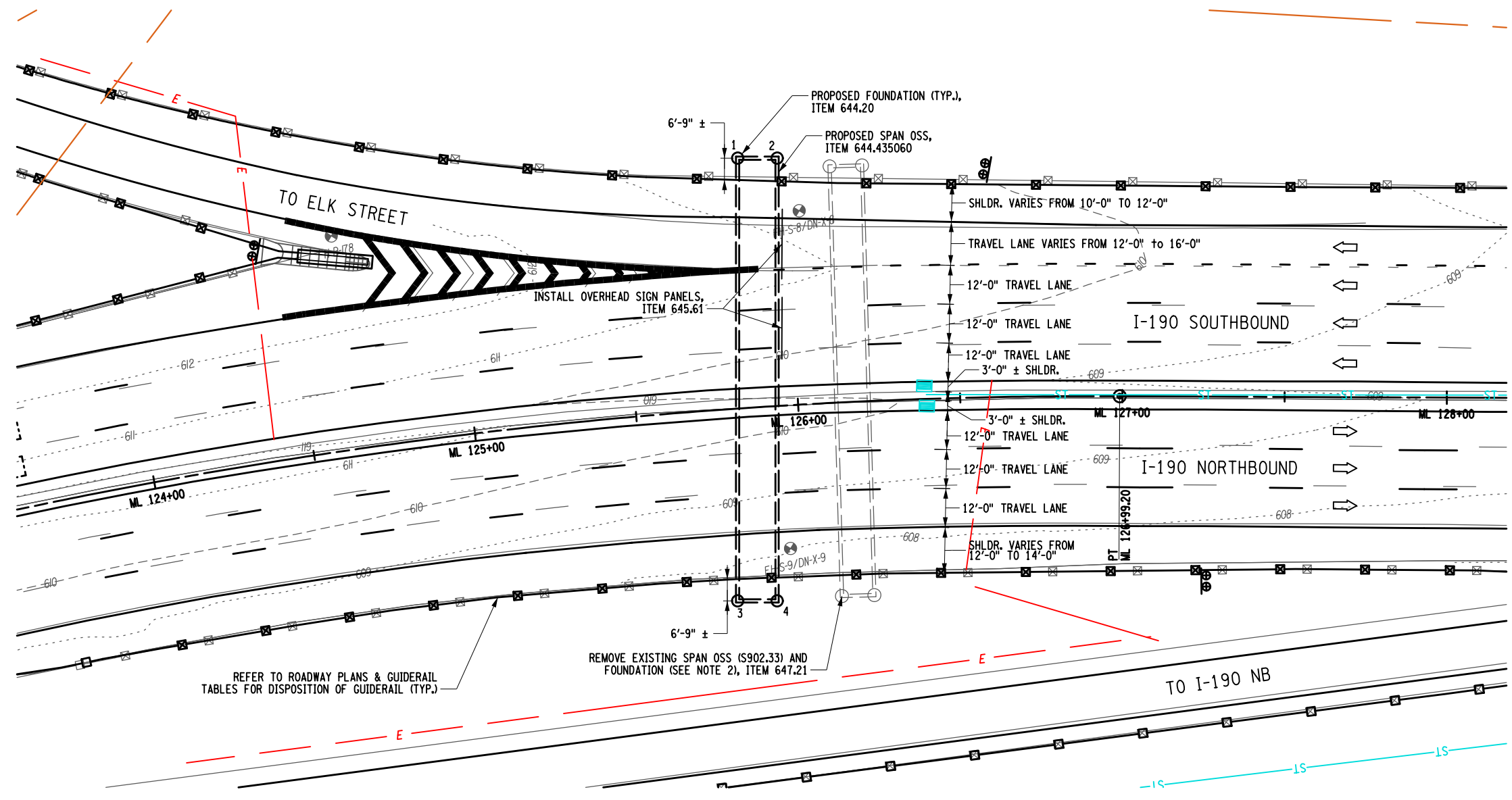
REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 TITLE OF DRAWING: OVERHEAD SIGN STRUCTURES ELEVATION LOCATION MP 902.19

CONTRACT NUMBER: TAN 24-12  
 DATE: MAY 2024  
 DRAWING NUMBER: OSP-08

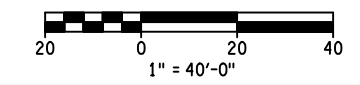
PROPOSED FOUNDATION C COORDINATES	
FOUNDATION NO.	COORDINATES
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2	N = 1044990.072 E = 1082264.496
3	N = 1045120.076 E = 1082307.845
4	N = 1045122.885 E = 1082296.007



**PLAN**  
LOCATION MP 902.33  
1" = 40'-0"

**NOTES:**

1. THE EXISTING SPAN OSS AND SIGN PANELS SHALL REMAIN IN PLACE AND FULLY FUNCTIONAL UNTIL THE REPLACEMENT OSS IS ERECTED, TESTED, AND ACCEPTED BY THE NYSTA.
2. UPON REMOVAL OF THE EXISTING SPAN OSS, THE EXISTING FOUNDATIONS SHALL BE REMOVED TO A DEPTH OF 1'-0" BELOW EXISTING GRADE AND THE SURFACE SHALL BE RESTORED TO MATCH THE SURROUNDING AREA.



CHECKED BY: S. LOFORTE

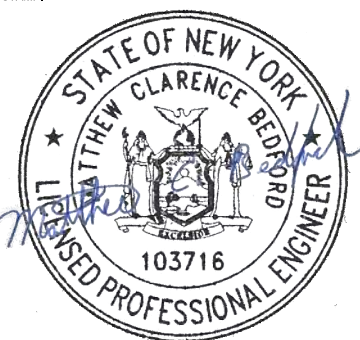
DRAFTED BY: M. OZOG

CHECKED BY: S. LOFORTE

DESIGNED BY: M. OZOG

DESIGN SUPERVISOR: M. BEDFORD

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP:



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES GENERAL PLAN LOCATION MP 902.33	DRAWING NUMBER: OSP-09

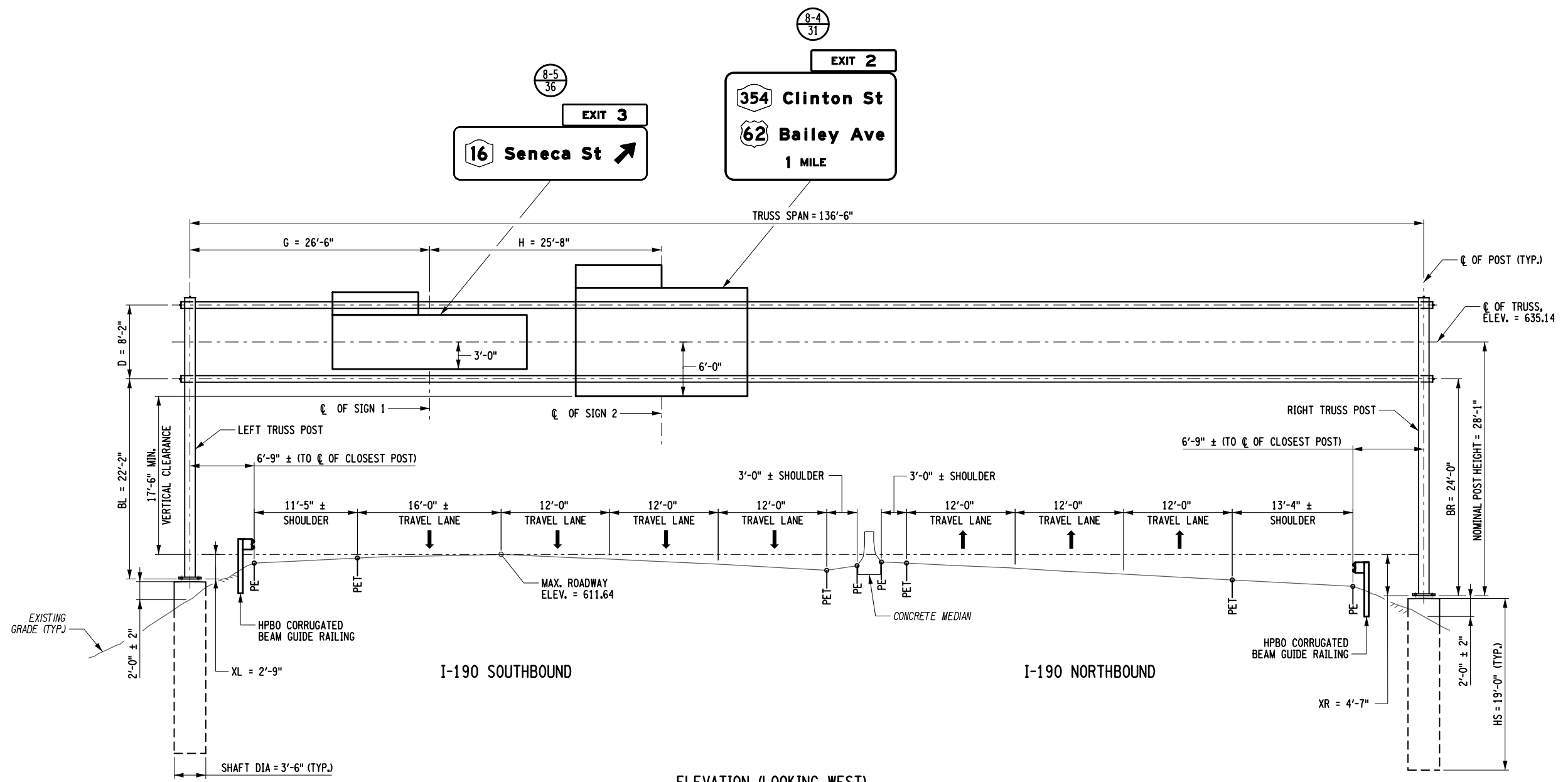
CHECKED BY: S. LOFORTE

DRAFTED BY: M. OZOG

CHECKED BY: S. LOFORTE

DESIGNED BY: M. OZOG

DESIGN SUPERVISOR: M. BEDFORD



ELEVATION (LOOKING WEST)  
LOCATION MP 902.33  
NOT TO SCALE

ALTERED ON:  
SIGNATURE:  
STAMP:

AFFIXED ON:  
**05/13/2024**  
SIGNATURE:  
STAMP:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYL

**Thruway Authority**

**POPPL DESIGN GROUP**

TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
OVERHEAD SIGN STRUCTURES  
ELEVATION  
LOCATION MP 902.33

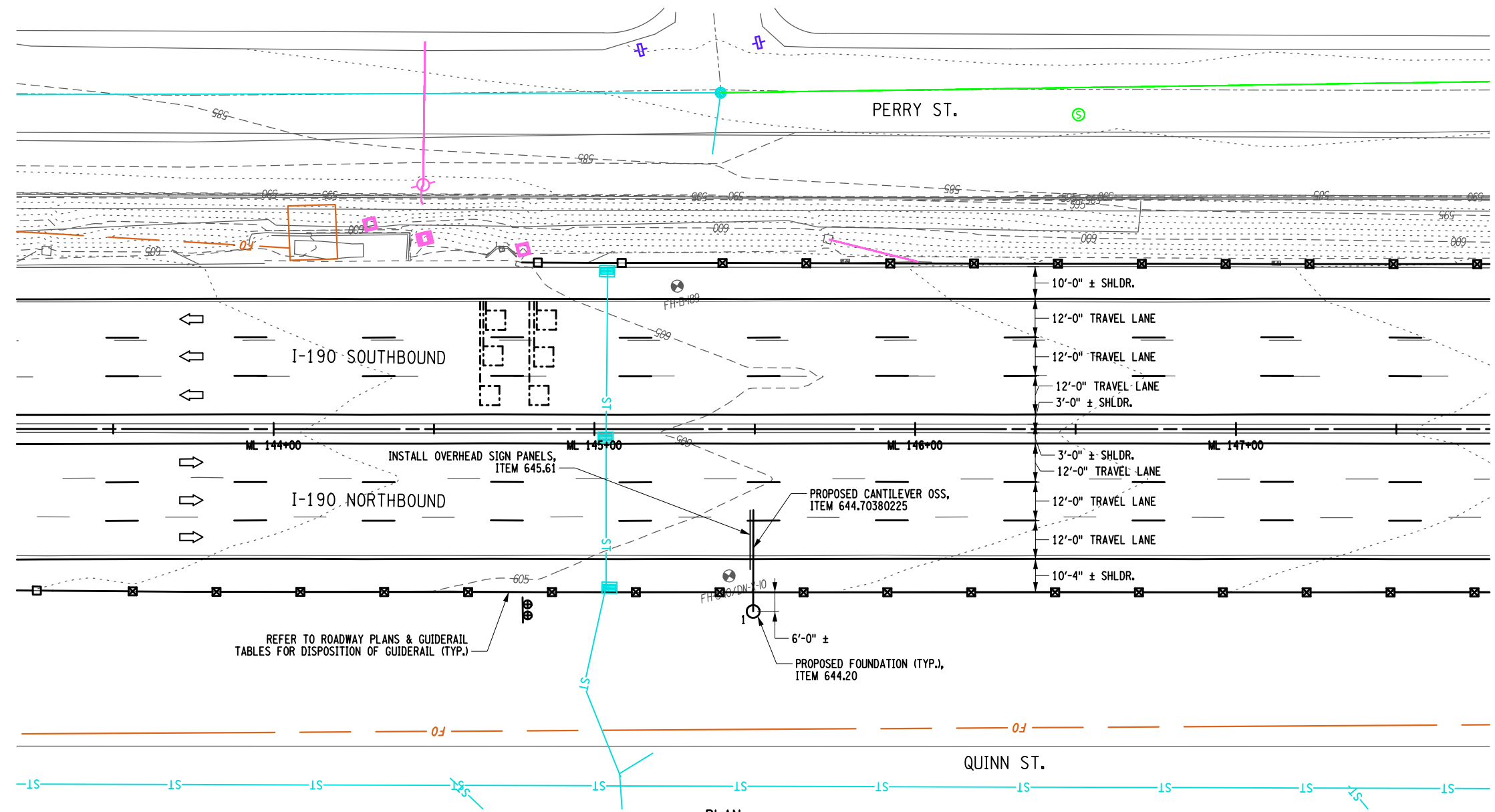
CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
OSP-10

- NOTES:**
1. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO THE FABRICATION OF THE SIGN STRUCTURE.
  2. SPAN TRUSS DIAGONALS, CROSS BRACING AND SPLICES NOT SHOWN FOR CLARITY.

PROPOSED FOUNDATION C COORDINATES	
FOUNDATION NO.	COORDINATES
1	N = 1045576.327 E = 1080393.656



**PLAN**  
LOCATION MP 902.69  
1" = 40'-0"

CHECKED BY: M. OZOG

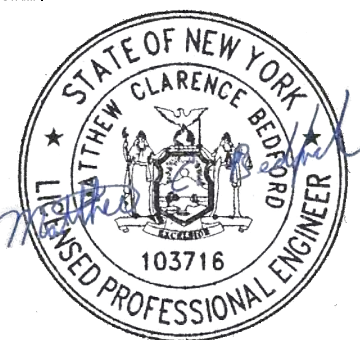
DRAFTED BY: S. LOFORTE

CHECKED BY: M. OZOG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP:

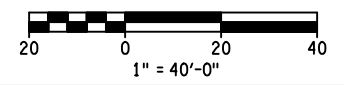


IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES GENERAL PLAN LOCATION MP 902.69	DRAWING NUMBER: OSP-11



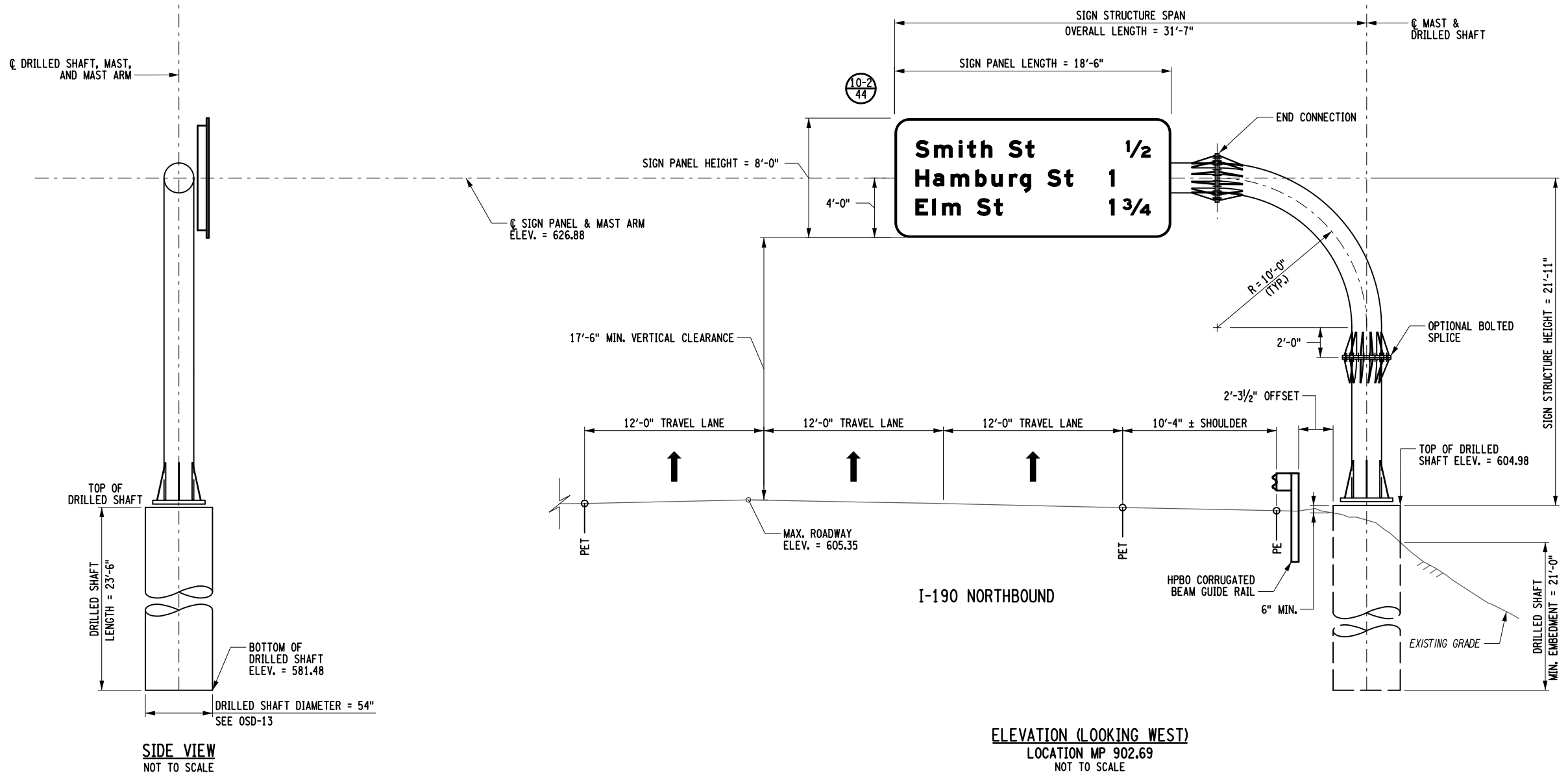
CHECKED BY: Q. WANG

DRAFTED BY: S. LOFORTE

CHECKED BY: Q. WANG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



**SIDE VIEW**  
NOT TO SCALE

**ELEVATION (LOOKING WEST)**  
LOCATION MP 902.69  
NOT TO SCALE

**NOTES:**

1. FABRICATOR HAS OPTION TO ADD OR ELIMINATE SPLICES.
2. PAYMENT FOR THE SIGN STRUCTURE SHALL BE MADE UNDER ITEM 644.70380225 - MONOPIPE OVERHEAD CANTILEVER SIGN STRUCTURE. PAYMENT FOR THE DRILLED SHAFT SHALL BE MADE UNDER ITEM 644.20 - DRILLED SHAFT FOR OVERHEAD STRUCTURE. ANCHOR BOLTS AND ASSOCIATED HARDWARE SHALL BE PAID FOR UNDER ITEM 644.11.
3. FOR CANTILEVER SIGN STRUCTURE DETAILS, SEE DWG. NO. OSD-10.
4. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO THE FABRICATION OF THE SIGN STRUCTURE.

ALTERED ON:

AFFIXED ON: **05/13/2024**

SIGNATURE:  
STAMP:

SIGNATURE:  
STAMP:



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

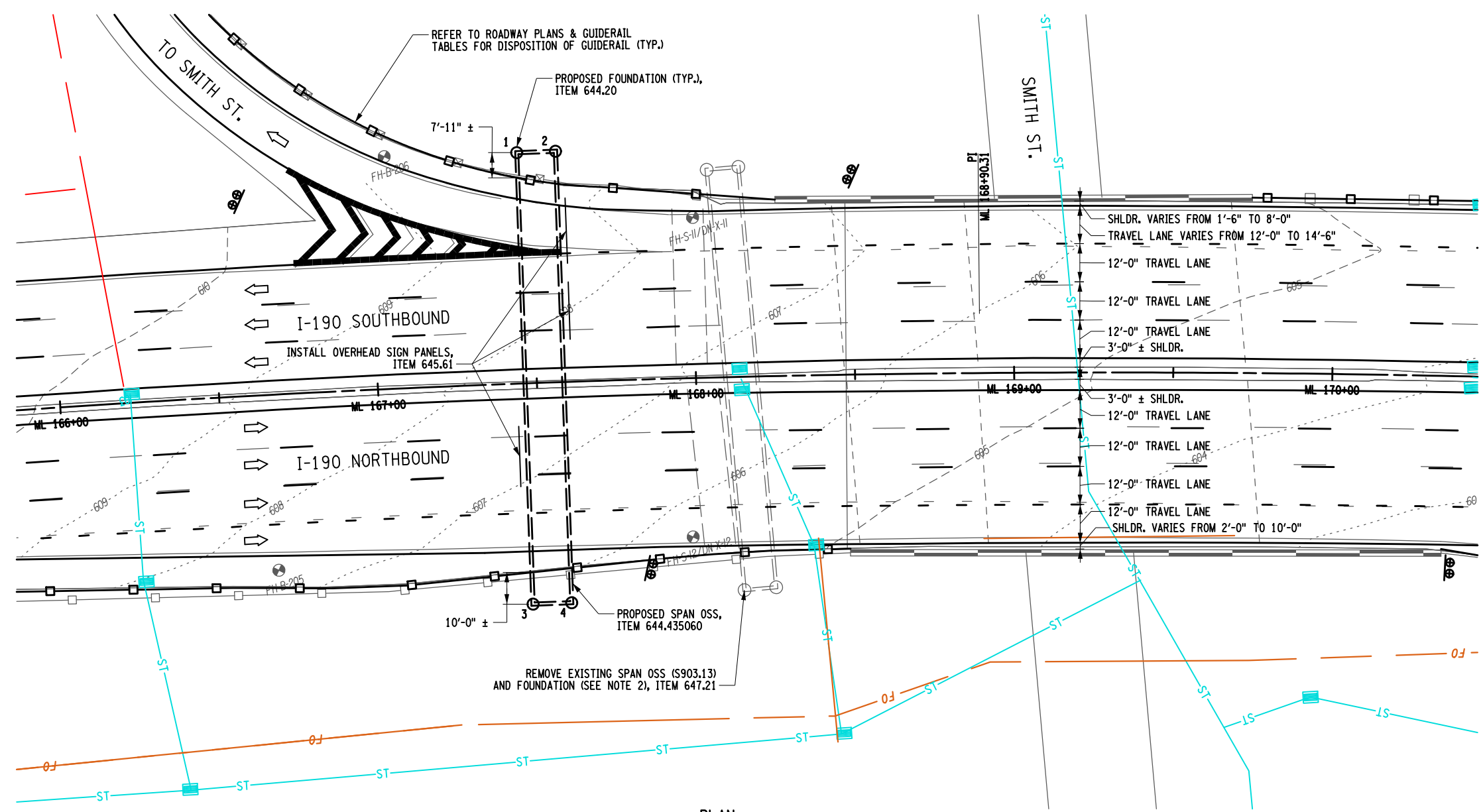
REVISIONS			
DATE	DESCRIPTION	BY	SYL



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2  
TITLE OF DRAWING  
OVERHEAD SIGN STRUCTURES  
ELEVATION  
LOCATION MP 902.69

CONTRACT NUMBER:  
TAN 24-12  
DATE:  
MAY 2024  
DRAWING NUMBER:  
OSP-12

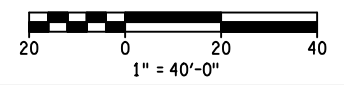
PROPOSED FOUNDATION C COORDINATES	
FOUNDATION NO.	COORDINATES
1	N = 1045976.308 E = 1078226.083
2	N = 1045979.856 E = 1078214.445
3	N = 1046112.134 E = 1078267.498
4	N = 1046115.683 E = 1078255.860



**PLAN**  
LOCATION MP 903.13  
1" = 40'-0"

**NOTES:**

1. THE EXISTING SPAN OSS AND SIGN PANELS SHALL REMAIN IN PLACE AND FULLY FUNCTIONAL UNTIL THE REPLACEMENT OSS IS ERECTED, TESTED, AND ACCEPTED BY THE NYSTA.
2. UPON REMOVAL OF THE EXISTING SPAN OSS, THE EXISTING FOUNDATIONS SHALL BE REMOVED TO A DEPTH OF 1'-0" BELOW EXISTING GRADE AND THE SURFACE SHALL BE RESTORED TO MATCH THE SURROUNDING AREA.



CHECKED BY: M. OZOG  
DRAFTED BY: S. LOFORTE  
CHECKED BY: M. OZOG  
DESIGNED BY: S. LOFORTE  
DESIGN SUPERVISOR: M. BEDFORD

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

<b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING OVERHEAD SIGN STRUCTURES GENERAL PLAN LOCATION MP 903.13	DRAWING NUMBER: OSP-13

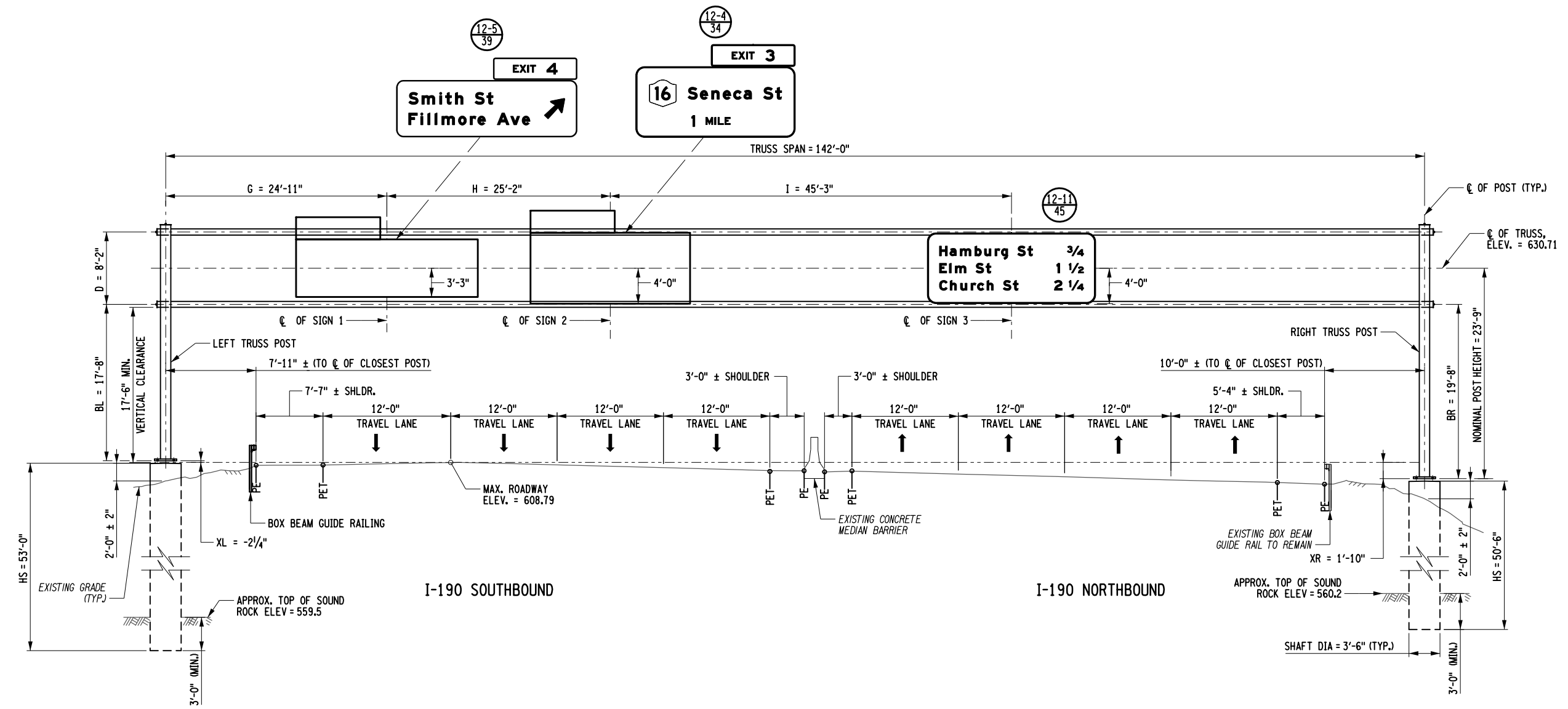
CHECKED BY: Q. WANG

DRAFTED BY: S. LOFORTE

CHECKED BY: Q. WANG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



**ELEVATION (LOOKING WEST)**  
LOCATION MP 903.13  
NOT TO SCALE

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

**NEW YORK STATE OF OPPORTUNITY**  
**Thruway Authority**

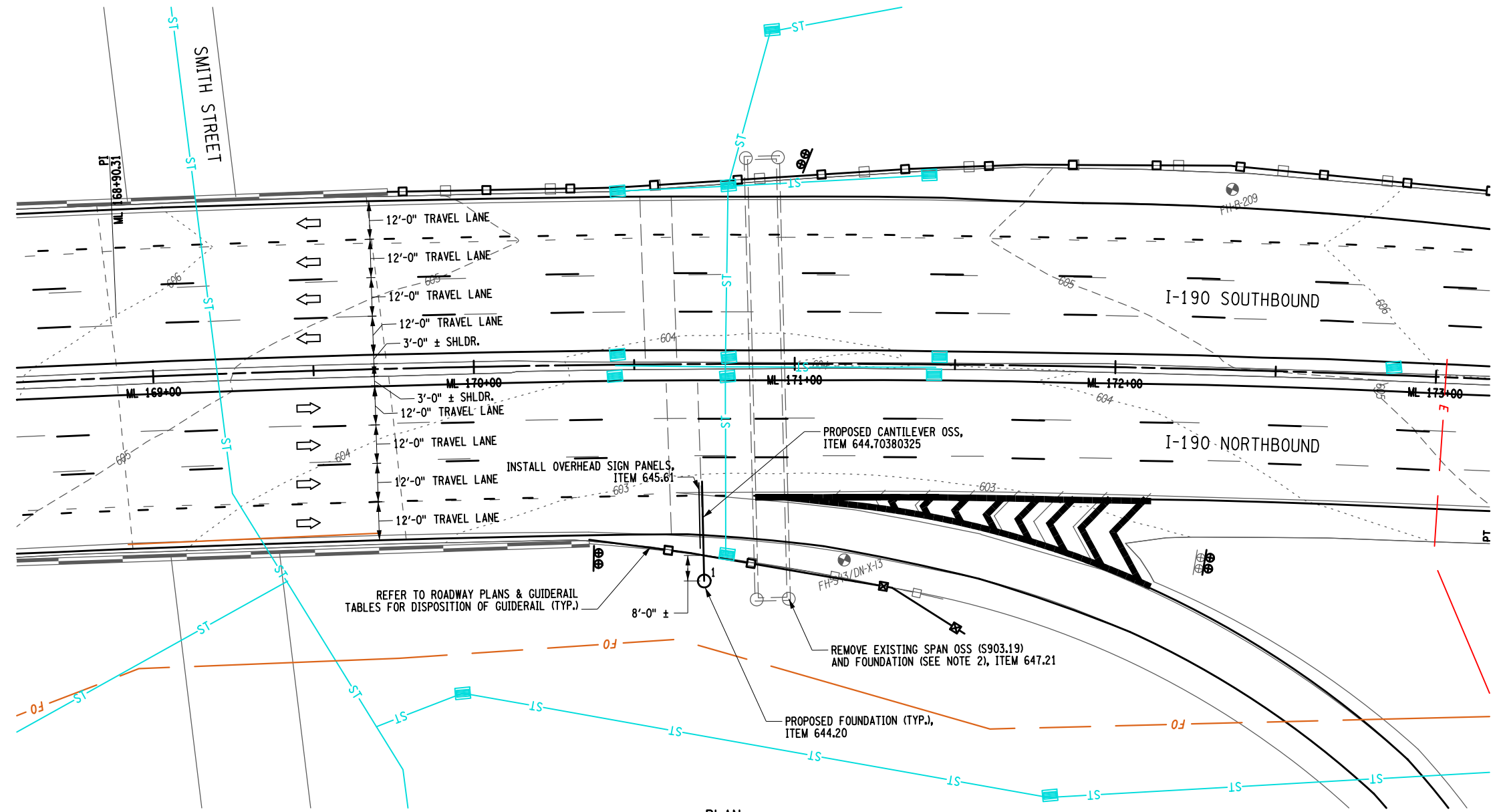
**PDG**  
POPPLI DESIGN GROUP

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES ELEVATION LOCATION MP 903.13	DRAWING NUMBER: OSP-14

- NOTES:**
1. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO THE FABRICATION OF THE SIGN STRUCTURE.
  2. SPAN TRUSS DIAGONALS, CROSS BRACING AND SPLICES NOT SHOWN FOR CLARITY.



PROPOSED FOUNDATION C COORDINATES	
FOUNDATION NO.	COORDINATES
1	N = 1046213.618 E = 1077963.223



DESIGNED BY: M. OZOG  
CHECKED BY: S. LOFORTE  
DRAFTED BY: M. OZOG  
CHECKED BY: S. LOFORTE

ALTERED ON:

SIGNATURE:  
STAMP:

AFFIXED ON:  
**05/13/2024**

SIGNATURE:  
STAMP:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYL



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
OVERHEAD SIGN STRUCTURES  
GENERAL PLAN  
LOCATION MP 903.19

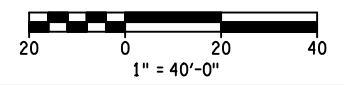
CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
OSP-15

**NOTES:**

- THE EXISTING SPAN OSS AND SIGN PANELS SHALL REMAIN IN PLACE AND FULLY FUNCTIONAL UNTIL THE REPLACEMENT OSS IS ERECTED, TESTED, AND ACCEPTED BY THE NYSTA.
- UPON REMOVAL OF THE EXISTING SPAN OSS, THE EXISTING FOUNDATION SHALL BE REMOVED TO A DEPTH OF 1'-0" BELOW EXISTING GRADE AND THE SURFACE SHALL BE RESTORED TO MATCH THE SURROUNDING AREA.



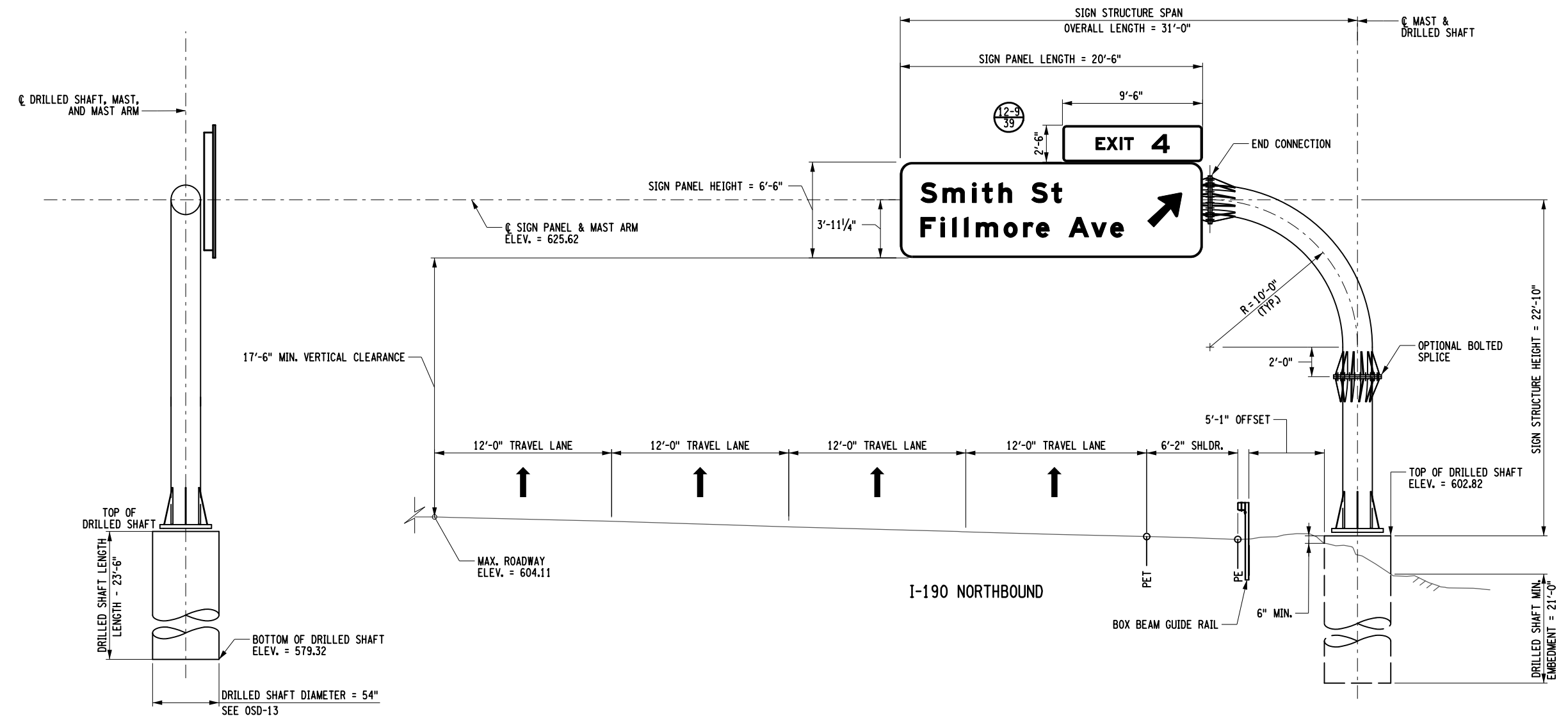
CHECKED BY: Q. WANG

DRAFTED BY: S. LOFORTE

CHECKED BY: Q. WANG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



**SIDE VIEW**  
NOT TO SCALE

**ELEVATION (LOOKING WEST)**  
LOCATION MP 903.19  
NOT TO SCALE

**NOTES:**

1. FABRICATOR HAS OPTION TO ADD OR ELIMINATE SPLICES.
2. PAYMENT FOR THE SIGN STRUCTURE SHALL BE MADE UNDER ITEM 644.70380325 - MONOPIPE OVERHEAD CANTILEVER SIGN STRUCTURE. PAYMENT FOR THE DRILLED SHAFT SHALL BE MADE UNDER ITEM 644.20 - DRILLED SHAFT FOR OVERHEAD STRUCTURE. ANCHOR BOLTS AND ASSOCIATED HARDWARE SHALL BE PAID FOR UNDER ITEM 644.11.
3. FOR CANTILEVER SIGN STRUCTURE DETAILS, SEE DWG. NO. OSD-10.
4. CONTRACTOR SHALL VERIFY ALL CONDITIONS AND DIMENSIONS PRIOR TO FABRICATION OF THE PROPOSED STRUCTURE.

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

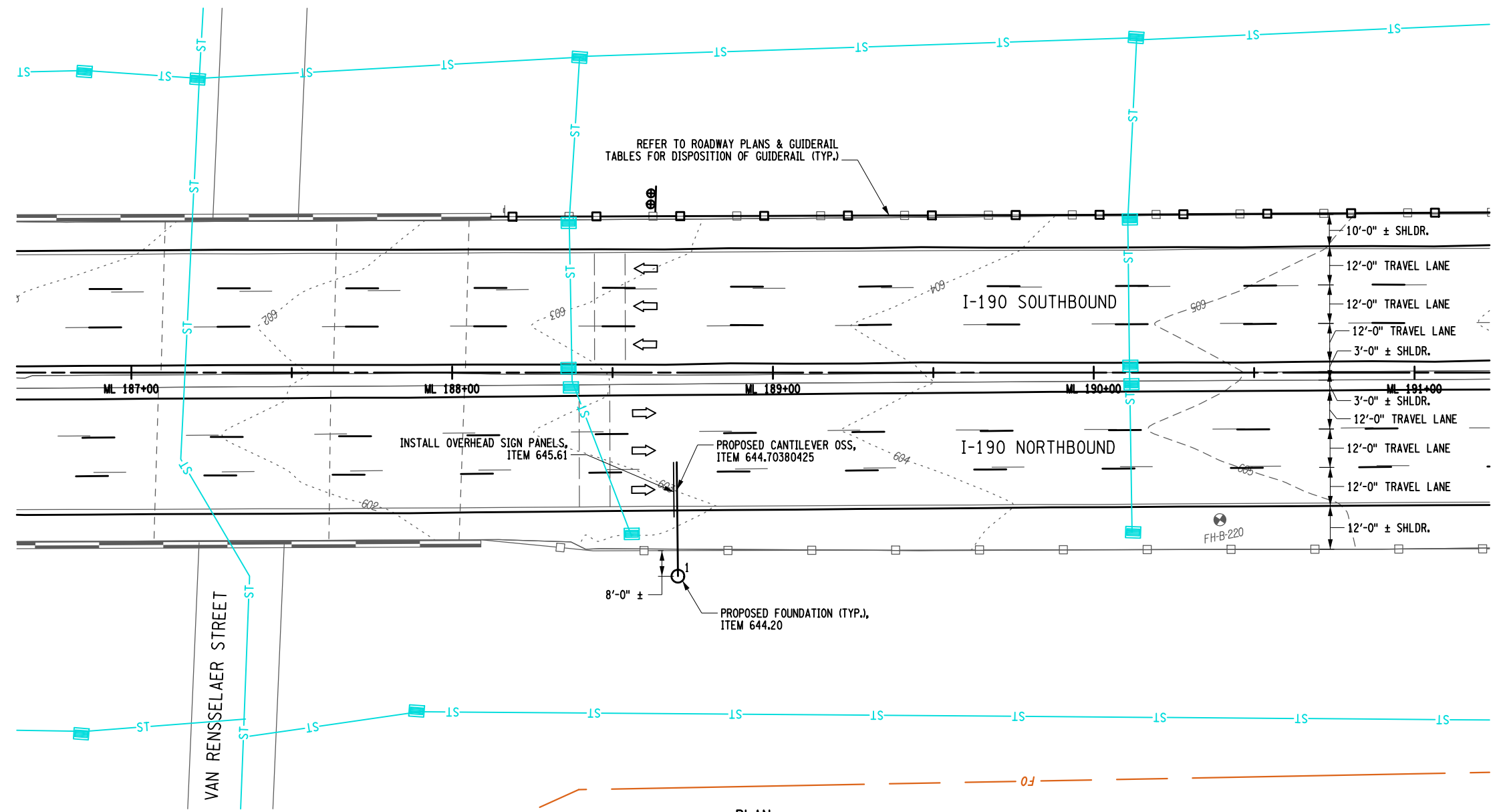
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYL



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES ELEVATION LOCATION MP 903.19	DRAWING NUMBER: OSP-16

PROPOSED FOUNDATION C COORDINATES	
FOUNDATION NO.	COORDINATES
1	N = 1046765.197 E = 1076245.897



PLAN  
LOCATION MP 903.51  
1" = 40'-0"

CHECKED BY: M. OZOG

DRAFTED BY: S. LOFORTE

CHECKED BY: M. OZOG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD

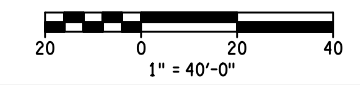
ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP:



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES GENERAL PLAN LOCATION MP 903.51	DRAWING NUMBER: OSP-17



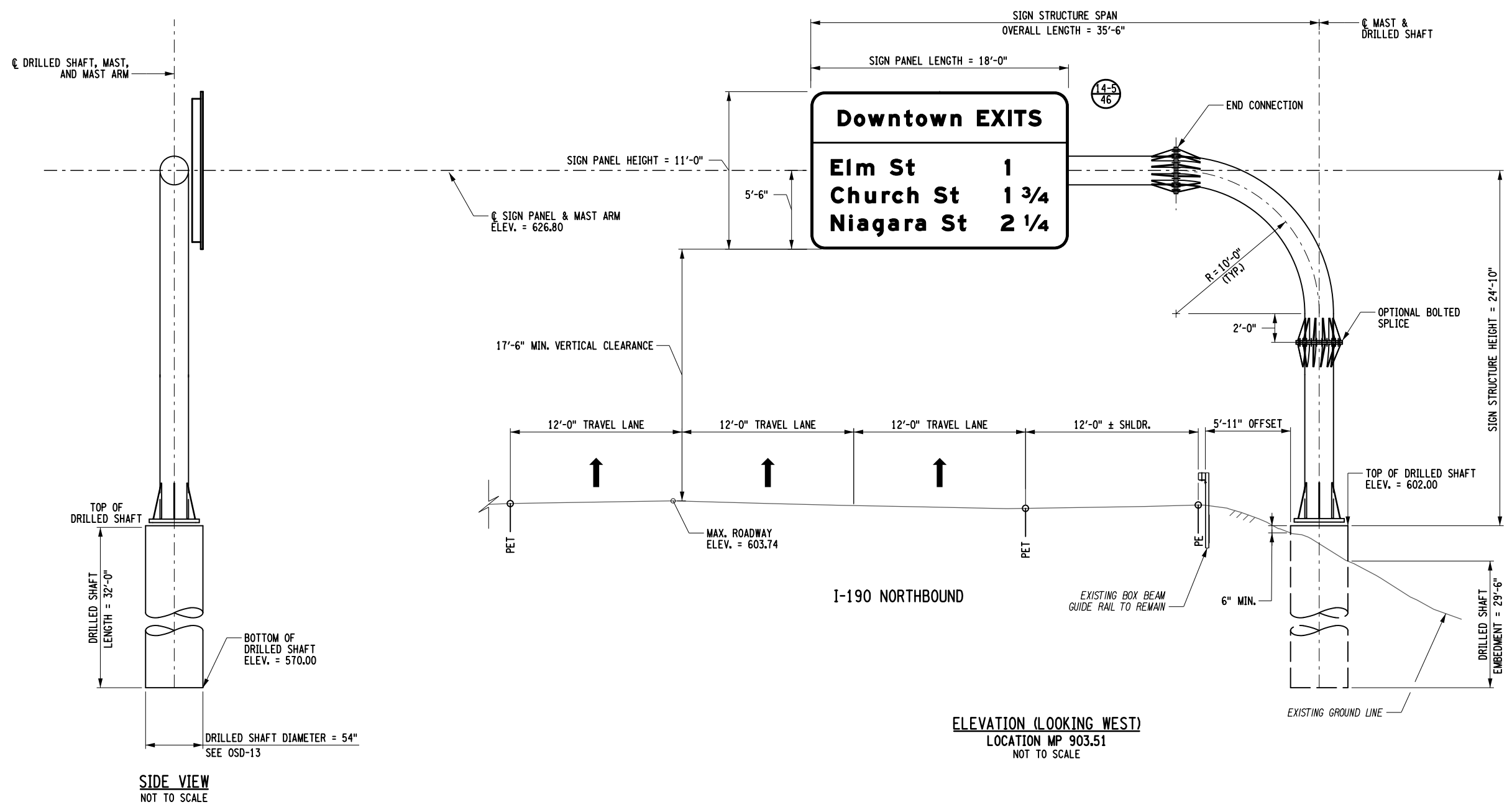
CHECKED BY: Q. WANG

DRAFTED BY: S. LOFORTE

CHECKED BY: Q. WANG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



ELEVATION (LOOKING WEST)  
LOCATION MP 903.51  
NOT TO SCALE

**NOTES:**

1. FABRICATOR HAS OPTION TO ADD OR ELIMINATE SPLICES.
2. PAYMENT FOR THE SIGN STRUCTURE SHALL BE MADE UNDER ITEM 644.70380425 - MONOPIPE OVERHEAD CANTILEVER SIGN STRUCTURE. PAYMENT FOR THE DRILLED SHAFT SHALL BE MADE UNDER ITEM 644.20 - DRILLED SHAFT FOR OVERHEAD STRUCTURE. ANCHOR BOLTS AND ASSOCIATED HARDWARE SHALL BE PAID FOR UNDER ITEM 644.11.
3. FOR CANTILEVER SIGN STRUCTURE DETAILS, SEE DWG. NO. OSD-10.
4. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO THE FABRICATION OF THE SIGN STRUCTURE.

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

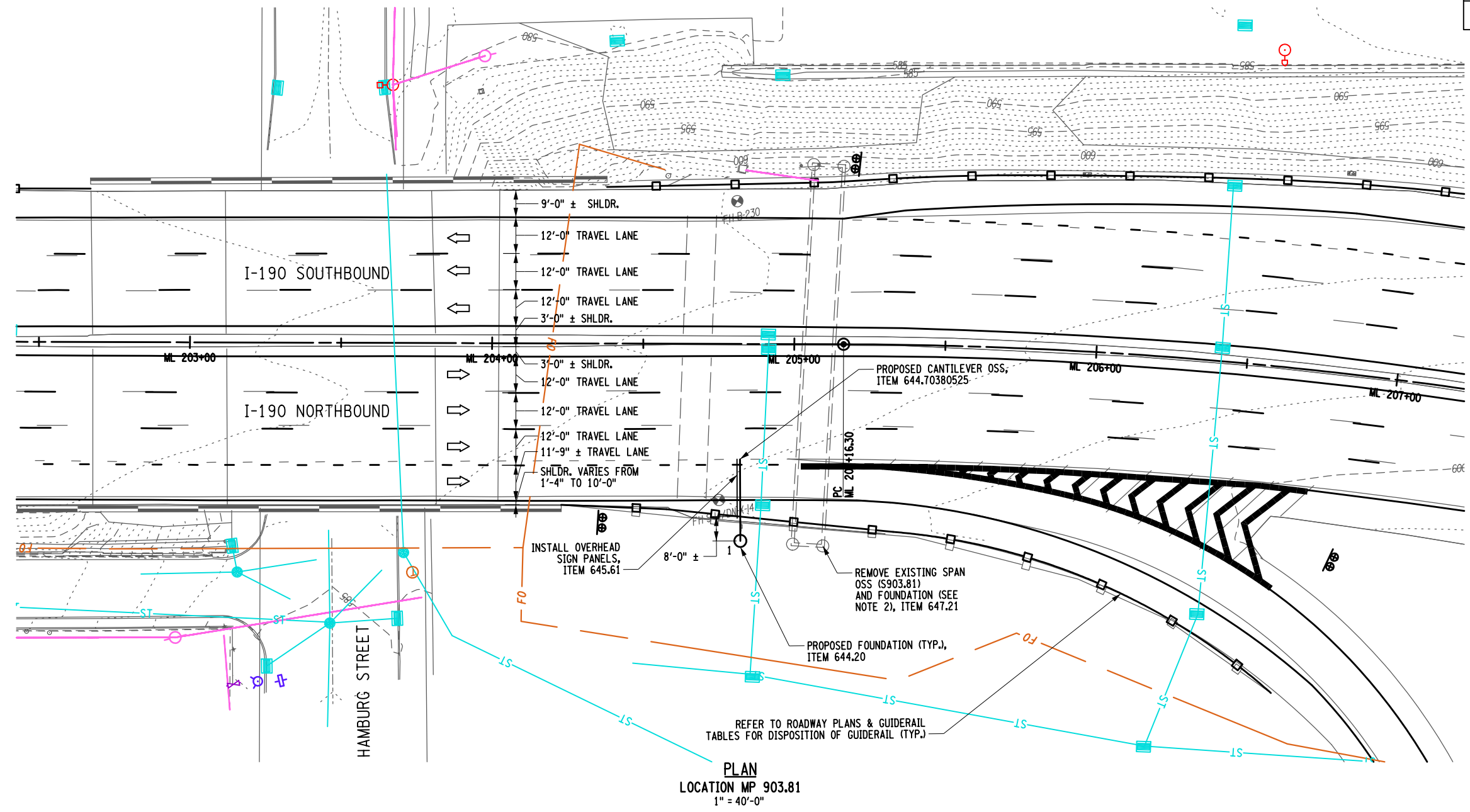
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYL.



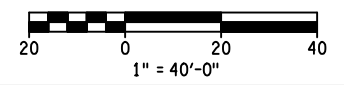
TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES ELEVATION LOCATION MP 903.51	DRAWING NUMBER: OSP-18

PROPOSED FOUNDATION C COORDINATES	
FOUNDATION NO.	COORDINATES
1	N = 1047126.990 E = 1074678.178



**NOTES:**

1. THE EXISTING SPAN OSS AND SIGN PANELS SHALL REMAIN IN PLACE AND FULLY FUNCTIONAL UNTIL THE REPLACEMENT OSS IS ERECTED, TESTED, AND ACCEPTED BY THE NYSTA.
2. UPON REMOVAL OF THE EXISTING SPAN OSS, THE EXISTING FOUNDATION SHALL BE REMOVED TO A DEPTH OF 1'-0" BELOW EXISTING GRADE AND THE SURFACE SHALL BE RESTORED TO MATCH THE SURROUNDING AREA.



CHECKED BY: S. LOFORTE  
DRAFTED BY: M. OZOG  
DESIGNED BY: M. OZOG  
DESIGN SUPERVISOR: M. BEDFORD

ALTERED ON:

AFFIXED ON: **05/13/2024**

SIGNATURE:  
STAMP:

SIGNATURE:  
STAMP:



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION

LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2

TITLE OF DRAWING  
OVERHEAD SIGN STRUCTURES  
GENERAL PLAN  
LOCATION MP 903.81

CONTRACT NUMBER:  
TAN 24-12

DATE:  
MAY 2024

DRAWING NUMBER:  
OSP-19

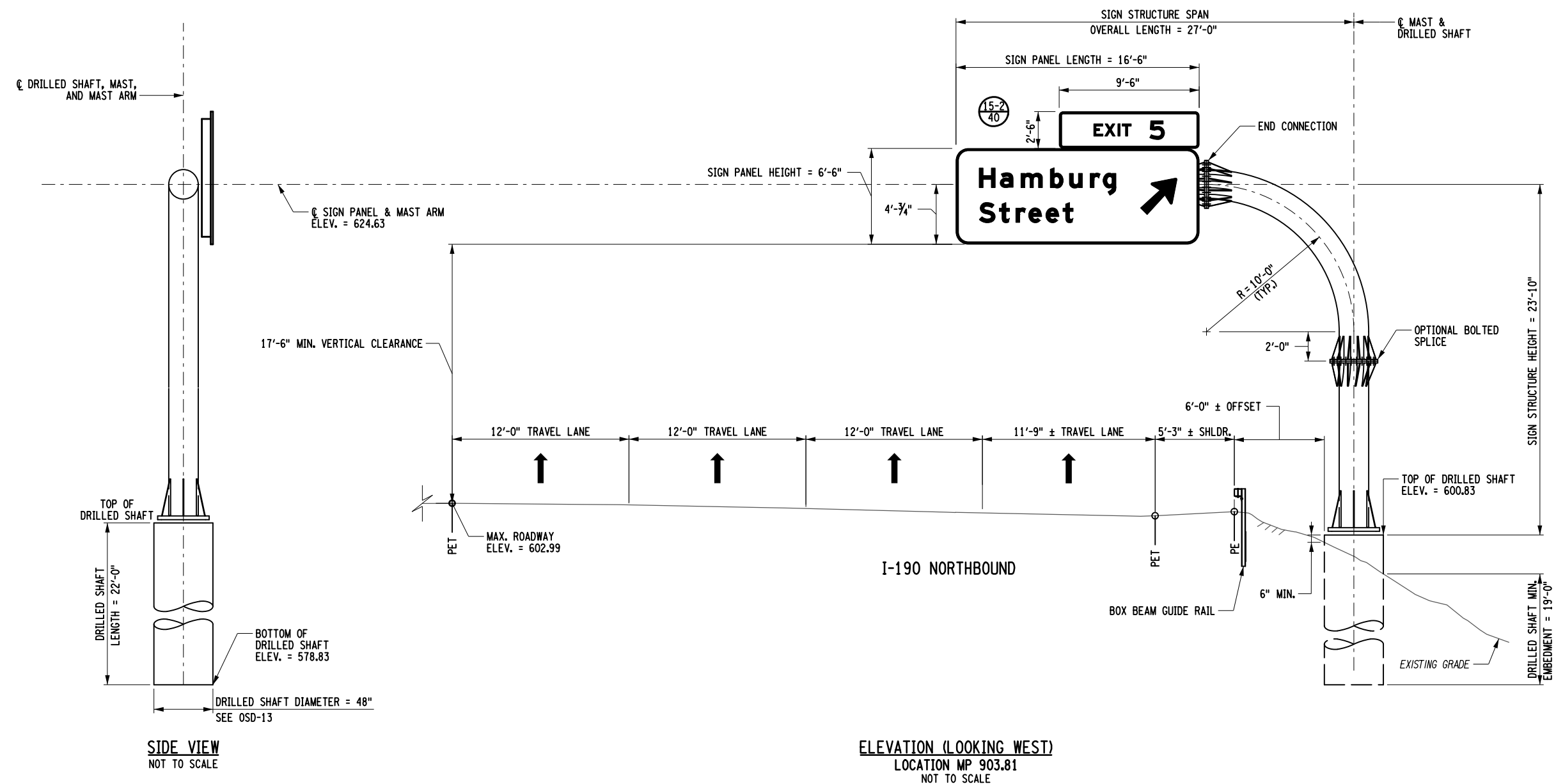
CHECKED BY: Q. WANG

DRAFTED BY: S. LOFORTE

CHECKED BY: Q. WANG

DESIGNED BY: S. LOFORTE


DESIGN SUPERVISOR: M. BEDFORD



ELEVATION (LOOKING WEST)  
LOCATION MP 903.81  
NOT TO SCALE

NOTES:

1. FABRICATOR HAS OPTION TO ADD OR ELIMINATE SPLICES.
2. PAYMENT FOR THE SIGN STRUCTURE SHALL BE MADE UNDER ITEM 644.70380525 - MONOPIPE OVERHEAD CANTILEVER SIGN STRUCTURE. PAYMENT FOR THE DRILLED SHAFT SHALL BE MADE UNDER ITEM 644.20 - DRILLED SHAFT FOR OVERHEAD STRUCTURE. ANCHOR BOLTS AND ASSOCIATED HARDWARE SHALL BE PAID FOR UNDER ITEM 644.11.
3. FOR CANTILEVER SIGN STRUCTURE DETAILS, SEE DWG. NO. OSD-10.
4. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO THE FABRICATION OF THE SIGN STRUCTURE.

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

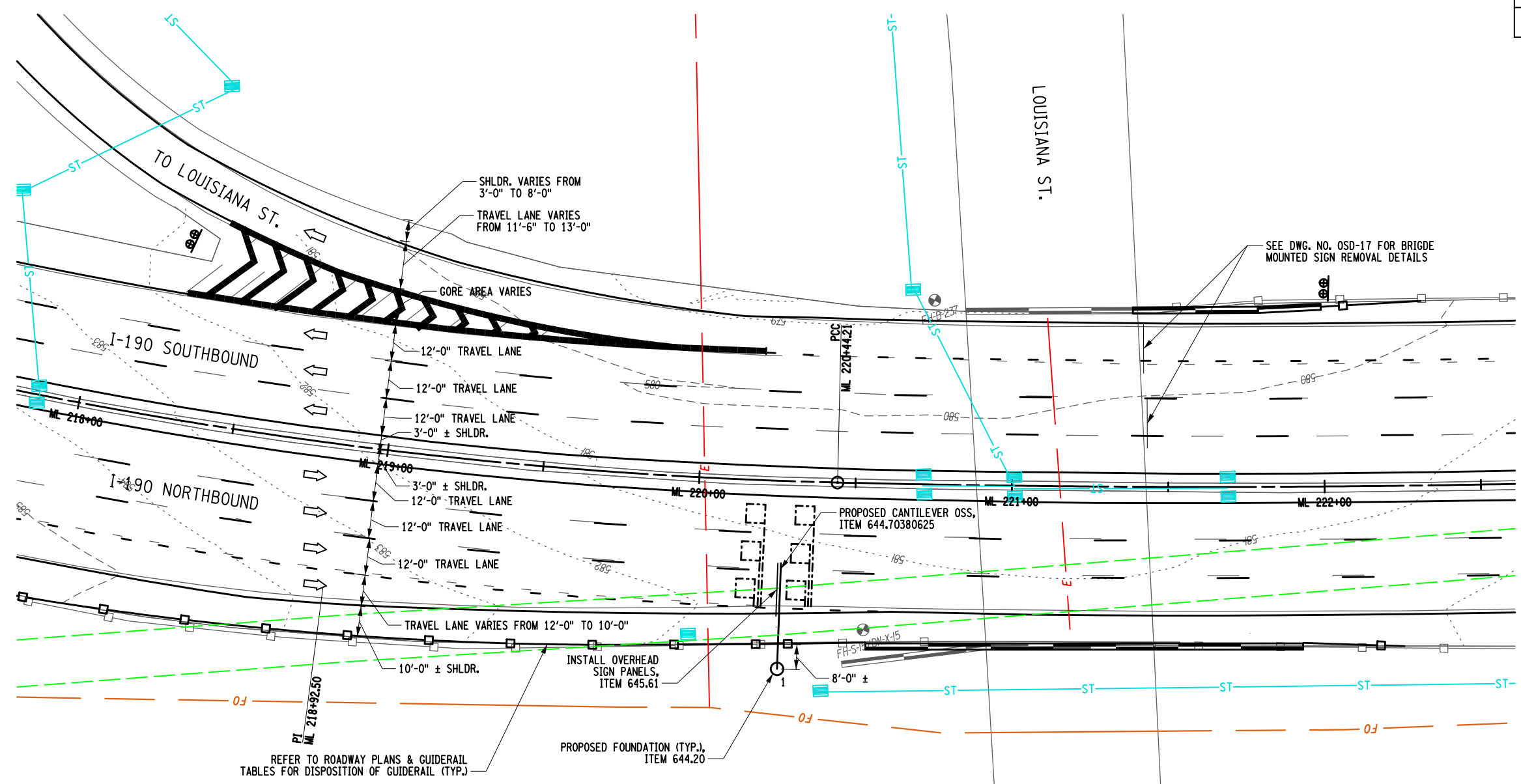
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REVISIONS			
DATE	DESCRIPTION	BY	SYML



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES ELEVATION LOCATION MP 903.81	DRAWING NUMBER: OSP-20

PROPOSED FOUNDATION C COORDINATES	
FOUNDATION NO.	COORDINATES
1	N = 1047883.216 E = 1073356.646



PLAN  
LOCATION MP 904.11  
1" = 40'-0"

CHECKED BY: M. OZOG

DRAFTED BY: S. LOFORTE

CHECKED BY: M. OZOG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP:

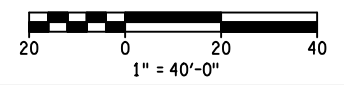


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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES GENERAL PLAN LOCATION MP 904.11	DRAWING NUMBER: OSP-21



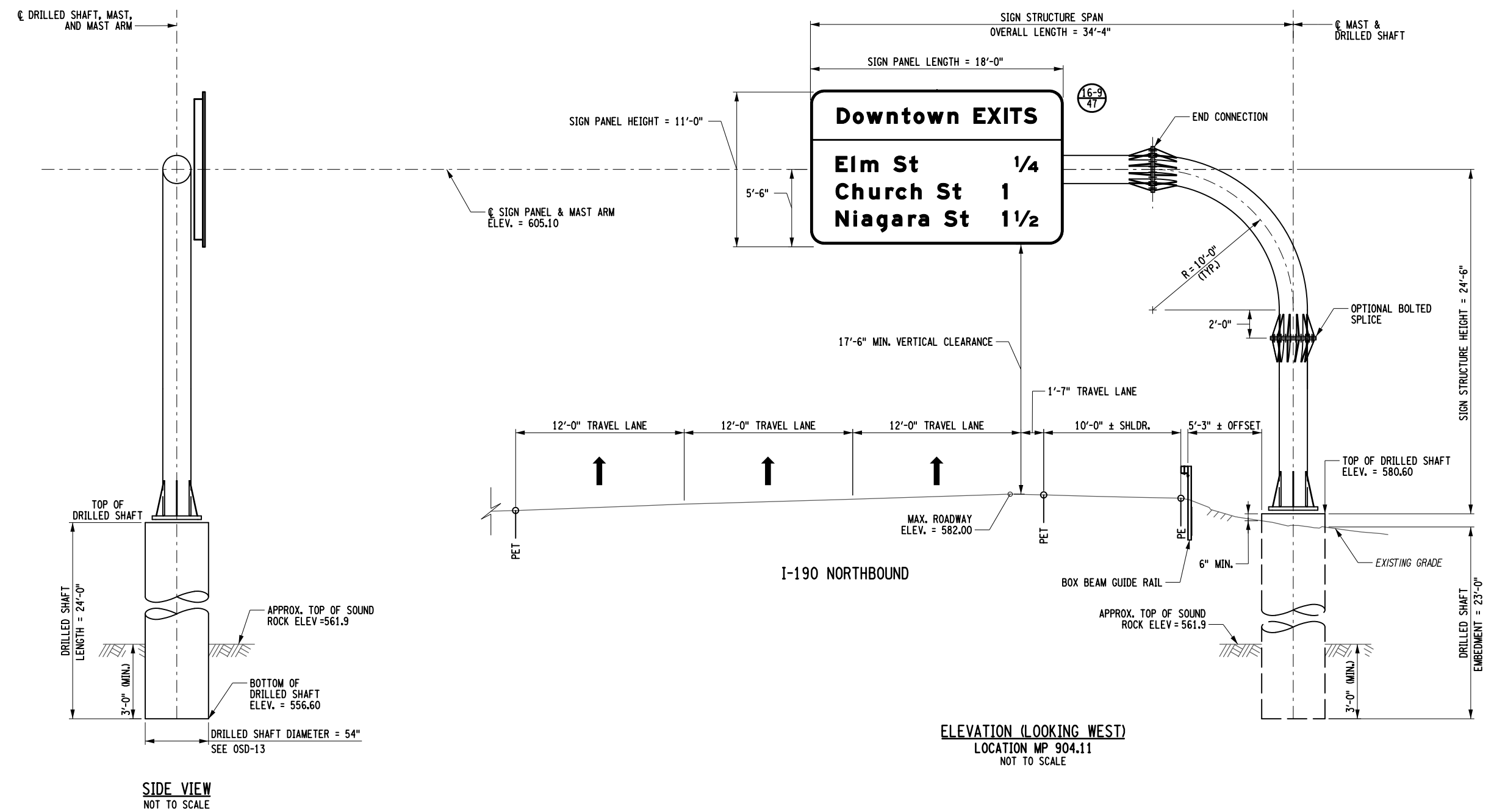
CHECKED BY: Q. WANG

DRAFTED BY: S. LOFORTE

CHECKED BY: Q. WANG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



SIDE VIEW  
NOT TO SCALE

ELEVATION (LOOKING WEST)  
LOCATION MP 904.11  
NOT TO SCALE

NOTES:

1. FABRICATOR HAS OPTION TO ADD OR ELIMINATE SPLICES.
2. PAYMENT FOR THE SIGN STRUCTURE SHALL BE MADE UNDER ITEM 644.70380625 - MONOPIPE OVERHEAD CANTILEVER SIGN STRUCTURE. PAYMENT FOR THE DRILLED SHAFT SHALL BE MADE UNDER ITEM 644.20 - DRILLED SHAFT FOR OVERHEAD STRUCTURE. ANCHOR BOLTS AND ASSOCIATED HARDWARE SHALL BE PAID FOR UNDER ITEM 644.11.
3. FOR CANTILEVER SIGN STRUCTURE DETAILS, SEE DWG. NO. OSD-10.
4. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO THE FABRICATION OF THE SIGN STRUCTURE.

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP:

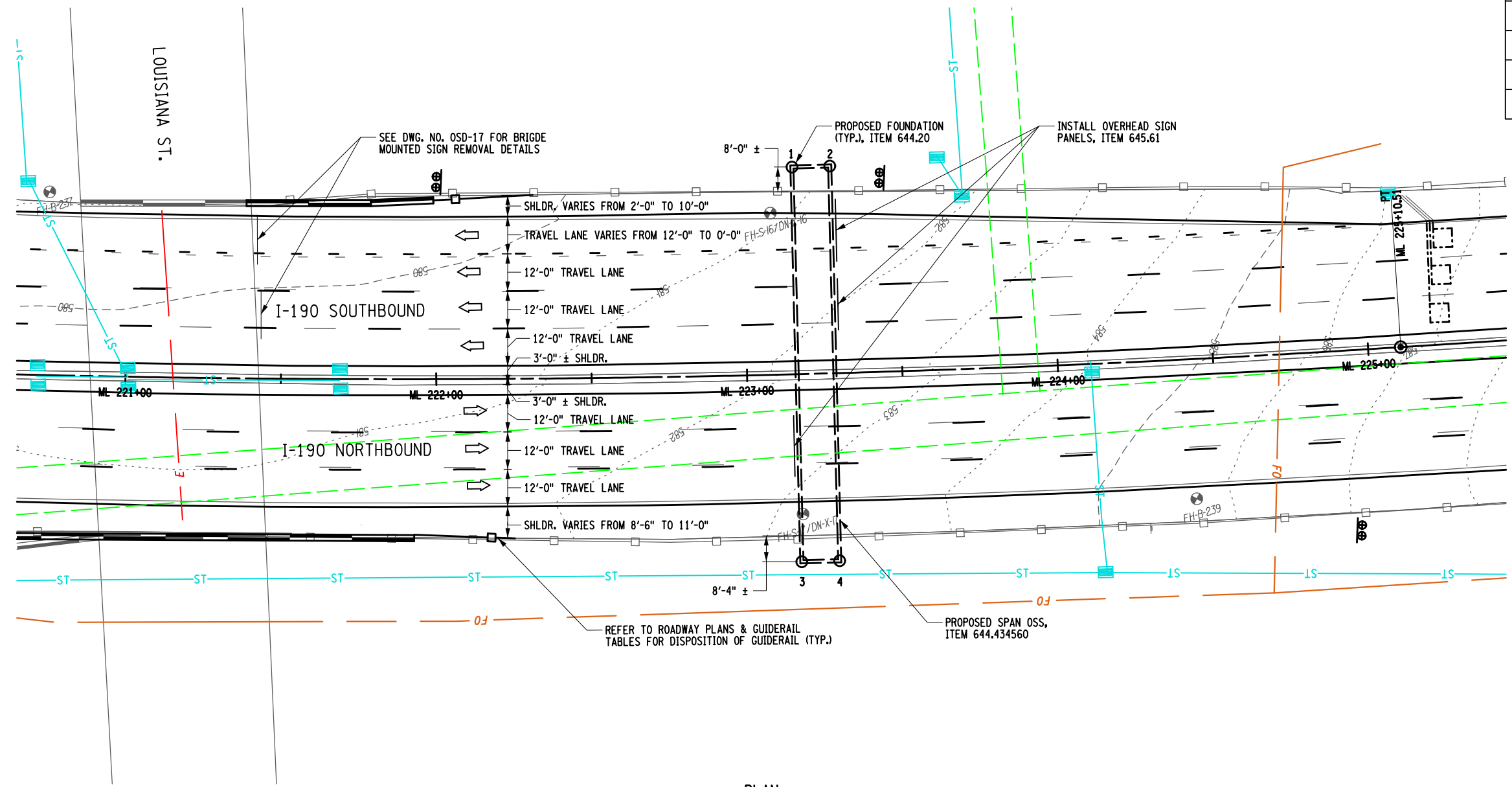
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING OVERHEAD SIGN STRUCTURES ELEVATION LOCATION MP 904.11	DRAWING NUMBER: OSP-22



PROPOSED FOUNDATION C COORDINATES	
FOUNDATION NO.	COORDINATES
1	N = 1047849.567 E = 1073042.526
2	N = 1047852.895 E = 1073030.824
3	N = 1047971.722 E = 1073077.269
4	N = 1047975.051 E = 1073065.567



**PLAN**  
LOCATION MP 904.16  
1" = 40'-0"

CHECKED BY: M. OZOG

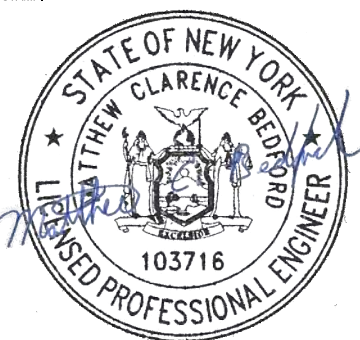
DRAFTED BY: S. LOFORTE

CHECKED BY: M. OZOG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP:

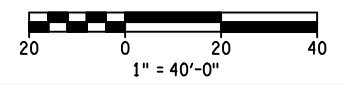


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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES GENERAL PLAN LOCATION MP 904.16	DRAWING NUMBER: OSP-23



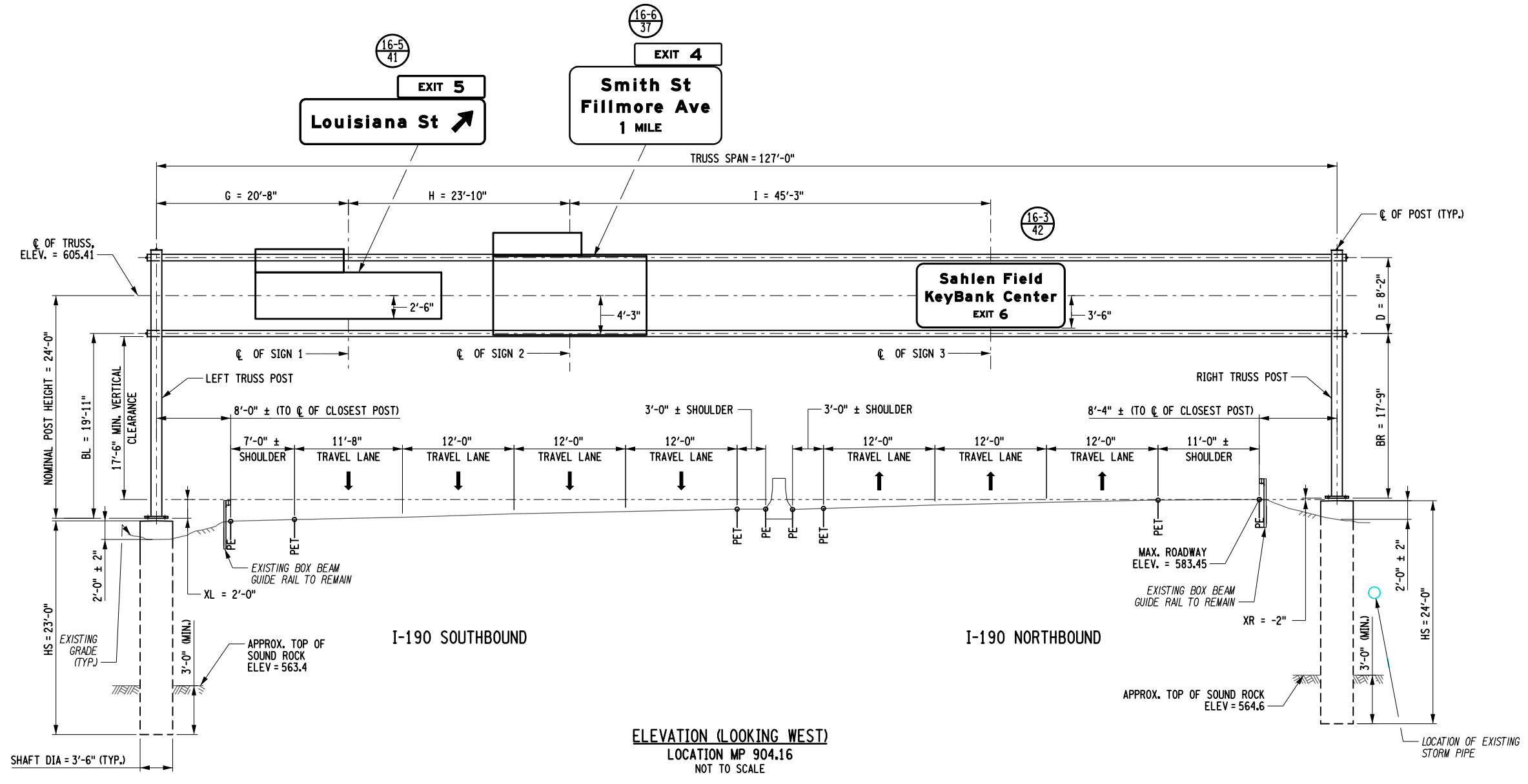
CHECKED BY: Q. WANG

DRAFTED BY: S. LOFORTE

CHECKED BY: Q. WANG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



ALTERED ON:  
SIGNATURE:  
STAMP:

AFFIXED ON:  
**05/13/2024**  
SIGNATURE:  
STAMP:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

**Thruway Authority**

**POPPL DESIGN GROUP**

TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2  
TITLE OF DRAWING  
OVERHEAD SIGN STRUCTURES  
ELEVATION  
LOCATION MP 904.16

CONTRACT NUMBER:  
TAN 24-12  
DATE:  
MAY 2024  
DRAWING NUMBER:  
OSP-24

- NOTES:**
1. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS PRIOR TO THE FABRICATION OF THE SIGN STRUCTURE.
  2. SPAN TRUSS DIAGONALS, CROSS BRACING AND SPLICES NOT SHOWN FOR CLARITY.

CHECKED BY: Q. WANG

DRAFTED BY: S. LOFORTE

CHECKED BY: Q. WANG

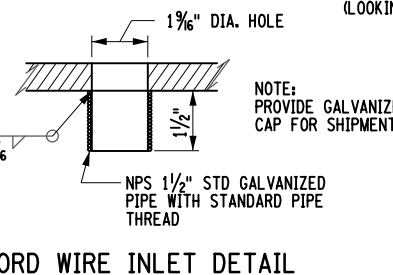
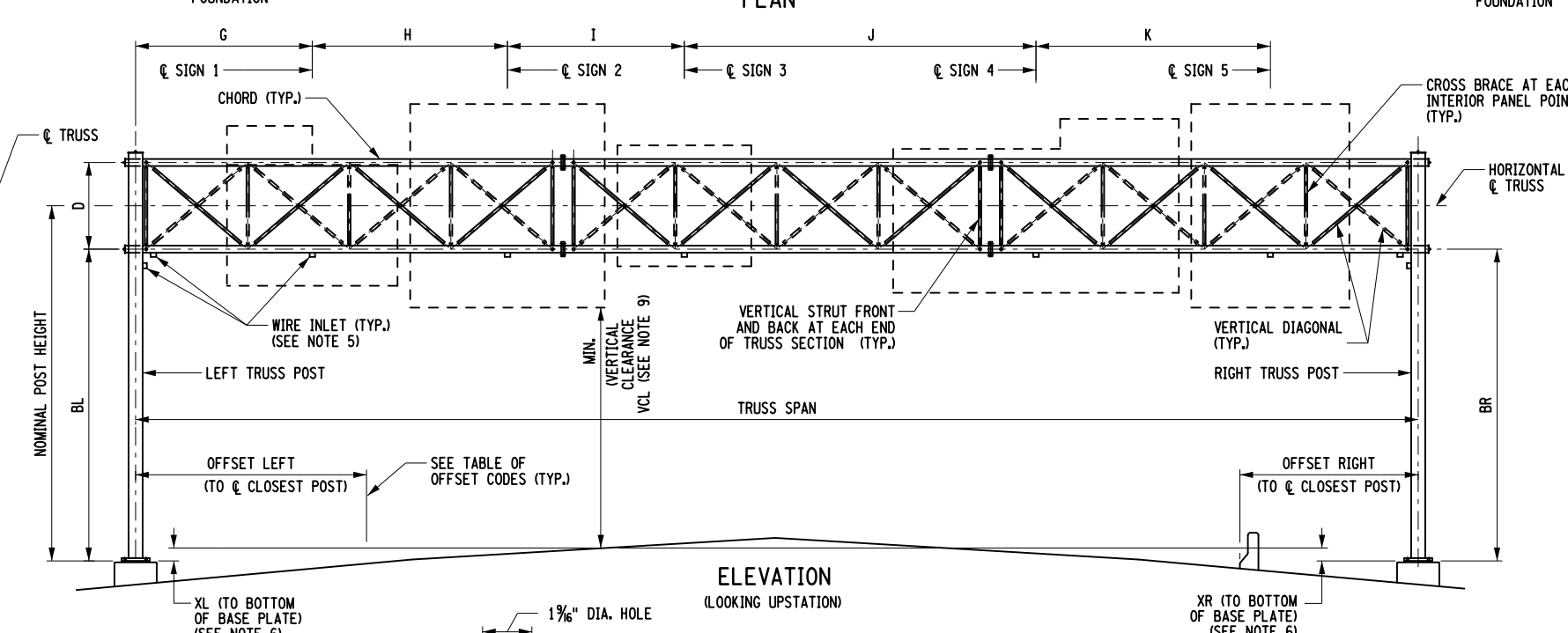
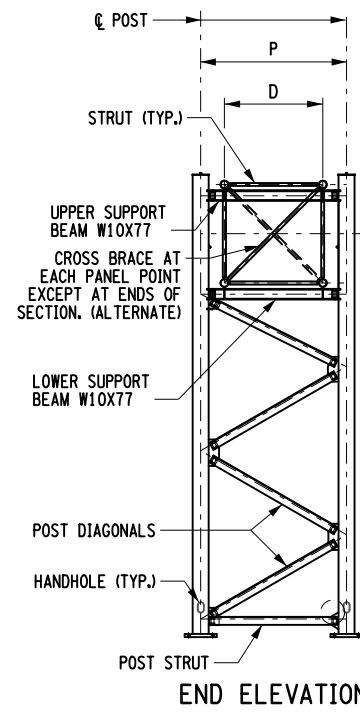
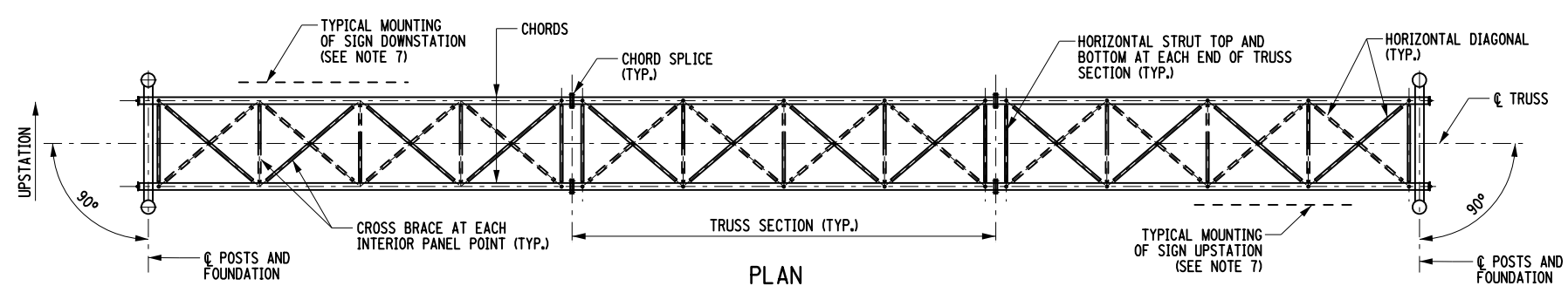
DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD

S.I.N.	ITEM NO.	LOC. NO. (MP)	STRUCTURE TYPE (NOTE 12)	TRUSS SPAN (ft.)	TOTAL SIGN AREA (ft.) <sup>2</sup>	NOMINAL POST HEIGHT (ft.) (NOTE 8)	MIN. VCL (ft.) (NOTE 9)	SPANS														SIGNS										CONSTRUCTED FOUNDATION TYPE (SEE NOTE 13)								
								LEFT						RIGHT						SIGN 1		SIGN 2		SIGN 3		SIGN 4		SIGN 5		LEFT	RIGHT									
								SUBSTRATE SOIL/ROCK (NOTE 10)	BL (ft.)	XL (ft.)	OFF-SET (ft.)	OFF-SET CODE	FOUND. TYPE (NOTE 11)	SUBSTRATE SOIL/ROCK (NOTE 10)	BR (ft.)	XR (ft.)	OFF-SET (ft.)	OFF-SET CODE	FOUND. TYPE (NOTE 11)	TEXT NO.	MIN NO. Z BARS (NOTE 4) (TYP.)	G (ft.)	US OR DS (NOTE 7) (TYP.)	TEXT NO.	MIN NO. Z BARS	H (ft.)	US OR DS	TEXT NO.	MIN NO. Z BARS			I (ft.)	US OR DS	TEXT NO.	MIN NO. Z BARS	J (ft.)	US OR DS	TEXT NO.	MIN NO. Z BARS	K (ft.)
S 900.38	644.432590	900.38	S30-90-9	70'-0"	711.00	25'-6"	17'-6"	ROCK	22'-3"	-1'-4 3/4"	10'-8"	FG	SHAFT	ROCK	20'-1"	-3'-7"	3'-4"	FB	SHAFT	49	7	42'-6"	DS																	
S 900.95	644.435090	900.95	S45-90-9	138'-0"	812.50	26'-4"	17'-6"	ROCK	22'-3"	1'-3 3/4"	14'-8"	FG	SHAFT	ROCK	21'-7"	7 1/2"	8'-5"	FG	SHAFT	29	3	27'-6"	DS	50	5	24'-3"	DS	32	4	49'-7"	US									
S 901.41	644.436090	901.41	S55-60-9	166'-6"	595.25	24'-4"	17'-6"	ROCK	20'-3"	2'-1"	18'-10"	FG	SHAFT	ROCK	18'-4"	2"	10'-0"	FG	SHAFT	33	4	39'-0"	DS	30	3	28'-7"	DS	33	4	74'-2"	US									
S 902.33	644.435060	902.33	S45-60-9	136'-6"	404.50	28'-1"	17'-6"	SOIL	22'-2"	2'-9"	6'-9"	FG	SHAFT	SOIL	24'-0"	4'-7"	6'-9"	FG	SHAFT	36	4	26'-6"	DS	31	4	25'-8"	DS													
S 903.13	644.435060	903.13	S45-60-9	142'-0"	476.75	23'-9"	17'-6"	ROCK	17'-8"	-2 1/4"	7'-11"	FG	SHAFT	ROCK	19'-8"	1'-10"	10'-0"	FG	SHAFT	39	4	24'-11"	DS	34	3	25'-2"	DS	45	4	45'-3"	US									
S 904.16	644.434560	904.16	S40-60-9	127'-0"	399.75	24'-0"	17'-6"	ROCK	19'-11"	2'-0"	8'-0"	FG	SHAFT	ROCK	17'-9"	-2"	8'-4"	FG	SHAFT	41	4	20'-8"	DS	37	3	23'-10"	DS	42	4	45'-3"	US									

S.I.N.	REMARKS
S 900.38	CUSTOM FOUNDATION. SEE TABLE ON OSD-03. NO OPTION ALLOWED AT FOUNDATIONS.
S 900.95	CUSTOM FOUNDATION. SEE TABLE ON OSD-03. NO OPTION ALLOWED AT FOUNDATIONS.
S 901.41	CUSTOM FOUNDATION. SEE TABLE ON OSD-03. NO OPTION ALLOWED AT FOUNDATIONS.
S 902.33	STANDARD FOUNDATION.
S 903.13	CUSTOM FOUNDATION. SEE TABLE ON OSD-03. NO OPTION ALLOWED AT FOUNDATIONS.
S 904.16	CUSTOM FOUNDATION. SEE TABLE ON OSD-03. NO OPTION ALLOWED AT FOUNDATIONS.

- NOTES:
- THESE STRUCTURES ARE DESIGNED IN ACCORDANCE WITH NYS DOT STANDARD DESIGN SPECIFICATIONS FOR OVERHEAD SIGN STRUCTURES. ALL CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATION SECTION 644. STEEL FABRICATION SHALL BE IN ACCORDANCE WITH THE NYS STEEL CONSTRUCTION MANUAL.
  - THE WEB MEMBERS IN EACH FACE OF THE TRUSS SHALL FORM CONTINUOUS TRUSSING FROM POST TO POST AND THERE SHALL BE AN EVEN NUMBER OF PANELS IN THE SPAN.
  - FOR VERTICAL ORIENTATION OF SIGN PANELS, SEE DWG. NO. OSD-09.
  - FOR SIGN PANEL ATTACHMENT DETAILS AND SPACING OF Z-BARS, SEE DWG. NO. OSD-09.
  - INSTALL WIRE INLETS IN BOTTOM CHORD (FRONT AND/OR BACK, AS NECESSARY) AT @ OF EACH SIGN AND AT FACE OF POST. IF SIGN LIGHTING IS NOT INCLUDED IN THE CONTRACT, WIRE INLETS SHALL BE OMITTED.
  - THE CONTRACTOR SHALL ESTABLISH THE TOP OF PEDESTAL OR SHAFT ELEVATIONS BASED ON THE INFORMATION SHOWN ON THE CONTRACT PLANS AND DATA DERIVED FROM FIELD SURVEYS. SPAN LENGTHS AND DIMENSIONS XL AND XR SHALL BE FIELD-VERIFIED PRIOR TO THE PLACEMENT OF THE FOUNDATIONS OR FABRICATION OF THE SIGN STRUCTURES AND ELEVATIONS AND DIMENSIONS SHALL BE PRESENTED ON THE SHOP DRAWINGS. A NEGATIVE XL OR XR INDICATES THAT THE BOTTOM OF BASE PLATE IS ABOVE THE POINT OF MINIMUM VERTICAL CLEARANCE. IF FIELD-VERIFIED SPAN LENGTHS, XL OR XR DIFFER FROM INFORMATION SHOWN ON THE CONTRACT PLANS, THE ENGINEER SHALL BE CONSULTED TO DETERMINE WHETHER DESIGN CHANGES ARE NEEDED.
  - SIGN MOUNTING LOCATION IS INDICATED BY US (UPSTATION) OR DS (DOWNSTATION) IN THE SPAN STRUCTURE TABLE (SEE SCHEMATIC IN PLAN VIEW).
  - NOMINAL POST HEIGHT IS THE LARGER OF LEFT OR RIGHT POSTS.
  - THE SIGN STRUCTURE HEIGHT IS DESIGNED TO PROVIDE THE MINIMUM VERTICAL CLEARANCE (VCL) INDICATED BETWEEN THE SIGN PANELS AND ROADWAY SURFACE, INCLUDING LIGHTING OR OTHER ATTACHMENTS, AT ANY POINT OVER THE ENTIRE ROADWAY WIDTH. THE REQUIRED MINIMUM VCL IS 1'-0" GREATER THAN THAT REQUIRED FOR NEW OVERPASS STRUCTURES ALONG THE ROUTE.
  - SUBSTRATE TYPE SHALL BE DESIGNATED S (SOIL), R (ROCK), OR '-' (SPECIAL FOUNDATION). FOR SHAFT FOUNDATIONS, WHEN ROCK IS SPECIFIED, THE TOTAL SHAFT LENGTH AND THE SHAFT LENGTH IN ROCK SHALL BE INDICATED IN PARENTHESIS (SEE DETAIL ON DWG. NO. OSD-03). FOR FOOTING DEPTH IN SOIL OR ROCK SEE FOUNDATION TABLE ON DWG. NO. OSD-03.
  - FOUNDATION TYPE SHALL BE DESIGNATED 'FOOTING', 'SHAFT', OR 'SPECIAL'.
  - FOR STANDARD SPAN STRUCTURES SELECTION TABLE AND DIMENSIONS D AND P, SEE DWG. NO. OSD-02.
  - FILLED OUT BY E.I.C. AT TIME OF AS-BUILTS.



OFFSET CODES			
EP	EDGE OF PAVEMENT	FB	TRAFFIC FACE OF BARRIER BASE
ES	EDGE OF SHOULDER	FG	FRONT FACE OF GUIDE RAIL
CF	CURB FACE	CL	HIGHWAY CENTER LINE

ALTERED ON:

AFFIXED ON: **05/13/2024**

SIGNATURE:

STAMP:

SIGNATURE:

STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

**NEW YORK STATE OF OPPORTUNITY** | **Thruway Authority**

**PDG**  
POPLI DESIGN GROUP

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
CONTRACT NUMBER: TAN 24-12

LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
DATE: MAY 2024

TITLE OF DRAWING: OVERHEAD SIGN STRUCTURES SPAN STRUCTURES  
DRAWING NUMBER: OSD-01

### STANDARD SPAN STRUCTURES

Table with columns for Structure Type, Max. Span Length, Max. Sign Area, Chord Spacing, Post Spacing, Truss (Chord NPS, Diagonal NPS, Truss Weight), Post (Max. Nominal Post Height, Post NPS, Wall Thickness, Diagonal Angle, End Frames Weight), Base Plate Type, Found. Type, Minimum Pedestal or Shaft Dia., Shaft Length HS, Shaft Conc. Qty. Includes rows for structures like S15-30-9, S15-30-11, S15-30-13, etc.

### STANDARD SPAN STRUCTURES

Table with columns for Structure Type, Max. Span Length, Max. Sign Area, Chord Spacing, Post Spacing, Truss (Chord NPS, Diagonal NPS, Truss Weight), Post (Max. Nominal Post Height, Post NPS, Wall Thickness, Diagonal Angle, End Frames Weight), Base Plate Type, Found. Type, Minimum Pedestal or Shaft Dia., Shaft Length HS, Shaft Conc. Qty. Includes rows for structures like S45-30-9, S45-30-11, S45-30-13, etc.

INDICATES INFORMATION NOT RELEVANT TO PROPOSED WORK

NOTES:

- 1. STRUCTURE TYPE EXAMPLE: S15-30-9. Includes a diagram showing Span, Max. Span Length, Max. Nominal Post Height, and Max. Sign Area.
- 2. FOR SHAFT AND RECTANGULAR FOOTING DETAILS, SEE DWG. NO. OSD-03.
- 3. SHAFT LENGTHS AND CONCRETE QUANTITIES ARE FOR SHAFTS IN S (SOIL) SUBSTRATES AS INDICATED IN THE SUBSTRATE COLUMNS ON DWG. NO. OSD-01. FOR SHAFTS IN R (ROCK) SUBSTRATES OR SPECIAL FOUNDATIONS, ADJUST QUANTITIES AS NECESSARY FOR BID PURPOSES IN ACCORDANCE WITH DWG. NO. OSD-03 OR SPECIAL FOUNDATION DRAWINGS.
- 4. SIGN HEIGHT, AS SHOWN ON DWG. NO. OSD-09, SHALL BE A MAXIMUM OF 19'-6".
- 5. SHAFT CONCRETE QUANTITY SHOWN IS FOR A SINGLE SHAFT. MULTIPLY BY THE NUMBER OF SHAFTS REQUIRED (TWO PER FOUNDATION) FOR BID QUANTITY PURPOSES.
- 6. TRUSS WEIGHTS SHOWN ARE BASED ON MAXIMUM SPAN LENGTH AND REFLECT THE OPTIONAL DOUBLE ANGLE SUBSTITUTION FOR HORIZONTAL DIAGONALS, STRUTS, AND CROSS BRACES AS SHOWN IN THE TABLE ON DWG. NO. OSD-07. TRUSS WEIGHTS DO NOT INCLUDE SIGN WEIGHT.
- 7. END FRAME WEIGHTS SHOWN INCLUDE BOTH END FRAMES AND ARE BASED ON MAXIMUM NOMINAL POST HEIGHT AND SINGLE ANGLE DIAGONALS.
- 8. PIPE SIZES SHALL BE BASED ON DIMENSIONS FROM ASTM A53.
- 9. FABRICATORS SHALL PLACE ALL SHIPPING WEIGHTS ON SHOP DRAWINGS.
- 10. THESE TABLES ARE FOR USE WITH 1/8" THICK ALUMINUM FLAT PANEL SIGNS ONLY.

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ALTERED ON: 05/13/2024  
AFFIXED ON:  
SIGNATURE: M. BEDFORD  
STAMP: [Professional Engineer Stamp for Matthew Clarence Bedford, No. 103716, State of New York]

REVISIONS table with columns: DATE, DESCRIPTION, BY, SYM.



TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
TITLE OF DRAWING: OVERHEAD SIGN STRUCTURES SPAN STRUCTURES SELECTION TABLE  
CONTRACT NUMBER: TAN 24-12  
DATE: MAY 2024  
DRAWING NUMBER: OSD-02

DESIGNED BY: S. LOFORTE  
CHECKED BY: M. OZOG  
DRAFTED BY: S. LOFORTE  
CHECKED BY: M. OZOG  
DESIGN SUPERVISOR: M. BEDFORD

CHECKED BY: Q. WANG

DRAFTED BY: S. LOFORTE

CHECKED BY: Q. WANG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD

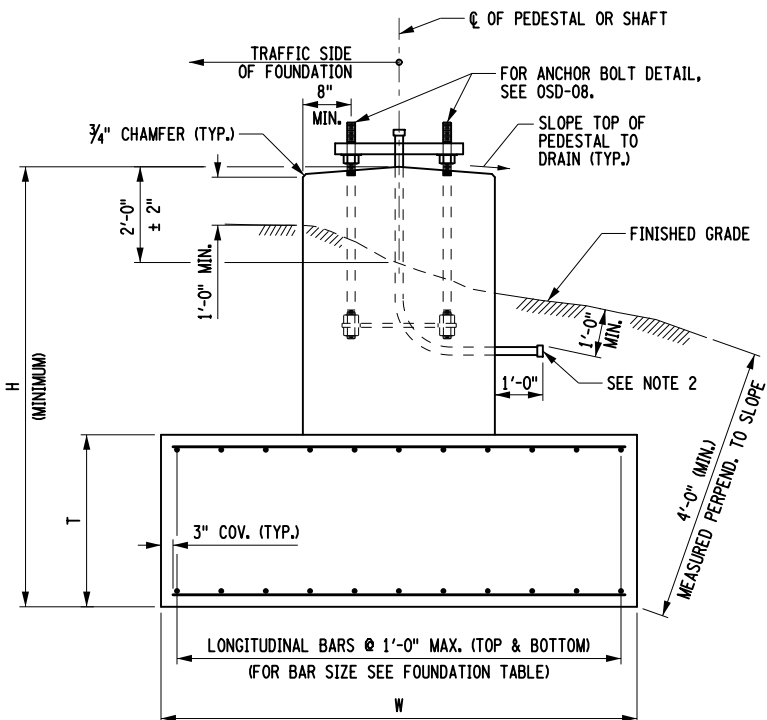
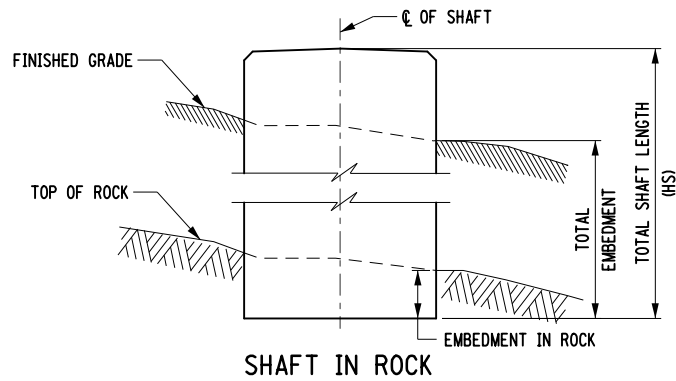
FOUNDATION TABLE

Table with columns for TYPE, SIZE (L, W, T), H, \* TOTAL CONCRETE QUANTITY (ft.³), LONG. REINF. T & B SIZE, TRANS. REINF. T & B SIZE, PEDESTAL/SHAFT VERTICAL REINFORCEMENT (NO. OF BARS, SIZE).

\* NOTE: TOTAL CONCRETE QUANTITY INCLUDES FOOTING AND PEDESTAL CONCRETE.

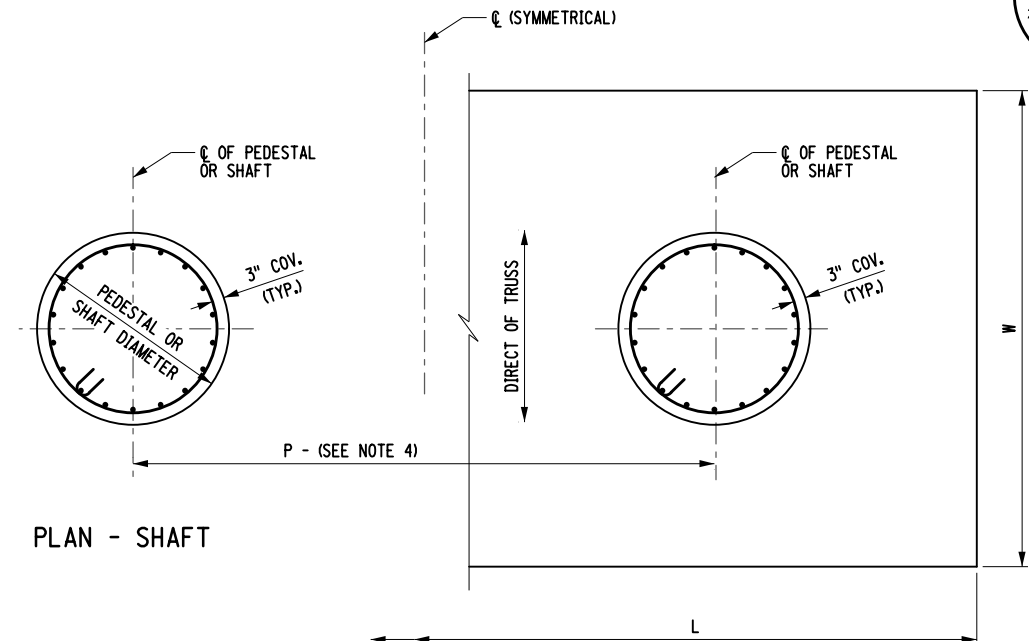
NOTES:

- 1. FOR DETAILS OF ANCHOR BOLTS AND TEMPLATES SEE DWG. NO. OSD-08.
2. INSTALL ONE NPS 2 STD GALVANIZED CONDUIT IN EACH FOUNDATION AT PEDESTAL OR SHAFT AT FACE OF STRUCTURE WHERE SIGNS ARE MOUNTED. OMIT IF SIGN LIGHTING IS NOT IN THE CONTRACT.
3. FOR FOUNDATION TYPE, SEE DWG. NO. OSD-01.
4. FOR POST SPACING (P), PEDESTAL AND SHAFT DIAMETER, SHAFT LENGTH (HS) AND CONCRETE QUANTITY WHEN IN SUBSTRATE TYPE S (SOIL), SEE DWG. NO. OSD-02.
5. WHERE A FOOTING IS TO BE PLACED ON ROCK, REMOVE ALL DISINTEGRATED OR SHATTERED MATERIAL TO LINES AND LEVELS ORDERED BY THE ENGINEER. IF TOP OF SOUND ROCK IS BELOW PLANNED ELEVATION OF BOTTOM OF FOOTING, BACKFILL WITH CLASS A CONCRETE TO THE PLANNED ELEVATION, OR PROCEED AS ORDERED BY THE ENGINEER. IF TOP OF SOUND ROCK IS ABOVE PLANNED ELEVATION OF BOTTOM OF FOOTING, SEE NOTE 10 AND PROCEED AS ORDERED BY THE ENGINEER. ROCK REMOVED BELOW THE ELEVATION DIRECTED BY THE ENGINEER AND BEYOND THE FACE OF THE FINISHED FOOTING, SHALL BE REPLACED BY BACKFILL OF CLASS A CONCRETE FOR WHICH NO PAYMENT WILL BE MADE.
6. THE GEOTECHNICAL ENGINEERING BUREAU SHALL BE CONSULTED UNDER THE FOLLOWING CIRCUMSTANCES:
A. FOOTING OR SHAFT IS PLACED IN SOFT CLAY OR ORGANIC DEPOSITS.
B. GROUNDWATER ELEVATION IS ABOVE BOTTOM OF FOOTING OR SHAFT.
C. UNANTICIPATED ROCK IS ENCOUNTERED WITHIN THE DEPTH OF THE SHAFT.
D. DEPTH TO SOUND ROCK FOR SHAFTS DIFFERS FROM THAT INDICATED ON DWG. NO. OSD-01.
E. SLOPE OF FINISHED GRADE EXCEEDS 1 ON 2.
7. ANY DRILLING OF ROCK FOR SHAFT FOUNDATIONS SHALL BE PERFORMED FROM THE EXISTING EARTH SURFACE THROUGH THE AUGER HOLE FORMED DURING THE DRILLING OPERATION. CASING, IF USED, SHALL BE REMOVED PRIOR TO PLACING SHAFT CONCRETE.
8. HEIGHT OF PEDESTAL OR LENGTH OF SHAFT MAY BE INCREASED AS ORDERED BY THE ENGINEER AS MUCH AS 1'-8" IF NECESSARY TO REACH GOOD FOUNDATION MATERIAL.
9. THE MINIMUM SHAFT DIAMETER SHALL BE BOLT CIRCLE DIAMETER + 8"
10. EXCAVATION, BACKFILL, FORM WORK, REINFORCEMENT, CONDUIT STUB-OUT AND ALL OTHER LABOR AND MATERIALS NECESSARY TO CONSTRUCT THE FOUNDATIONS SHALL BE INCLUDED IN THE PRICE BID FOR THE FOUNDATION CONCRETE.
11. THE FOOTINGS SHOWN HAVE BEEN DESIGNED USING THE SOIL OVERBURDEN TO RESIST OVERTURNING. IF IT IS DETERMINED THAT THE FOOTING DEPTH NEEDS TO BE REDUCED, CONTACT THE GEOTECHNICAL ENGINEERING BUREAU.
12. (G) DENOTES HOT-DIP GALVANIZED REINFORCEMENT.
13. SIGN STRUCTURE FOUNDATIONS WERE DESIGNED USING A CONCRETE MINIMUM COMPRESSIVE STRENGTH OF 3000 PSI AT 28 DAYS.

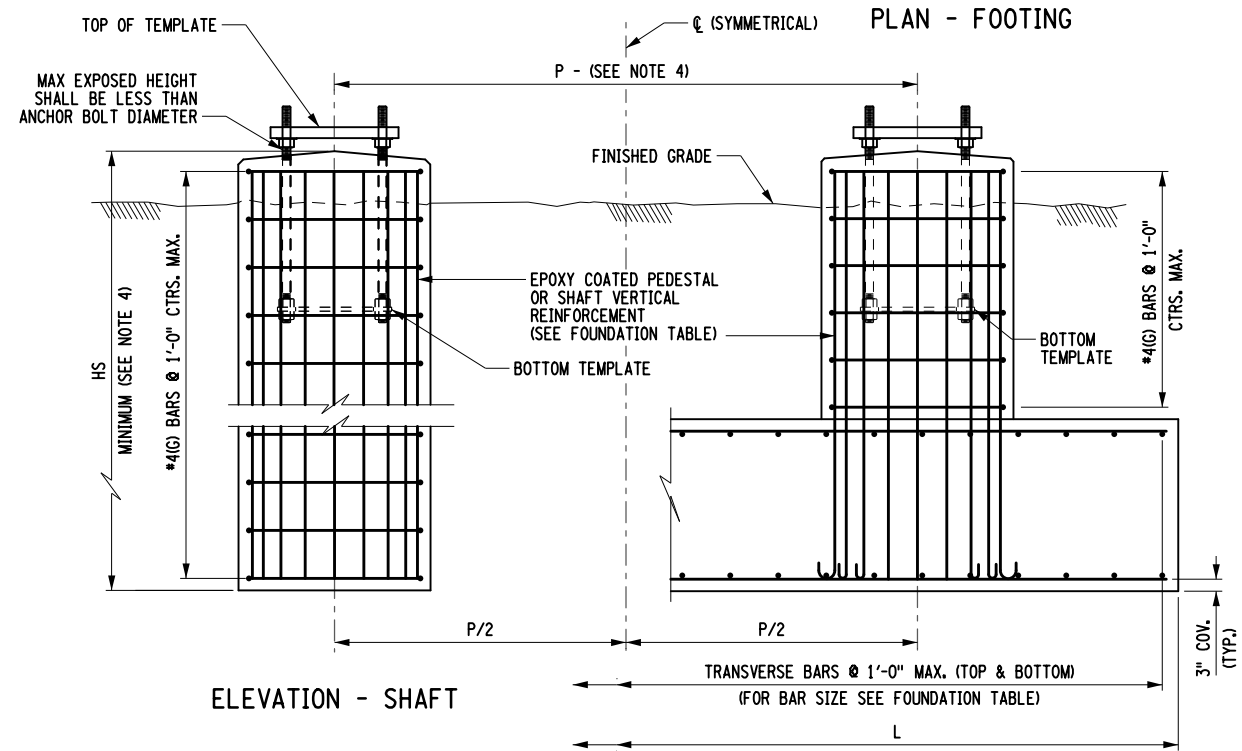


END ELEVATION (VERTICAL REINFORCEMENT NOT SHOWN) (FOOTING SHOWN, SHAFT SIMILAR TO PEDESTAL PORTION OF DETAIL)

FOUNDATION ITEM NUMBERS table with columns for FOUNDATION TYPE (SHAFT, FOOTING) and ITEM NUMBER (644.20, 644.30).



PLAN - SHAFT



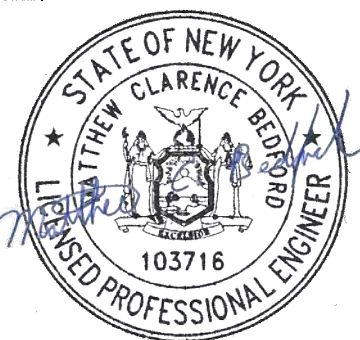
ELEVATION - SHAFT

SIDE ELEVATION - FOOTING

CUSTOM FOUNDATION PROPERTIES table with columns for S.I.N., SHAFT PROPERTIES (DIAMETER, LENGTH, CONC. QTY), SHAFT VERT. REIN., SHAFT TRANS. REINFORCEMENT, and RIGHT POST properties.

NOTE: DIMENSIONS SHOWN IN PARENTHESIS APPLY TO ROCK SOCKET WHERE NECESSARY

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REVISIONS table with columns for DATE, DESCRIPTION, BY, and SYM.



Project information table including TITLE OF PROJECT (I-190 HIGHWAY REHABILITATION), LOCATION OF PROJECT (I-190 NIAGARA THRUWAY), TITLE OF DRAWING (OVERHEAD SIGN STRUCTURES SPAN STRUCTURES FOUNDATION DETAILS), CONTRACT NUMBER (TAN 24-12), DATE (MAY 2024), and DRAWING NUMBER (OSD-03).

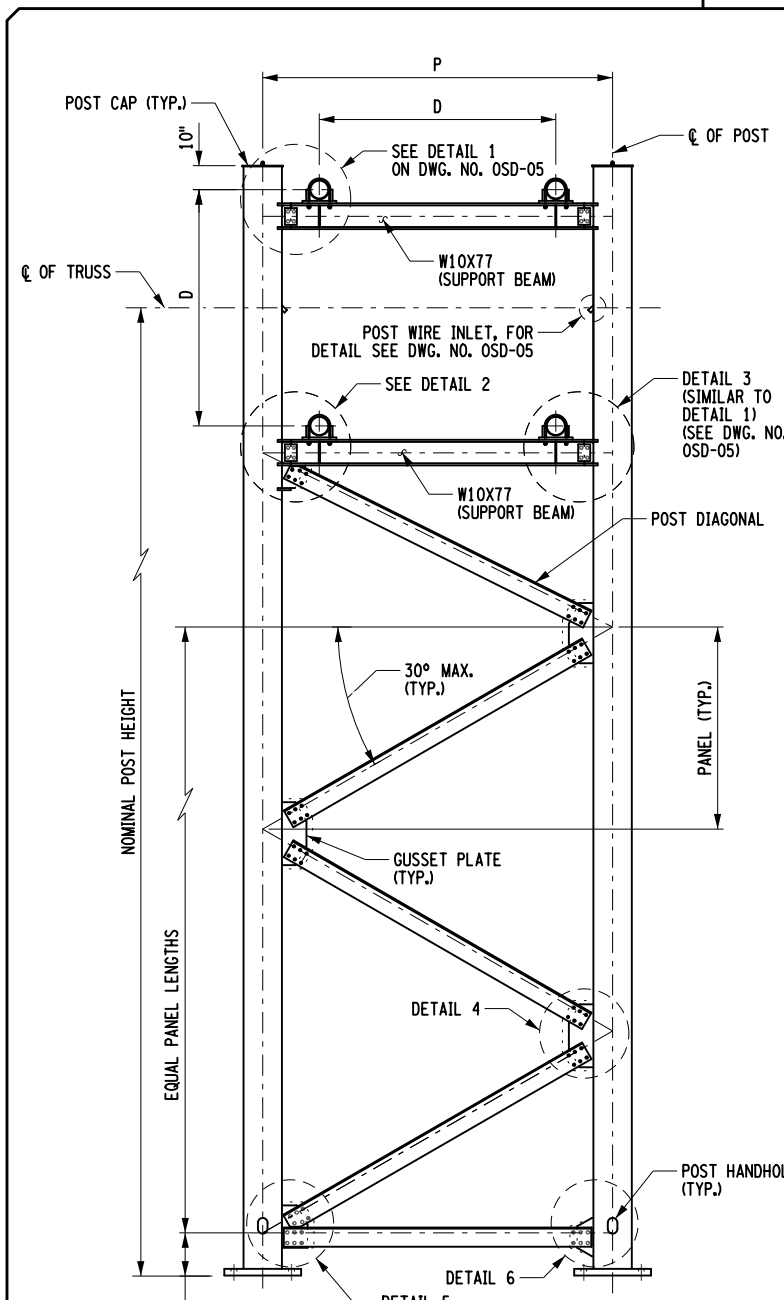
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DRAFTED BY: S. LOFORTE

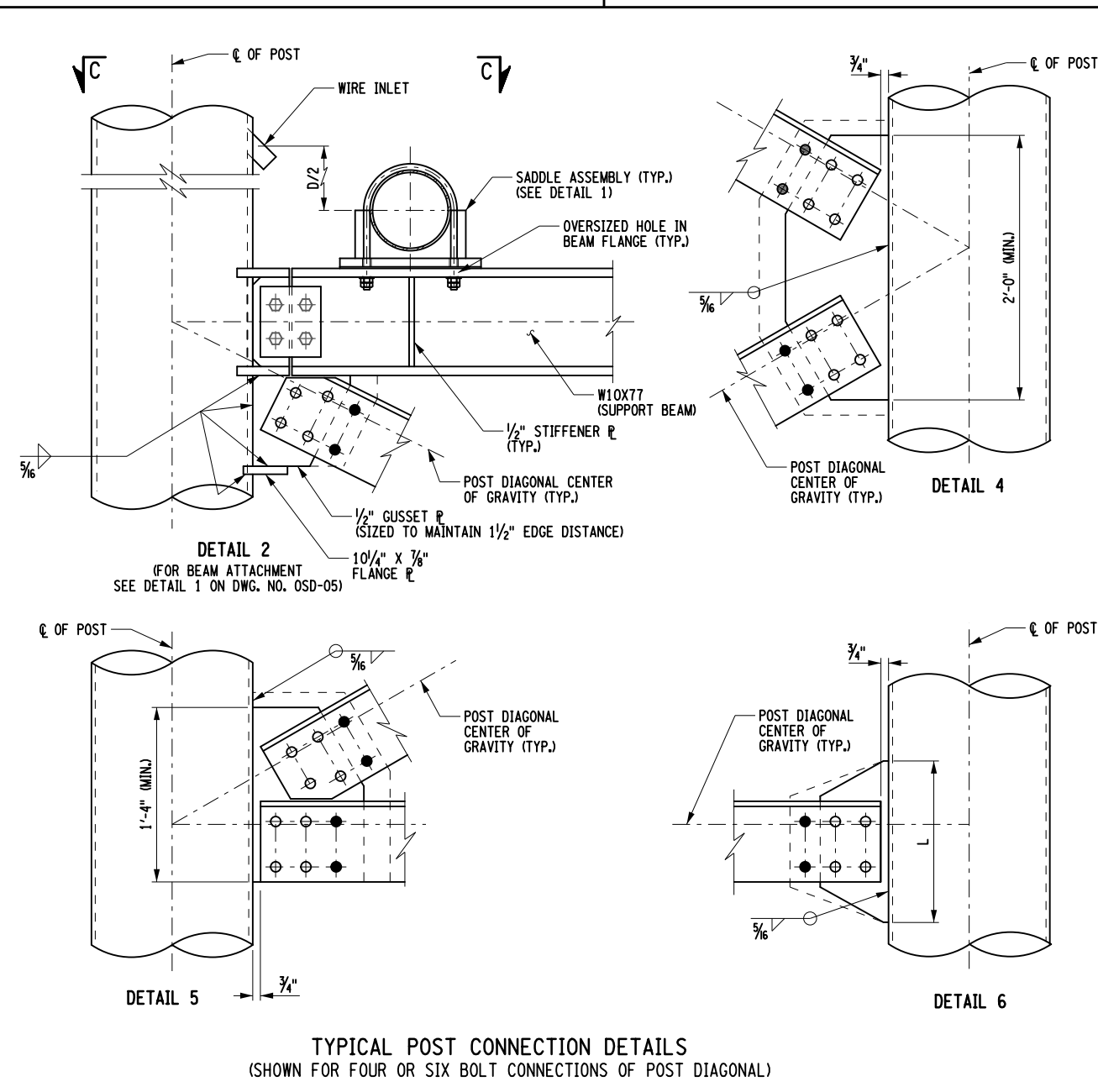
CHECKED BY: M. BEDFORD

DESIGNED BY: S. LOFORTE

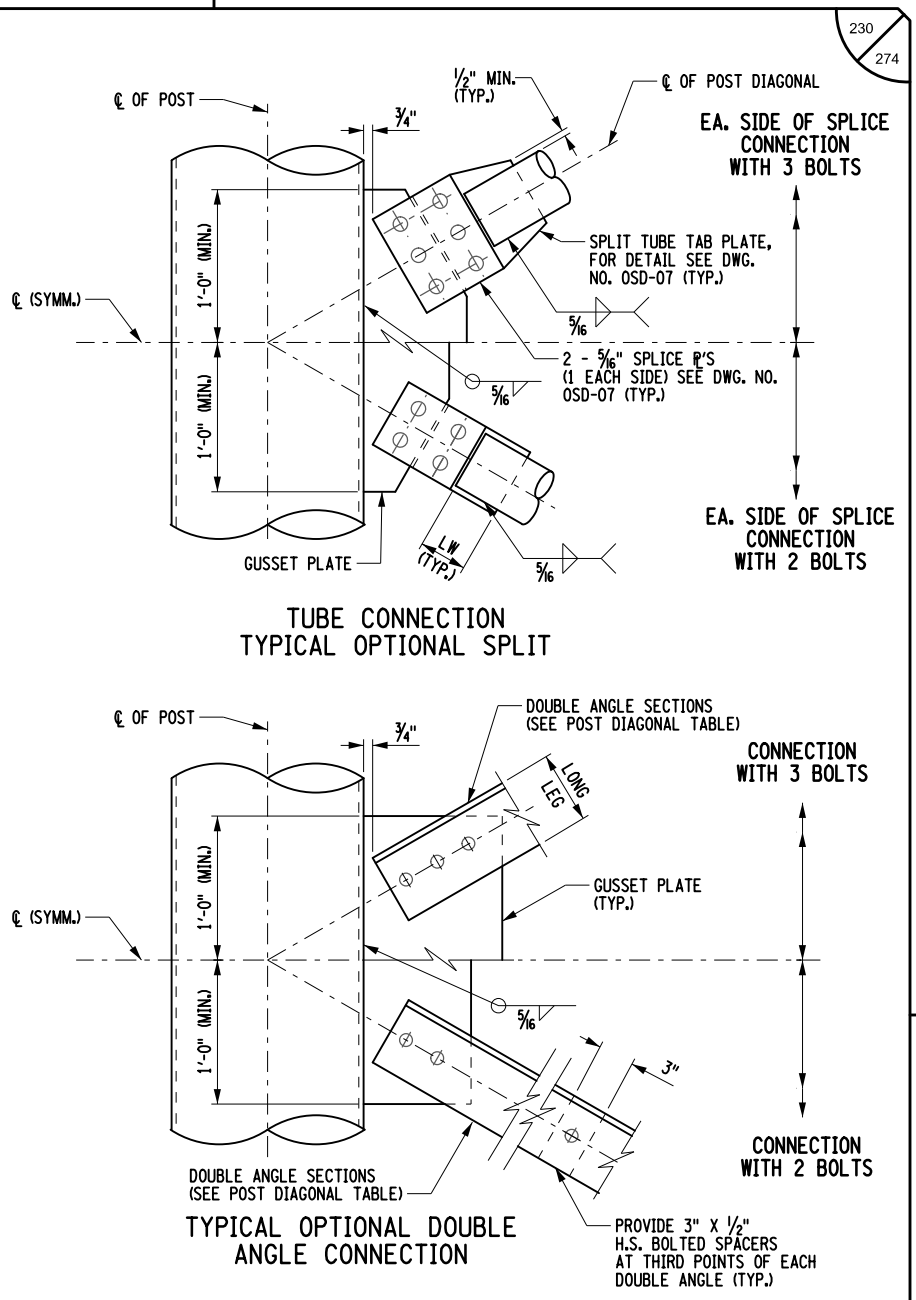
DESIGN SUPERVISOR: M. BEDFORD



POST ELEVATION



TYPICAL POST CONNECTION DETAILS  
(SHOWN FOR FOUR OR SIX BOLT CONNECTIONS OF POST DIAGONAL)



TUBE CONNECTION  
TYPICAL OPTIONAL SPLIT

TYPICAL OPTIONAL DOUBLE  
ANGLE CONNECTION

NOTES:

1. PLACE HANDHOLES OPPOSITE TRAFFIC. FOR DETAIL SEE DWG. NO. OSD-08.
2. FOR POST CAP DETAILS, SEE DWG. NO. OSD-08.
3. FOR DETAILS 1 AND 3 AND SECTION C-C SEE DWG. NO. OSD-05.
4. ALL BOLTS SHALL BE HIGH STRENGTH 7/8" DIA. IN STANDARD HOLES, UNLESS OTHERWISE NOTED.
5. GUSSET AND SPLIT TUBE TAB PLATES SHALL BE 1/2" THICK UNLESS OTHERWISE NOTED.
6. BOLTS DESIGNATED AS ● ARE FOR SIX BOLT CONNECTIONS ONLY.
7. TOP SUPPORT BEAMS SHALL BE INSTALLED AFTER TRUSS HAS BEEN ERECTED AND LEVELED.
8. A POST WIRE INLET SHALL BE INSTALLED ADJACENT TO EACH TRUSS FACE CARRYING A LIGHTED SIGN PANEL. FOR DETAIL, SEE DWG. NO. OSD-05.
9. FOR SPAN SELECTION TABLE AND DIMENSIONS P AND D, SEE DWG. NO. OSD-02.

POST DIAGONAL				
SINGLE ANGLE SIZE	L MIN.	OPTIONAL SPLIT TUBE		OPTIONAL DOUBLE ANGLE
		PIPE NPS	LW MIN.	ANGLE SIZE
L8"x8"x1/2"	1'-1"	4 STD	3"	2-L4"x3"x3/8"
L8"x8"x3/16"	1'-2"	5 STD	4"	2-L6"x4"x3/8"
L8"x8"x5/16"	1'-3"	5 STD	4"	2-L6"x4"x3/8"
L8"x8"x3/4"	1'-4"	5 STD	4"	2-L6"x4"x3/8"

SPAN LENGTH (ft.)	SIGN AREA (ft. <sup>2</sup> )	NUMBER OF BOLTS IN DIAGONAL CONNECTION		
		SINGLE ANGLE	SPLIT TUBE (EACH SIDE OF SPLICE)	DOUBLE ANGLE
≤ 164'	ALL	4	2	2
180'	UP TO 645	4	2	2
ALL OTHERS	ALL	6	3	3

ALTERED ON:  
SIGNATURE:  
STAMP:

AFFIXED ON:  
**05/13/2024**  
STATE OF NEW YORK  
ATTHEW CLARENCE BEDFORD  
LICENSED PROFESSIONAL ENGINEER  
103716

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2  
TITLE OF DRAWING  
OVERHEAD SIGN STRUCTURES  
SPAN STRUCTURES  
POST DETAILS (SHEET 1 OF 2)

CONTRACT NUMBER:  
TAN 24-12  
DATE:  
MAY 2024  
DRAWING NUMBER:  
OSD-04

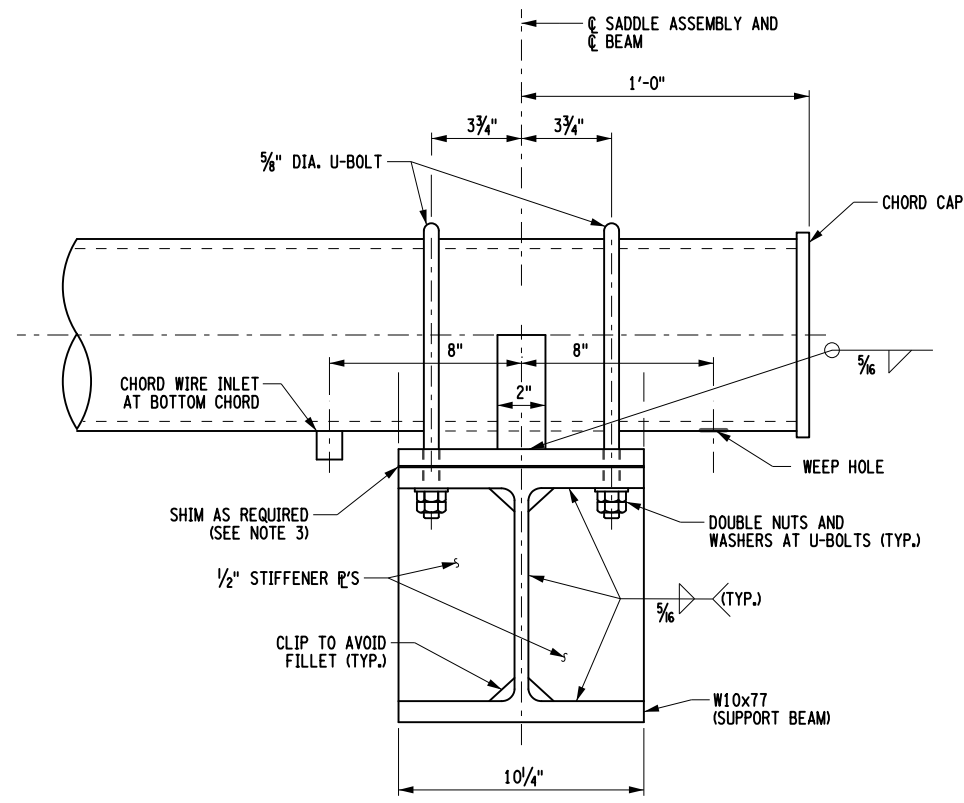
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DRAFTED BY: S. LOFORTE

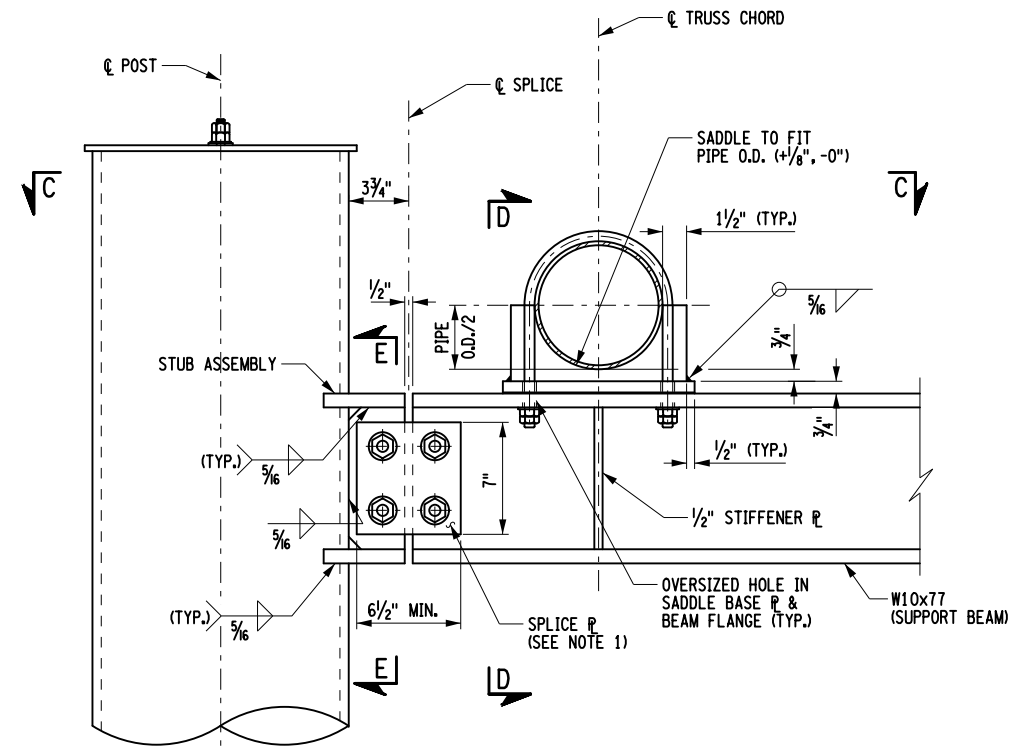
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DESIGNED BY: S. LOFORTE

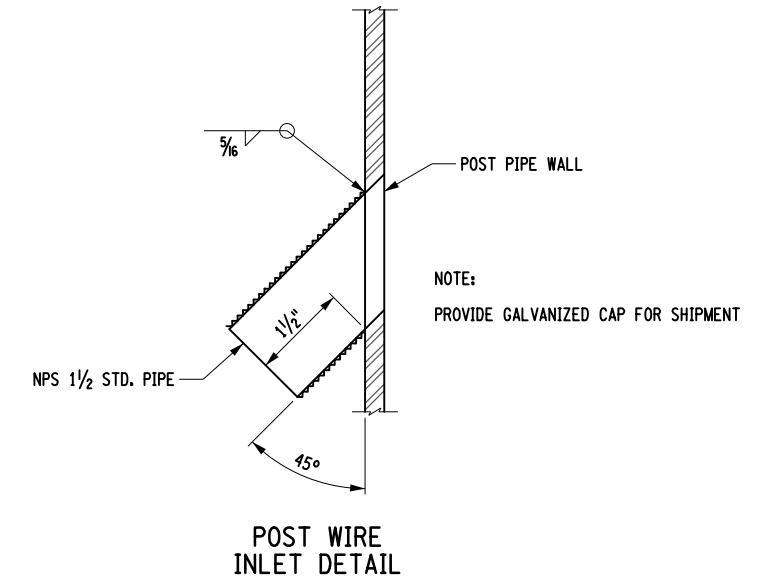
DESIGN SUPERVISOR: M. BEDFORD



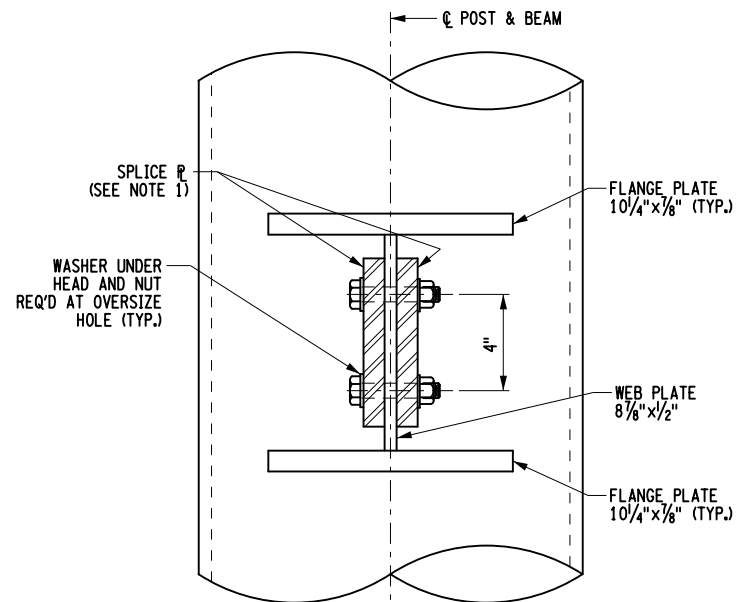
SECTION D-D



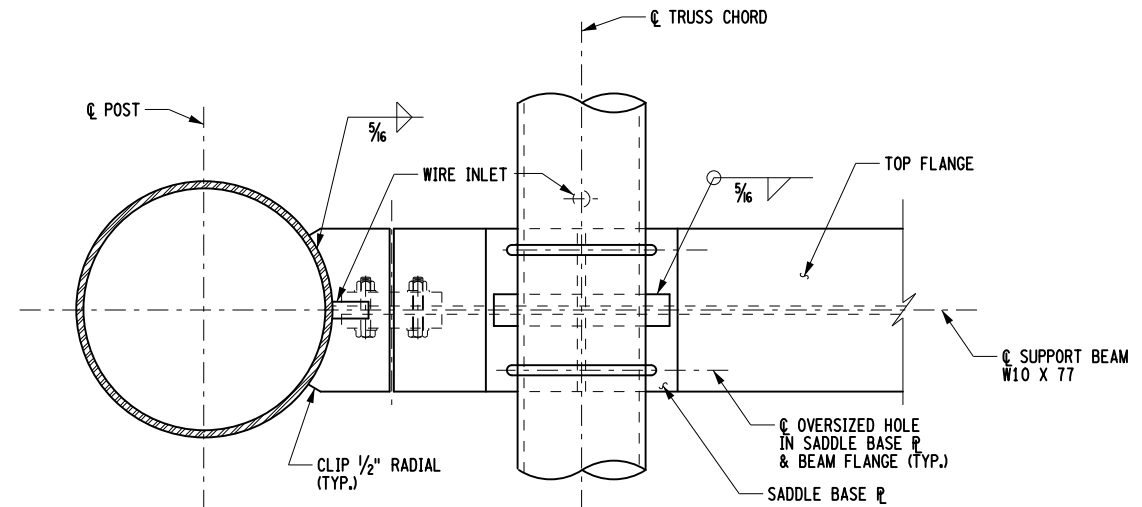
DETAIL 1  
(DETAIL 3 SIMILAR, OPPOSITE HAND)



POST WIRE INLET DETAIL  
NOTE:  
ONLY REQUIRED WHEN LIGHTING ON STRUCTURE.



SECTION E-E



SECTION C-C

NOTE:  
STUB MAY BE FABRICATED FROM W10X77  
OR WELDED ASSEMBLY AS SHOWN.

- NOTES:
1. SPLICE PLATES SHALL BE 5/8" THICK AT TOP BEAM CONNECTIONS, 3/8" THICK AT LOWER BEAM CONNECTIONS. HOLES IN WEB PLATES AT TOP BEAM AND HOLES IN LOWER BEAM CONNECTION SHALL BE STANDARD. HOLES IN TOP BEAM SPLICE PLATES MAY BE OVERSIZE.
  2. FOR LOCATION OF DETAILS 1 AND 3, SEE DWG. NO. OSD-04.
  3. SADDLE ASSEMBLIES SHALL BE SHIMMED AS REQUIRED TO PROVIDE LEVEL SEAT FOR CHORD WHEN ALL DEAD LOADS ARE APPLIED. VARIABLE THICKNESS SHIM PACKS SHALL BE PROVIDED. THE MAXIMUM ACCEPTABLE GAP SHALL BE 1/16". SHIMS SHALL HAVE OVERSIZE HOLES TO ACCEPT 5/8" U-BOLTS. ALL SHIMS SHALL BE GALVANIZED STEEL.
  4. FOR CHORD WIRE INLET DETAIL, SEE DWG. NO. OSD-01.
  5. FOR CHORD CAP DETAILS, SEE DWG. NO. OSD-08.

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES SPAN STRUCTURES POST DETAILS (SHEET 2 OF 2)	DRAWING NUMBER: OSD-05

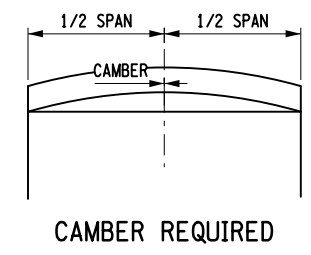
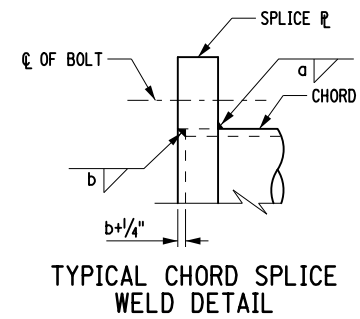
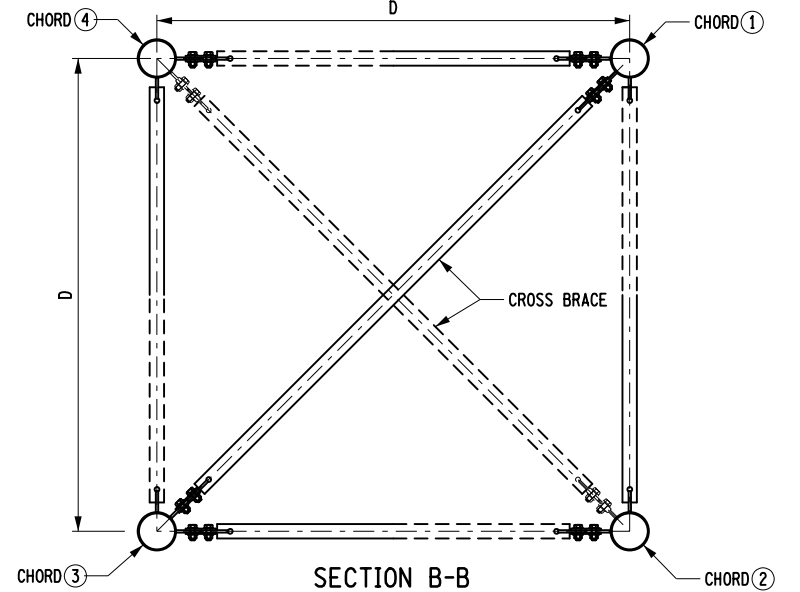
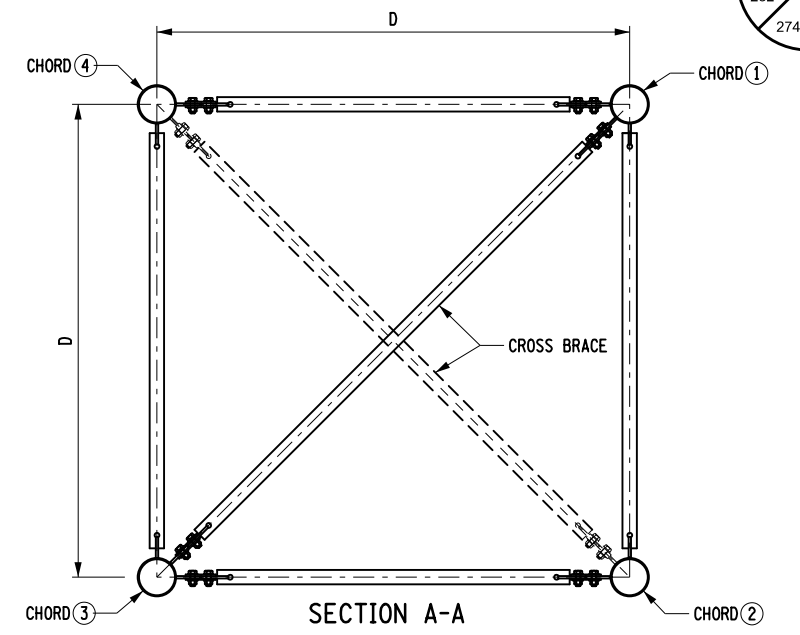
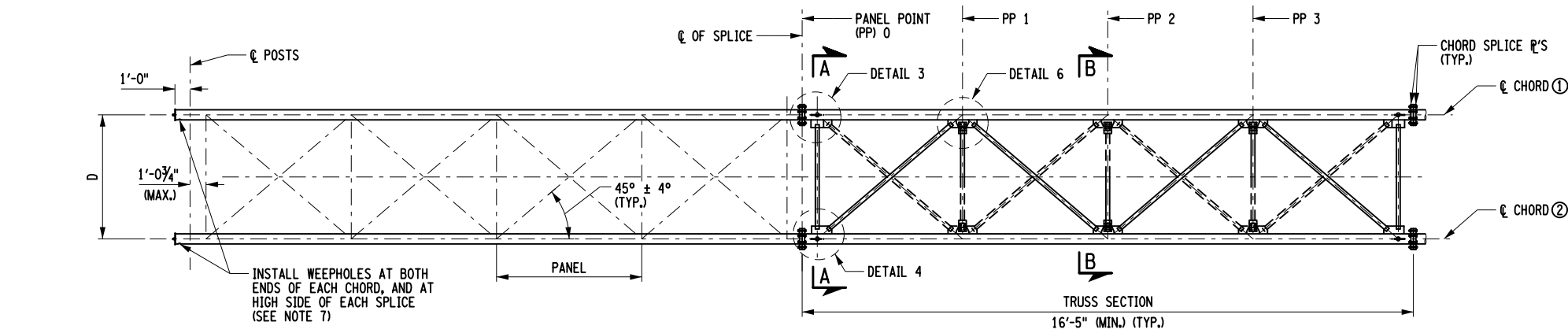
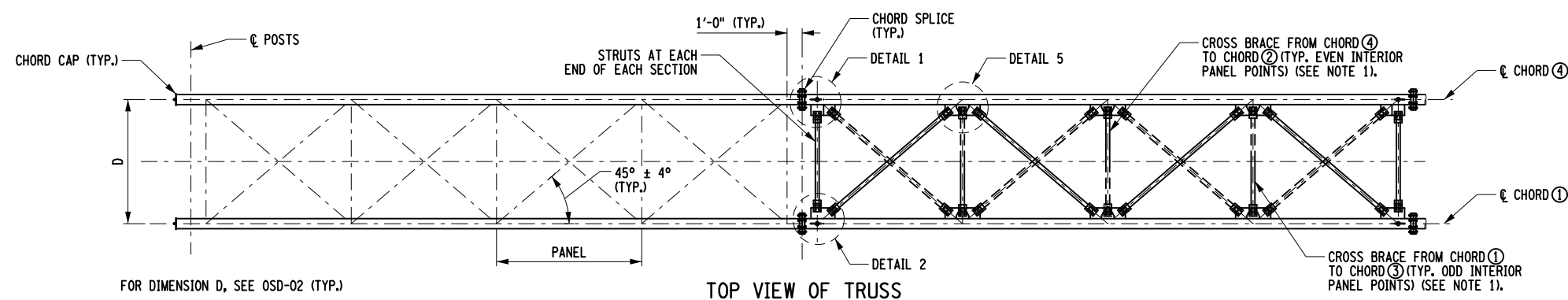
CHECKED BY: M. BEDFORD

DRAFTED BY: S. LOFORTE

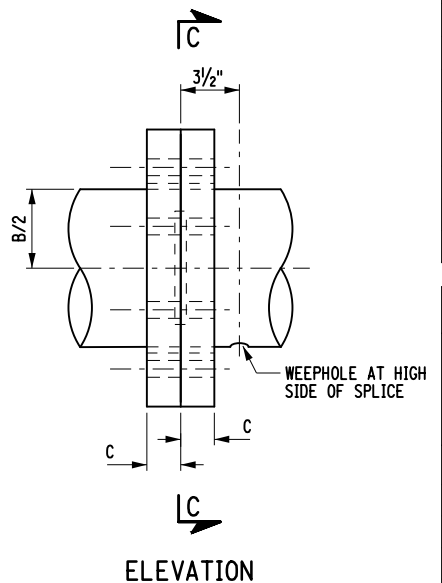
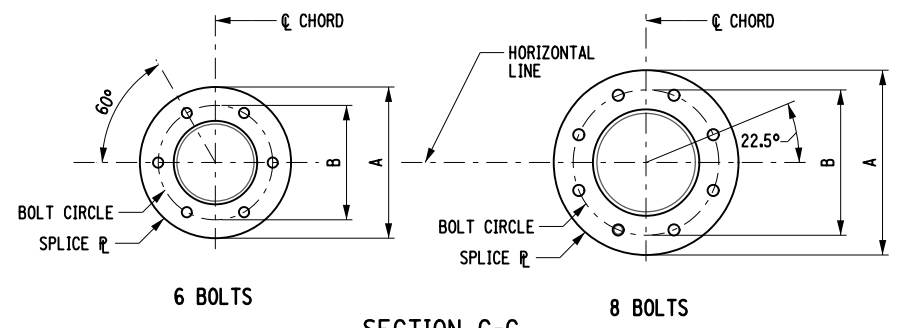
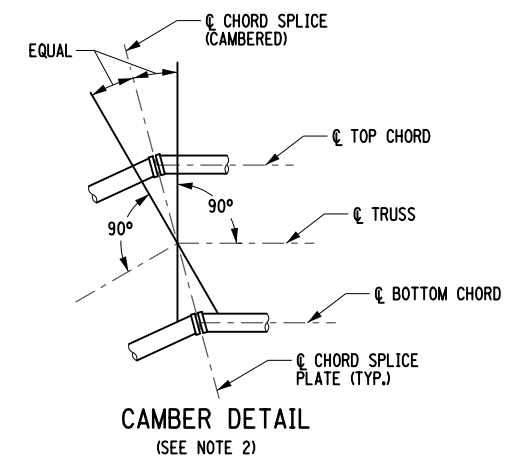
CHECKED BY: M. BEDFORD

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



CAMBER TABLE	
MAX. SPAN	CAMBER
49'-2 1/2"	3/4"
65'-7 1/2"	1 3/16"
82'-0 1/4"	1 1/2"
98'-5 1/8"	2"
114'-10"	2 3/8"
131'-2 3/4"	2 3/4"
147'-7 5/8"	3 1/8"
164'-0"	4"
180'-5 1/16"	4 3/4"
196'-10 1/4"	5 1/2"
213'-3"	7"



CHORD NPS	PLATE			HIGH STRENGTH BOLTS		FILLET WELD	
	A	B	C	NUMBER	DIA.	a	b
5 STD	1'-1"	10"	1 1/2"	6	7/8"	5/16"	1/4"
6 STD	1'-1 1/2"	10 1/4"	1 1/2"	6	7/8"	5/16"	1/4"
8 STD	1'-4 1/2"	1'-1"	2"	8	1"	3/8"	1/4"
10 STD	1'-6"	1'-2 1/2"	2"	8	1"	3/8"	1/4"
12 STD	1'-9 1/4"	1'-5 1/4"	2 3/8"	8	1 1/8"	1/2"	5/16"
12 XS	1'-10"	1'-6"	2 3/8"	8	1 1/4"	1/2"	3/8"

- NOTES:
1. THE CROSS BRACING SHALL BRACE THE TRUSS CHORD MIDWAY BETWEEN HORIZONTAL DIAGONAL CONNECTION POINTS, SO THAT EACH CHORD OF THE TRUSS IS BRACED HORIZONTALLY AT EACH PANEL POINT BY A HORIZONTAL DIAGONAL, A HORIZONTAL STRUT, OR A CROSS BRACE.
  2. CAMBER SHALL BE OBTAINED BY INCREASING THE TOP CHORD LENGTHS AND DECREASING THE BOTTOM CHORD LENGTHS AS SHOWN. CHORD ENDS AND SPLICE PLATES SHALL BE PREPARED TO THE PROPER ANGLE BEFORE SPLICE PLATES ARE WELDED TO THE CHORDS.
  3. FOR DETAILS 1 THRU 6, SEE DWG. NO. OSD-07.
  4. FOR DETAIL OF CHORD WIRE INLET, SEE DWG. NO. OSD-01.
  5. FOR CHORD CAP DETAIL, SEE DWG. NO. OSD-08.
  6. BOLT HOLES FOR CHORD SPLICES SHALL BE 1/8" LARGER THAN BOLT DIAMETER.
  7. WEEP HOLES SHALL BE 3/8" DIAMETER DRILLED HOLES AT CENTERLINE OF BOTH TOP AND BOTTOM CHORDS.

ALTERED ON: \_\_\_\_\_ AFFIXED ON: **05/13/2024**

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

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REVISIONS			
DATE	DESCRIPTION	BY	SYL.

**NEW YORK STATE OF OPPORTUNITY** Thruway Authority

**PDG** POPPLI DESIGN GROUP

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES SPAN STRUCTURES TRUSS DETAILS (SHEET 1 OF 2)	DRAWING NUMBER: OSD-06



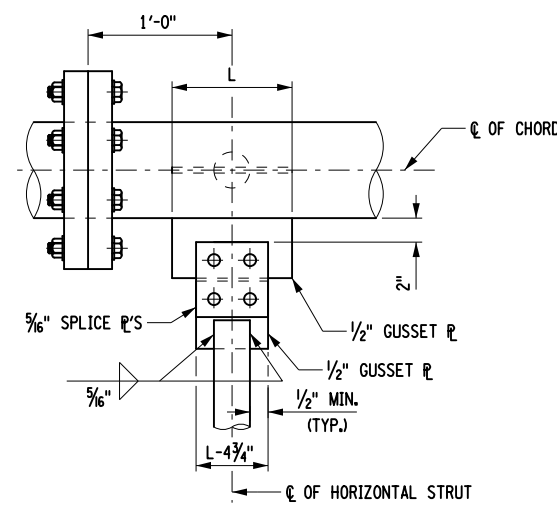
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DRAFTED BY: S. LOFORTE

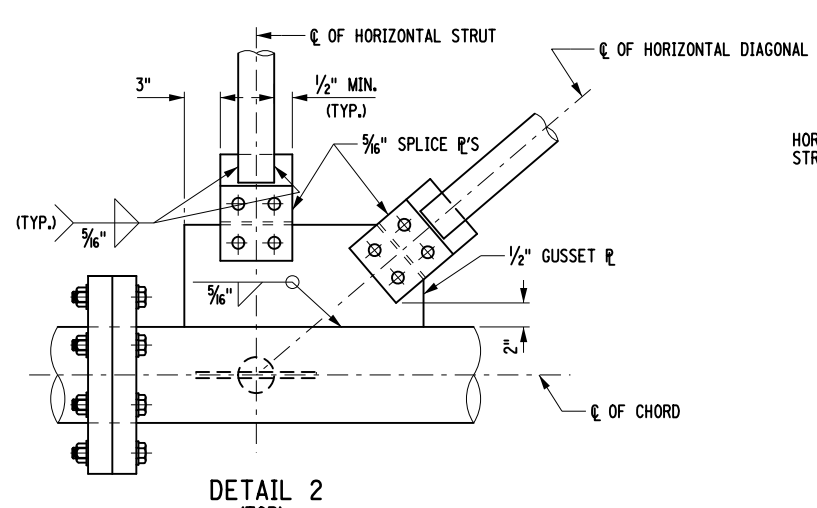
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DESIGNED BY: S. LOFORTE

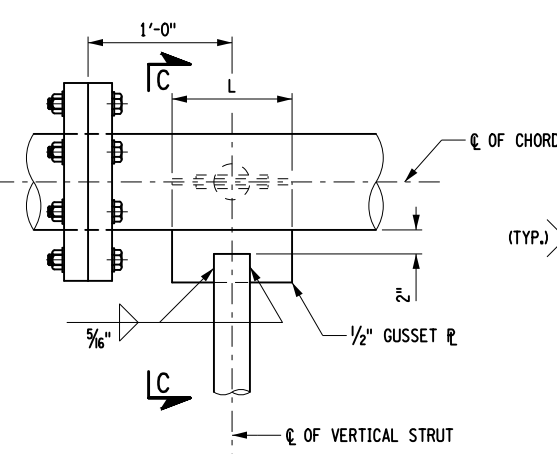
DESIGN SUPERVISOR: M. BEDFORD



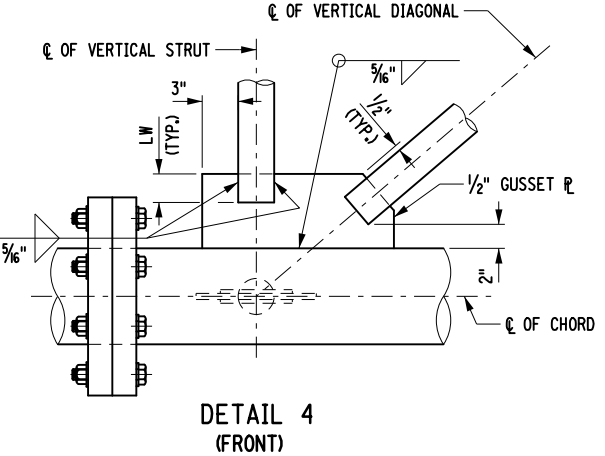
DETAIL 1 (TOP)



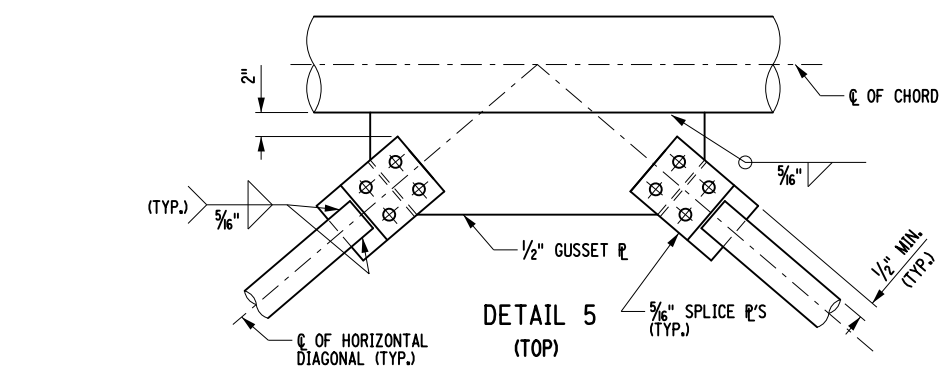
DETAIL 2 (TOP)



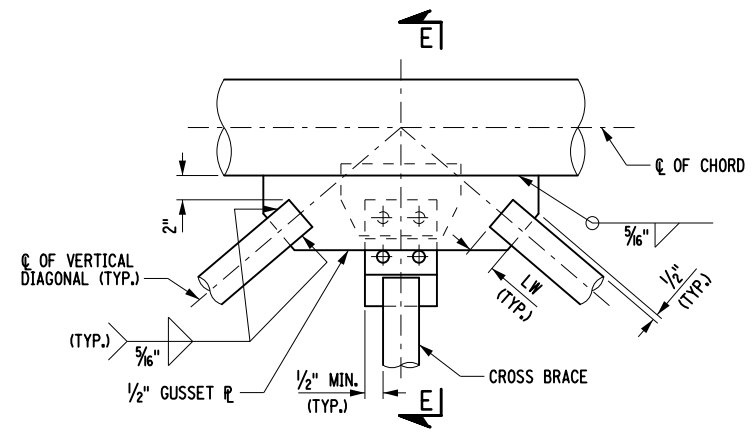
DETAIL 3 (FRONT)



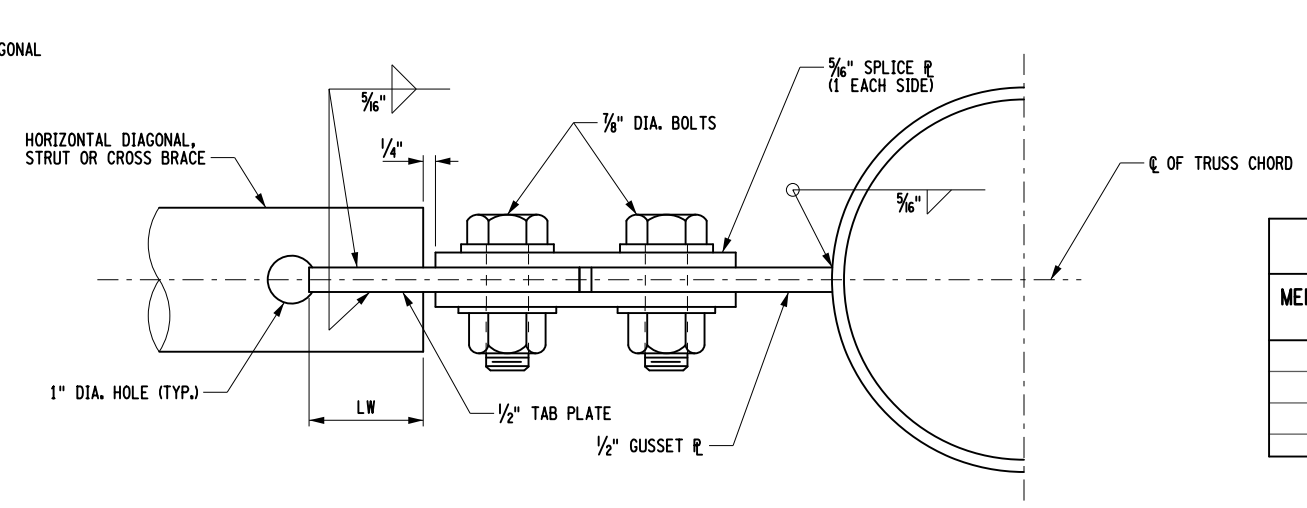
DETAIL 4 (FRONT)



DETAIL 5 (TOP)



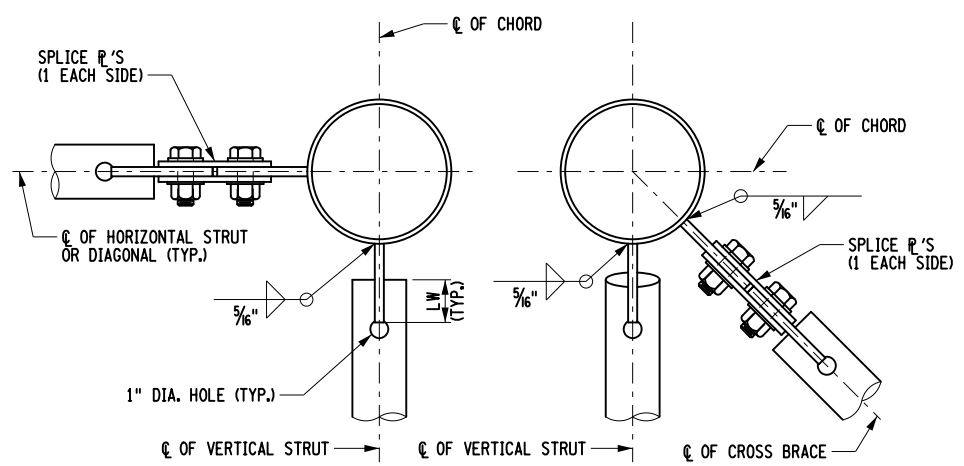
DETAIL 6 (FRONT)



SPLIT TUBE BOLTED CONNECTION DETAIL  
(TYPICAL FOR ALL BOLTED TRUSS CONNECTIONS)

DIAGONAL/STRUT AND CROSS BRACE TABLE		
MEMBER SIZE (NPS)	L (MIN.)	LW (MIN.)
2.5 STD.	10"	2 3/8"
3 STD.	10"	2 3/8"
4 STD.	1'-0 3/8"	3 1/8"
5 STD.	1'-2 1/8"	4"

HORIZONTAL DIAGONALS, STRUTS AND CROSS BRACES	
SPLIT TUBE (NPS)	OPTIONAL DOUBLE ANGLE
2.5 STD.	2 L 3"x 2"x 3/8"
3 STD.	2 L 3"x 2 1/2"x 3/8"
4 STD.	2 L 4"x 3"x 3/8"
5 STD.	2 L 6"x 4"x 3/8"

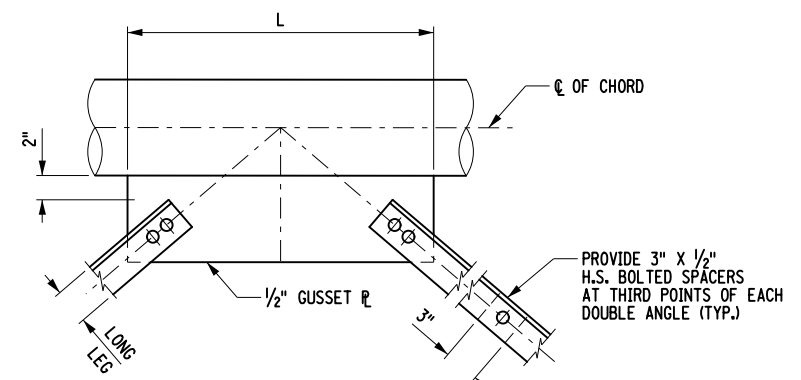


SECTION C-C

SECTION E-E

NOTES:

- ALL BOLTED CONNECTIONS SHALL BE MADE WITH 7/8" DIA. HIGH STRENGTH BOLTS FULLY TIGHTENED. HOLES IN GUSSET AND TAB PLATES SHALL BE STANDARD. HOLES IN BOLTED CONNECTION SPLICE PLATES OR DOUBLE ANGLES MAY BE OVERSIZE. WHERE BOLTS ARE INSTALLED VERTICALLY (OR SEMI-VERTICALLY) THE BOLT SHALL BE INSTALLED WITH THE BOLT HEAD UP.
- FOR LOCATIONS OF DETAILS, SEE DWG. NO. OSD-06.
- ALL VERTICAL TRUSS CONNECTIONS SHALL BE WELDED. DOUBLE ANGLES MAY NOT BE SUBSTITUTED FOR VERTICAL TRUSS MEMBERS.
- FULLY WELDED SPLIT TUBES MAY BE USED IN LIEU OF THE BOLTED CONNECTIONS SHOWN IN DETAILS 4, 5 AND 6, IF GALVANIZING TANKS PERMIT.
- DETAIL 4 MAY BE SUBSTITUTED FOR DETAIL 1 AT THE FABRICATOR'S DISCRETION.



TYPICAL DETAIL FOR OPTIONAL DOUBLE ANGLES AT HORIZONTAL DIAGONALS, STRUTS AND CROSS BRACES  
(FOR MINIMUM L AT STRUT, SEE DIAGONAL/STRUT AND CROSS BRACE TABLE)

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING OVERHEAD SIGN STRUCTURES SPAN STRUCTURES TRUSS DETAILS (SHEET 2 OF 2)	DRAWING NUMBER: OSD-07

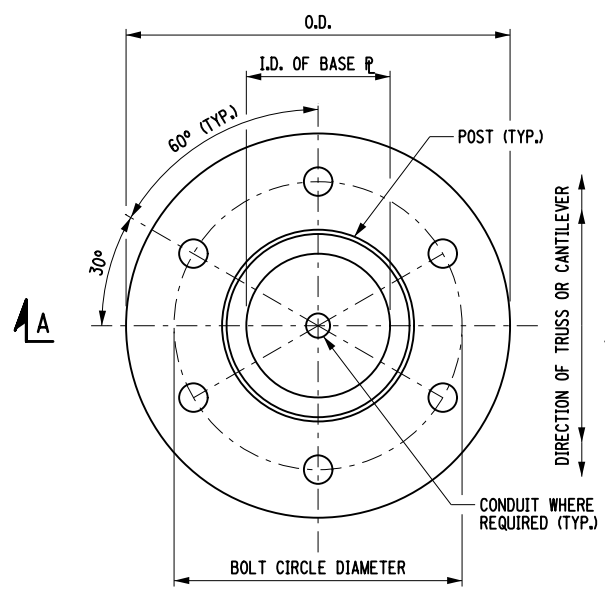
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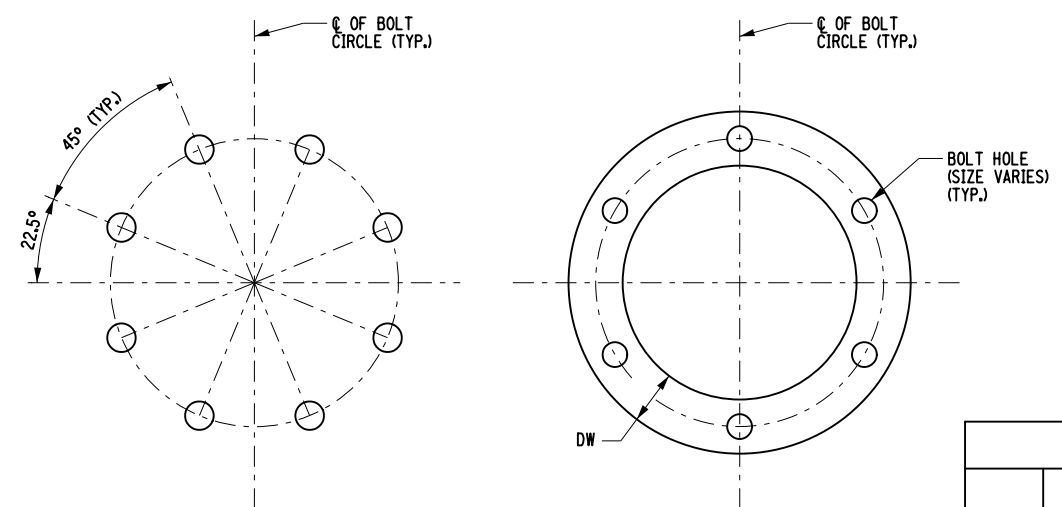
CHECKED BY: M. BEDFORD

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD

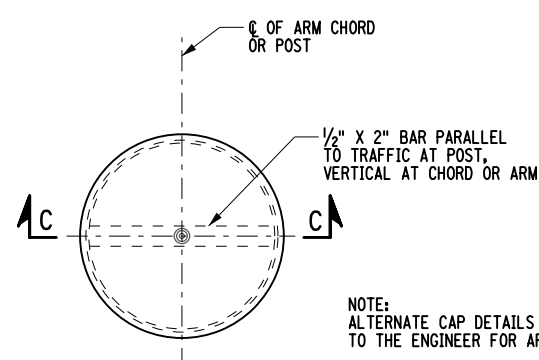


BASE PLATE DETAIL  
6 BOLT ARRANGEMENT



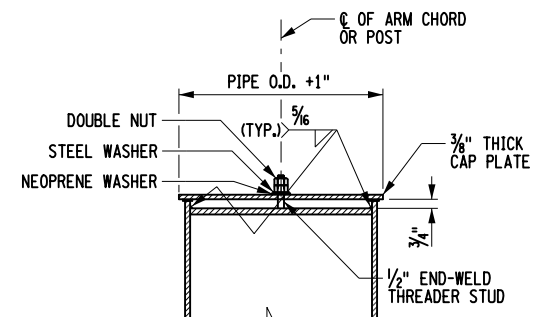
BOLT CIRCLE DETAIL  
8 BOLT ARRANGEMENT

TYPICAL TEMPLATE DETAIL  
6 BOLT ARRANGEMENT SHOWN  
(8 BOLT AS NECESSARY)



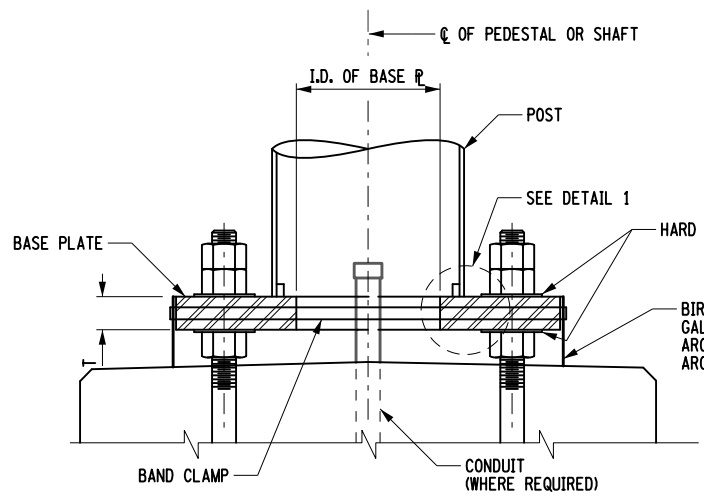
PLAN

CAP DETAILS

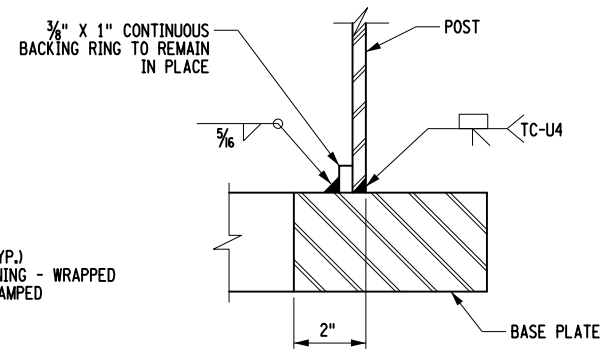


SECTION C-C

NOTE:  
ALTERNATE CAP DETAILS MAY BE SUBMITTED  
TO THE ENGINEER FOR APPROVAL.



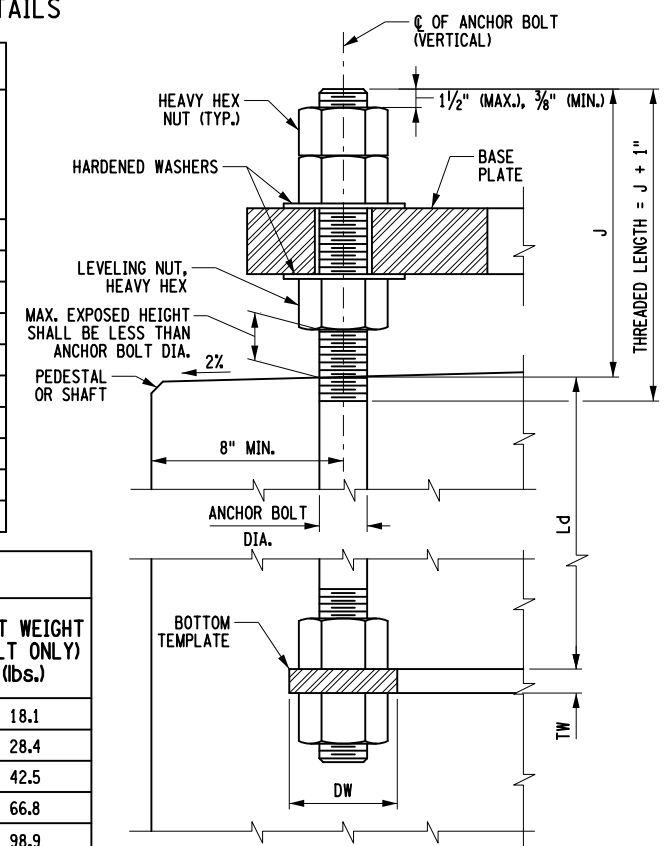
SECTION A-A



DETAIL 1

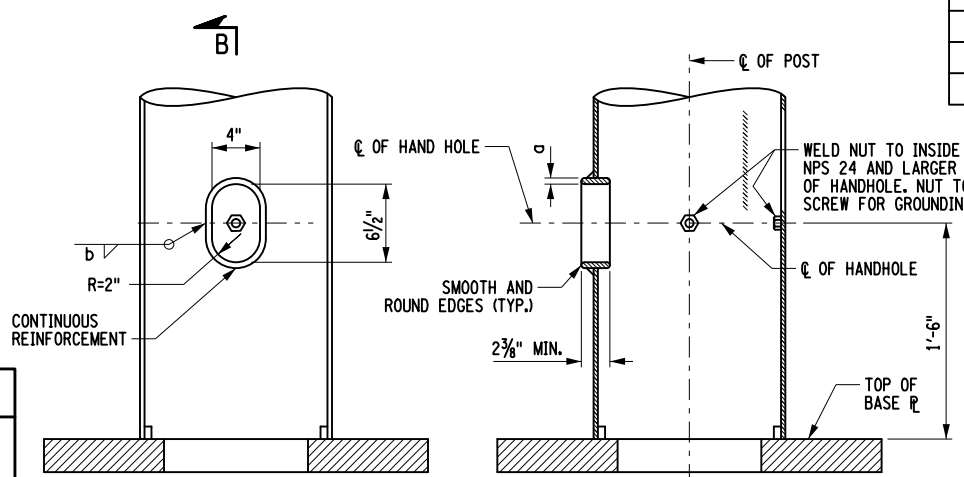
BASE PLATE TABLE							
BASE PLATE TYPE	POST (NPS)	PLATE		ANCHOR BOLTS		HOLE DIA.	
		O.D.	T	NO.	SIZE DIA.		
BP-10	10	2'-2 3/4"	2 3/8"	1'-6 3/4"	6	1 1/2"	1 7/8"
BP-12	12	2'-4 1/2"	2 3/8"	1'-8 1/2"	6	1 3/4"	2 1/8"
BP-14	14	2'-6"	2 3/8"	1'-10"	6	1 3/4"	2 1/8"
BP-16	16	2'-8"	2 3/4"	2'-0"	6	2"	2 3/8"
BP-18	18	2'-10"	2 3/4"	2'-2"	6	2 1/4"	2 5/8"
BP-20	20	3'-0"	2 3/4"	2'-4"	6	2 1/4"	2 5/8"
BP-24	24	3'-2"	2 3/4"	2'-8"	6	2 1/2"	2 7/8"
BP-26	26	3'-4"	2 3/4"	3'-10"	6	2 1/2"	2 7/8"
BP-30	30	3'-10 1/2"	2 3/4"	3'-2 1/2"	6	2 1/2"	2 7/8"
BP-36	36	4'-7 1/8"	2 3/4"	3'-11 1/8"	8	2 1/2"	2 7/8"

ANCHOR BOLT TABLE (ITEM 644.11)						
BOLT SIZE	Ld MIN. EMBED.	J EXPOSED LENGTH	TEMPLATE DATA			BOLT WEIGHT (BOLT ONLY) (lbs.)
			DW	TW	HOLE DIA.	
1 1/2"	2'-5"	9 1/2"	3 1/2"	3/4"	1 1/16"	18.1
1 3/4"	2'-9"	10 1/2"	4"	7/8"	1 3/16"	28.4
2"	3'-2"	1'-0"	4 1/2"	1"	2 1/16"	42.5
2 1/4"	3'-8"	1'-1 1/2"	5"	1 1/8"	2 3/8"	66.8
2 1/2"	4'-3"	1'-2 3/4"	6"	1 1/4"	2 3/4"	98.9

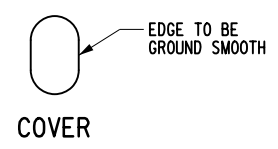


ANCHOR BOLT DETAIL

HANDHOLE TABLE		
PIPE WALL THICKNESS	a REINFORCEMENT THICKNESS (MIN.)	b WELD SIZE
LESS THAN 1/2"	1/2"	3/8"
1/2" TO 5/8"	5/8"	1/2"



SECTION B-B  
POST HANDHOLE DETAIL



COVER

DETAILS OF THE COVER TO BE DETERMINED BY THE FABRICATOR TO FIT TIGHTLY, EXCLUDE WATER, AND BE REMOVABLE WITH A WRENCH. ALL FASTENERS, NUTS, AND WASHERS SHALL BE STAINLESS STEEL.

- NOTES:
1. ANCHOR BOLTS, NUTS, WASHERS AND TEMPLATES SHALL BE GALVANIZED.
  2. FOR TEMPLATE REQUIREMENT, SEE SPECIFICATIONS.
  3. TOP TEMPLATE SHALL BE REMOVED PRIOR TO THE INSTALLATION OF THE BASE PLATE.
  4. S.I.N. AND CONTRACT NO. SHALL BE STAMPED ON BASE PLATE, SEE SPECIFICATIONS.

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2  
TITLE OF DRAWING  
OVERHEAD SIGN STRUCTURES  
SPAN STRUCTURES  
BASE AND CAP DETAILS

CONTRACT NUMBER:  
TAN 24-12  
DATE:  
MAY 2024  
DRAWING NUMBER:  
OSD-08



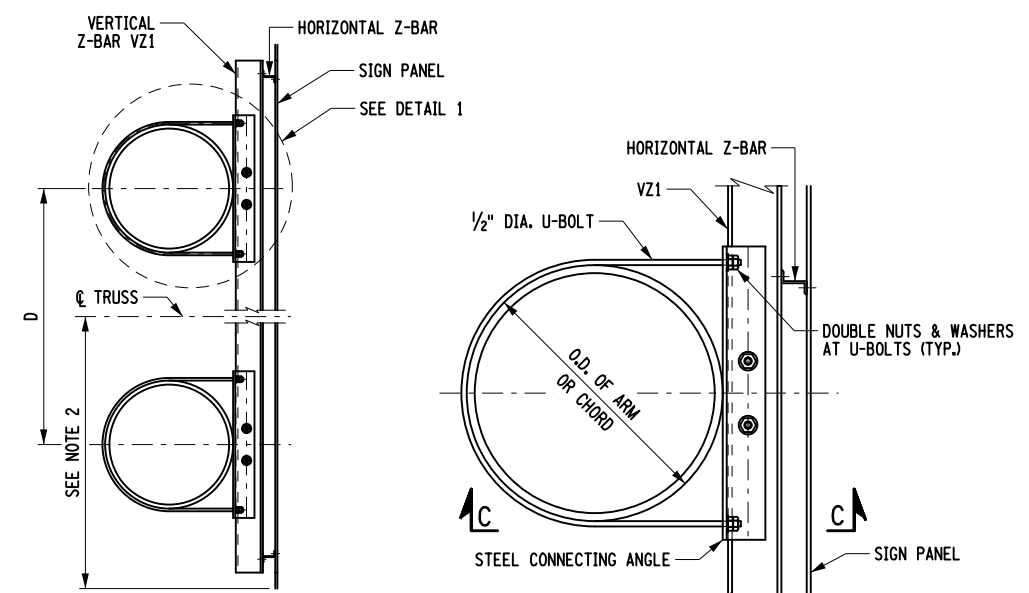
CHECKED BY: M. BEDFORD

DRAFTED BY: S. LOFORTE

CHECKED BY: M. BEDFORD

DESIGNED BY: S. LOFORTE

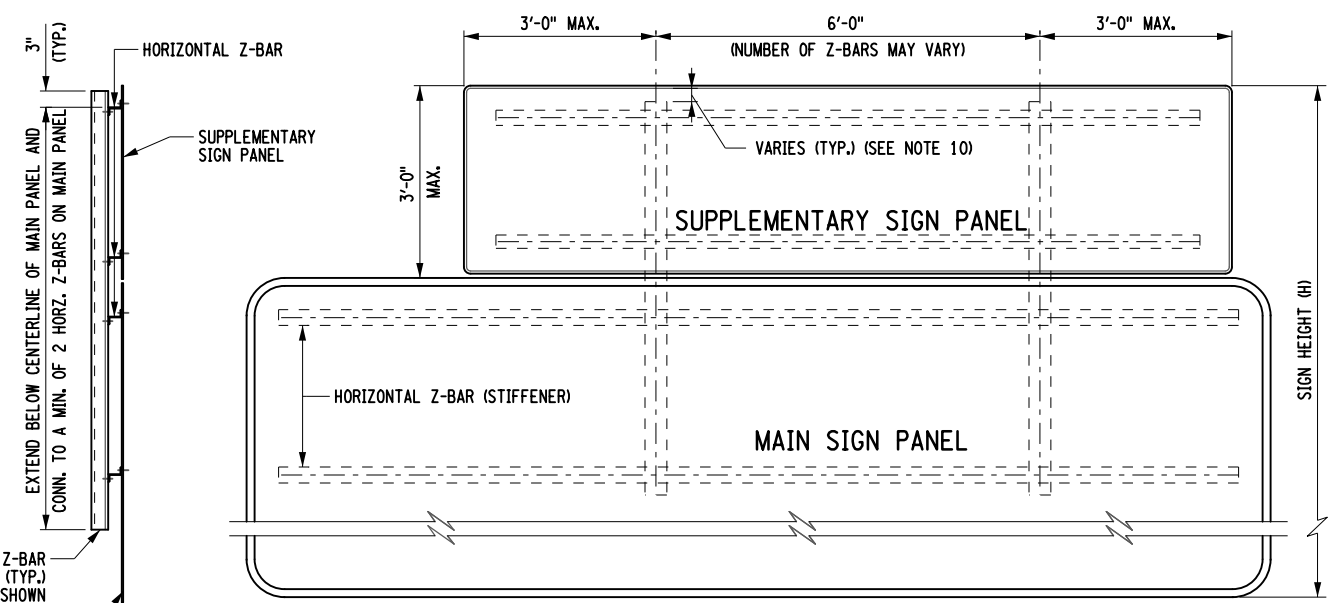
DESIGN SUPERVISOR: M. BEDFORD



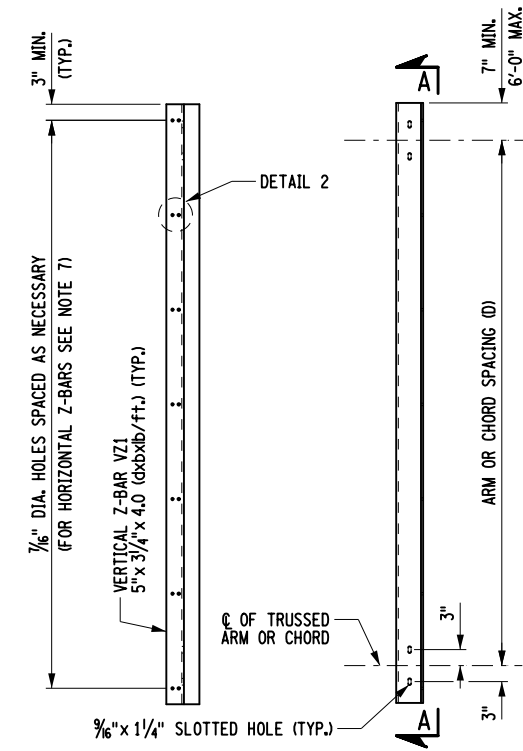
TYPICAL SECTION  
ATTACHMENT DETAILS AT TRUSSED ARMS OR CHORD

SUPPLEMENTARY VERTICAL Z-BAR  
5" x 3/4" x 4.0 (dx b x lb / ft.) (TYP.)  
MAIN VERTICAL Z-BARS NOT SHOWN  
FOR CLARITY

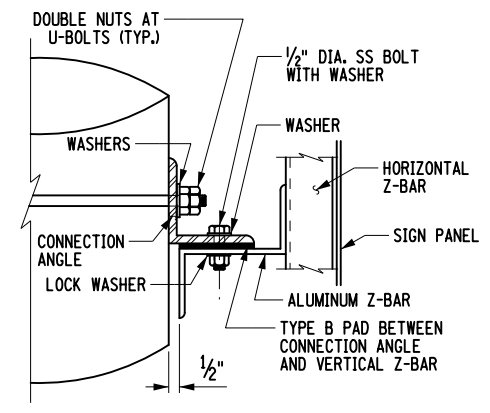
MAIN SIGN PANEL



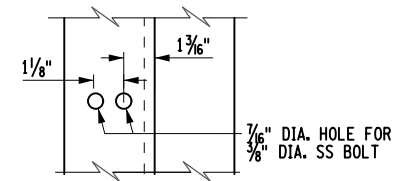
NOTE: IN LIEU OF THIS DETAIL, THE VERTICAL Z-BARS BEHIND THE MAIN SIGN PANEL MAY BE EXTENDED TO CARRY THE SUPPLEMENTARY PANEL. THE OVERHANG OF THE SUPPLEMENTARY PANEL SHALL NOT EXCEED 3'-0".  
**SUPPLEMENTARY SIGN PANEL MOUNTING DETAILS**  
(MAIN SIGN PANEL VERTICAL Z-BARS AND ADDITIONAL HORIZONTAL Z-BARS NOT SHOWN. SEE NOTE 10)



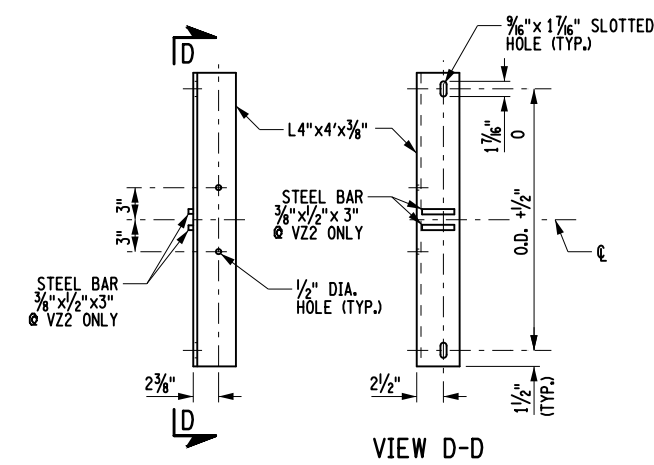
VIEW A-A  
ELEVATION  
DETAIL OF VERTICAL Z-BAR VZ1



SECTION C-C



DETAIL 2



VIEW D-D  
STEEL CONNECTION ANGLE

NOTES:

- Z-BARS SHALL BE NON-WELDED ALUMINUM ALLOY 6061-T6.
- EACH SET OF MAIN AND SUPPLEMENTARY SIGN PANELS ON SPAN STRUCTURES SHALL HAVE THE MAIN PANEL VERTICALLY CENTERED ON THE TRUSS. IF THE COMBINED VERTICAL CENTROID OF THE MAIN AND SUPPLEMENTARY SIGN PANELS IS GREATER THAN 9/16" AWAY FROM THE VERTICAL CENTROID OF THE MAIN PANEL ALONE, THEN THE COMBINED VERTICAL CENTROID SHALL BE CENTERED ON THE TRUSS.
- STEEL BARS AND STEEL CONNECTION ANGLES SHALL BE GALVANIZED.
- ALL STAINLESS STEEL BOLTING MATERIAL SHALL BE ASTM A193, GRADE B8.
- THERE SHALL BE A MINIMUM OF TWO VERTICAL Z-BARS PER SIGN. SPACING SHALL BE 6'-0" MAXIMUM. END VERTICAL Z-BARS SHALL BE NO MORE THAN 3'-0" FROM THE EDGE OF THE SIGN PANEL. VERTICAL Z-BARS SHALL BE ARRANGED TO AVOID INTERFERENCE WITH CHORD GUSSET PLATES OR WITH CHORD SPLICES. IF INTERFERENCE CANNOT BE RESOLVED BY RESPACING VERTICAL Z-BARS, THE SIGN PANEL MAY BE RELOCATED BY AS MUCH AS 6" FROM ITS ORIGINALLY DESIGNATED LOCATION.
- SPACING OF SIGN PANEL HORIZONTAL Z-BAR STIFFENERS SHALL BE AS SHOWN ON THE CURRENT SIGN PANEL STANDARD SHEET.
- A LOCK WASHER SHALL BE USED UNDER EACH NUT FOR NON-COUNTERSUNK STAINLESS STEEL BOLTS. IN LIEU OF THE LOCK WASHER, AN APPROVED STOP NUT MAY BE USED. LOCK WASHERS SHALL NOT BE USED AT A SLOTTED HOLE. A FLAT WASHER SHALL BE USED UNDER EACH BOLT HEAD.
- SIGN PANEL, HORIZONTAL AND VERTICAL Z-BARS AND FASTENING ASSEMBLIES FOR ATTACHING SIGNS TO STRUCTURE SHALL BE PAID UNDER THE OVERHEAD SIGN PANEL ITEM 645.
- SEE NYSDOT STANDARD SHEET 645-09 FOR ADDITIONAL SIGN PANEL MOUNTING DETAILS.

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SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING OVERHEAD SIGN STRUCTURES SPAN STRUCTURES SIGN PANEL ATTACHMENT DETAILS	DRAWING NUMBER: OSD-09

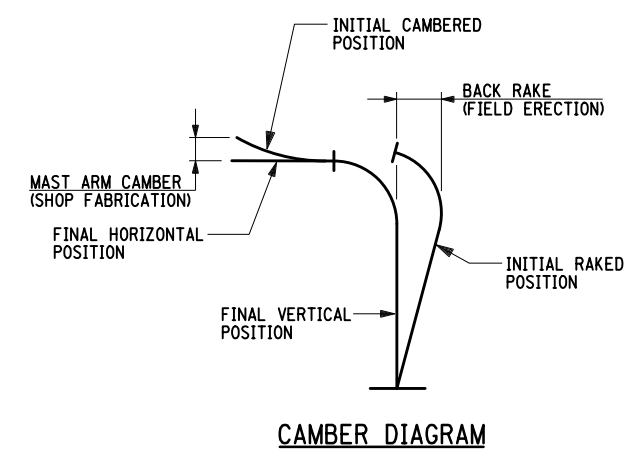
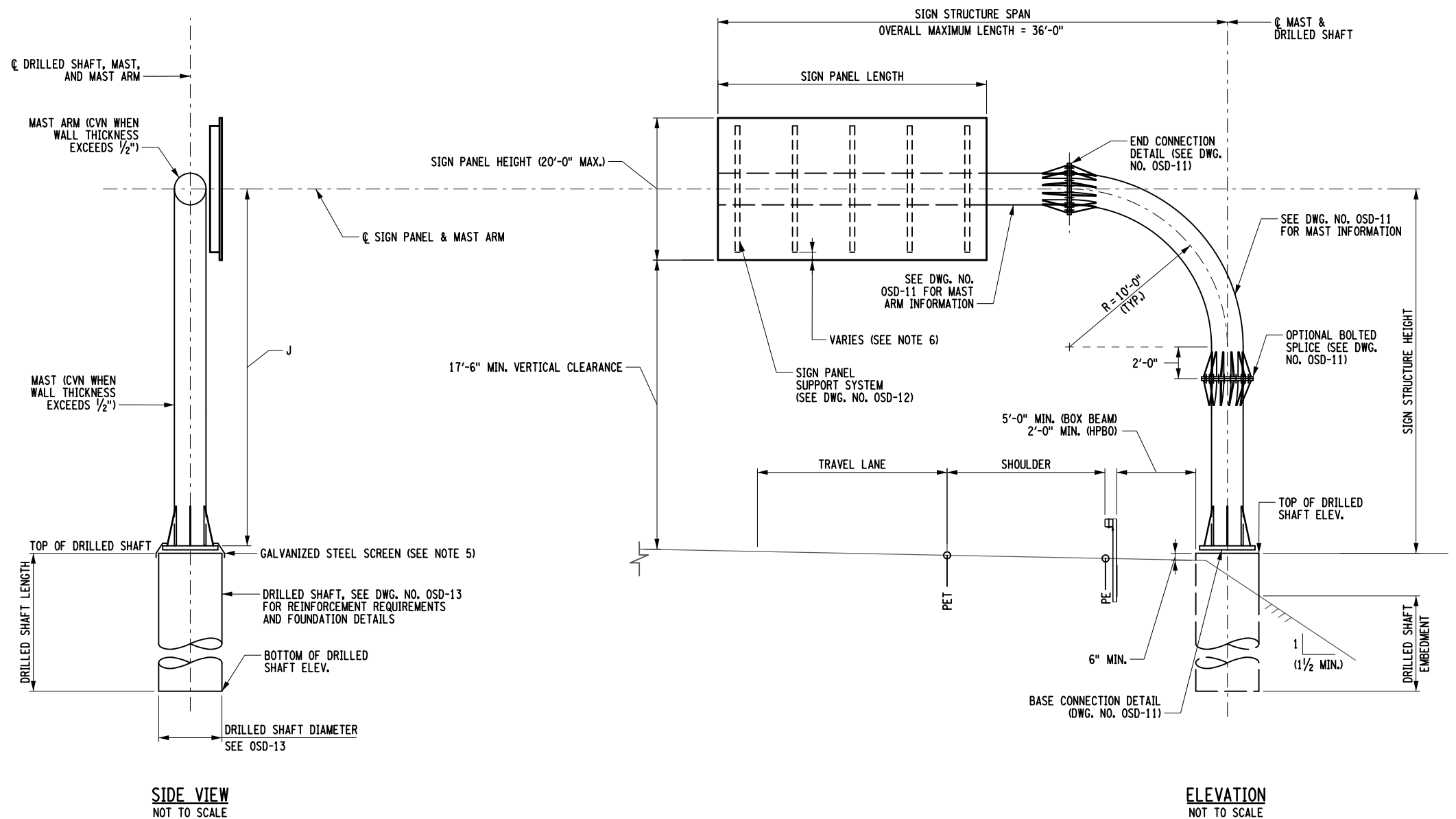
CHECKED BY: Q. WANG

DRAFTED BY: S. LOFORTE

CHECKED BY: Q. WANG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



**NOTE TO FABRICATOR:**

THE ARM CAMBER INDICATED ON THE SCHEMATIC IS MEASURED FROM THE FIXED END CONNECTION (ZERO CAMBER) TO THE CANTILEVER END OF THE ARM (MAXIMUM CAMBER). THIS CAMBER IS UPWARD AND PARABOLIC IN SHAPE IN ORDER TO COUNTER THE DOWNWARD PARABOLIC DEFLECTION OF THIS CANTILEVER PIPE SECTION DUE TO THE SELF WEIGHT OF THE PIPE AND WEIGHT OF THE APPURTANANCES ATTACHED TO THE ARM.

**NOTE TO CONTRACTOR:**

RAKING OF THE MAST SHALL BE ACCOMPLISHED USING THE LEVELING NUTS AND LOWER TOP NUTS PRIOR TO THE INSTALLATION OF THE ARM.

**NOTE TO ERECTOR:**

MAST BACK RAKE SHALL BE PERFORMED USING THE LEVELING NUTS AND LOWER TOP NUTS PRIOR TO INSTALLATION OF THE ARM. THE LOWER TOP NUTS SHALL BE NO MORE THAN "SNUG TIGHT" AT THE BEGINNING OF THIS OPERATION. THE DIMENSION SHOWN FOR THE BACK RAKE IN THE SCHEMATIC ABOVE IS THE HORIZONTAL DISPLACEMENT OF THE THEORETICAL TOP OF MAST (DIMENSION "J") ON THE SIDE VIEW, FROM THE CENTERLINE OF THE MAST AT THE BASE PLATE. THE MAST SHALL BE RAKED BACK TO THIS POSITION AND LEVELING NUTS/LOWER TOP NUTS SNUG TIGHT AGAIN UNTIL THE STRUCTURE IS COMPLETELY ASSEMBLED. AFTER THE INSTALLATION OF THE ARM AND ALL SPECIFIED SIGN APPURTANANCES, THE RAKE SHALL BE ADJUSTED AS NEEDED TO ACHIEVE A VERTICAL MAST AND HORIZONTAL ARM. ONLY AFTER THIS FINAL POSITIONING IS ACHIEVED AND APPROVED BY THE ENGINEER, SHALL LOWER TOP NUTS BE FULLY TENSIONED AGAINST THE LEVELING NUTS AND THE UPPER TOP NUTS INSTALLED SNUG TIGHT AGAINST THE LOWER TOP NUTS.

**SIDE VIEW**  
NOT TO SCALE

**ELEVATION**  
NOT TO SCALE

**SIGN STRUCTURE TABLE**

SIGN ID NO. (SIN)	ITEM NO.	LOCATION MP	SIGN TEXT ID	SPAN LENGTH	PANEL AREA (SF.)	SIGN STRUCTURE HEIGHT	MAST ARM CAMBER (IN.)	BACK RAKE (IN.)	TOP OF DRILLED SHAFT (ELEV.)	BOT. OF DRILLED SHAFT (ELEV.)	DRILLED SHAFT LENGTH	DRILLED SHAFT MIN. EMBEDMENT	Ø SIGN PANEL (ELEV.)	SIGN PANEL LENGTH	SIGN PANEL HEIGHT
S902.19	644.70380125	902.19	37	33'-0"	164.00	24'-2"	0.20	0.40	606.82	560.82	46'-0" ±	42'-0" ±	630.97	16'-6" 9'-6"	8'-6" 2'-6"
S902.69	644.70380225	902.69	44	31'-7"	148.00	21'-11"	0.15	0.30	604.98	581.48	23'-6"	21'-0"	626.88	18'-6"	8'-0"
S903.19	644.70380325	903.19	39	31'-0"	157.00	22'-10"	0.15	0.30	602.82	579.32	23'-6"	21'-0"	625.62	20'-6" 9'-6"	6'-6" 2'-6"
S903.51	644.70380425	903.51	46	35'-6"	198.00	24'-10"	0.30	0.55	602.00	570.00	32'-0"	29'-6"	626.80	18'-0"	11'-0"
S903.81	644.70380525	903.81	40	27'-0"	131.00	23'-10"	0.05	0.30	600.83	578.83	22'-0"	19'-0"	624.63	16'-6" 9'-6"	6'-6" 2'-6"
S904.11	644.70380625	904.11	47	34'-4"	198.00	24'-6"	0.20	0.50	580.60	556.60	24'-0"	23'-0"	605.10	18'-0"	11'-0"

**NOTES:**

- MAST ARM AND END CONNECTION COMPONENTS AS WELL AS MAST AND BASE CONNECTION COMPONENTS SHOWN ON DWG. NO. OSD-11.
- DRILLED SHAFT DIAMETER AND REINFORCING SHOWN ON DWG. NO. OSD-13.
- PROVIDE A MAXIMUM 2'-0" SPACE BETWEEN ADJACENT SIGN PANELS WHEN PRESENT.
- TERMINATE WELDS 1/4" SHORT OF STIFFENER ENDS.
- SEAL BASE PLATE TO FOUNDATION GAP WITH GALVANIZED STEEL SCREEN, 1/2" BY 1/2" MESH AND 0.063" DIAMETER WIRES. SCREEN IS TO PREVENT ENTRY OF RODENTS WHILE PERMITTING DRAINAGE. SCREEN IS TO BE REMOVEABLE AND ATTACHED TO BASEPLATE WITH STAINLESS STEEL HARDWARE. COST TO BE INCLUDED IN SIGN STRUCTURE ITEM NUMBER.
- SEE PENNDOT "TRAFFIC CONTROL - PAVEMENT MARKINGS AND SIGNING STANDARDS TC-8700 SERIES" FOR ADDITIONAL SIGN PANEL MOUNTING DETAILS.

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES CANTILEVER STRUCTURES MISC. DETAILS (SHEET 1 OF 4)	DRAWING NUMBER: OSD-10

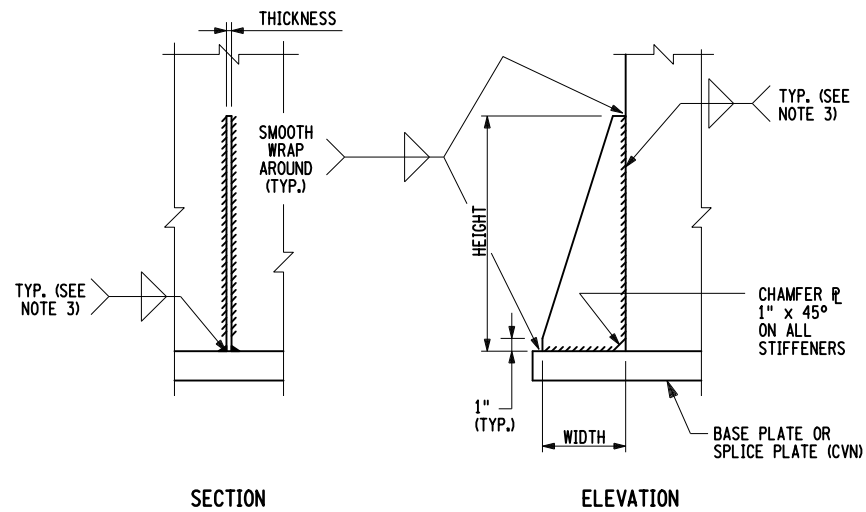
CHECKED BY: M. BEDFORD

DRAFTED BY: S. LOFORTE

CHECKED BY: M. BEDFORD

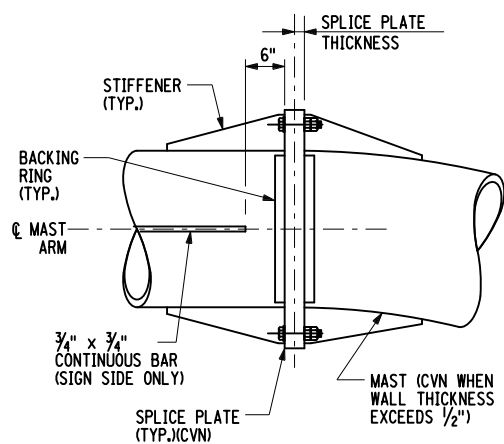
DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD

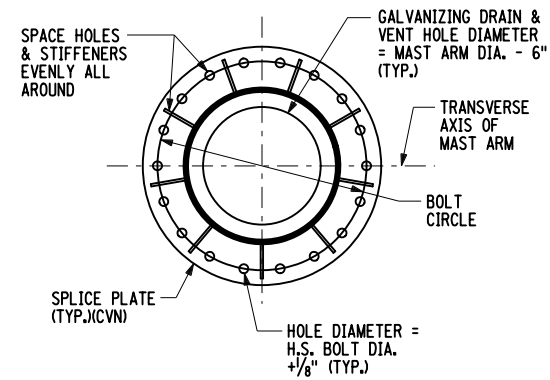


SECTION

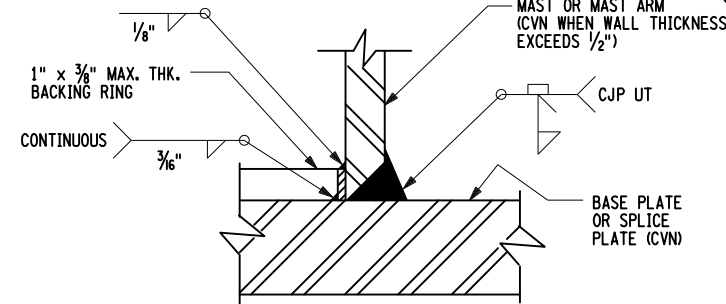
ELEVATION



ELEVATION



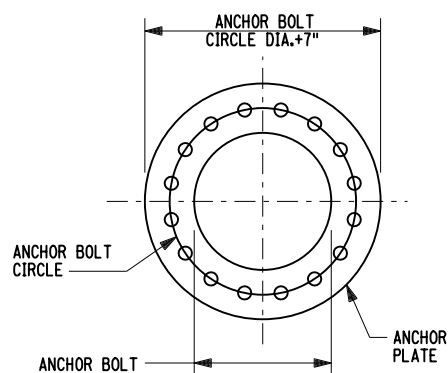
SECTION



WELD DETAIL

NOTE:  
BACKING RING MUST BE FITTED/SIZED TO THE PIPE COLUMN AND CONTINUOUSLY FILLET WELDED TO THE BASE PLATE BEFORE THE FULL PENETRATION GROOVE WELD IS MADE. BACKING RING MUST BE FABRICATED AS A CONTINUOUS RING.

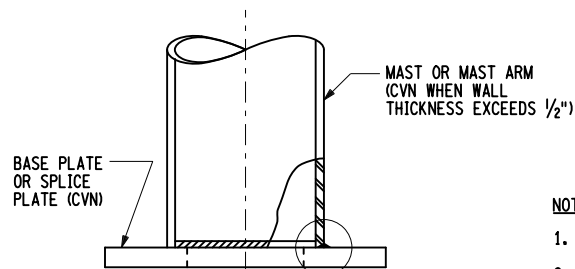
STIFFENER DETAILS



PLAN

ANCHOR PLATE DETAIL

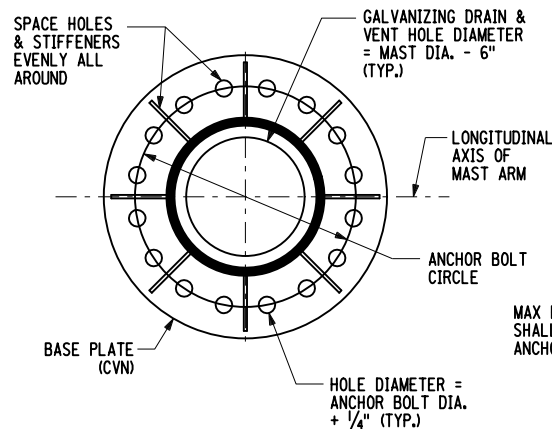
(16 BOLT CONFIGURATION SHOWN, 18 BOLT CONFIGURATION SIMILAR)



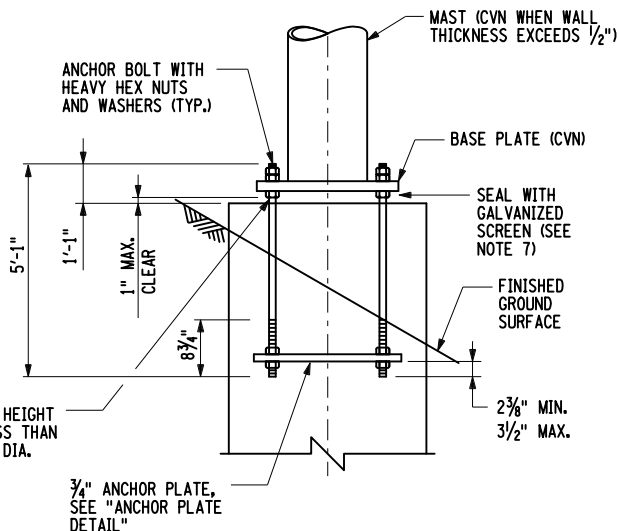
PIPE TO PLATE CONNECTION DETAIL

END CONNECTION DETAILS

(OPTIONAL MAST SPLICE CONNECTION SIMILAR)  
(18 BOLT CONFIGURATION SHOWN, 20 BOLT CONFIGURATION SIMILAR)

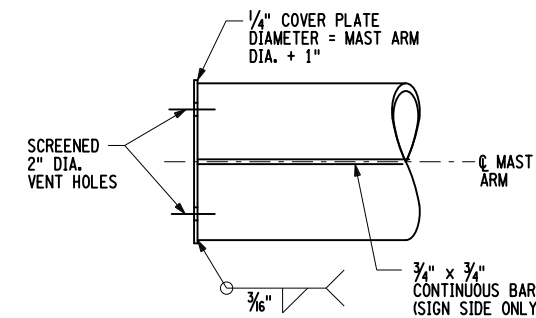


PLAN



ELEVATION

NOTE: STIFFENERS NOT SHOWN FOR CLARITY



MAST ARM END DETAIL  
(CANTILEVER STRUCTURES)

BASE CONNECTION DETAILS

(16 BOLT CONFIGURATION SHOWN, 18 BOLT CONFIGURATION SIMILAR)

NOTES:

- FOR GENERAL NOTES, SEE DWG. NO. OSN-02.
- PIPE DIAMETERS SHOWN IN THE TABLE ARE OUTSIDE DIAMETERS.
- TERMINATE WELDS 1/4" SHORT OF STIFFENER CORNER CHAMFER.
- PROVIDE STIFFENERS AS INDICATED IN CONNECTION COMPONENT SELECTION TABLES.
- FOR DRILLED SHAFT INFORMATION, SEE DWG. NO. OSD-13.
- FOR SIGN PANEL SUPPORT BEAM DEATILS, SEE DWG. NO. OSD-12.
- SEAL BASE PLATE TO FOUNDATION GAP WITH GALVANIZED STEEL SCREEN, 1/2" BY 1/2" MESH AND 0.063" DIAMETER WIRES. SCREEN IS TO PREVENT ENTRY OF RODENTS WHILE PERMITTING DRAINAGE. SCREEN IS TO BE REMOVEABLE AND ATTACHED TO BASEPLATE WITH STAINLESS STEEL HARDWARE.
- INSTALL MAST ARM SPLICE BOLTS IN ACCORDANCE WITH SECTION 10 OF NYSSCM.
- TIGHTEN ANCHOR BOLTS USING TURN-OF-THE-NUT METHOD (PER TABLE 1001.3b OF THE NYSSCM) ONCE SIGN STRUCTURE AND SIGN PANEL ARE LEVELLED.
- INSTALL TOP NUTS SNUG-TIGHT AFTER COMPLETION OF NOTE 9.

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MAST ARM & END CONNECTION COMPONENT SELECTION TABLE (CANTILEVER STRUCTURES)

LOCATION(S)	MAX. SPAN (FEET)	MAX. PANEL AREA (S.F.)	MAST ARM		NO.	H.S. BOLTS		SPLICE PLATE		STIFFENERS				
			DIAMETER (INCHES)	THICKNESS (INCHES)		DIAMETER (INCHES)	CIRCLE (INCHES)	DIAMETER (INCHES)	THICKNESS (INCHES)	NO.	THICKNESS (INCHES)	WIDTH (INCHES)	HEIGHT (INCHES)	WELD (INCHES)
902.19, 902.69, 903.19, 903.51, 904.11	36	250	24	0.500 (WT. XS)	18	1 3/8	29	34	2	9	3/8	4	11	5/8
903.81	27	250	24	0.375 (SCH. 20)	18	1	27 1/2	31	2	9	3/8	4	11	5/8

MAST & BASE CONNECTION COMPONENT SELECTION TABLE (CANTILEVER STRUCTURES)

LOCATIONS	SPAN (FEET)	MAX. PANEL AREA (S.F.)	MAST		NO.	ANCHOR BOLTS		BASE PLATE		STIFFENERS				
			DIAMETER (INCHES)	THICKNESS (INCHES)		DIAMETER (INCHES)	CIRCLE (INCHES)	DIAMETER (INCHES)	THICKNESS (INCHES)	NO.	THICKNESS (INCHES)	WIDTH (INCHES)	HEIGHT (INCHES)	WELD (INCHES)
902.19, 902.69, 903.19, 903.51, 904.11	36	250	24	0.688 (SCH. 40)	18	1 3/4	35	41	2	9	3/8	7 1/2	21	5/8
903.81	27	250	24	0.500 (WT.XS)	18	1 1/2	30 1/2	35 1/2	2	9	3/8	4 3/4	13 1/2	5/8

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05/13/2024

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DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES CANTILEVER STRUCTURES MISC. DETAILS (SHEET 2 OF 4)	DRAWING NUMBER: OSD-11

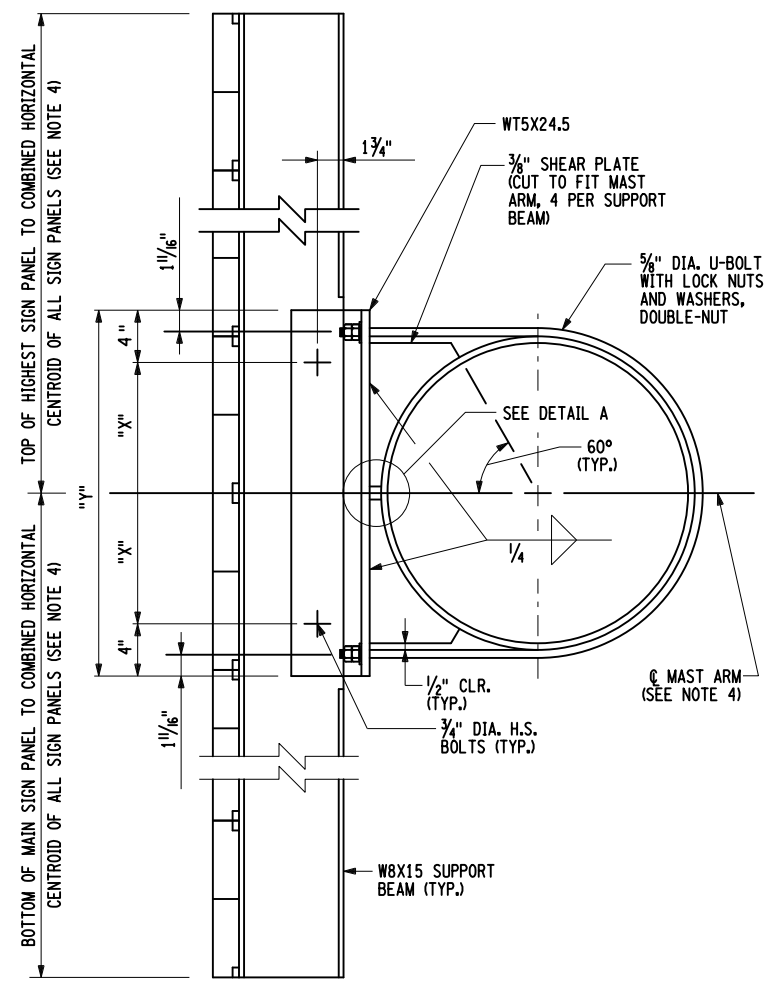
CHECKED BY: M. BEDFORD

DRAFTED BY: S. LOFORTE

CHECKED BY: M. BEDFORD

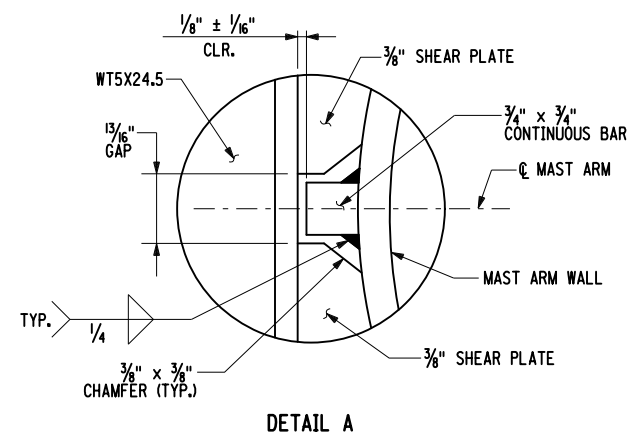
DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



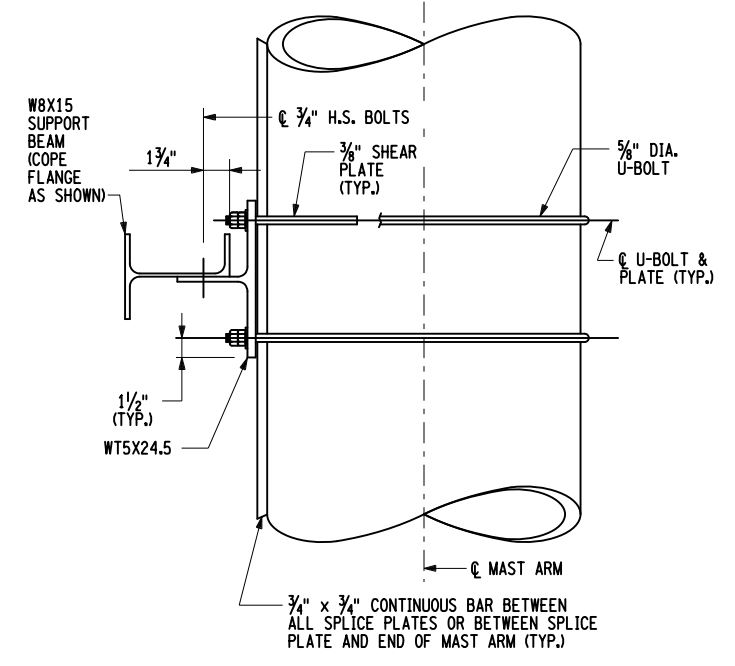
SINGLE SIGN PANEL SECTION

DISTANCE		
PIPE OUTSIDE DIAMETER	"X"	"Y"
24"	10"	2'-4"



DETAIL A

SUPPORT BEAM TO MAST ARM CONNECTION DETAILS



SINGLE SIGN PANEL PLAN

NOTES:

- FOR GENERAL NOTES, SEE SHEET OSN-02.
- FOR SIGN PANEL DETAILS, SEE SHEET SDS-01 THRU SDS-06.
- LOCATE SUPPORT BEAMS TO AVOID END AND SPLICE CONNECTIONS. MAXIMUM SPACING = 5'-0". MAXIMUM DISTANCE TO PANEL EDGE = 2'-6".
- SIGN PANEL SUPPORT BEAM DETAILS GIVEN ON THIS SHEET ARE ONLY VALID FOR SIGNS WHERE THE COMBINED HORIZONTAL CENTERLINE OF THE SIGN PANEL(S) IS AT THE SAME LOCATION AS THE CENTERLINE OF MAST ARM. PROVIDE SIGN PANEL SUPPORT BEAM DETAILS ON SHOP DRAWINGS WHEN THE COMBINED HORIZONTAL CENTERLINE OF THE SIGN PANEL(S) IS NOT AT THE SAME LOCATION AS THE CENTERLINE OF THE MAST ARM.
- SEE PENNDOT "TRAFFIC CONTROL - PAVEMENT MARKINGS AND SIGNING STANDARDS TC-8700 SERIES" FOR ADDITIONAL SIGN PANEL MOUNTING DETAILS.

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REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES CANTILEVER STRUCTURES MISC. DETAILS (SHEET 3 OF 4)	DRAWING NUMBER: OSD-12

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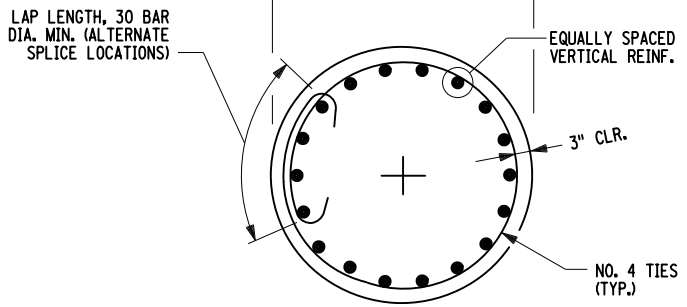
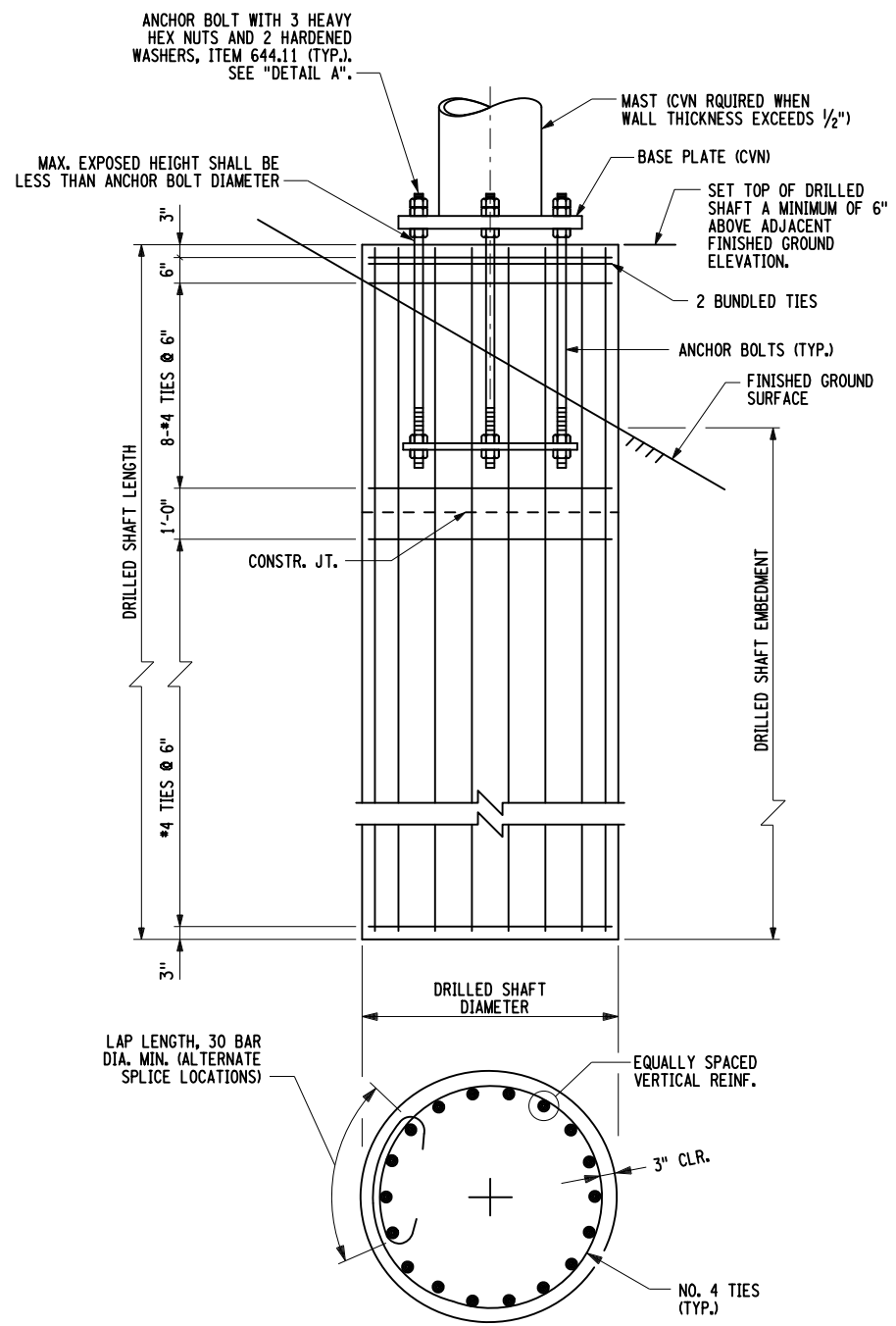
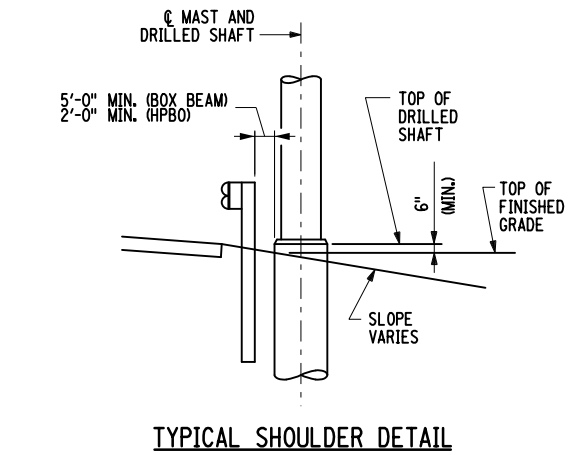
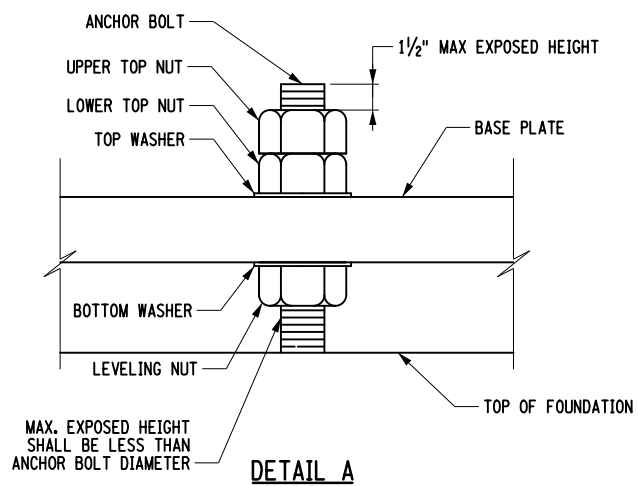
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DRAFTED BY: S. LOFORTE

CHECKED BY: Q. WANG

DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD



**DRILLED SHAFT FOUNDATION DETAILS  
ROADSIDE INSTALLATION**

LOCATION	SPAN (FEET)	PANEL AREA (S.F.)	DRILLED SHAFT DIAMETER (INCHES)	DRILLED SHAFT EMBEDMENT	DRILLED SHAFT LENGTH	DRILLED SHAFT CONCRETE QTY. (CY)	VERT. REINF.	
							NO.	SIZE
902.19	33'-0"	164.00	60	42'-0"	46'-0"	33.5	40	#8(G)
902.69	31'-7"	148.00	54	21'-0"	23'-6"	13.8	30	#8(G)
903.19	31'-0"	157.00	54	21'-0"	23'-6"	13.8	30	#8(G)
903.51	35'-6"	198.00	54	29'-6"	32'-0"	18.8	30	#8(G)
903.81	27'-0"	131.00	48	19'-0"	22'-0"	10.2	30	#8(G)
904.11	34'-4"	198.00	54	23'-0" (3'-0")	24'-0" (3'-0")	14.1	30	#8(G)

NOTE: DIMENSIONS SHOWN IN PARENTHESIS APPLY TO ROCK SOCKET WHERE NECESSARY

**DRILLED SHAFT DRILLING AND INSTALLATION NOTES:**

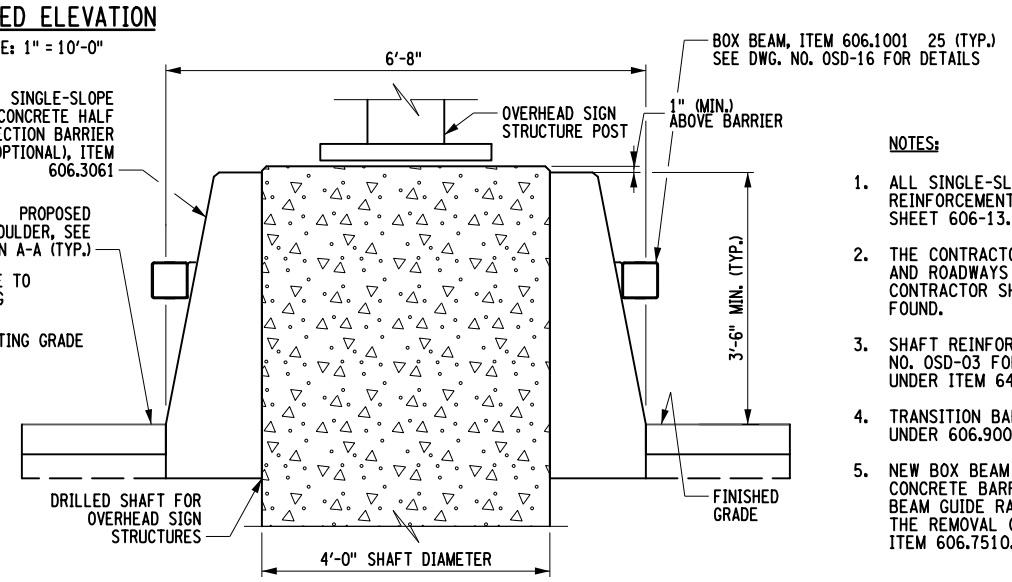
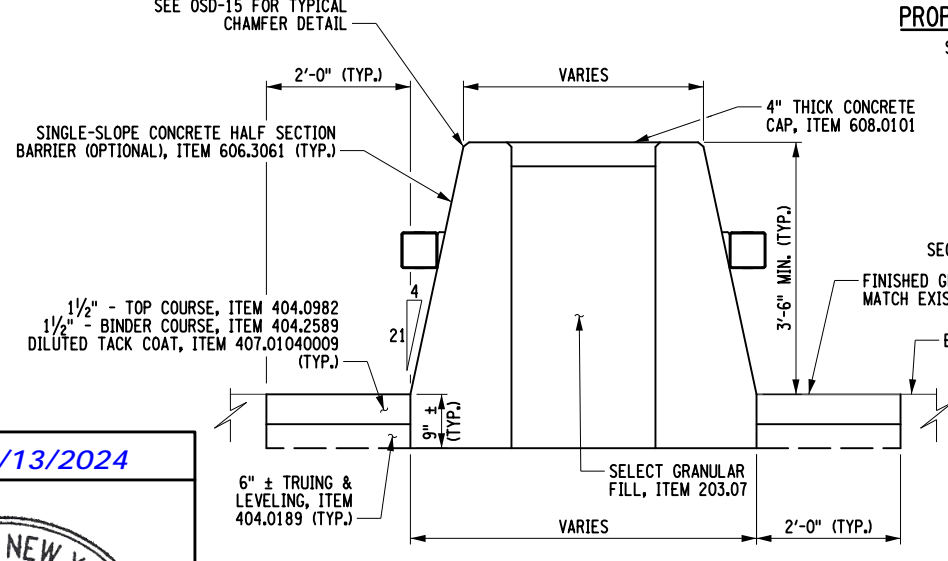
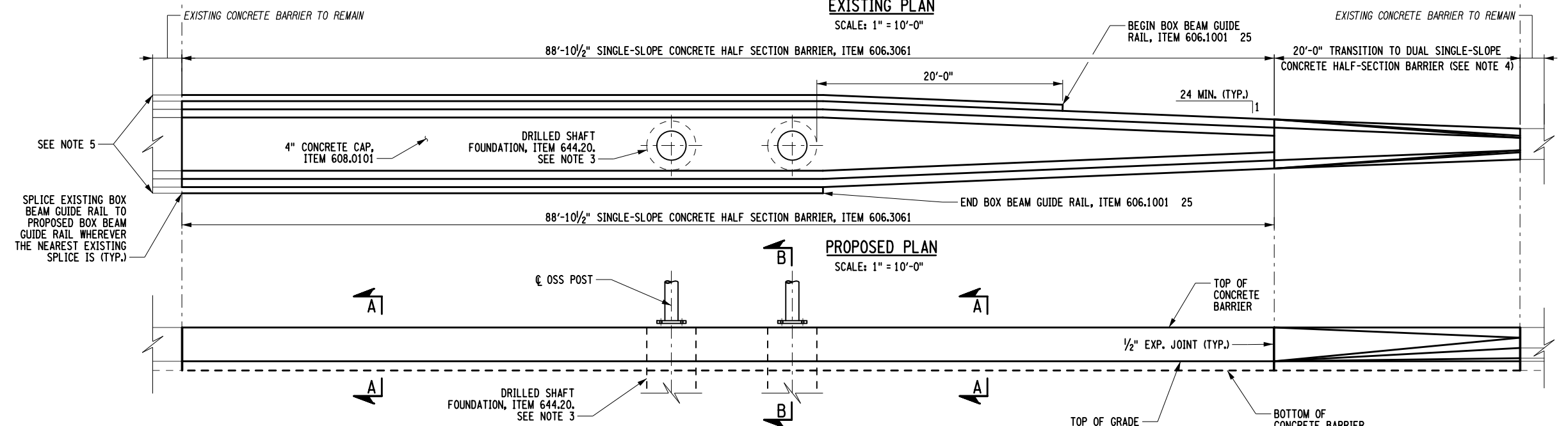
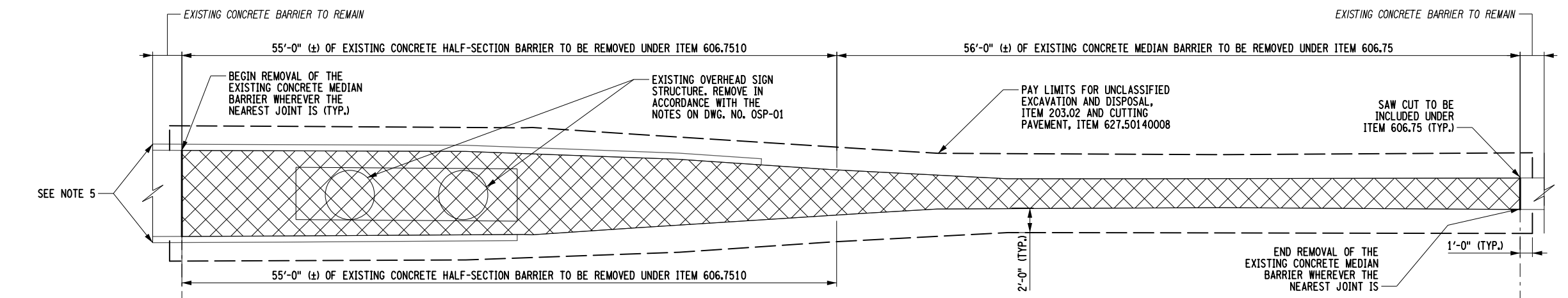
- FOR DETAILS OF ANCHOR BOLTS AND TEMPLATES SEE DWG. NO. OSD-11.
- IF UNANTICIPATED SOUND ROCK IS ENCOUNTERED WITHIN ONE DIAMETER OF THE BOTTOM OF A SHAFT EXCAVATION, USE THE FULL SHAFT LENGTH. IF UNANTICIPATED SOUND ROCK IS ENCOUNTERED AT HIGHER ELEVATIONS, THE TOTAL SHAFT LENGTH MAY BE DECREASED FROM THAT SHOWN, SUCH THAT THE SHAFT PENETRATES A MINIMUM OF ONE DIAMETER INTO SOUND ROCK. HOWEVER, THE TOTAL EMBEDMENT SHALL NOT BE LESS THAN ONE DIAMETER PLUS 2'-0". ALL CHANGES TO SHAFT LENGTHS MUST BE APPROVED AND AS ORDERED BY THE ENGINEER.
- ANY DRILLING OF THE ROCK FOR SHAFT FOUNDATIONS SHALL BE PERFORMED FROM THE EXISTING EARTH SURFACE THROUGH THE AUGER HOLE FORMED DURING THE DRILLING OPERATION. CASING SHALL BE REMOVED PRIOR TO PLACING CONCRETE.
- CONTACT THE NYSTA GEOTECHNICAL ENGINEER IF ANY OF THE FOLLOWING SOIL CONDITIONS ARE ENCOUNTERED DURING DRILLING:
  - A) THE SOIL HAS A HIGH ORGANIC CONTENT OR CONSISTS OF SATURATED SILT AND CLAY.
  - B) THE SITE WILL NOT SUPPORT THE WEIGHT OF THE DRILLING RIG.
  - C) FIRM BEDROCK IS ENCOUNTERED.
  - D) GROUNDWATER IS ABOVE BOTTOM OF SHAFT.
- LENGTH OF SHAFT MAY BE INCREASED AS ORDERED BY THE ENGINEER AS MUCH AS 1'-8" IF NECESSARY TO REACH GOOD FOUNDATION MATERIAL.
- EXCAVATION, BACKFILL, FORMWORK, REINFORCEMENT, CONDUIT STUB-OUT AND ALL OTHER LABOR AND MATERIALS NECESSARY TO CONSTRUCT THE FOUNDATIONS SHALL BE INCLUDED IN THE PRICE BID FOR THE FOUNDATIONS' CONCRETE.
- NEW DRILLED SHAFTS SHALL BE PAID FOR UNDER ITEM 644.20 - DRILLED SHAFT FOR OVERHEAD SIGN STRUCTURE.
- EXISTING FOUNDATIONS SHALL BE REMOVED TO 1'-0" BELOW EXISTING GRADE AND BURIED WITH SUITABLE EMBANKMENT AND SEEDED, ITEM 647.20.
- SUBMIT AN AS-BUILT SURVEY OF EACH DRILLED SHAFT FOUNDATION TO THE REPRESENTATIVE THAT IDENTIFIES ANCHOR BOLT LOCATION, ANCHOR BOLT ORIENTATION, TOP OF ANCHOR BOLT ELEVATIONS, TOP OF DRILLED SHAFT ELEVATIONS, AND ADJACENT FINISHED GROUND ELEVATIONS. INCLUDE A COPY OF THE SURVEY NOTES. RECONCILE ANY DIFFERENCES BETWEEN SURVEY INFORMATION AND DATA ON THE APPROVED SHOP DRAWINGS. SUBMIT ALL PROPOSED ADJUSTMENTS OR MODIFICATIONS TO THE REPRESENTATIVE FOR ACCEPTANCE.
- (G) DENOTES HOT-DIP GALVANIZED REINFORCEMENT.

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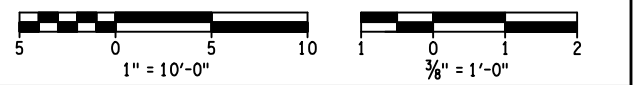
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING OVERHEAD SIGN STRUCTURES CANTILEVER STRUCTURES MISC. DETAILS (SHEET 4 OF 4)	DRAWING NUMBER: OSD-13



- NOTES:**
1. ALL SINGLE-SLOPE CONCRETE HALF SECTION BARRIER REINFORCEMENT SHALL BE IN ACCORDANCE WITH NYS DOT STANDARD SHEET 606-13.
  2. THE CONTRACTOR SHALL VERIFY DIMENSIONS OF EXISTING BARRIERS AND ROADWAYS PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL NOTICE THE ENGINEER IF ANY DISCREPANCY IS FOUND.
  3. SHAFT REINFORCEMENT SHALL BE PLACED IN ACCORDANCE WITH DWG. NO. OSD-03 FOR THE FULL HEIGHT OF THE SHAFT AND PAID FOR UNDER ITEM 644.20.
  4. TRANSITION BARRIERS AND EXPANSION JOINTS SHALL BE PAID UNDER 606.9001, SEE DWG. NO. OSD-15.
  5. NEW BOX BEAM GUIDE RAIL SHALL BE ATTACHED TO THE PROPOSED CONCRETE BARRIER AND SPLICED TO THE EXISTING RUN OF BOX BEAM GUIDE RAIL AT THE NEAREST EXISTING SPLICE. THE COST OF THE REMOVAL OF THE BOX BEAM GUIDE RAIL SHALL BE INCLUDED IN ITEM 606.7510.



CHECKED BY: M. BEDFORD  
 DRAFTED BY: S. LOFORTE  
 CHECKED BY: M. BEDFORD  
 DESIGNED BY: S. LOFORTE  
 DESIGN SUPERVISOR: M. BEDFORD

ALTERED ON:  
SIGNATURE:  
STAMP:

AFFIXED ON:  
**05/13/2024**  
SIGNATURE:  
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DATE	DESCRIPTION	BY	SYMBOL



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING OVERHEAD SIGN STRUCTURES BARRIER DETAILS LOCATION MP 900.38 (SHEET 1 OF 3)	DRAWING NUMBER: OSD-14



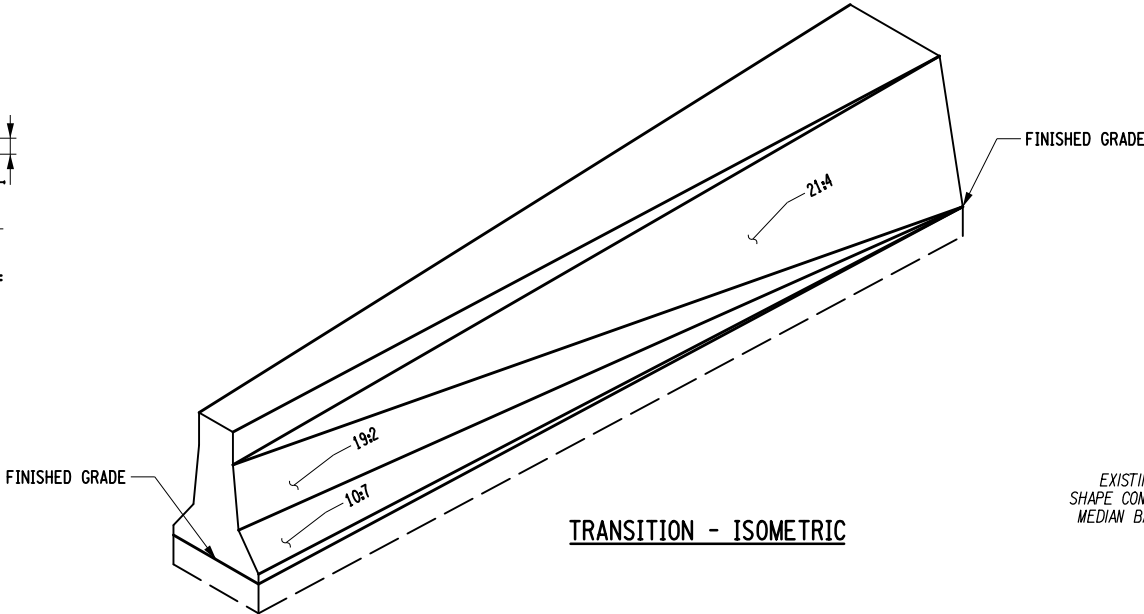
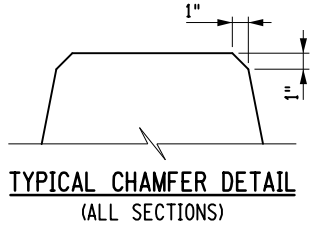
CHECKED BY: Q. WANG

DRAFTED BY: S. LOFORTE

CHECKED BY: Q. WANG

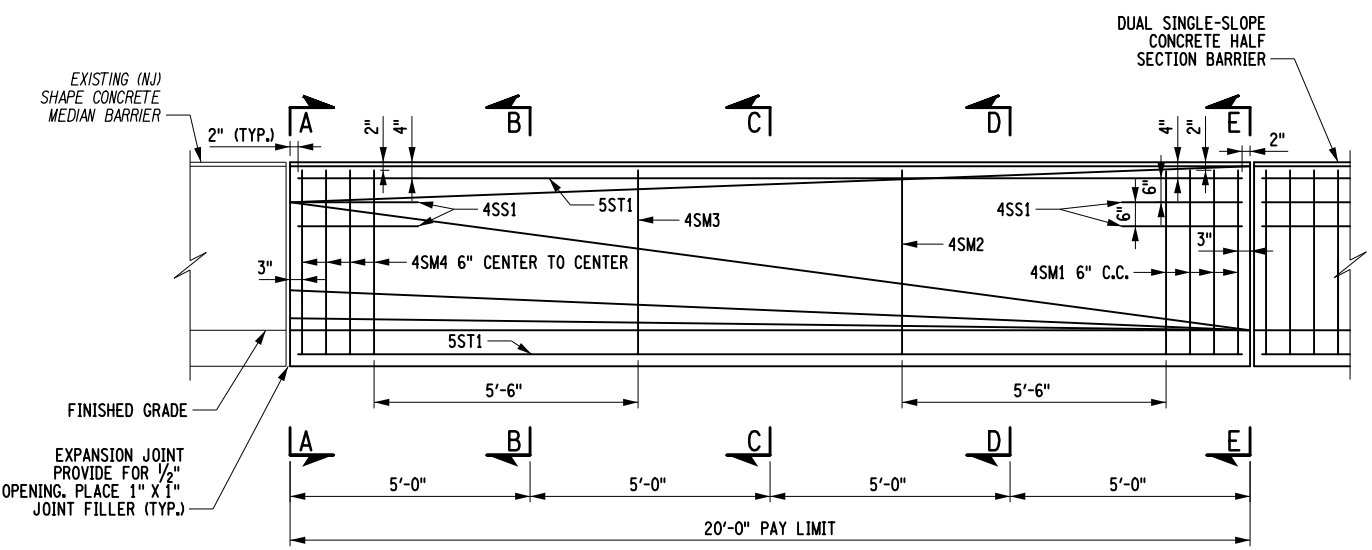
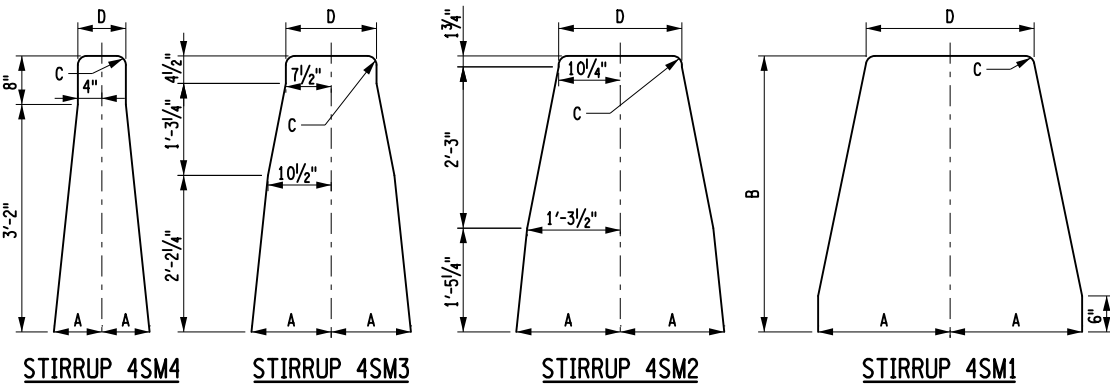
DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD

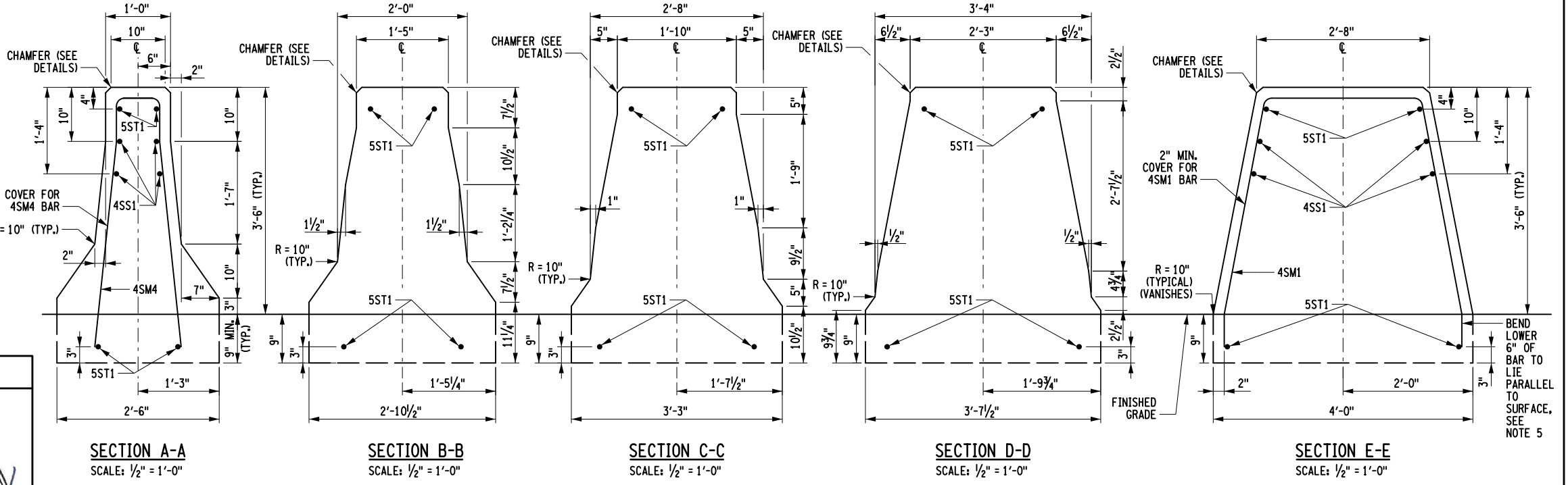
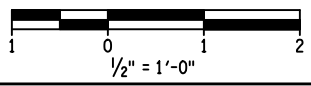


BAR LIST										
MARK	SIZE	NO.	LENGTH	USE	A	B	C	D	LOCATION	
4SM1	#4	4	10'-1/2"	STIRRUP	1'-10"	3'-10"	1 1/2"	2'-4"	4 AT 6" CENTERS AT END ADJACENT TO THE SINGLE SLOPE BARRIER	
4SM2	#4	1	9'-4 1/2"	STIRRUP	1'-5 1/4"	3'-10"	1 1/2"	1'-8 1/2"	5'-5 1/2" FROM 4SM1 BARS	
4SM3	#4	1	8'-10 3/4"	STIRRUP	1'-1 1/4"	3'-10"	1 1/2"	1'-3"	5'-5 1/2" FROM 4SM4 BARS	
4SM4	#4	4	8'-3 1/4"	STIRRUP	8"	3'-10"	1 1/2"	8"	4 AT 6" CENTERS AT END ADJACENT TO THE JERSEY BARRIER	
4SS1	#4	8	2'-6"	STRAIGHT	—	—	—	—	4 AT EACH END OF THE TRANSITION	
4ST1	#5	4	19'-8"	STRINGER	—	—	—	—	LONGITUDINAL 2 IN TOP - 2 IN BOTTOM	

BARS ARE TO BE EPOXY COATED GRADE 60, COST TO BE INCLUDED IN ITEM 606.9001



- NOTES:**
- SURFACES SHALL BE SMOOTH.
  - MATERIALS SHALL CONFORM TO §606-2.14, TRANSITIONS CONSTRUCTED OF, OR WITH, CONCRETE.
  - CONSTRUCTION SHALL CONFORM TO §606-3.19, TRANSITIONS CONSTRUCTED OF, OR WITH, CONCRETE.
  - UNLESS INDICATED OTHERWISE: EXCAVATION SHALL BE PERFORMED IN ACCORDANCE WITH §206-3.01\* GRANULAR BACKFILL SHALL CONFORM WITH §304 AND SHOULD MATCH THE SUBBASE COURSE TYPE USED ON THE ADJACENT ROADWAY.
  - BENDING OF BOTTOM OF STIRRUPS SHOWN IN SECTION E-E IS NOT NECESSARY, PROVIDED COVER REQUIREMENTS ARE SATISFIED.



ALTERED ON: AFFIXED ON: **05/13/2024**

SIGNATURE: STAMP:

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY** **Thruway Authority**

**PDG** POPPLI DESIGN GROUP

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 TITLE OF DRAWING: OVERHEAD SIGN STRUCTURES BARRIER DETAILS LOCATION MP 900.38 (SHEET 2 OF 3)

CONTRACT NUMBER: TAN 24-12  
 DATE: MAY 2024  
 DRAWING NUMBER: OSD-15

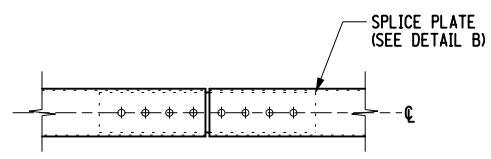
CHECKED BY: M. BEDFORD

DRAFTED BY: S. LOFORTE

CHECKED BY: M. BEDFORD

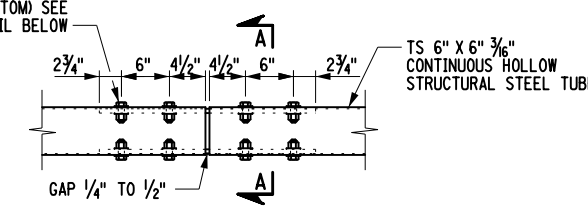
DESIGNED BY: S. LOFORTE

DESIGN SUPERVISOR: M. BEDFORD

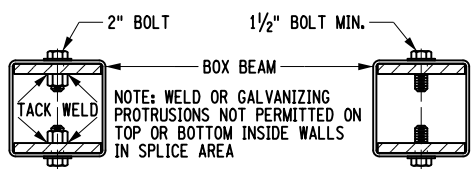


**TYPICAL SPLICE PLAN**  
NOT TO SCALE

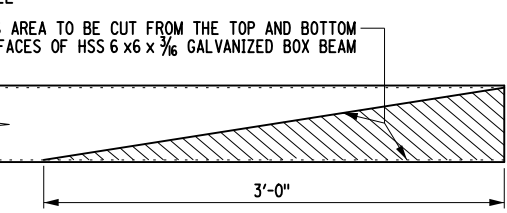
(8) - 3/4" BOLTS  
ASTM A325 WITH  
F436 WIDE WASHERS  
(TYP.) 1 1/8" Ø HOLES  
IN BOX BEAM (TOP  
AND BOTTOM) SEE  
DETAIL BELOW



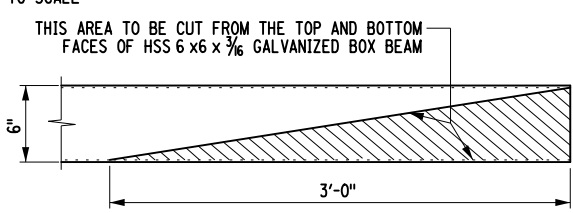
**TYPICAL SPLICE DETAIL**  
NOT TO SCALE



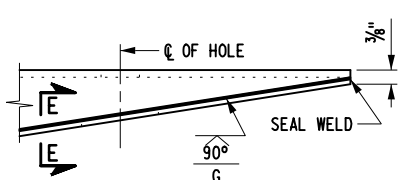
**SECTION A-A  
SPLICE PLATE SHOWING  
3/4" HEX NUTS WELDED  
TO PLATE**  
NOT TO SCALE



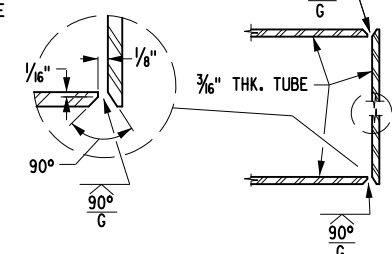
**SECTION A-A  
SPLICE PLATE SHOWING  
TAPPED BOLT HOLES**  
NOT TO SCALE



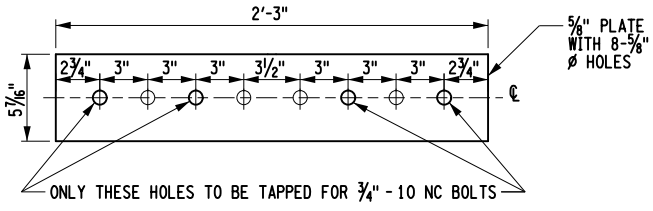
**CUT DETAIL - END CONNECTION**  
NOT TO SCALE



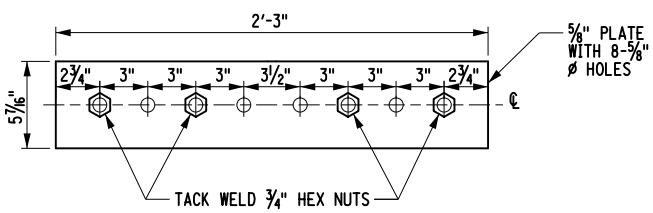
**DETAIL "A"**  
NOT TO SCALE



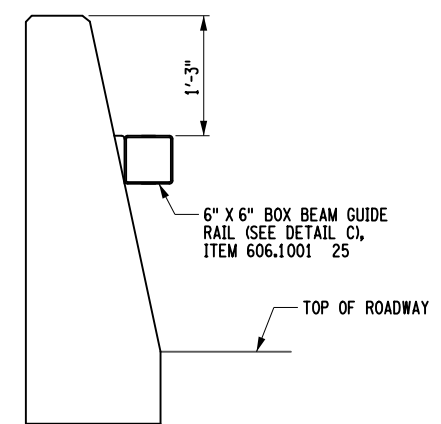
**SECTION E-E**  
NOT TO SCALE



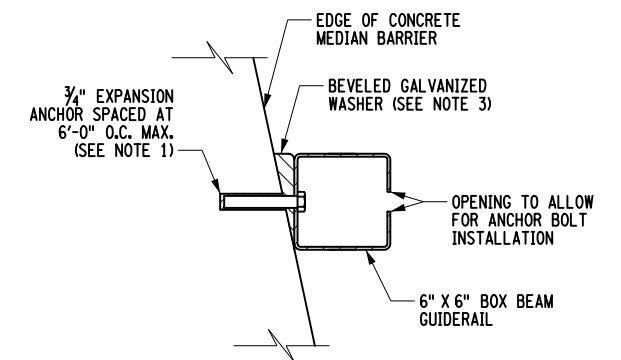
**DETAIL "B" SPLICE PLATE SHOWING  
TAPPED BOLT HOLES**  
NOT TO SCALE



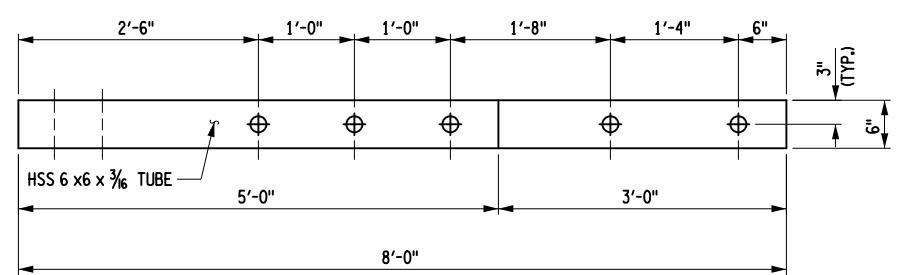
**DETAIL "B" SPLICE PLATE SHOWING 3/4"  
HEX NUTS WELDED TO PLATE**  
NOT TO SCALE



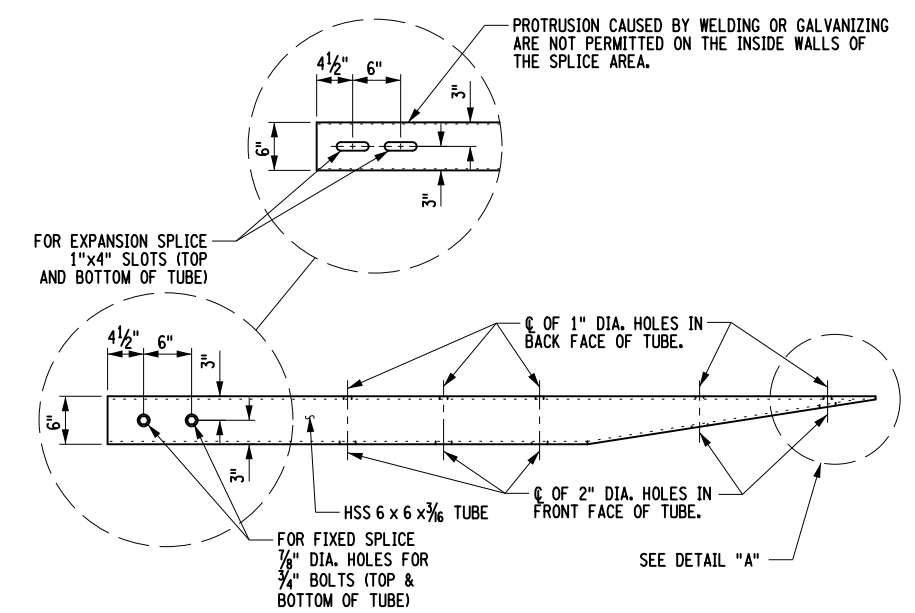
**GUIDE RAIL DETAIL**  
NOT TO SCALE



**DETAIL "C"**  
NOT TO SCALE



**ELEVATION**



**PLAN**

**END CONNECTION FOR BOX BEAM GUIDE RAIL**  
NOT TO SCALE

- NOTES:**
1. EXPANSION ANCHORS FOR THE 6" X 6" TUBE CONNECTIONS SHALL BE 3/4" Ø EXPANSION ANCHORS WITH A MINIMUM EMBEDMENT OF 3/4" AND A MINIMUM PULLOUT STRENGTH OF 13,000 LBS. DRILL HOLE SIZE AND METHOD SHALL BE SPECIFIED BY THE MANUFACTURER.
  2. THE COST OF THE ABOVE WORK INCLUDING DRILLING AND SUPPLYING THE NECESSARY ANCHORS, NUTS, BOLTS AND WASHERS SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 606.1001 25.
  3. GALVANIZED BEVELED WASHERS FOR MOUNTING OF GUIDE RAIL END SECTION WILL VARY IN THICKNESS. CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING THE CORRECT WASHERS.

ALTERED ON:  
SIGNATURE:  
STAMP:

AFFIXED ON:  
**05/13/2024**  
SIGNATURE:  
STAMP:

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT  
I-190 HIGHWAY REHABILITATION  
LOCATION OF PROJECT  
I-190 NIAGARA THRUWAY  
MP 900.7 TO MP 904.2  
TITLE OF DRAWING  
OVERHEAD SIGN STRUCTURES  
BARRIER DETAILS  
LOCATION MP 900.38  
(SHEET 3 OF 3)

CONTRACT NUMBER:  
TAN 24-12  
DATE:  
MAY 2024  
DRAWING NUMBER:  
OSD-16

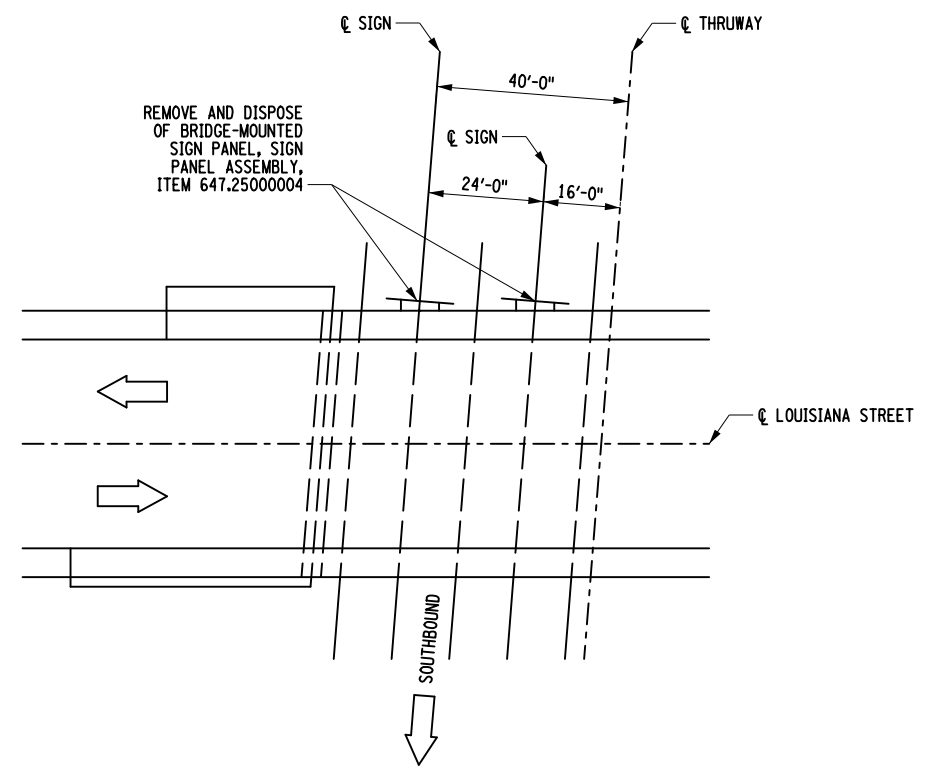
CHECKED BY: M. BEDFORD

DRAFTED BY: S. LOFORTE

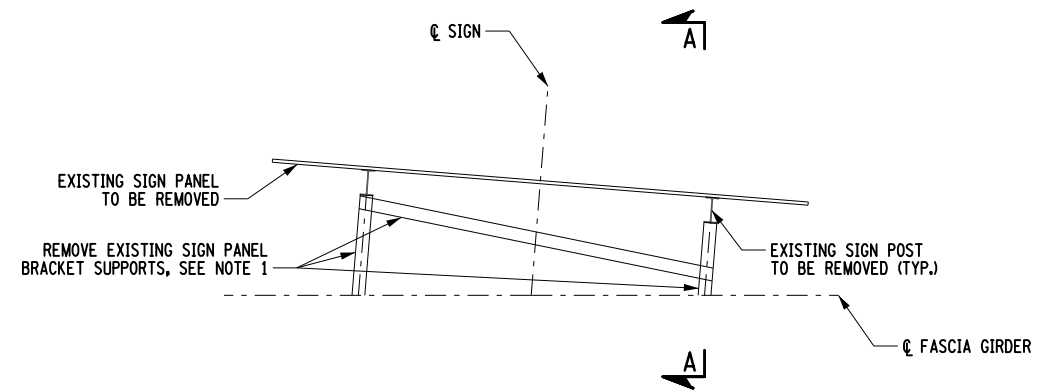
CHECKED BY: M. BEDFORD

DESIGNED BY: S. LOFORTE

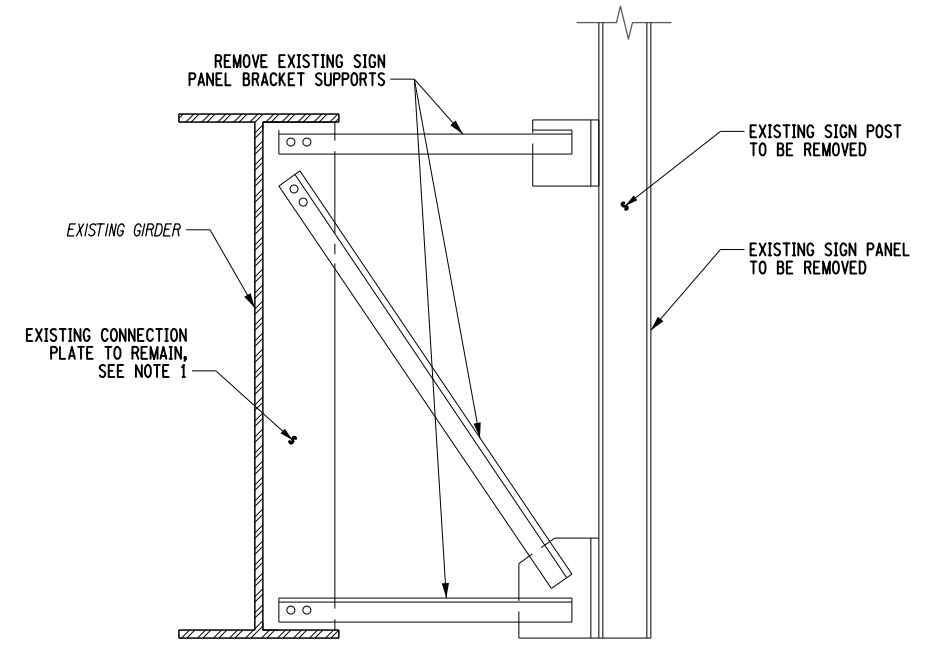
DESIGN SUPERVISOR: M. BEDFORD



**LOUISIANA STATE BRIDGE**  
SIGN LOCATION PLAN  
NOT TO SCALE



**BRIDGE MOUNTED SIGN PLAN**  
NOT TO SCALE



**SECTION A-A**  
NOT TO SCALE

**NOTES:**

1. ALL OPEN BOLT HOLES LEFT AFTER THE REMOVAL OF EXISTING SIGN STRUCTURES SHALL BE FILLED WITH A HIGH STRENGTH BOLT OF THE APPLICABLE SIZE IN ACCORDANCE WITH §715-14. TIGHTEN ALL SUCH BOLTS TO A SNUG TIGHT CONDITION. COST TO BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 647.25000004.

ALTERED ON:	AFFIXED ON: <b>05/13/2024</b>
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY  
**Thruway Authority**

**PDG**  
POPPLI DESIGN GROUP

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING BRIDGE MOUNTED SIGN REMOVAL DETAILS BIN 5512290	DRAWING NUMBER: OSD-17

BRIDGE ESTIMATE OF QUANTITIES

Table with columns: ITEM NUMBER, DESCRIPTION, UNIT, BRIDGE OVER (CLINTON ST, BAILEY AVE, SENECA ST, MILTON ST, BABCOCK ST, PEABODY ST), and sub-columns for NB, SB, TOTAL for each location.

BRIDGE ESTIMATE OF QUANTITIES (CONT'D)

Table with columns: ITEM NUMBER, DESCRIPTION, UNIT, BRIDGE OVER (SELKIRK ST, SMITH ST, ERIE RR, VAN RENSSLAER ST, CSX RR, HAMBURG ST), and sub-columns for NB, SB, TOTAL for each location, plus a TOTALS column.

ALTERED ON: 5/10/2024. SIGNATURE: [Signature]. STAMP: [Professional Engineer Seal for YOUNES DRIHM, LICENSED PROFESSIONAL ENGINEER, 100805, 5/10/24].

NOTES: 1. THE ESTIMATE OF QUANTITIES AND INDEX SHOWN ON THIS SHEET ARE FOR THE BRIDGE JOINT AND DECK REPAIR DRAWINGS ONLY. IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY...

BRIDGE INDEX OF DRAWINGS table with columns: SHEET NO., DWG. NO., DESCRIPTION. Rows include EQ-1, BT-1, BP-1 to BP-7, CS-1.

BRIDGE INDEX OF DRAWINGS (CONT'D) table with columns: SHEET NO., DWG. NO., DESCRIPTION. Rows include JD-1 to JD-7, BR-1, BL-1 to BL-10, ST-1 to ST-3.

REVISIONS table with columns: DATE, DESCRIPTION, BY, SYM.

NEW YORK STATE OF THRUWAY Authority logo and GMA logo.

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION. LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2. CONTRACT NUMBER: TAN 24-12. DATE: MAY 2024. DRAWING NUMBER: EQ-1.

CHECKED BY: Y DRIHM, S SCHUKRAFT. DRAFTED BY: S SCHUKRAFT. DESIGNED BY: S SCHUKRAFT. DESIGN SUPERVISOR: D MARSH.

CHECKED BY: Y. DRIHMI

DRAFTED BY: S. SCHUKRAFT

CHECKED BY: Y. DRIHMI

DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH

SUMMARY OF DECK AND APPROACH SLAB AREAS				
BRIDGE OVER	LOCATION	NB AREA (SF)	SB AREA (SF)	TOTAL (SF)
CLINTON ST M.P. 901.56 BIN 5046459	BEGIN APPROACH SLAB	2,056	2,056	4,112
	SPAN 1 DECK	2,278	2,278	21,540
	SPAN 2 DECK	5,878	5,878	
	SPAN 3 DECK	2,614	2,614	
	END APPROACH SLAB	2,056	2,056	4,112
SENECA ST M.P. 902.22 BIN 5011959	BEGIN APPROACH SLAB	2,465	2,465	4,930
	SPAN 1 DECK	3,135	3,135	25,546
	SPAN 2 DECK	6,303	6,303	
	SPAN 3 DECK	3,335	3,335	
	END APPROACH SLAB	2,465	2,465	
MILTON ST M.P. 902.44 BIN 5512209	BEGIN APPROACH SLAB	2,209	1,602	3,811
	SPAN 1 DECK	1,953	1,780	12,886
	SPAN 2 DECK	2,910	2,652	
	SPAN 3 DECK	1,879	1,712	
	END APPROACH SLAB	2,209	1,602	
BABCOCK ST M.P. 902.63 BIN 5512219	BEGIN APPROACH SLAB	1,602	1,602	3,204
	SPAN 1 DECK	1,720	1,720	13,834
	SPAN 2 DECK	3,445	3,445	
	SPAN 3 DECK	1,752	1,752	
	END APPROACH SLAB	1,602	1,602	
PEABODY ST M.P. 902.84 BIN 5512229	BEGIN APPROACH SLAB	1,490	1,490	2,980
	SPAN 1 DECK	1,655	1,655	12,462
	SPAN 2 DECK	2,673	2,673	
	SPAN 3 DECK	1,903	1,903	
	END APPROACH SLAB	1,490	1,490	
SMITH ST M.P. 903.14 BIN 5512249	BEGIN APPROACH SLAB	2,027	2,027	4,054
	SPAN 1 DECK	2,176	2,176	17,198
	SPAN 2 DECK	4,423	4,423	
	SPAN 3 DECK	2,000	2,000	
	END APPROACH SLAB	2,027	2,027	
ERIE RR. M.P. 903.32 BIN 5512259	BEGIN APPROACH SLAB	2,455	1,741	4,196
	SPAN 1 DECK	2,868	2,614	17,746
	SPAN 2 DECK	3,548	3,234	
	SPAN 3 DECK	2,868	2,614	
	END APPROACH SLAB	2,455	1,741	
VAN RENSSELAER ST M.P. 903.48 BIN 5512269	BEGIN APPROACH SLAB	1,467	1,467	2,934
	SPAN 1 DECK	1,871	1,871	12,832
	SPAN 2 DECK	2,574	2,574	
	SPAN 3 DECK	1,971	1,971	
	END APPROACH SLAB	1,467	1,467	
CSX RR M.P. 903.66 BIN 5512279	BEGIN APPROACH SLAB	1,765	1,765	3,530
	SPAN 1 DECK	2,702	2,702	24,800
	SPAN 2 DECK	3,026	3,026	
	SPAN 3 DECK	4,154	4,154	
	SPAN 4 DECK	2,518	2,518	
END APPROACH SLAB	1,765	1,765	3,530	
HAMBURG ST M.P. 903.79 BIN 5512289	BEGIN APPROACH SLAB	1,924	1,442	3,366
	SPAN 1 DECK	2,316	2,111	15,660
	SPAN 2 DECK	3,666	3,341	
	SPAN 3 DECK	2,211	2,015	
	END APPROACH SLAB	1,924	1,442	

SUMMARY OF DECK AND APPROACH SLAB AREAS				
BRIDGE OVER	LOCATION	NB AREA (SF)	SB AREA (SF)	TOTAL (SF)
BAILEY AVE & NS RR M.P. 901.88 BIN 5512199	BEGIN APPROACH SLAB	1,739	1,734	3,473
	SPAN 1 DECK	3,956	3,454	97,480
	SPAN 2 DECK	4,700	4,200	
	SPAN 3 DECK	3,395	3,539	
	SPAN 4 DECK	3,924	3,570	
	SPAN 5 DECK	3,902	0	
	SPAN 6 DECK	3,137	3,276	
	SPAN 7 DECK	6,494	5,959	
	SPAN 8 DECK	0	3,759	
	SPAN 9 DECK	3,262	3,806	
	SPAN 10 DECK	0	3,755	
	SPAN 11 DECK	4,788	3,837	
	SPAN 12 DECK	0	6,311	
	SPAN 13 DECK	11,348	3,108	
END APPROACH SLAB	2,460	2,443	4,903	
SELKIRK ST & RR YARD M.P. 902.99 BIN 5512239	BEGIN APPROACH SLAB	1,589	1,589	3,178
	SPAN 1 DECK	2,662	2,913	59,172
	SPAN 2 DECK	4,961	5,427	
	SPAN 3 DECK	5,130	5,612	
	SPAN 4 DECK	5,180	5,667	
	SPAN 5 DECK	4,421	4,837	
	SPAN 6 DECK	3,573	3,217	
	SPAN 7 DECK	2,661	2,911	
END APPROACH SLAB	1,440	1,440	2,880	

THE DECK AND APPROACH SLAB AREAS SHOWN IN THESE TABLES ARE APPROXIMATE AND ARE SHOWN TO HELP FACILITATE BIDDING. THE CONTRACTOR WILL BE RESPONSIBLE FOR FIELD VERIFYING ALL PERTINANT DIMENSIONS.

FOR THE PURPOSE OF ESTIMATING QUANTITIES, THE AREAS SHOWN REPRESENT THE "EFFECTIVE" DECK AREA, AND EXCLUDE THE FOOTPRINT OF THE CONCRETE BARRIERS.

SHEET-APPLIED WATERPROOFING MEMBRANE				
ITEM 595.5000018				
BRIDGE OVER	LOCATION	NB AREA (SF)	SB AREA (SF)	TOTAL (SF)
CLINTON ST M.P. 901.56 BIN 5046459	BEGIN APPROACH SLAB	-	-	21,540
	SPAN 1 DECK	2,278	2,278	
	SPAN 2 DECK	5,878	5,878	
	SPAN 3 DECK	2,614	2,614	
	END APPROACH SLAB	-	-	
SENECA ST M.P. 902.22 BIN 5011959	BEGIN APPROACH SLAB	-	-	25,546
	SPAN 1 DECK	3,135	3,135	
	SPAN 2 DECK	6,303	6,303	
	SPAN 3 DECK	3,335	3,335	
	END APPROACH SLAB	-	-	
MILTON ST M.P. 902.44 BIN 5512209	BEGIN APPROACH SLAB	-	-	12,886
	SPAN 1 DECK	1,953	1,780	
	SPAN 2 DECK	2,910	2,652	
	SPAN 3 DECK	1,879	1,712	
	END APPROACH SLAB	-	-	
BABCOCK ST M.P. 902.63 BIN 5512219	BEGIN APPROACH SLAB	-	-	13,834
	SPAN 1 DECK	1,720	1,720	
	SPAN 2 DECK	3,445	3,445	
	SPAN 3 DECK	1,752	1,752	
	END APPROACH SLAB	-	-	
PEABODY ST M.P. 902.84 BIN 5512229	BEGIN APPROACH SLAB	-	-	12,462
	SPAN 1 DECK	1,655	1,655	
	SPAN 2 DECK	2,673	2,673	
	SPAN 3 DECK	1,903	1,903	
	END APPROACH SLAB	-	-	
SMITH ST M.P. 903.14 BIN 5512249	BEGIN APPROACH SLAB	-	-	17,198
	SPAN 1 DECK	2,176	2,176	
	SPAN 2 DECK	4,423	4,423	
	SPAN 3 DECK	2,000	2,000	
	END APPROACH SLAB	-	-	
ERIE RR. M.P. 903.32 BIN 5512259	BEGIN APPROACH SLAB	-	-	17,746
	SPAN 1 DECK	2,868	2,614	
	SPAN 2 DECK	3,548	3,234	
	SPAN 3 DECK	2,868	2,614	
	END APPROACH SLAB	-	-	
VAN RENSSELAER ST M.P. 903.48 BIN 5512269	BEGIN APPROACH SLAB	-	-	12,832
	SPAN 1 DECK	1,871	1,871	
	SPAN 2 DECK	2,574	2,574	
	SPAN 3 DECK	1,971	1,971	
	END APPROACH SLAB	-	-	
CSX RR M.P. 903.66 BIN 5512279	BEGIN APPROACH SLAB	-	-	24,800
	SPAN 1 DECK	2,702	2,702	
	SPAN 2 DECK	3,026	3,026	
	SPAN 3 DECK	4,154	4,154	
	SPAN 4 DECK	2,518	2,518	
END APPROACH SLAB	-	-		
HAMBURG ST M.P. 903.79 BIN 5512289	BEGIN APPROACH SLAB	-	-	15,660
	SPAN 1 DECK	2,316	2,111	
	SPAN 2 DECK	3,666	3,341	
	SPAN 3 DECK	2,211	2,015	
	END APPROACH SLAB	-	-	
<b>TOTAL</b>				<b>174,504</b>

THE DECK AREAS SHOWN IN THIS TABLE ARE APPROXIMATE AND ARE SHOWN TO HELP FACILITATE BIDDING. THE CONTRACTOR WILL BE RESPONSIBLE FOR FIELD VERIFYING ALL PERTINANT DIMENSIONS.

FOR THE PURPOSE OF ESTIMATING QUANTITIES, THE AREAS SHOWN REPRESENT THE "EFFECTIVE" DECK AREA, AND EXCLUDE THE FOOTPRINT OF THE CONCRETE BARRIERS.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

STATE OF NEW YORK  
YOUNES DRIHMI  
LICENSED PROFESSIONAL ENGINEER  
100805  
5/10/24

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING BRIDGE TABLES	DRAWING NUMBER: BT-1

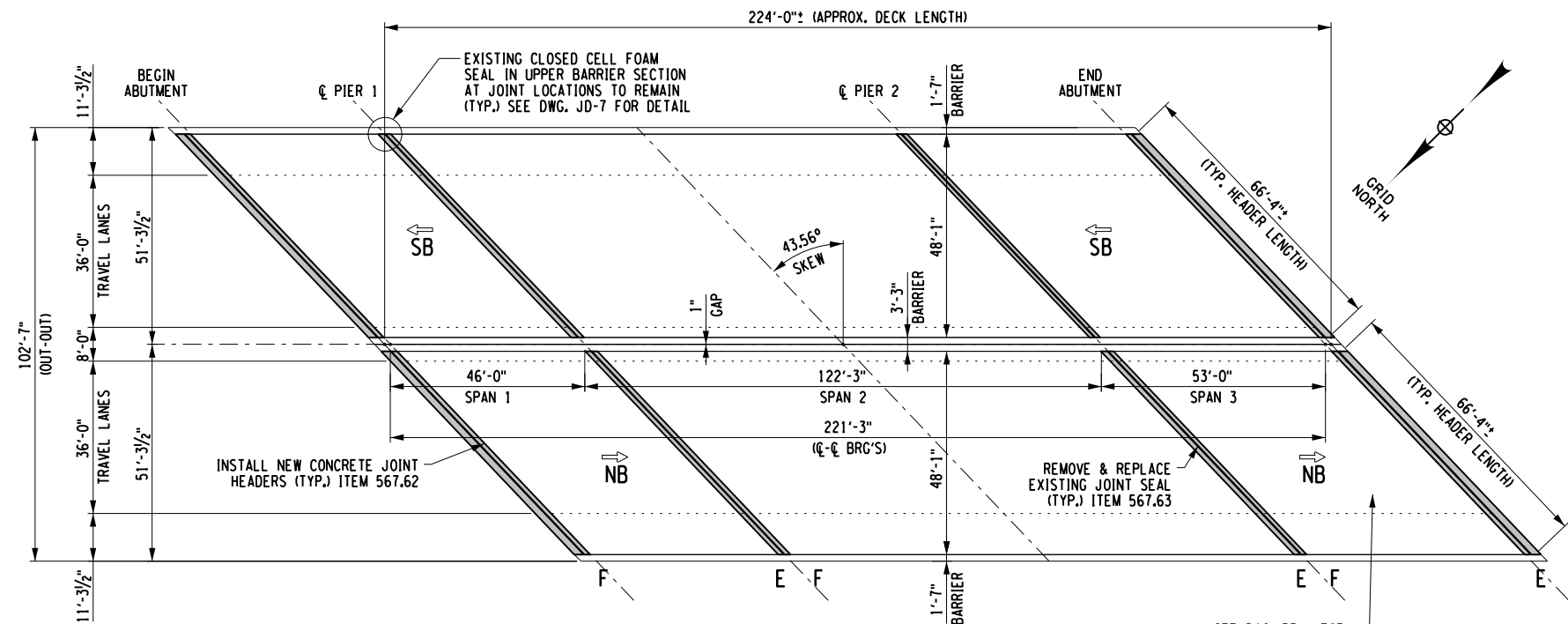
CHECKED BY: Y DRIHMI

DRAFTED BY: S SCHUKRAFT

CHECKED BY: Y DRIHMI

DESIGNED BY: S SCHUKRAFT

DESIGN SUPERVISOR: D MARSH



JOINT HEADER INSTALLATION - ITEM 567.62; (NB) 275' + (SB) 275' = 550'  
 JOINT SEAL REMOVAL & REPLACEMENT - ITEM 567.63; (NB) 275' + (SB) 275' = 550'  
 FOR JOINT REPAIR DETAILS, SEE DWG'S JD-3 TO JD-7.  
 FOR PARTIAL-DEPTH CONCRETE DECK REPAIRS, SEE DWG. BR-1.

**BRIDGE OVER CLINTON STREET**  
**PLAN VIEW M.P. 901.56**  
 SCALE: 1" = 40'

SEE DWG. BR-1 FOR  
 TYPICAL BRIDGE DECK  
 REPAIR DETAILS

E = EXPANSION  
 F = FIXED

**PIER 1**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 1/8
35	1 1/8
40	1 1/8
45	1 1/8
50	1 1/8
55	1 1/8
60	1 1/8
65	1 1/8
70	1 1/8
75	1 1/8
80	1 1/8
85	1
90	1
95	1
100	1
105	1
110	1

NOTES:  
 MIN. OPENING (IN.) = 1  
 MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.375  
 TOTAL MOVEMENT (IN.) = 0.5

**PIER 2**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 3/4
35	1 3/4
40	1 3/4
45	1 5/8
50	1 5/8
55	1 1/2
60	1 1/2
65	1 1/2
70	1 1/2
75	1 3/8
80	1 3/8
85	1 1/4
90	1 1/4
95	1 1/8
100	1 1/8
105	1 1/8
110	1 1/8

NOTES:  
 MIN. OPENING (IN.) = 1.125  
 MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 2.25  
 TOTAL MOVEMENT (IN.) = 1.5

**END ABUTMENT**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 1/4
35	1 1/4
40	1 1/4
45	1 1/8
50	1 1/8
55	1 1/8
60	1 1/8
65	1 1/8
70	1 1/8
75	1 1/8
80	1 1/8
85	1 1/8
90	1 1/8
95	1
100	1
105	1
110	1

NOTES:  
 MIN. OPENING (IN.) = 1  
 MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.5  
 TOTAL MOVEMENT (IN.) = 0.625

- NOTES:
1. THE "JOINT OPENING" SHALL BE DETERMINED AT THE TIME OF PLACEMENT, USING THE TABLES PROVIDED ON THIS SHEET.
  2. THE SUPERSTRUCTURE TEMPERATURE USED TO DETERMINE THE JOINT OPENING SHALL BE THE AVERAGE TEMPERATURE OF THE BOTTOM OF THE GIRDER TOP FLANGES. A MINIMUM OF TWO TEMPERATURE READINGS PER SPAN SHALL BE USED TO CALCULATE THE AVERAGE SUPERSTRUCTURE TEMPERATURE.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING BRIDGE PLAN (1 OF 7)	DRAWING NUMBER: BP-1

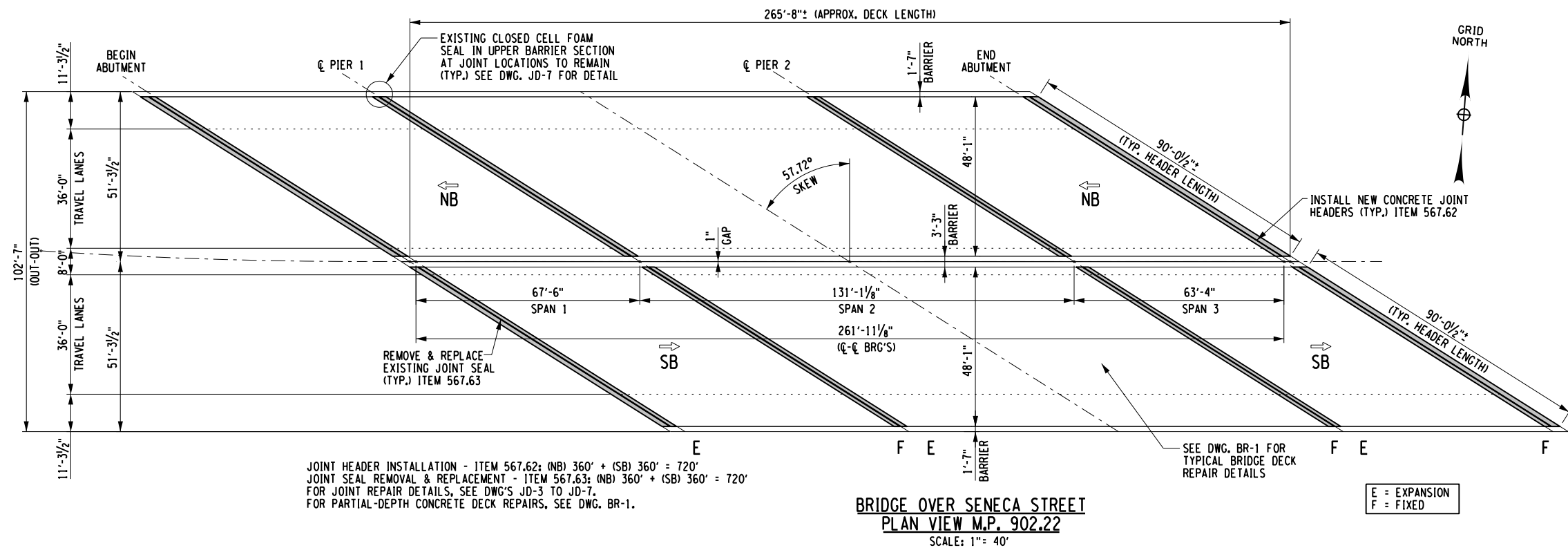
CHECKED BY: Y. DRIHMI

DRAFTED BY: S. SCHUKRAFT

CHECKED BY: Y. DRIHMI

DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH



**BEGIN ABUTMENT**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 3/8
35	1 3/8
40	1 3/8
45	1 3/8
50	1 3/8
55	1 3/8
60	1 3/8
65	1 3/8
70	1 1/4
75	1 1/4
80	1 1/4
85	1 1/4
90	1 1/8
95	1 1/8
100	1 1/8
105	1 1/8
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.75  
TOTAL MOVEMENT (IN.) = 0.875

**PIER 1**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	2 1/4
35	2 1/4
40	2 1/8
45	2 1/8
50	2
55	2
60	1 7/8
65	1 7/8
70	1 3/4
75	1 3/4
80	1 3/4
85	1 3/4
90	1 5/8
95	1 5/8
100	1 1/2
105	1 1/2
110	1 3/8

NOTES:  
MIN. OPENING (IN.) = 1.375  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 2.75  
TOTAL MOVEMENT (IN.) = 1.625

**PIER 2**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 3/8
35	1 3/8
40	1 3/8
45	1 3/8
50	1 3/8
55	1 1/4
60	1 1/4
65	1 1/4
70	1 1/4
75	1 1/8
80	1 1/8
85	1 1/8
90	1 1/8
95	1 1/8
100	1
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.75  
TOTAL MOVEMENT (IN.) = 0.75

- NOTES:**
1. THE "JOINT OPENING" SHALL BE DETERMINED AT THE TIME OF PLACEMENT, USING THE TABLES PROVIDED ON THIS SHEET.
  2. THE SUPERSTRUCTURE TEMPERATURE USED TO DETERMINE THE JOINT OPENING SHALL BE THE AVERAGE TEMPERATURE OF THE BOTTOM OF THE GIRDER TOP FLANGES. A MINIMUM OF TWO TEMPERATURE READINGS PER SPAN SHALL BE USED TO CALCULATE THE AVERAGE SUPERSTRUCTURE TEMPERATURE.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY**  
**Thruway Authority**

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING BRIDGE PLAN (2 OF 7)	DRAWING NUMBER: BP-2

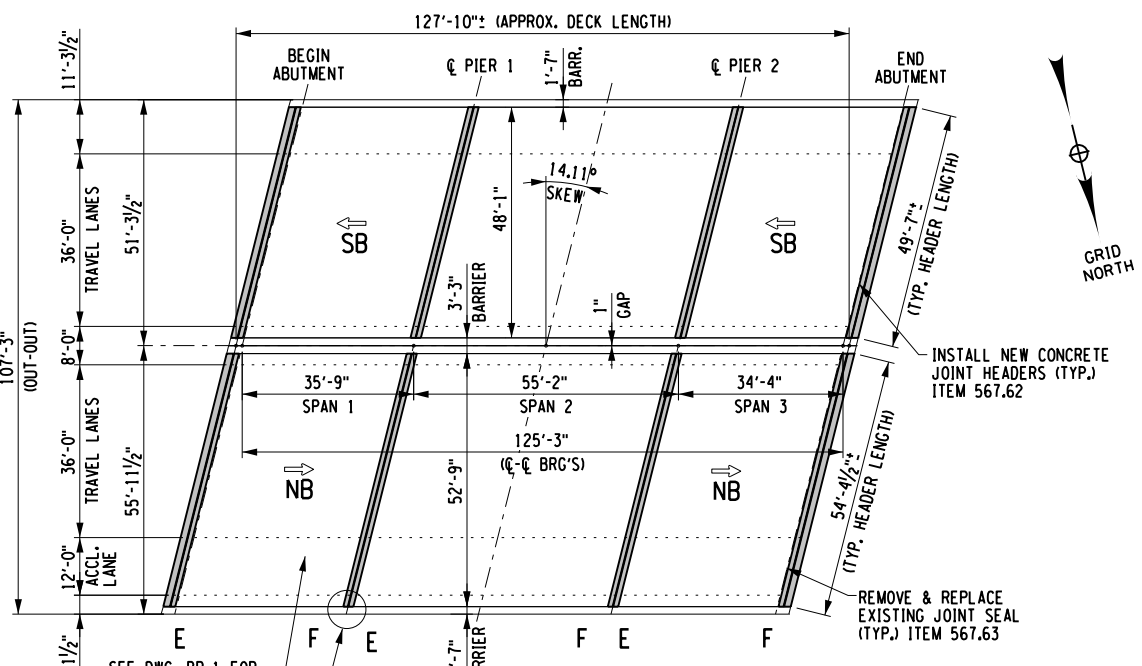
CHECKED BY: Y. DRIHMI

DRAFTED BY: S. SCHUKRAFT

CHECKED BY: Y. DRIHMI

DRAFTED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH



**BRIDGE OVER MILTON STREET**  
PLAN VIEW M.P. 902.44  
SCALE: 1" = 40'

JOINT HEADER INSTALLATION - ITEM 567.62; (NB) 218' + (SB) 199' = 417'  
JOINT SEAL REMOVAL & REPLACEMENT - ITEM 567.63; (NB) 218' + (SB) 199' = 417'  
FOR JOINT REPAIR DETAILS, SEE DWG'S JD-3 TO JD-7.  
FOR PARTIAL-DEPTH CONCRETE DECK REPAIRS, SEE DWG. BR-1.

**BEGIN ABUTMENT**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 1/4
35	1 1/4
40	1 1/8
45	1 1/8
50	1 1/8
55	1 1/8
60	1 1/8
65	1 1/8
70	1 1/8
75	1
80	1
85	1
90	1
95	1
100	1
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.375  
TOTAL MOVEMENT (IN.) = 0.375

**PIER 1**

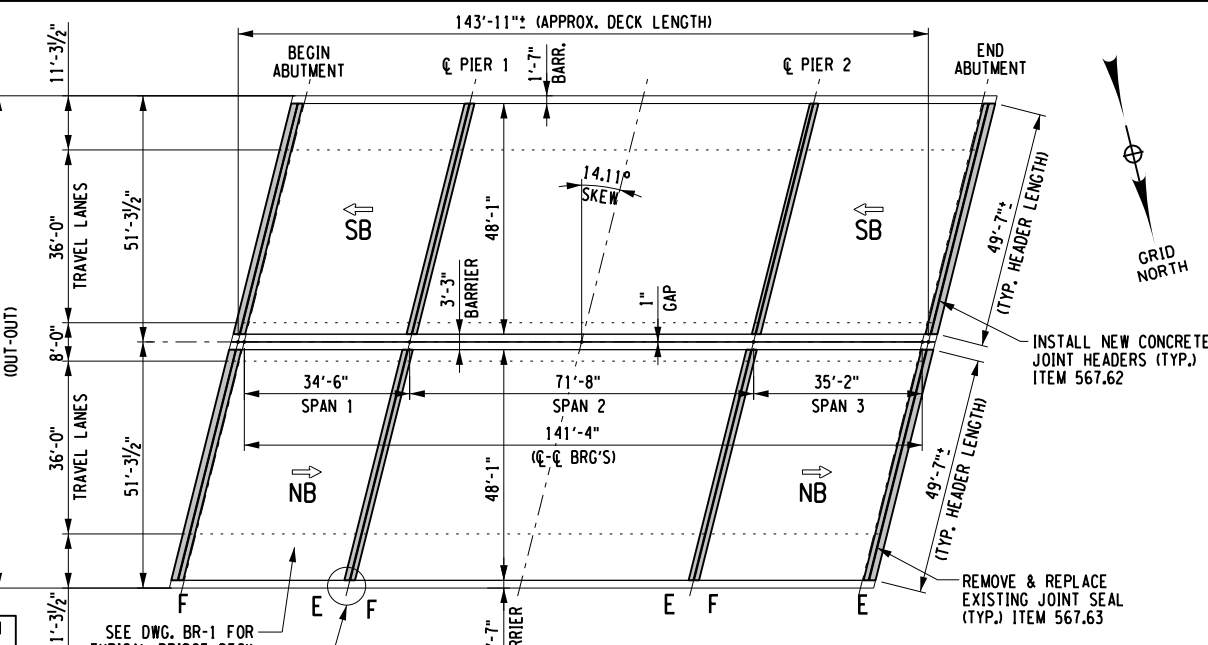
EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 3/8
35	1 3/8
40	1 3/8
45	1 1/4
50	1 1/4
55	1 1/4
60	1 1/4
65	1 1/8
70	1 1/8
75	1 1/8
80	1 1/8
85	1 1/8
90	1
95	1
100	1
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.625  
TOTAL MOVEMENT (IN.) = 0.625

**PIER 2**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 1/4
35	1 1/4
40	1 1/8
45	1 1/8
50	1 1/8
55	1 1/8
60	1 1/8
65	1 1/8
70	1 1/8
75	1 1/8
80	1
85	1
90	1
95	1
100	1
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.375  
TOTAL MOVEMENT (IN.) = 0.375



**BRIDGE OVER BABCOCK STREET**  
PLAN VIEW M.P. 902.63  
SCALE: 1" = 40'

JOINT HEADER INSTALLATION - ITEM 567.62; (NB) 199' + (SB) 199' = 398'  
JOINT SEAL REMOVAL & REPLACEMENT - ITEM 567.63; (NB) 199' + (SB) 199' = 398'  
FOR JOINT REPAIR DETAILS, SEE DWG'S JD-3 TO JD-7.  
FOR PARTIAL-DEPTH CONCRETE DECK REPAIRS, SEE DWG. BR-1.

**PIER 1**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 1/4
35	1 1/4
40	1 1/8
45	1 1/8
50	1 1/8
55	1 1/8
60	1 1/8
65	1 1/8
70	1 1/8
75	1 1/8
80	1
85	1
90	1
95	1
100	1
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.375  
TOTAL MOVEMENT (IN.) = 0.375

**PIER 2**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 1/2
35	1 1/2
40	1 1/2
45	1 1/2
50	1 3/8
55	1 3/8
60	1 3/8
65	1 1/4
70	1 1/4
75	1 1/4
80	1 1/4
85	1 1/8
90	1 1/8
95	1 1/8
100	1 1/8
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.875  
TOTAL MOVEMENT (IN.) = 0.875

**END ABUTMENT**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 1/4
35	1 1/4
40	1 1/8
45	1 1/8
50	1 1/8
55	1 1/8
60	1 1/8
65	1 1/8
70	1 1/8
75	1
80	1
85	1
90	1
95	1
100	1
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.375  
TOTAL MOVEMENT (IN.) = 0.375

- NOTES:
1. THE "JOINT OPENING" SHALL BE DETERMINED AT THE TIME OF PLACEMENT, USING THE TABLES PROVIDED ON THIS SHEET.
  2. THE SUPERSTRUCTURE TEMPERATURE USED TO DETERMINE THE JOINT OPENING SHALL BE THE AVERAGE TEMPERATURE OF THE BOTTOM OF THE GIRDER TOP FLANGES. A MINIMUM OF TWO TEMPERATURE READINGS PER SPAN SHALL BE USED TO CALCULATE THE AVERAGE SUPERSTRUCTURE TEMPERATURE.

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

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REVISIONS

DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING BRIDGE PLAN (3 OF 7)	DRAWING NUMBER: BP-3



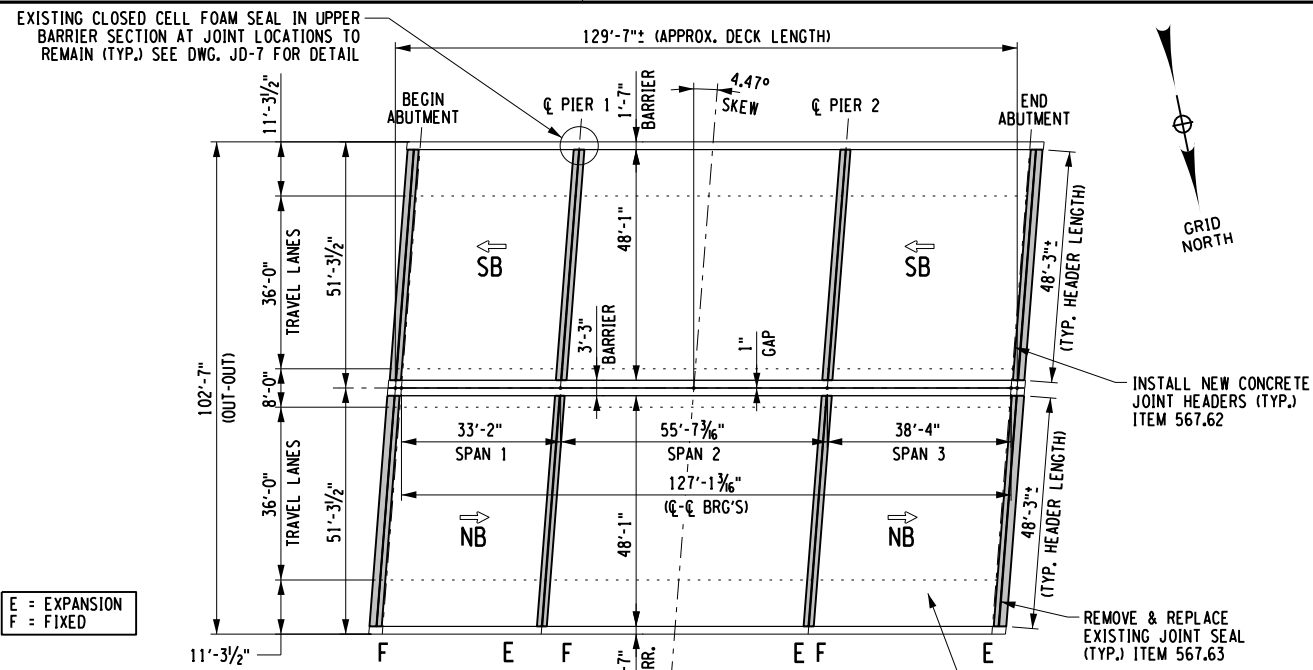
CHECKED BY: Y. DRIHMI

DRAFTED BY: S. SCHUKRAFT

CHECKED BY: Y. DRIHMI

DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH



JOINT HEADER INSTALLATION - ITEM 567.62; (NB) 193' + (SB) 193' = 386'  
 JOINT SEAL REMOVAL & REPLACEMENT - ITEM 567.63; (NB) 193' + (SB) 193' = 386'  
 FOR JOINT REPAIR DETAILS, SEE DWG'S JD-3 TO JD-7.  
 FOR PARTIAL-DEPTH CONCRETE DECK REPAIRS, SEE DWG. BR-1.

BRIDGE OVER PEABODY STREET  
 PLAN VIEW M.P. 902.84

SCALE: 1" = 40'

PIER 1

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 1/4
35	1 1/4
40	1 1/4
45	1 1/8
50	1 1/8
55	1 1/8
60	1 1/8
65	1 1/8
70	1 1/8
75	1 1/8
80	1 1/8
85	1
90	1
95	1
100	1
105	1
110	1

NOTES:  
 MIN. OPENING (IN.) = 1  
 MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.375  
 TOTAL MOVEMENT (IN.) = 0.375

PIER 2

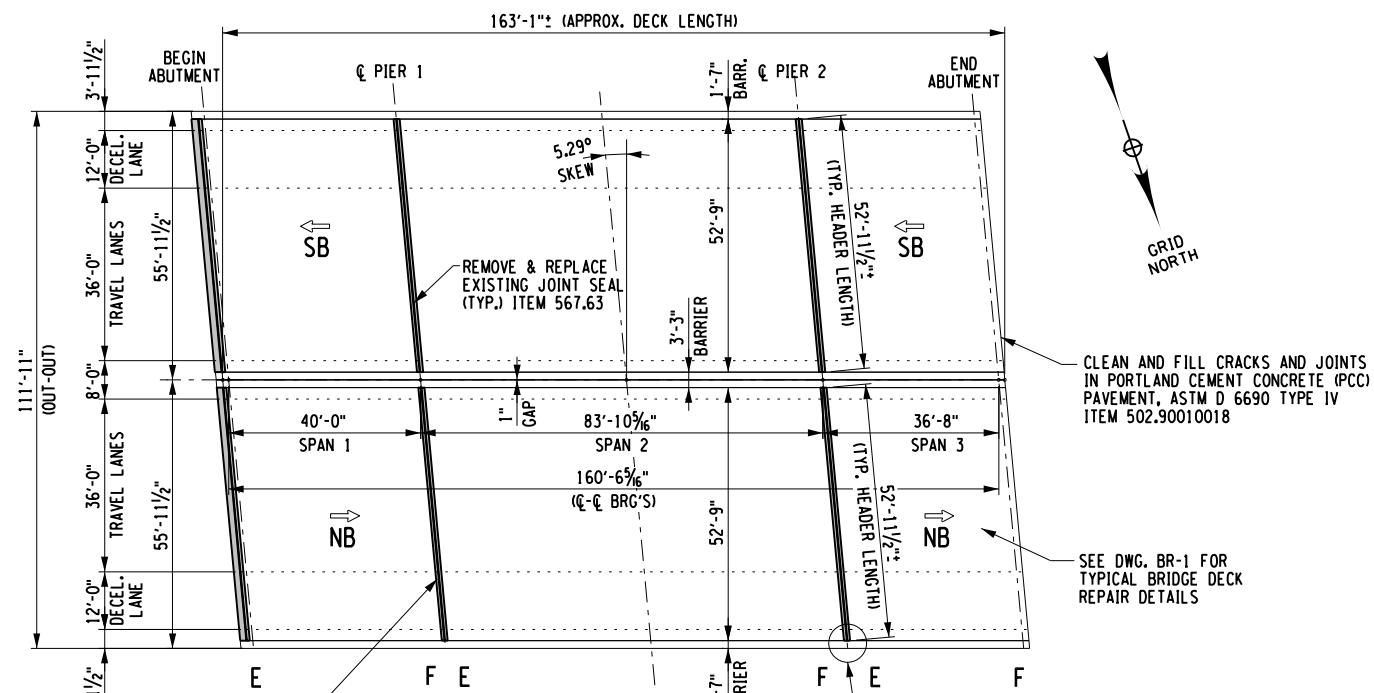
EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 3/8
35	1 3/8
40	1 3/8
45	1 1/4
50	1 1/4
55	1 1/4
60	1 1/4
65	1 1/8
70	1 1/8
75	1 1/8
80	1 1/8
85	1 1/8
90	1
95	1
100	1
105	1
110	1

NOTES:  
 MIN. OPENING (IN.) = 1  
 MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.625  
 TOTAL MOVEMENT (IN.) = 0.625

END ABUTMENT

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 3/8
35	1 1/4
40	1 1/4
45	1 1/4
50	1 1/4
55	1 1/4
60	1 1/4
65	1 1/4
70	1 1/8
75	1 1/8
80	1 1/8
85	1 1/8
90	1 1/8
95	1 1/8
100	1 1/8
105	1
110	1

NOTES:  
 MIN. OPENING (IN.) = 1  
 MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.5  
 TOTAL MOVEMENT (IN.) = 0.5



INSTALL NEW CONCRETE JOINT HEADERS (TYP.) ITEM 567.62

BRIDGE OVER SMITH STREET  
 PLAN VIEW M.P. 903.14

SCALE: 1" = 40'

JOINT HEADER INSTALLATION ITEM 567.62; (NB) 159' + (SB) 159' = 318'  
 JOINT SEAL REMOVAL & REPLACEMENT - ITEM 567.63; (NB) 159' + (SB) 159' = 318'  
 CLEAN AND FILL JOINT - ITEM 502.90010018; (NB) 159' + (SB) 159' = 318'  
 FOR JOINT REPAIR DETAILS, SEE DWG'S JD-3 TO JD-7.  
 FOR PARTIAL-DEPTH CONCRETE DECK REPAIRS, SEE DWG. BR-1.

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BEGIN ABUTMENT

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 3/8
35	1 1/4
40	1 1/4
45	1 1/4
50	1 1/4
55	1 1/4
60	1 1/4
65	1 1/8
70	1 1/8
75	1 1/8
80	1 1/8
85	1 1/8
90	1 1/8
95	1 1/8
100	1
105	1
110	1

NOTES:  
 MIN. OPENING (IN.) = 1  
 MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.5  
 TOTAL MOVEMENT (IN.) = 0.5

PIER 1

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 7/8
35	1 7/8
40	1 3/4
45	1 3/4
50	1 3/4
55	1 5/8
60	1 5/8
65	1 5/8
70	1 1/2
75	1 1/2
80	1 1/2
85	1 3/8
90	1 3/8
95	1 3/8
100	1 1/4
105	1 1/4
110	1 1/4

NOTES:  
 MIN. OPENING (IN.) = 1.25  
 MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 2.25  
 TOTAL MOVEMENT (IN.) = 1

PIER 2

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 3/8
35	1 1/4
40	1 1/4
45	1 1/4
50	1 1/4
55	1 1/4
60	1 1/4
65	1 1/4
70	1 1/4
75	1 1/8
80	1 1/8
85	1 1/8
90	1 1/8
95	1 1/8
100	1 1/8
105	1 1/8
110	1

NOTES:  
 MIN. OPENING (IN.) = 1  
 MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.5  
 TOTAL MOVEMENT (IN.) = 0.5

- NOTES:
1. THE "JOINT OPENING" SHALL BE DETERMINED AT THE TIME OF PLACEMENT, USING THE TABLES PROVIDED ON THIS SHEET.
  2. THE SUPERSTRUCTURE TEMPERATURE USED TO DETERMINE THE JOINT OPENING SHALL BE THE AVERAGE TEMPERATURE OF THE BOTTOM OF THE GIRDER TOP FLANGES. A MINIMUM OF TWO TEMPERATURE READINGS PER SPAN SHALL BE USED TO CALCULATE THE AVERAGE SUPERSTRUCTURE TEMPERATURE.

ALTERED ON: 5/10/2024

AFFIXED ON: 5/10/2024

SIGNATURE: [Signature]

STATE OF NEW YORK  
 YOUNES DRIHMI  
 LICENSED PROFESSIONAL ENGINEER  
 100805  
 5/10/24

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY

Thruway Authority

2018 Mid-Range Tower  
 60 Park West  
 6th Fl., NY 10023-3723  
 616.463.4800 • www.thruwayauthority.com

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING BRIDGE PLAN (4 OF 7)	DRAWING NUMBER: BP-4

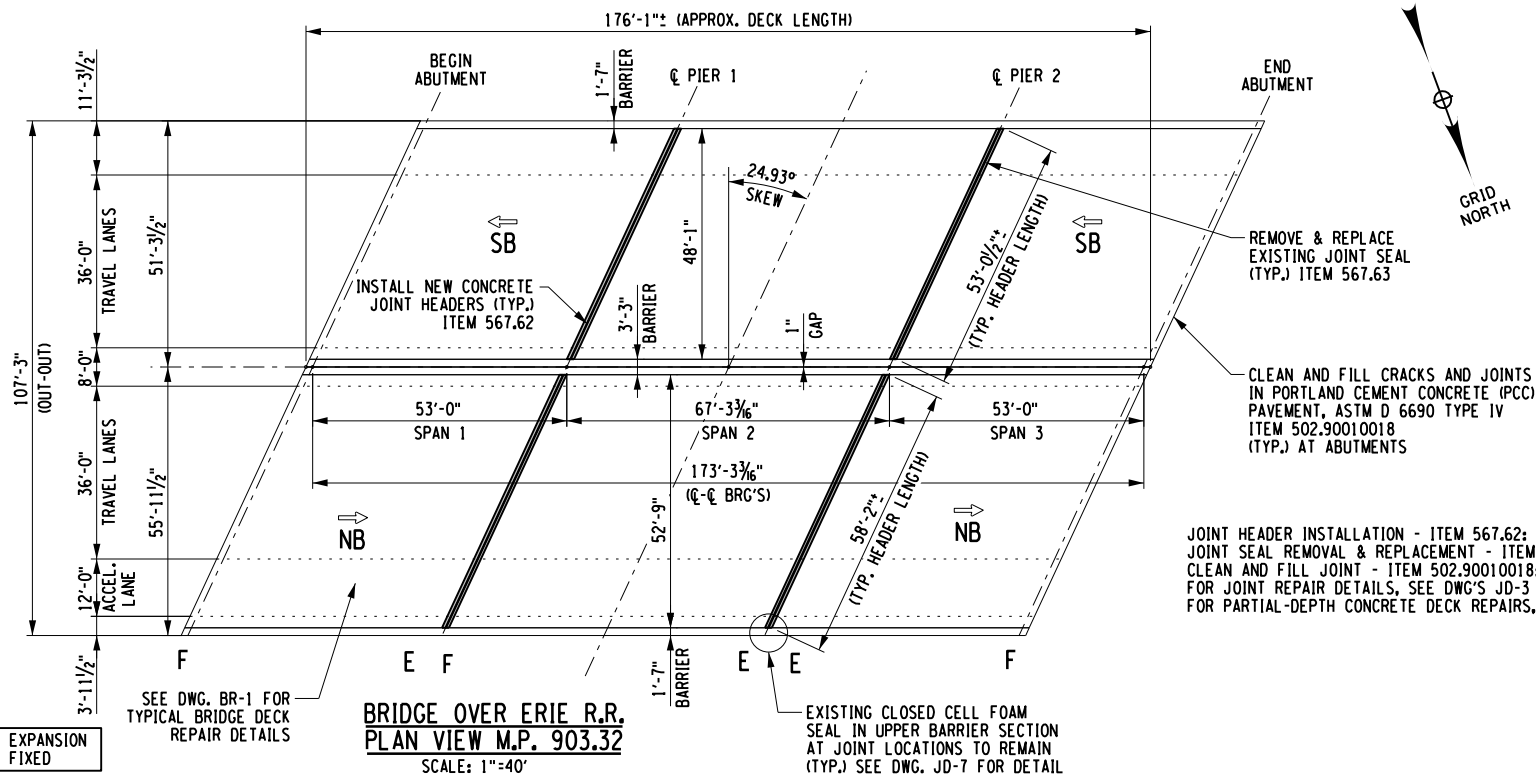
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DRAFTED BY: S. SCHUKRAFT

CHECKED BY: Y. DRIHMI

DRAFTED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH



**PIER 1**

**EXPANSION JOINT OPENING TABLE**

SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 3/8
35	1 3/8
40	1 3/8
45	1 1/4
50	1 1/4
55	1 1/4
60	1 1/4
65	1 1/4
70	1 1/8
75	1 1/8
80	1 1/8
85	1 1/8
90	1 1/8
95	1
100	1
105	1
110	1

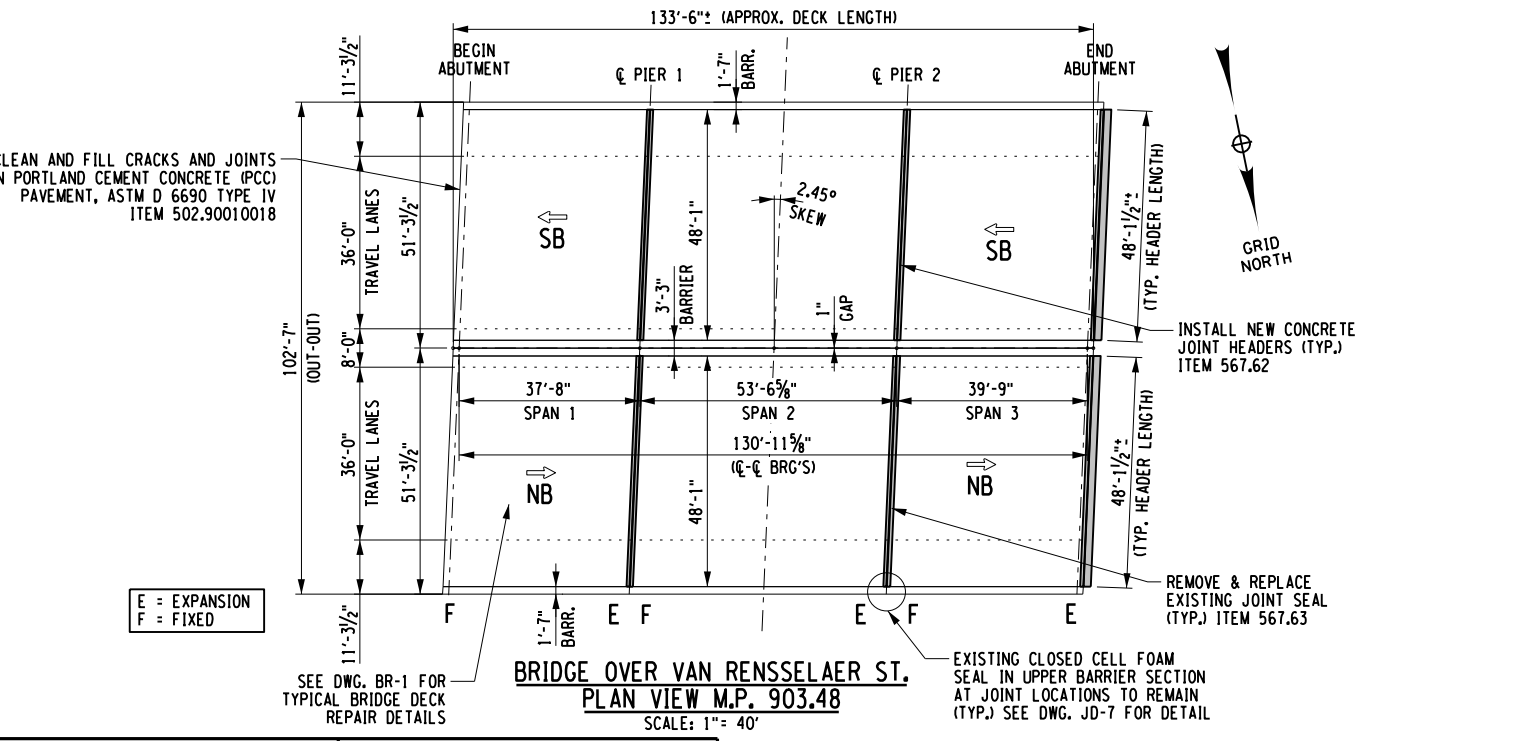
NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.625  
TOTAL MOVEMENT (IN.) = 0.625

**PIER 2**

**EXPANSION JOINT OPENING TABLE**

SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	2 1/8
35	2 1/8
40	2 1/8
45	2
50	2
55	1 7/8
60	1 7/8
65	1 7/8
70	1 7/8
75	1 3/4
80	1 3/4
85	1 3/4
90	1 5/8
95	1 5/8
100	1 1/2
105	1 1/2
110	1 3/8

NOTES:  
MIN. OPENING (IN.) = 1.375  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 2.75  
TOTAL MOVEMENT (IN.) = 1.5



**PIER 1**

**EXPANSION JOINT OPENING TABLE**

SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 3/8
35	1 1/4
40	1 1/4
45	1 1/4
50	1 1/4
55	1 1/4
60	1 1/4
65	1 1/4
70	1 1/8
75	1 1/8
80	1 1/8
85	1 1/8
90	1 1/8
95	1 1/8
100	1 1/8
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.5  
TOTAL MOVEMENT (IN.) = 0.5

**PIER 2**

**EXPANSION JOINT OPENING TABLE**

SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 3/8
35	1 3/8
40	1 3/8
45	1 1/4
50	1 1/4
55	1 1/4
60	1 1/4
65	1 1/4
70	1 1/8
75	1 1/8
80	1 1/8
85	1 1/8
90	1 1/8
95	1
100	1
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.625  
TOTAL MOVEMENT (IN.) = 0.625

**END ABUTMENT**

**EXPANSION JOINT OPENING TABLE**

SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 3/8
35	1 1/4
40	1 1/4
45	1 1/4
50	1 1/4
55	1 1/4
60	1 1/4
65	1 1/8
70	1 1/8
75	1 1/8
80	1 1/8
85	1 1/8
90	1 1/8
95	1 1/8
100	1
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.5  
TOTAL MOVEMENT (IN.) = 0.5

JOINT HEADER INSTALLATION - ITEM 567.62; (NB) 147'+ (SB) 147' = 294'  
JOINT SEAL REMOVAL & REPLACEMENT - ITEM 567.63; (NB) 147'+ (SB) 147' = 294'  
CLEAN AND FILL JOINT - ITEM 502.90010018; (NB) 48'+ (SB) 48' = 96'  
FOR JOINT REPAIR DETAILS, SEE DWG'S JD-3 TO JD-7.  
FOR PARTIAL-DEPTH CONCRETE DECK REPAIRS, SEE DWG. BR-1.

- NOTES:
1. THE "JOINT OPENING" SHALL BE DETERMINED AT THE TIME OF PLACEMENT, USING THE TABLES PROVIDED ON THIS SHEET.
  2. THE SUPERSTRUCTURE TEMPERATURE USED TO DETERMINE THE JOINT OPENING SHALL BE THE AVERAGE TEMPERATURE OF THE BOTTOM OF THE GIRDER TOP FLANGES. A MINIMUM OF TWO TEMPERATURE READINGS PER SPAN SHALL BE USED TO CALCULATE THE AVERAGE SUPERSTRUCTURE TEMPERATURE.

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REVISIONS

DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY**

**Thruway Authority**

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TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING BRIDGE PLAN (5 OF 7)	DRAWING NUMBER: BP-5

ALTERED ON: 5/10/2024

AFFIXED ON: 5/10/2024

SIGNATURE: [Signature]

STATE OF NEW YORK  
YOUNES DRIHMI  
LICENSED PROFESSIONAL ENGINEER  
100805  
5/10/24

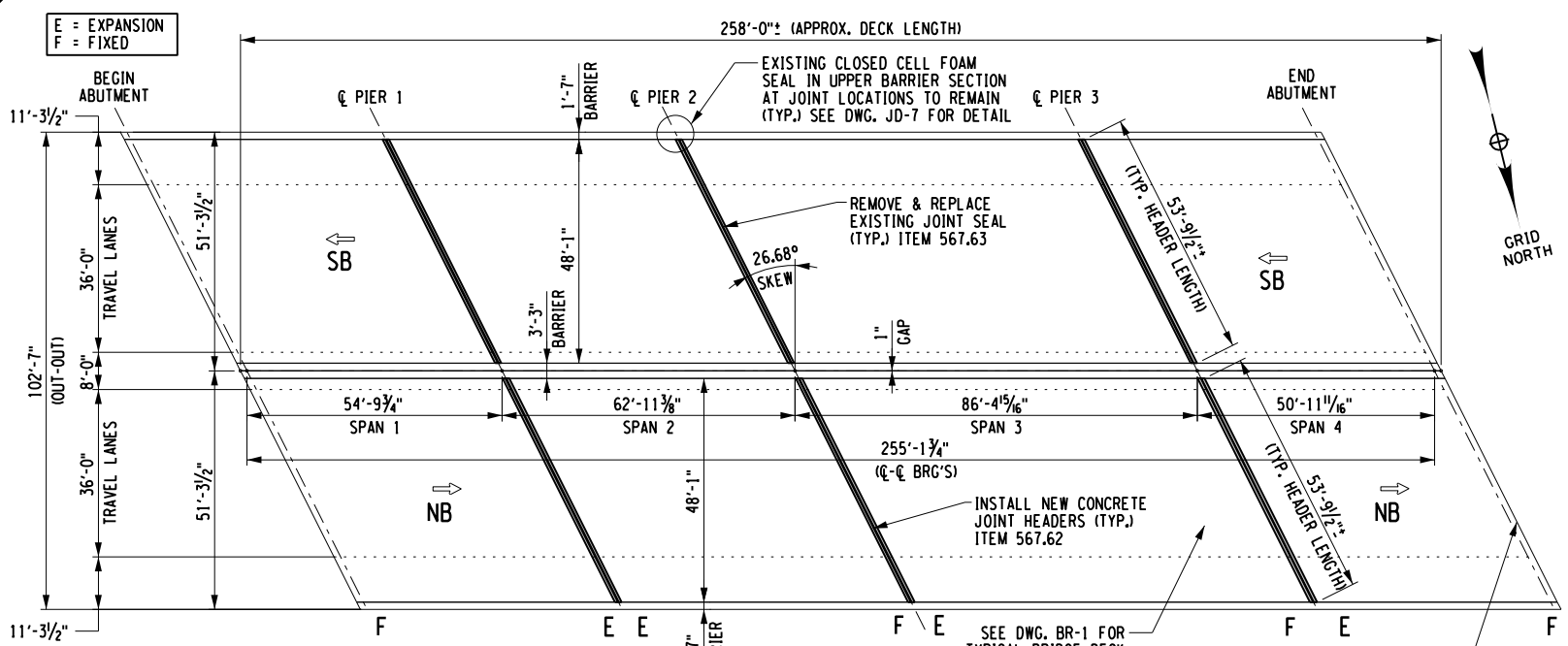
CHECKED BY: Y. DRIHMI

DRAFTED BY: S. SCHUKRAFT

CHECKED BY: Y. DRIHMI

DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH



**BRIDGE OVER CSX RR  
PLAN VIEW M.P. 903.66**  
SCALE: 1" = 40'

JOINT HEADER INSTALLATION - ITEM 567.62: (NB) 162'+ (SB) 162' = 324'  
JOINT SEAL REMOVAL & REPLACEMENT - ITEM 567.63: (NB) 162'+ (SB) 162' = 324'  
CLEAN AND FILL JOINT - ITEM 502.90010018: (NB) 108'+ (SB) 108' = 216'  
FOR JOINT REPAIR DETAILS, SEE DWG'S JD-3 TO JD-7.  
FOR PARTIAL-DEPTH CONCRETE DECK REPAIRS, SEE DWG. BR-1.

**PIER 1**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	2 1/4
35	2 1/8
40	2 1/8
45	2
50	2
55	1 7/8
60	1 7/8
65	1 7/8
70	1 3/4
75	1 3/4
80	1 5/8
85	1 5/8
90	1 5/8
95	1 5/8
100	1 1/2
105	1 1/2
110	1 1/2

NOTES:  
MIN. OPENING (IN.) = 1.5  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 2.75  
TOTAL MOVEMENT (IN.) = 1.375

**PIER 2**

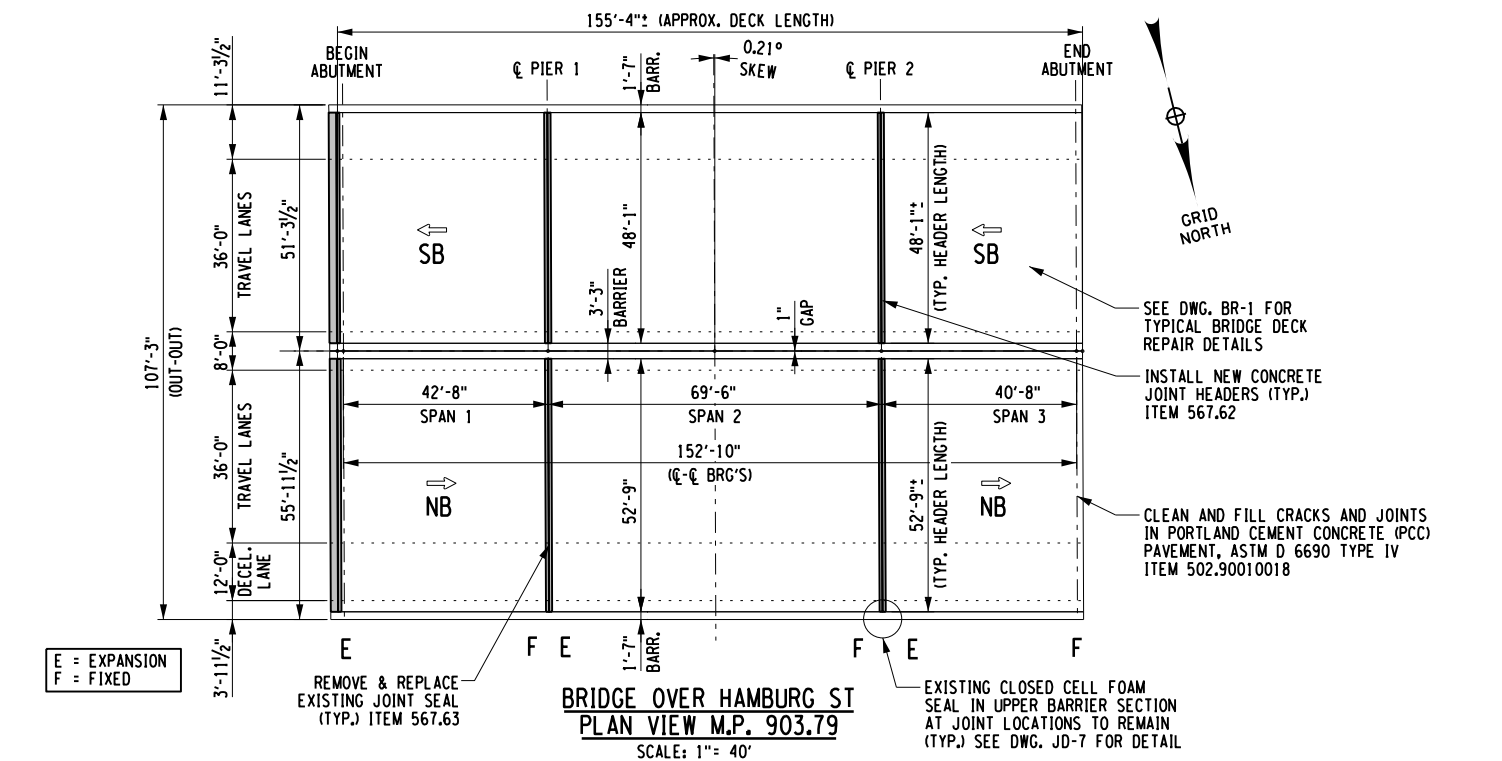
EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 5/8
35	1 5/8
40	1 5/8
45	1 5/8
50	1 1/2
55	1 1/2
60	1 1/2
65	1 1/2
70	1 3/8
75	1 3/8
80	1 3/8
85	1 1/4
90	1 1/4
95	1 1/4
100	1 1/8
105	1 1/8
110	1 1/8

NOTES:  
MIN. OPENING (IN.) = 1.125  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 2.125  
TOTAL MOVEMENT (IN.) = 1

**PIER 3**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 3/8
35	1 3/8
40	1 3/8
45	1 3/8
50	1 1/4
55	1 1/4
60	1 1/4
65	1 1/4
70	1 1/4
75	1 1/8
80	1 1/8
85	1 1/8
90	1 1/8
95	1 1/8
100	1
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.625  
TOTAL MOVEMENT (IN.) = 0.625



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

5/10/24

JOINT HEADER INSTALLATION - ITEM 567.62: (NB) 159'+ (SB) 145' = 304'  
JOINT SEAL REMOVAL & REPLACEMENT - ITEM 567.63: (NB) 159'+ (SB) 145' = 304'  
CLEAN AND FILL JOINT - ITEM 502.90010018: (NB) 53'+ (SB) 53' = 106'  
FOR JOINT REPAIR DETAILS, SEE DWG'S JD-3 TO JD-7.  
FOR PARTIAL-DEPTH CONCRETE DECK REPAIRS, SEE DWG. BR-1.

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**BEGIN ABUTMENT**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 1/4
35	1 1/4
40	1 1/4
45	1 1/4
50	1 1/4
55	1 1/4
60	1 1/8
65	1 1/8
70	1 1/8
75	1 1/8
80	1 1/8
85	1 1/8
90	1
95	1
100	1
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.5  
TOTAL MOVEMENT (IN.) = 0.5

**PIER 1**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 1/2
35	1 1/2
40	1 1/2
45	1 1/2
50	1 3/8
55	1 3/8
60	1 3/8
65	1 3/8
70	1 1/4
75	1 1/4
80	1 1/4
85	1 1/4
90	1 1/8
95	1 1/8
100	1 1/8
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.875  
TOTAL MOVEMENT (IN.) = 0.875

**PIER 2**

EXPANSION JOINT OPENING TABLE	
SUPERSTRUCTURE TEMPERATURE (°F)	JOINT OPENING (IN.)
30	1 1/4
35	1 1/4
40	1 1/4
45	1 1/4
50	1 1/4
55	1 1/4
60	1 1/4
65	1 1/8
70	1 1/8
75	1 1/8
80	1 1/8
85	1 1/8
90	1 1/8
95	1
100	1
105	1
110	1

NOTES:  
MIN. OPENING (IN.) = 1  
MAX. OPENING / REQ. MIN. SEAL WIDTH (IN.) = 1.5  
TOTAL MOVEMENT (IN.) = 0.5

- NOTES:
1. THE "JOINT OPENING" SHALL BE DETERMINED AT THE TIME OF PLACEMENT, USING THE TABLES PROVIDED ON THIS SHEET.
  2. THE SUPERSTRUCTURE TEMPERATURE USED TO DETERMINE THE JOINT OPENING SHALL BE THE AVERAGE TEMPERATURE OF THE BOTTOM OF THE GIRDER TOP FLANGES. A MINIMUM OF TWO TEMPERATURE READINGS PER SPAN SHALL BE USED TO CALCULATE THE AVERAGE SUPERSTRUCTURE TEMPERATURE.

REVISIONS

DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY** | **Thruway Authority**

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
CONTRACT NUMBER: TAN 24-12

LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
DATE: MAY 2024

TITLE OF DRAWING: BRIDGE PLAN (6 OF 7)  
DRAWING NUMBER: BP-6

CHECKED BY: Y. DRIHMI

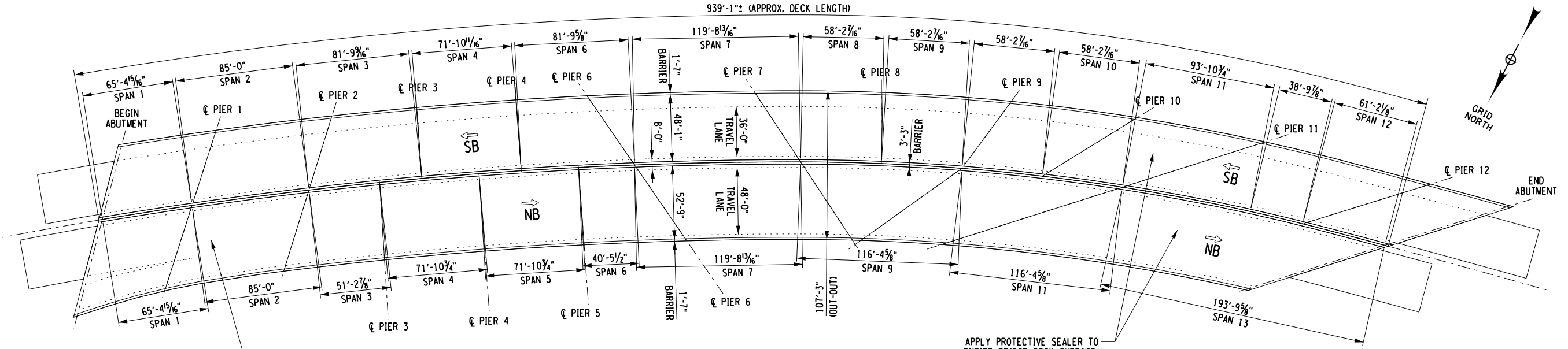
DRAFTED BY: S. SCHUKRAFT

CHECKED BY: Y. DRIHMI

DRAFTED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH

939'-1" (APPROX. DECK LENGTH)

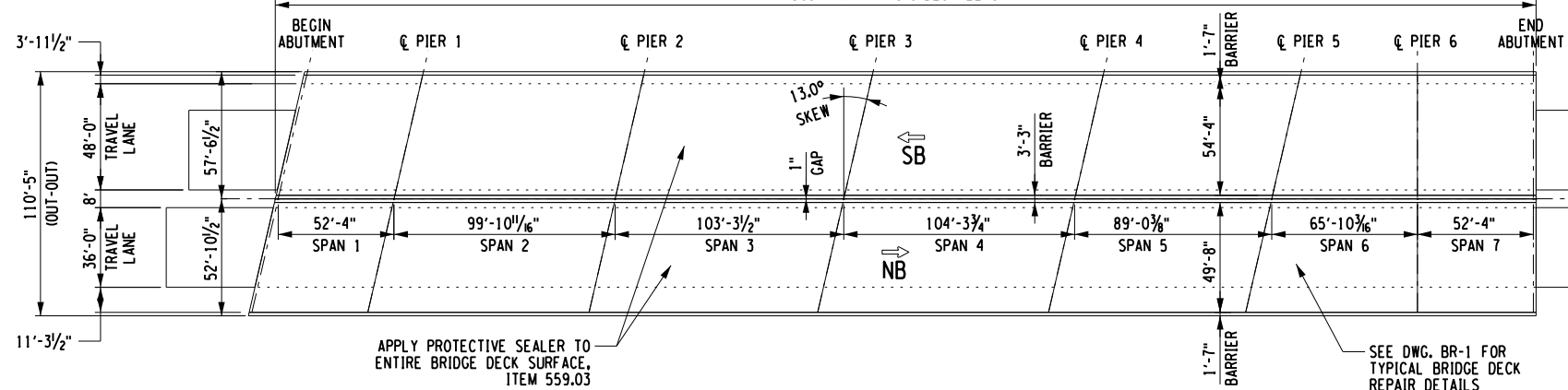


**BRIDGE OVER BAILEY AVENUE & NS RR**  
**PLAN VIEW M.P. 901.88**  
 SCALE: 1" = 80'

SEE DWG. BR-1 FOR TYPICAL BRIDGE DECK REPAIR DETAILS

APPLY PROTECTIVE SEALER TO ENTIRE BRIDGE DECK SURFACE, ITEM 559.03

569'-7" (APPROX. DECK LENGTH)



**BRIDGE OVER SELKIRK STREET & RR YARD**  
**PLAN VIEW M.P. 902.99**  
 SCALE: 1" = 80'

APPLY PROTECTIVE SEALER TO ENTIRE BRIDGE DECK SURFACE, ITEM 559.03

SEE DWG. BR-1 FOR TYPICAL BRIDGE DECK REPAIR DETAILS

PROTECTIVE SEALING OF EXISTING BRIDGE DECKS				
ITEM 559.03				
BRIDGE OVER	LOCATION	NB AREA (SF)	SB AREA (SF)	TOTAL (SF)
BAILEY AVE & NS RR M.P. 901.88 BIN 5512199	SPAN 1 DECK	3,956	3,454	97,480
	SPAN 2 DECK	4,700	4,200	
	SPAN 3 DECK	3,395	3,539	
	SPAN 4 DECK	3,924	3,570	
	SPAN 5 DECK	3,902	0	
	SPAN 6 DECK	3,137	3,276	
	SPAN 7 DECK	6,494	5,959	
	SPAN 8 DECK	0	3,759	
	SPAN 9 DECK	3,262	3,806	
	SPAN 10 DECK	0	3,755	
	SPAN 11 DECK	4,788	3,837	
	SPAN 12 DECK	0	6,311	
	SPAN 13 DECK	11,348	3,108	
SELKIRK ST & RR YARD M.P. 902.99 BIN 5512239	SPAN 1 DECK	2,662	2,913	59,172
	SPAN 2 DECK	4,961	5,427	
	SPAN 3 DECK	5,130	5,612	
	SPAN 4 DECK	5,180	5,667	
	SPAN 5 DECK	4,421	4,837	
	SPAN 6 DECK	3,573	3,217	
	SPAN 7 DECK	2,661	2,911	
TOTAL				156,652

- NOTES:**
- NO JOINT REPAIRS REQUIRED ON THE FOLLOWING TWO BRIDGES:  
 BRIDGE OVER BAILEY AVENUE & NS RR - M.P. 901.88  
 BRIDGE OVER SELKIRK STREET & RR YARD - M.P. 902.99

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING BRIDGE PLAN (7 OF 7)	DRAWING NUMBER: BP-7

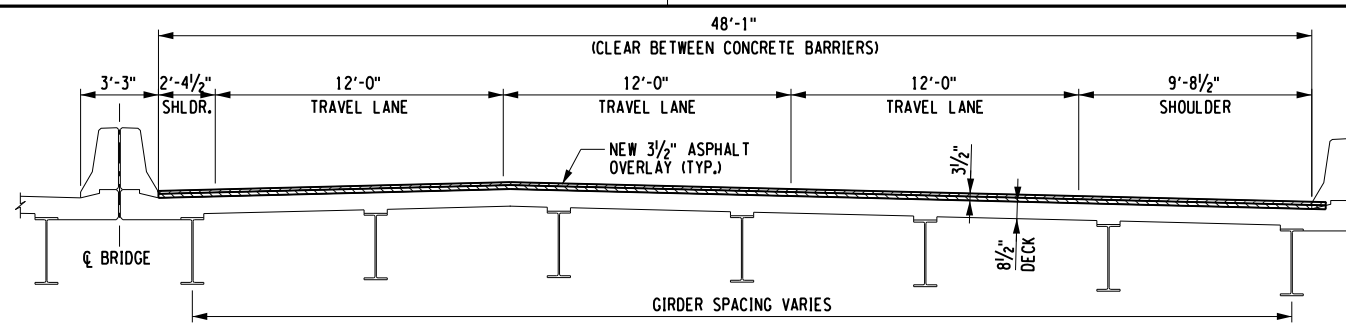
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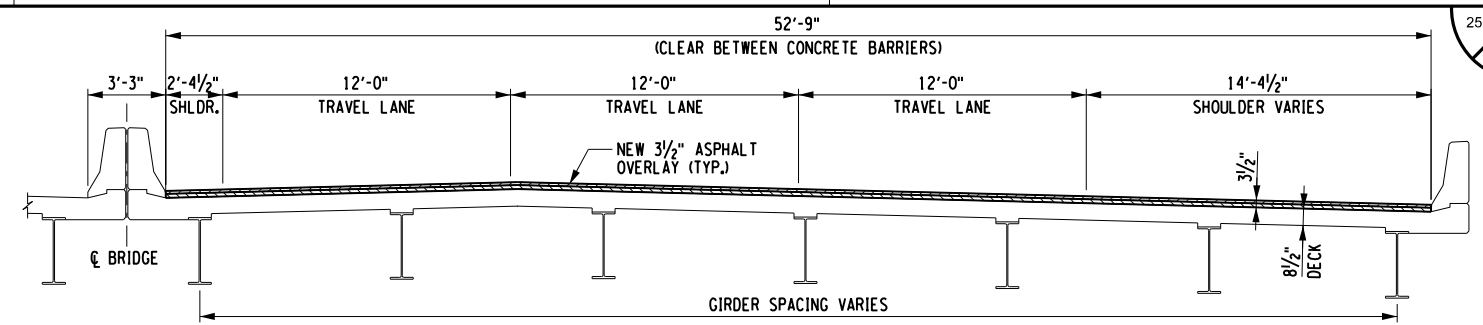
CHECKED BY: Y. DRIHMI

DESIGNED BY: S. SCHUKRAFT

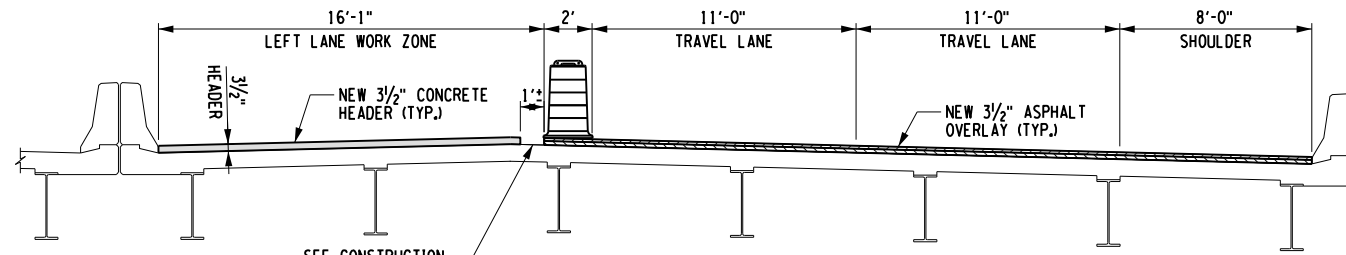
DESIGN SUPERVISOR: D. MARSH



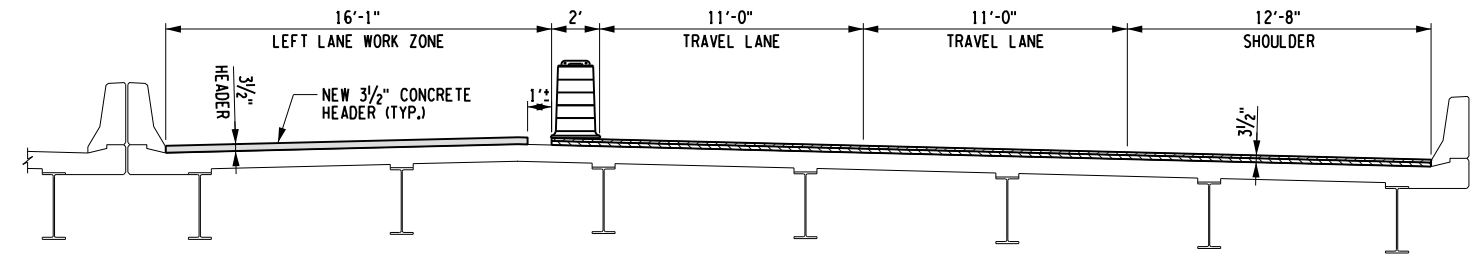
**TYPICAL BRIDGE SECTION - 3 LANES**  
(NORTHBOUND SHOWN, SOUTHBOUND SIMILAR)



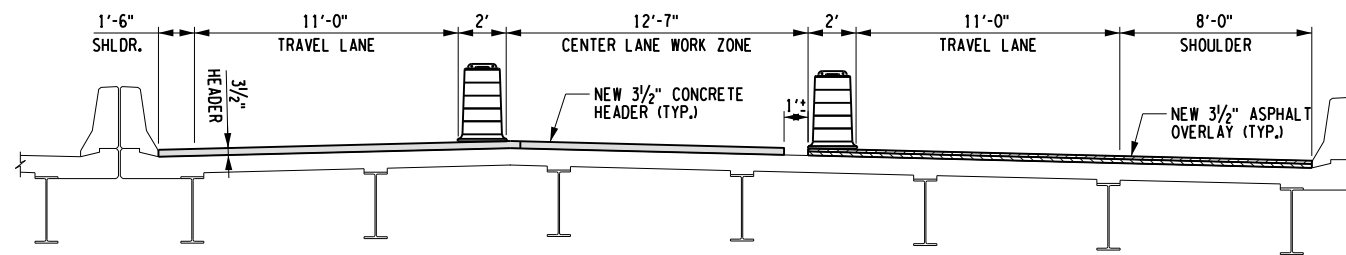
**TYPICAL BRIDGE SECTION - 3 LANE & ACCEL/DECAL RAMP**  
(NORTHBOUND SHOWN, SOUTHBOUND SIMILAR)



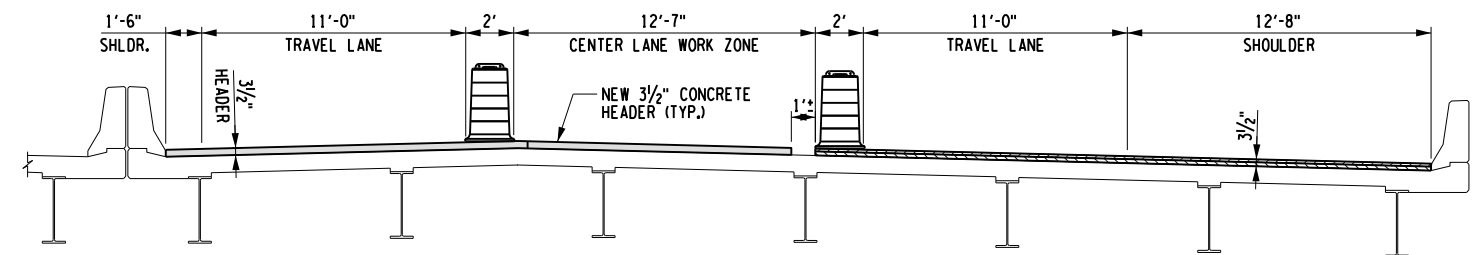
**STAGE I  
LEFT LANE CLOSURE**



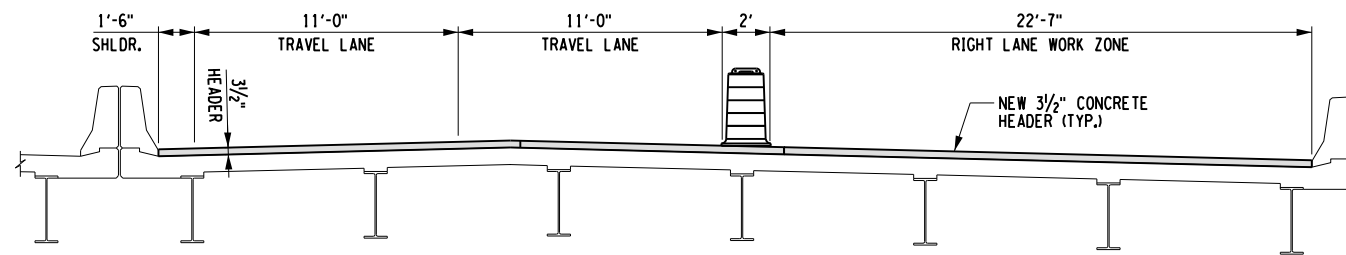
**STAGE I  
LEFT LANE CLOSURE**



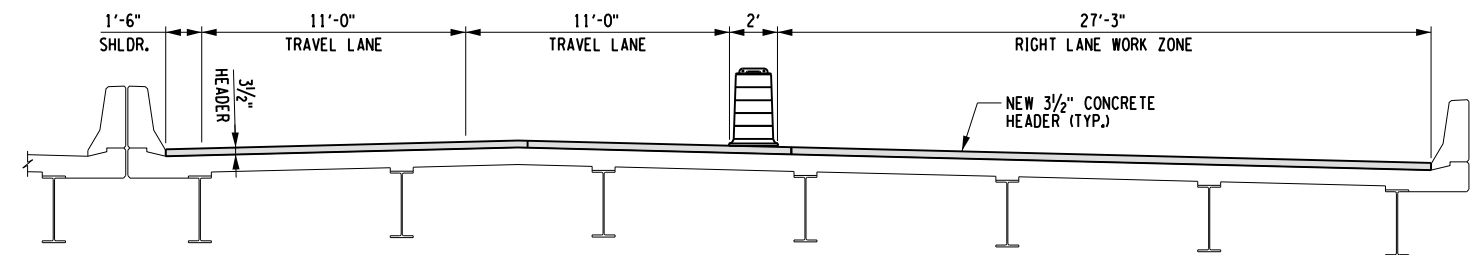
**STAGE II  
CENTER LANE CLOSURE**



**STAGE II  
CENTER LANE CLOSURE**



**STAGE III  
RIGHT LANE/SHOULDER CLOSURE**



**STAGE III  
RIGHT LANE/ACCEL/DECAL RAMP CLOSURE**

BRIDGE OVER	DISTANCE BETWEEN BARRIERS		BRIDGE OVER	DISTANCE BETWEEN BARRIERS		BRIDGE OVER	DISTANCE BETWEEN BARRIERS	
	NB	SB		NB	SB		NB	SB
CLINTON ST M.P. 901.56	48'-1"	48'-1"	PEABODY ST M.P. 902.84	48'-1"	48'-1"	M.P. 903.66 CSX RR	48'-1"	48'-1"
SENECA ST M.P. 902.22	48'-1"	48'-1"	SMITH ST M.P. 903.14	52'-9"	52'-9"	HAMBURG ST M.P. 903.79	52'-9"	48'-1"
MILTON ST M.P. 902.44	52'-9"	48'-1"	ERIE RR. M.P. 903.32	52'-9"	48'-1"			
BABCOCK ST M.P. 902.63	48'-1"	48'-1"	VAN RENSSELAER M.P. 903.48	48'-1"	48'-1"			

NOTE: DISTANCE BETWEEN CONCRETE BARRIERS SHOWN ON THE TYPICAL SECTIONS AND IN THE TABLES ABOVE ARE MEASURED PERPENDICULAR TO CENTERLINE OF ROADWAY.

**NOTES:**

- BRIDGE JOINT CONSTRUCTION LANE CLOSURE WIDTHS SHOWN ON THIS DRAWING ARE ONLY A SUGGESTED STAGING SEQUENCE. THE CONTRACTOR WILL BE RESPONSIBLE FOR SUBMITTING A WORK ZONE TRAFFIC CONTROL PLAN FOR APPROVAL.

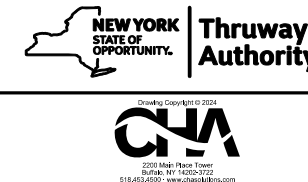
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING CONSTRUCTION STAGING FOR BRIDGE JOINT HEADER REPAIRS	DRAWING NUMBER: CS-1

BRIDGE JOINT TABLE - NORTHBOUND

BRIDGE OVER	LOCATION	SIDE OF JOINT	EXPANSION LENGTH (FT)	SKEW (°)	DECK WIDTH BTWN. BARRIERS (PERP.)	EXISTING DIM.		PROPOSED DIM.		LENGTH OF JOINT SEAL		JOINT REPAIR METHOD (A OR B)		
						H (IN)	J (@ 35° F) (IN)	H' (IN)	J' (@ 68° F) (IN)	PORTION ON DECK (FT)	PORTION ON BARRIER (FT) **			
						CLINTON ST M.P. 901.56 BIN 5046459	BEG. ABUT.	APPR. DECK	0.00	43.56	48.08		18.50	3.38
PIER 1	BEG	11.00	3.38	12.19	1.125	66.35	1.35	A						
		11.00	3.38	12.13	1.125	66.35	1.35	A						
	END	10.88	3.38	12.00	1.125	66.35	1.35	A						
		12.38	3.88	13.56	1.500	66.35	1.33	A						
PIER 2	BEG	9.63	3.88	10.81	1.500	66.35	1.33	A						
	END	11.75	3.25	12.81	1.125	66.35	1.46	A						
END ABUT.	DECK	18.63	3.25	19.69	1.125	66.35	1.46	A						
	APPR.							B						
SENECA ST M.P. 902.22 BIN 5011959	BEG. ABUT.	APPR. DECK	67.50	57.72	48.08	14.63	2.63	15.28	1.313			90.04	1.36	A
PIER 1	BEG	11.63	2.63			12.28	1.313	90.04	1.36			A		
		10.88	4.00			11.97	1.813	90.04	1.36			A		
	END	11.50	4.00			12.59	1.813	90.04	1.36	A				
		11.13	3.38			12.19	1.250	90.04	1.33	A				
PIER 2	BEG	10.00	3.38			11.06	1.250	90.04	1.33	A				
	END	11.00	3.13			12.06	1.000	90.04	1.33	A				
END ABUT.	DECK	15.25	3.13			16.31	1.000	90.04	1.33	A				
	APPR.									A				
MILTON ST M.P. 902.44 BIN 5512209	BEG. ABUT.	APPR. DECK	35.75			14.11	52.75	15.75	2.75	16.56	1.125	54.39	1.40	A
PIER 1	BEG	11.00	2.75					11.81	1.125	54.39	1.29	A		
		10.50	2.88					11.38	1.125	54.39	1.29	A		
	END	11.25	2.88	12.13	1.125			54.39	1.29	A				
		10.50	2.75	11.31	1.125			54.39	1.31	A				
PIER 2	BEG	11.50	2.75	12.31	1.125			54.39	1.31	A				
	END	11.50	2.88	12.44	1.000			54.39	1.33	A				
END ABUT.	DECK	15.25	2.88	16.19	1.000			54.39	1.33	A				
	APPR.									A				
BABCOCK ST M.P. 902.63 BIN 5512219	BEG. ABUT.	APPR. DECK	0.00	14.11	48.08			14.88	3.00	15.88	1.000	49.58	1.25	A
PIER 1	BEG	11.63	3.00					12.63	1.000	49.58	1.25	A		
		11.50	3.50					12.69	1.125	49.58	1.33	A		
	END	10.63	3.50			11.81	1.125	49.58	1.33	A				
		10.50	3.38			11.56	1.250	49.58	1.31	A				
PIER 2	BEG	11.25	3.38			12.31	1.250	49.58	1.31	A				
	END	11.25	3.00			12.19	1.125	49.58	1.27	A				
END ABUT.	DECK	14.75	3.00			15.69	1.125	49.58	1.27	A				
	APPR.									A				
PEABODY ST M.P. 902.84 BIN 5512229	BEG. ABUT.	APPR. DECK	0.00			4.47	48.08	16.38	3.25	17.50	1.000	48.23	1.52	A
PIER 1	BEG	11.00	3.25					12.13	1.000	48.23	1.52	A		
		10.63	3.63					11.88	1.125	48.23	1.52	A		
	END	10.25	3.63	11.50	1.125			48.23	1.52	A				
		10.63	3.50	11.81	1.125			48.23	1.54	A				
PIER 2	BEG	11.00	3.50	12.19	1.125			48.23	1.54	A				
	END	11.00	3.00	11.91	1.188			48.23	1.56	A				
END ABUT.	DECK	17.00	3.00	17.91	1.188			48.23	1.56	A				
	APPR.									B				

\* EXISTING JOINT OPENINGS WERE MEASURED AT A TEMPERATURE OF 35° F.  
 \*\* PORTION OF BRIDGE JOINT SEAL THAT EXTENDS UP THE VERTICAL FACE OF THE BARRIER. LENGTH SHOWN IS FOR FASCIA BARRIER, CENTER MEDIAN BARRIER SIMILAR.

BRIDGE JOINT TABLE - NORTHBOUND (CONT'D)

BRIDGE OVER	LOCATION	SIDE OF JOINT	EXPANSION LENGTH (FT)	SKEW (°)	DECK WIDTH BTWN. BARRIERS (PERP.)	EXISTING DIM.		PROPOSED DIM.		LENGTH OF JOINT SEAL		JOINT REPAIR METHOD (A OR B)		
						H (IN)	J (@ 35° F) (IN)	H' (IN)	J' (@ 68° F) (IN)	PORTION ON DECK (FT)	PORTION ON BARRIER (FT) **			
						SMITH ST M.P. 903.14 BIN 5512249	BEG. ABUT.	APPR. DECK	40.00	5.29	52.75		15.00	2.00
PIER 1	BEG	7.00	2.00	7.44	1.125	52.98	1.83	A						
		6.38	2.63	6.91	1.563	52.98	1.79	A						
	END	6.25	2.63	6.78	1.563	52.98	1.79	A						
		6.75	2.13	7.19	1.250	52.98	1.79	A						
PIER 2	BEG	6.38	2.13	6.81	1.250	52.98	1.79	A						
	END	0.00	1.00	0.00	0.000	52.98	0.00	A						
END ABUT.	DECK	0.00	1.00	0.00	0.000	52.98	0.00	A						
	APPR.							A						
ERIE RR. M.P. 903.32 BIN 5512259	BEG. ABUT.	APPR. DECK	0.00	24.93	52.75	0.00	1.00	0.00	0.000			58.17	0.00	A
PIER 1	BEG	0.00	1.00			0.00	0.000	58.17	0.00			A		
		7.00	2.13			7.47	1.188	58.17	1.73			A		
	END	6.38	2.13			6.84	1.188	58.17	1.73	A				
		6.25	2.50			6.56	1.875	58.17	1.75	A				
PIER 2	BEG	6.63	2.50			6.94	1.875	58.17	1.75	A				
	END	0.00	1.00			0.00	0.000	58.17	0.00	A				
END ABUT.	DECK	0.00	1.00			0.00	0.000	58.17	0.00	A				
	APPR.									A				
VAN RENSSELAER ST M.P. 903.48 BIN 5512269	BEG. ABUT.	APPR. DECK	0.00			2.45	48.08	0.00	1.13	0.00	0.000	48.13	0.00	A
PIER 1	BEG	0.00	1.13					0.00	0.000	48.13	0.00	A		
		6.88	2.13					7.34	1.188	48.13	1.67	A		
	END	6.88	2.13	7.34	1.188			48.13	1.67	A				
		6.13	3.75	7.41	1.188			48.13	1.81	A				
PIER 2	BEG	7.63	3.75	8.91	1.188			48.13	1.81	A				
	END	6.13	3.75	7.44	1.125			48.13	1.69	A				
END ABUT.	DECK	16.75	3.75	18.06	1.125			48.13	1.69	B				
	APPR.									A				
CSX RR M.P. 903.66 BIN 5512279	BEG. ABUT.	APPR. DECK	0.00	26.68	48.08			0.00	1.50	0.00	0.000	53.81	0.00	A
PIER 1	BEG	0.00	1.50					0.00	0.000	53.81	0.00	A		
		6.50	2.38					6.78	1.813	53.81	1.60	A		
	END	6.50	2.38			6.78	1.813	53.81	1.60	A				
		6.00	2.63			6.59	1.438	53.81	1.78	A				
PIER 2	BEG	6.63	2.63			7.22	1.438	53.81	1.78	A				
	END	7.13	2.13			7.56	1.250	53.81	1.71	A				
PIER 3	BEG	7.25	2.13			7.69	1.250	53.81	1.71	A				
	END	0.00	1.25			0.00	0.000	53.81	0.00	A				
END ABUT.	DECK	0.00	1.25			0.00	0.000	53.81	0.00	A				
	APPR.									A				
HAMBURG ST M.P. 903.79 BIN 5512289	BEG. ABUT.	APPR. DECK	42.67			0.21	52.75	17.00	3.50	18.19	1.125	52.75	1.81	A
PIER 1	BEG	6.75	3.50	7.94	1.125			52.75	1.81	A				
		6.13	3.25	7.09	1.313			52.75	1.75	A				
	END	6.38	3.25	7.34	1.313			52.75	1.75	A				
		6.25	2.13	6.75	1.125			52.75	1.79	A				
PIER 2	BEG	7.00	2.13	7.50	1.125			52.75	1.79	A				
	END	0.00	1.25	0.00	0.000			52.75	0.00	A				
END ABUT.	DECK	0.00	1.25	0.00	0.000			52.75	0.00	A				
	APPR.									A				



\* EXISTING JOINT OPENINGS WERE MEASURED AT A TEMPERATURE OF 35° F.  
 \*\* PORTION OF BRIDGE JOINT SEAL THAT EXTENDS UP THE VERTICAL FACE OF THE BARRIER. LENGTH SHOWN IS FOR FASCIA BARRIER, CENTER MEDIAN BARRIER SIMILAR.

NOTES:

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD VERIFYING ALL DIMENSIONS PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL FIELD VERIFY EXISTING JOINT OPENINGS WITH PRIMARY MEMBER TEMPERATURE TAKEN INTO ACCOUNT.
- SEE DWG. JD-3 TO JD-7 FOR EXISTING & PROPOSED JOINT DETAILS.
- THE CONTRACTOR SHALL VERIFY THE WIDTH OF EXISTING JOINT IN THE FIELD. THE WIDTH MAY VARY FROM JOINT TO JOINT, AND ALSO MAY VARY ACROSS THE ROADWAY'S WIDTH. NO EXTRA COMPENSATION SHALL BE PAID DUE TO THESE VARIATIONS IN JOINT WIDTH. THE CONTRACTOR SHALL RECORD THE TEMPERATURE AT THE TIME OF JOINT WIDTH MEASUREMENT.
- THE JOINT WIDTH AND TEMPERATURE AT THE TIME OF SEALANT INSTALLATION SHALL BE SUCH THAT THE MOVEMENT RANGE (AS SPECIFIED BY THE MANUFACTURER) CAN ACCOMMODATE THE ANTICIPATED MAXIMUM AND MINIMUM JOINT WIDTHS SHOWN IN THE TABLES.
- CONTRACTOR SHALL TAKE CARE NOT TO DAMAGE EXISTING REINFORCING BARS IN THE DECK/APPROACH SLABS AND HEADERS. IF ANY BAR IS DAMAGED DURING CONSTRUCTION, CONTRACTOR SHALL REPLACE IT AT NO COST TO THE THRUWAY.
- DURING OPERATIONS, THE CONTRACTOR SHALL TAKE CARE TO NOT DROP ANY MATERIAL TO THE AREA BELOW THE BRIDGE. PLATFORMS, NETS OR OTHER PROTECTIVE DEVICES SHALL BE USED TO CATCH THE MATERIAL. IF ANY MATERIAL FALLS BELOW THE BRIDGE, IT SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AT NO COST TO THE THRUWAY. IF THE ENGINEER DETERMINES THAT ADEQUATE PROTECTIVE DEVICES ARE NOT BEING EMPLOYED, THE WORK SHALL BE SUSPENDED UNTIL ADEQUATE PROTECTION IS PROVIDED.
- CONTRACTOR IS ADVISED THAT THE NEW ELASTOMERIC HEADER OVERHANG SHALL NOT EXCEED 1 INCH MAXIMUM.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.


 <b>Thruway Authority</b>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING BRIDGE JOINT TABLE NORTHBOUND	DRAWING NUMBER: JD-1

CHECKED BY: Y. DRHMI

DRAFTED BY: S. SCHUKRAFT

CHECKED BY: Y. DRHMI

DESIGNED BY: S. SCHUKRAFT

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

BRIDGE JOINT TABLE - SOUTHBOUND

BRIDGE OVER	LOCATION	SIDE OF JOINT	EXPANSION LENGTH (FT)	SKEW (°)	DECK WIDTH BTWN. BARRIERS (PERP.)	EXISTING DIM.		PROPOSED DIM.		LENGTH OF JOINT SEAL		JOINT REPAIR METHOD (A OR B)		
						H (IN)	J (@ 35° F) (IN)	H' (IN)	J' (@ 68° F) (IN)	PORTION ON DECK (FT)	PORTION ON BARRIER (FT) **			
						CLINTON ST M.P. 901.56 BIN 5046459	BEG. ABUT.	APPR. DECK	0.00	43.56	48.08		17.50	3.38
PIER 1	BEG	END	46.00	10.38	3.38	14.00	1.125	66.35	1.60			A		
	END			12.88	11.38	1.125	1.60							
PIER 2	BEG	END	122.25	8.88	4.00	10.13	1.500	66.35	1.58			A		
	END			9.00	10.25	1.500	1.58							
END ABUT.	DECK	APPR.	53.00	10.50	3.00	11.44	1.125	66.35	1.56			A		
	APPR.			18.50	19.44	1.125	1.56							
SENECA ST M.P. 902.22 BIN 5011959	BEG. ABUT.	APPR. DECK	67.50	57.72	48.08	14.50	3.13	15.41	1.313			90.04	1.60	A
PIER 1	BEG	END	131.09			10.75	4.25	11.66	1.813			90.04	1.58	A
	END					10.00	11.22	1.813	1.58					
PIER 2	BEG	END	63.33			10.38	3.25	11.59	1.250			90.04	1.65	A
	END					10.75	11.75	1.250	1.65					
END ABUT.	DECK	APPR.	0.00			10.00	4.00	11.00	1.000	90.04	1.71	A		
	APPR.					11.00	12.50	1.000	1.71					
MILTON ST M.P. 902.44 BIN 5512209	BEG. ABUT.	APPR. DECK	35.75			14.11	48.08	15.25	2.88	16.13	1.125	49.58	1.58	A
PIER 1	BEG	END	55.17					11.00	3.68	11.88	1.125	49.58	1.63	A
	END							11.00	12.03	1.125	1.63			
PIER 2	BEG	END	34.33					10.75	2.88	12.38	1.125	49.58	1.58	A
	END							11.50	11.75	1.125	1.58			
END ABUT.	DECK	APPR.	0.00	12.00	2.75			12.88	1.000	49.58	1.65	A		
	APPR.			15.00	15.88			1.000	1.65					
BABCOCK ST M.P. 902.63 BIN 5512219	BEG. ABUT.	APPR. DECK	0.00	14.11	48.08			15.50	2.75	16.38	1.000	49.58	1.58	A
PIER 1	BEG	END	34.50					11.25	3.38	12.00	1.125	49.58	1.58	A
	END							10.88	11.13	1.125	1.58			
PIER 2	BEG	END	71.67					7.50	2.88	8.31	1.250	49.58	1.63	A
	END							10.88	11.69	1.250	1.63			
END ABUT.	DECK	APPR.	35.17			11.13	2.88	12.00	1.125	49.58	1.60	A		
	APPR.					14.63	15.50	1.125	1.60					
PEABODY ST M.P. 902.84 BIN 5512229	BEG. ABUT.	APPR. DECK	0.00			4.47	48.08	13.75	3.38	14.94	1.000	48.23	1.63	A
PIER 1	BEG	END	33.17					10.75	3.50	11.94	1.125	48.23	1.77	A
	END							11.50	12.69	1.125	1.77			
PIER 2	BEG	END	55.60					11.38	2.88	12.56	1.125	48.23	1.65	A
	END							12.38	10.88	1.125	1.65			
END ABUT.	DECK	APPR.	38.33	10.00	2.00			10.88	1.188	48.23	1.50	A		
	APPR.			11.88	12.28			1.188	1.50					

\* EXISTING JOINT OPENINGS WERE MEASURED AT A TEMPERATURE OF 35° F.  
 \*\* PORTION OF BRIDGE JOINT SEAL THAT EXTENDS UP THE VERTICAL FACE OF THE BARRIER. LENGTH SHOWN IS FOR FASCIA BARRIER, CENTER MEDIAN BARRIER SIMILAR.

BRIDGE JOINT TABLE - SOUTHBOUND (CONT'D)

BRIDGE OVER	LOCATION	SIDE OF JOINT	EXPANSION LENGTH (FT)	SKEW (°)	DECK WIDTH BTWN. BARRIERS (PERP.)	EXISTING DIM.		PROPOSED DIM.		LENGTH OF JOINT SEAL		JOINT REPAIR METHOD (A OR B)		
						H (IN)	J (@ 35° F) (IN)	H' (IN)	J' (@ 68° F) (IN)	PORTION ON DECK (FT)	PORTION ON BARRIER (FT) **			
						SMITH ST M.P. 903.14 BIN 5512249	BEG. ABUT.	APPR. DECK	40.00	5.29	52.75		16.50	3.50
PIER 1	BEG	END	83.86	7.00	2.50	8.19	1.563	52.98	1.67			A		
	END			6.25	7.47	1.563	1.67							
PIER 2	BEG	END	36.67	7.00	2.13	7.47	1.250	52.98	1.78			A		
	END			6.50	6.94	1.250	1.78							
END ABUT.	DECK	APPR.	0.00	6.50	1.00	6.94	0.000	52.98	0.00			A		
	APPR.			0.00	0.00	0.000	0.00							
ERIE RR. M.P. 903.32 BIN 5512259	BEG. ABUT.	APPR. DECK	0.00	24.93	48.08	0.00	1.25	0.00	0.000			53.02	0.00	A
PIER 1	BEG	END	53.00			0.00	2.88	7.84	1.188			53.02	1.67	A
	END					7.00	7.34	1.188	1.67					
PIER 2	BEG	END	120.27			6.50	2.25	6.81	1.875			53.02	1.88	A
	END					6.25	6.44	1.875	1.88					
END ABUT.	DECK	APPR.	0.00			0.00	1.25	0.00	0.000	53.02	0.00	A		
	APPR.					0.00	0.00	0.000	0.00					
VAN RENSSELAER ST M.P. 903.48 BIN 5512269	BEG. ABUT.	APPR. DECK	0.00			2.45	48.08	0.00	1.00	0.00	0.000	48.13	0.00	A
PIER 1	BEG	END	37.67					0.00	2.13	6.97	1.188	48.13	1.54	A
	END							7.13	7.59	1.188	1.54			
PIER 2	BEG	END	53.55					6.50	2.63	7.22	1.188	48.13	1.71	A
	END							6.63	7.34	1.188	1.71			
END ABUT.	DECK	APPR.	39.75	6.38	3.75			7.69	1.125	48.13	1.70	A		
	APPR.			16.75	18.06			1.125	1.70					
CSX RR M.P. 903.66 BIN 5512279	BEG. ABUT.	APPR. DECK	0.00	26.68	48.08			0.00	1.13	0.00	0.000	53.81	0.00	A
PIER 1	BEG	END	117.76					0.00	2.38	6.53	1.813	53.81	1.77	A
	END							6.25	6.28	1.813	1.77			
PIER 2	BEG	END	86.41					6.25	2.63	6.84	1.438	53.81	1.79	A
	END							6.63	7.22	1.438	1.79			
PIER 3	BEG	END	50.97			5.13	3.13	6.06	1.250	53.81	1.63	A		
	END					6.63	7.56	1.250	1.63					
END ABUT.	DECK	APPR.	0.00			0.00	1.25	0.00	0.000	53.81	0.00	A		
	APPR.					0.00	0.00	0.000	0.00					
HAMBURG ST M.P. 903.79 BIN 5512289	BEG. ABUT.	APPR. DECK	42.67			0.21	48.08	17.25	3.50	18.44	1.125	48.08	1.67	A
PIER 1	BEG	END	69.50					5.88	2.75	7.06	1.313	48.08	1.67	A
	END							6.50	7.22	1.313	1.67			
PIER 2	BEG	END	40.67	7.00	2.75			7.72	1.125	48.08	1.50	A		
	END			5.88	7.44			1.125	1.50					
END ABUT.	DECK	APPR.	0.00	6.63	1.63			7.44	0.000	48.08	1.42	A		
	APPR.			0.00	0.00			0.000	1.42					

\* EXISTING JOINT OPENINGS WERE MEASURED AT A TEMPERATURE OF 35° F.  
 \*\* PORTION OF BRIDGE JOINT SEAL THAT EXTENDS UP THE VERTICAL FACE OF THE BARRIER. LENGTH SHOWN IS FOR FASCIA BARRIER, CENTER MEDIAN BARRIER SIMILAR.

NOTES:

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR FIELD VERIFYING ALL DIMENSIONS PRIOR TO CONSTRUCTION.
2. THE CONTRACTOR SHALL FIELD VERIFY EXISTING JOINT OPENINGS WITH PRIMARY MEMBER TEMPERATURE TAKEN INTO ACCOUNT.
3. SEE DWG. JD-3 TO JD-7 FOR EXISTING & PROPOSED JOINT DETAILS.
4. THE CONTRACTOR SHALL VERIFY THE WIDTH OF EXISTING JOINT IN THE FIELD. THE WIDTH MAY VARY FROM JOINT TO JOINT, AND ALSO MAY VARY ACROSS THE ROADWAY'S WIDTH. NO EXTRA COMPENSATION SHALL BE PAID DUE TO THESE VARIATIONS IN JOINT WIDTH. THE CONTRACTOR SHALL RECORD THE TEMPERATURE AT THE TIME OF JOINT WIDTH MEASUREMENT.
5. THE JOINT WIDTH AND TEMPERATURE AT THE TIME OF SEALANT INSTALLATION SHALL BE SUCH THAT THE MOVEMENT RANGE (AS SPECIFIED BY THE MANUFACTURER) CAN ACCOMMODATE THE ANTICIPATED MAXIMUM AND MINIMUM JOINT WIDTHS SHOWN IN THE TABLES.
6. CONTRACTOR SHALL TAKE CARE NOT TO DAMAGE EXISTING REINFORCING BARS IN THE DECK/APPROACH SLABS AND HEADERS. IF ANY BAR IS DAMAGED DURING CONSTRUCTION, CONTRACTOR SHALL REPLACE IT AT NO COST TO THE THRUWAY.
7. DURING OPERATIONS, THE CONTRACTOR SHALL TAKE CARE TO NOT DROP ANY MATERIAL TO THE AREA BELOW THE BRIDGE. PLATFORMS, NETS OR OTHER PROTECTIVE DEVICES SHALL BE USED TO CATCH THE MATERIAL. IF ANY MATERIAL FALLS BELOW THE BRIDGE, IT SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AT NO COST TO THE THRUWAY. IF THE ENGINEER DETERMINES THAT ADEQUATE PROTECTIVE DEVICES ARE NOT BEING EMPLOYED, THE WORK SHALL BE SUSPENDED UNTIL ADEQUATE PROTECTION IS PROVIDED.
8. CONTRACTOR IS ADVISED THAT THE NEW ELASTOMERIC HEADER OVERHANG SHALL NOT EXCEED 1 INCH MAXIMUM.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING BRIDGE JOINT TABLE SOUTHBOUND	DRAWING NUMBER: JD-2

CHECKED BY: Y. DRHIMI

DRAFTED BY: S. SCHUKRAFT

CHECKED BY: Y. DRHIMI

DESIGNED BY: S. SCHUKRAFT

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

DESIGN SUPERVISOR: D. MARSH

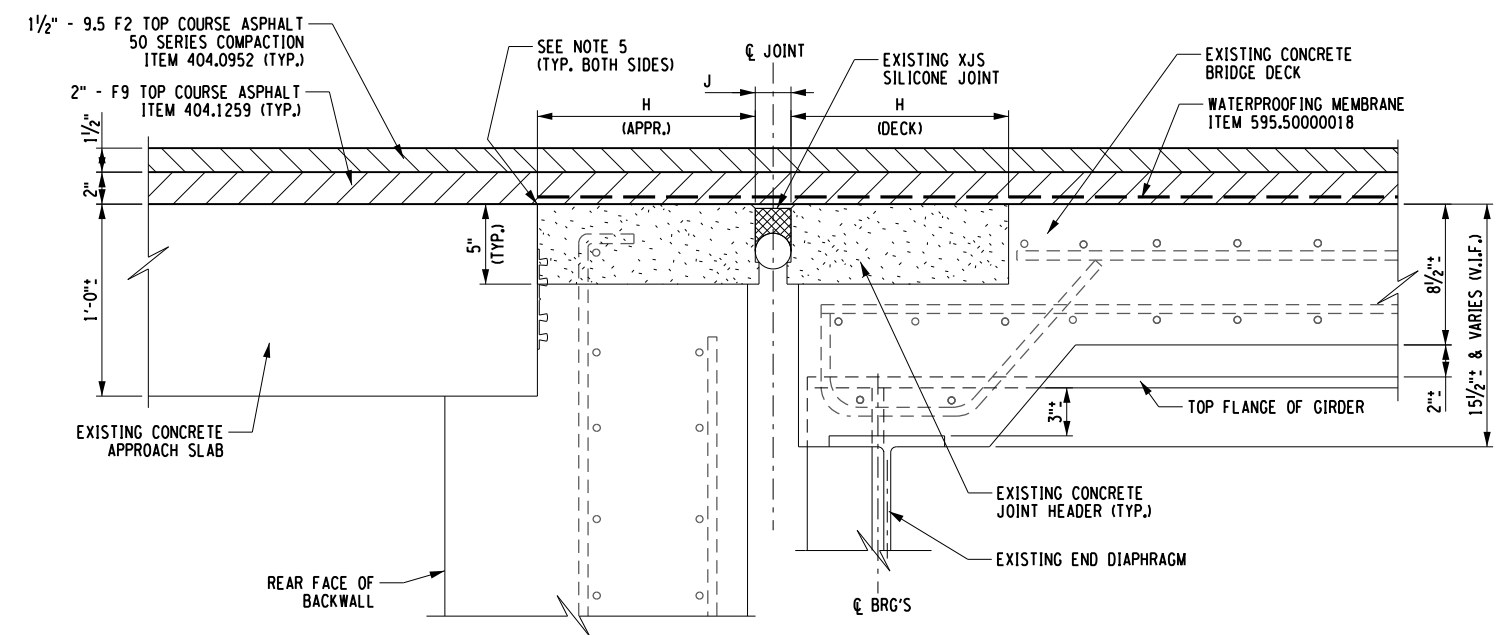
CHECKED BY: Y. DRHMI

DRAFTED BY: S. SCHUKRAFT

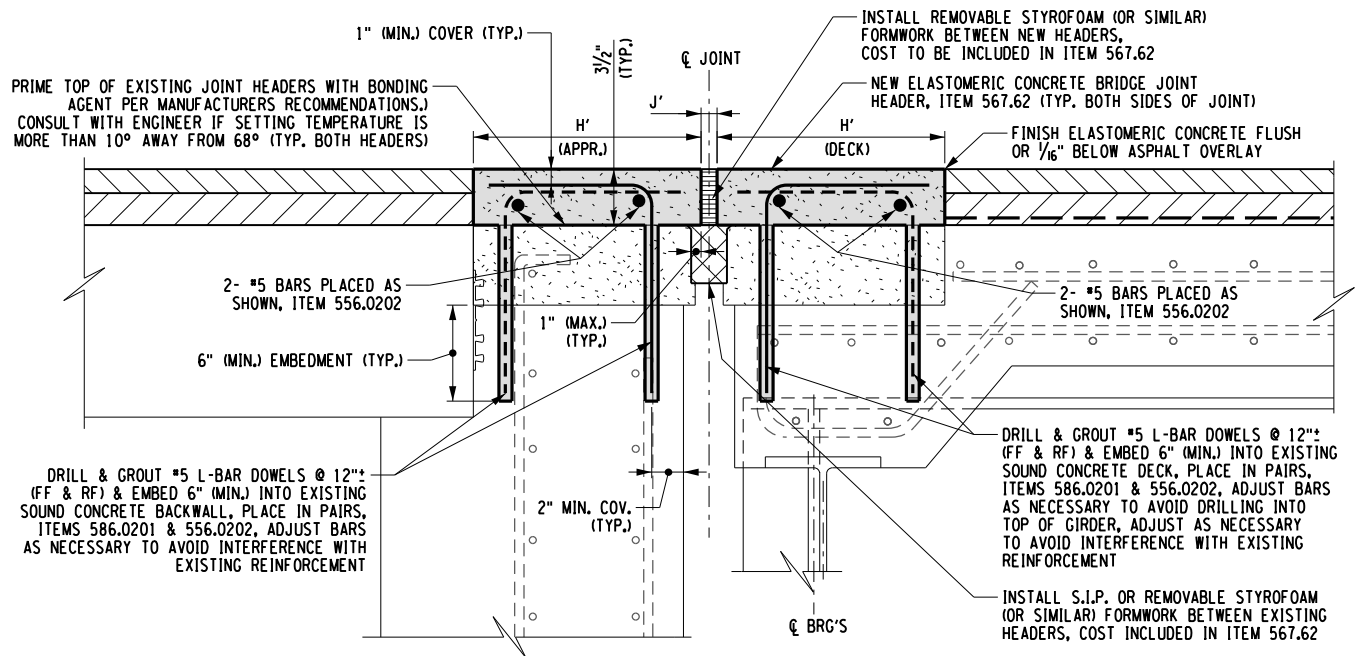
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DESIGNED BY: S. SCHUKRAFT

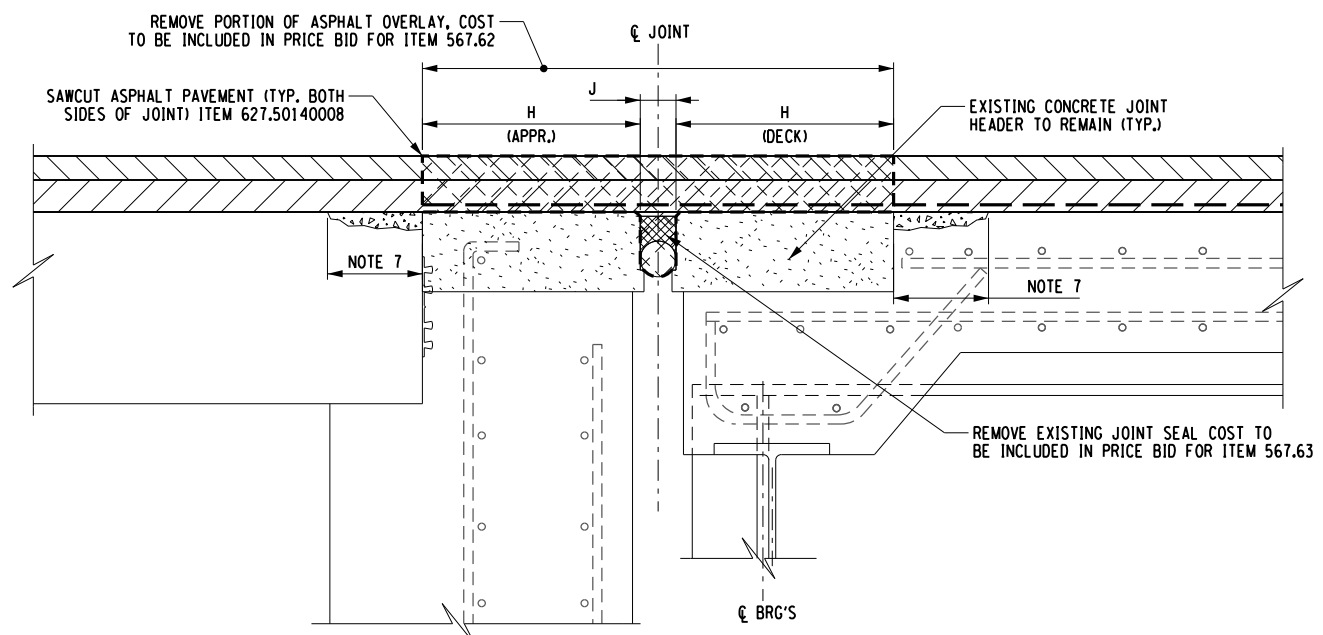
DESIGN SUPERVISOR: D. MARSH



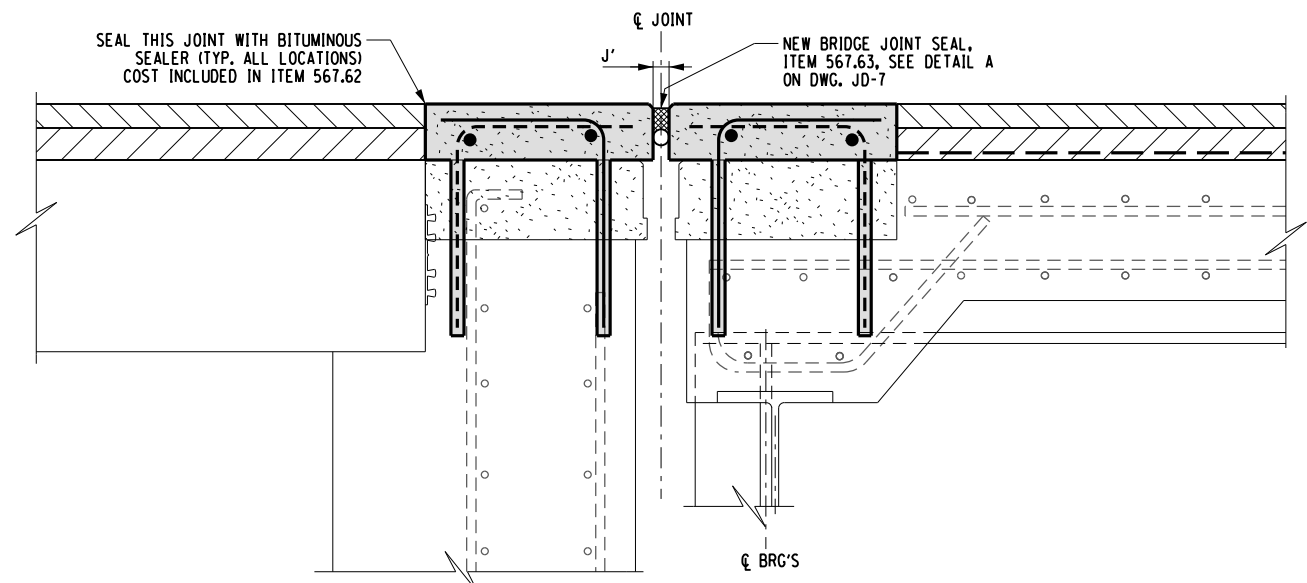
**PHASE I - PLACE ASPHALT OVERLAY OVER EXISTING JOINT**  
N.T.S.



**PHASE III - DRILL & GROUT DOWELS, CONSTRUCT ELASTOMERIC CONCRETE HEADERS**  
N.T.S.



**PHASE II - SAWCUT & REMOVE PORTION OF ASPHALT OVERLAY**  
N.T.S.



**PHASE IV - REMOVE TEMPORARY FORMWORK & INSTALL JOINT SEAL**  
N.T.S.

**NOTES:**

- FOR "J" DIMENSIONS, AND "H" VALUES, SEE BRIDGE JOINT TABLES ON DWG'S JD-1 AND JD-2.
- THE JOINT HEADER REPAIR DETAILS SHOWN ON THIS DRAWING ARE BASED ON THE ASSUMPTION THAT THE EXISTING CONCRETE JOINT HEADERS ARE IN SATISFACTORY AND SOUND CONDITION AND FREE OF LOOSE OR DETERIORATED CONCRETE, AS DETERMINED BY THE ENGINEER IN CHARGE.
- IF EXISTING JOINT HEADERS ARE IN POOR CONDITION (AS DETERMINED BY E.I.C.) AND CONSTRUCTING NEW HEADERS ON TOP OF THE EXISTING HEADERS IS NOT FEASIBLE, USE JOINT REPLACEMENT METHOD B.
- PRIOR TO PLACING THE NEW ASPHALT OVERLAY, THE CONTRACTOR SHALL INSPECT ALL JOINT HEADERS AND CONSULT WITH THE E.I.C. TO DETERMINE WHICH JOINT REPLACEMENT METHOD WILL BE USED AT EACH BRIDGE LOCATION.
- PRIOR TO PLACING THE NEW ASPHALT OVERLAY, THE CONTRACTOR SHALL FIELD MEASURE THE LIMITS OF THE EXISTING JOINT HEADERS, RECORD MEASUREMENTS AND/OR MAKE TEMPORARY REFERENCE MARKS ON THE EXISTING CONCRETE BARRIERS IN ORDER TO EASILY DETERMINE THE LOCATIONS OF THE PAVEMENT SAWCUT LIMITS DURING PHASE II.
- DIMENSIONS SHOWN ON THESE DETAILS PERTAINING TO THE EXISTING BRIDGE GEOMETRY AS WELL AS PROPOSED REPAIRS ARE APPROXIMATE AND BASED ORIGINAL DESIGN PLANS & VARIOUS REHABILITATION PROJECTS INFO. THE CONTRACTOR WILL BE RESPONSIBLE FOR FIELD VERIFYING ALL DIMENSIONS AT EACH BRIDGE LOCATION AND ADJUSTING QUANTITIES AS NECESSARY.
- BRIDGE DECK AND APPROACH SLAB AREAS ADJACENT THE JOINT HEADERS THAT CONTAIN LOOSE, DETERIORATED OR SPALLED CONCRETE (AS DETERMINED BY THE E.I.C.) WILL REQUIRE PARTIAL-DEPTH REPAIRS. SEE DWG. BR-1 FOR TYPICAL REPAIR DETAILS.
- REFER TO DWG. JD-1 FOR ADDITIONAL NOTES.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

JOINT REPLACEMENT - METHOD A	
TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING BRIDGE JOINT REPAIR DETAILS (AT ABUTMENTS) METHOD A	DRAWING NUMBER: JD-3

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:



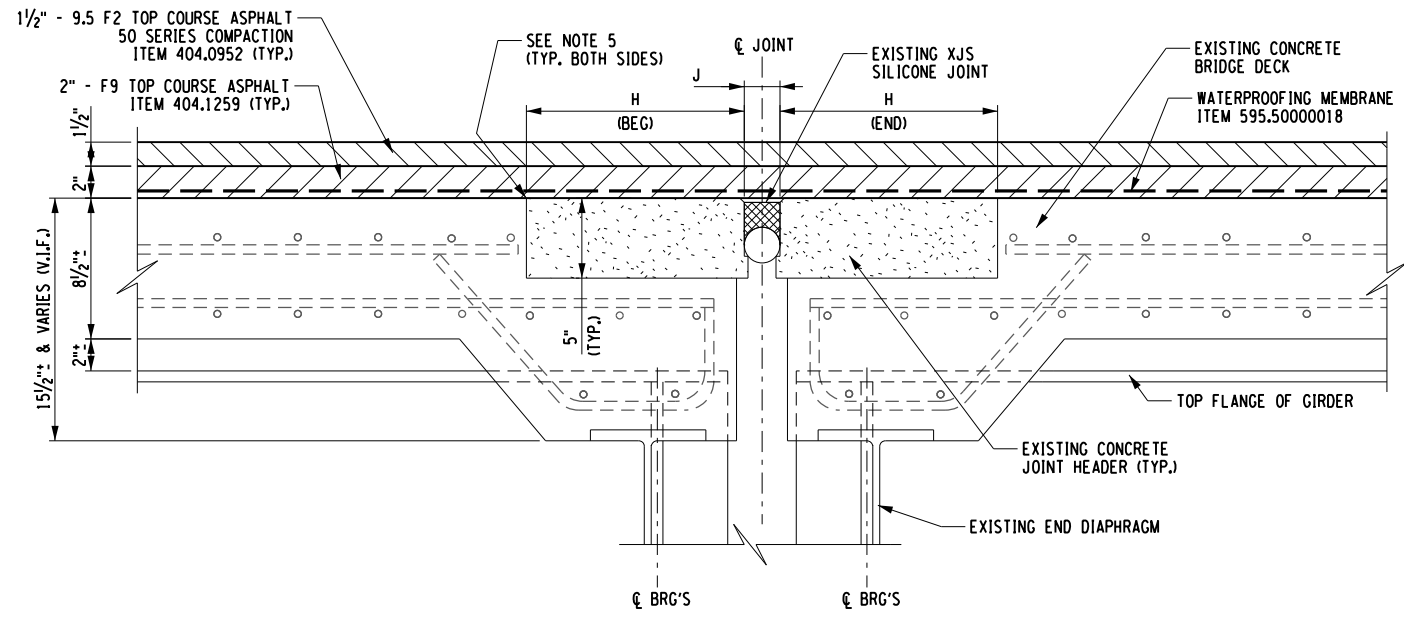
CHECKED BY: Y. DRIHMI

DRAFTED BY: S. SCHUKRAFT

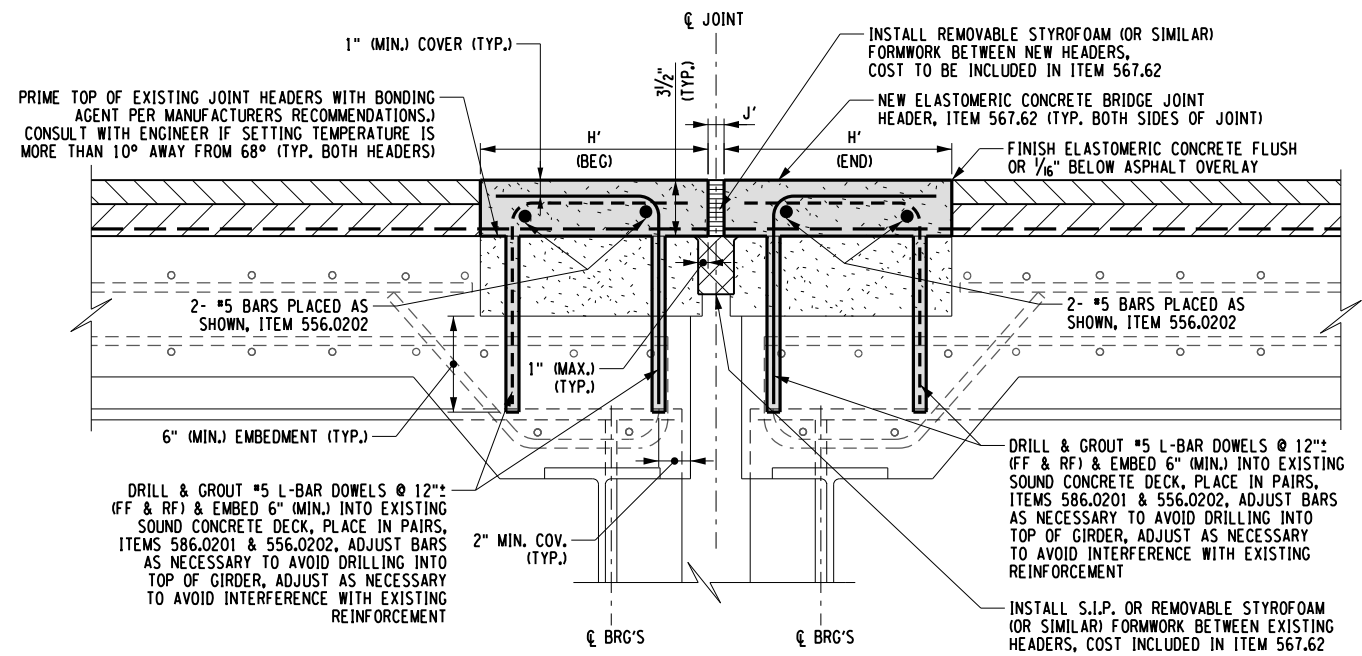
CHECKED BY: Y. DRIHMI

DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH

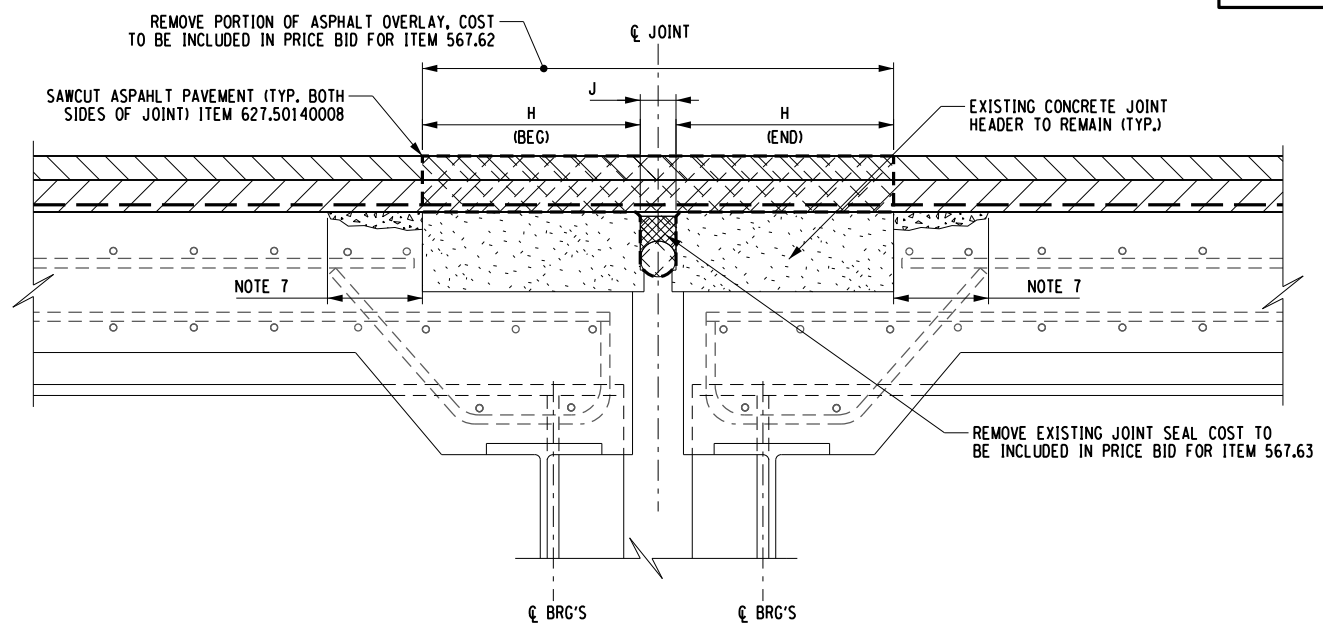


**PHASE I - PLACE ASPHALT OVERLAY OVER EXISTING JOINT**  
N.T.S.

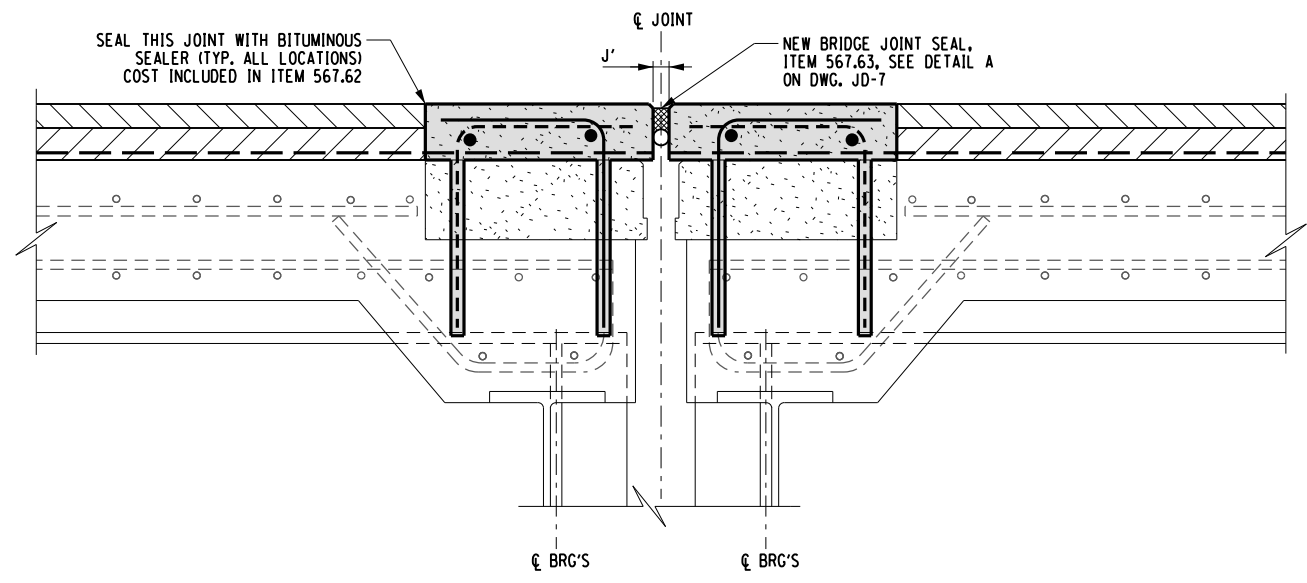


**PHASE III - DRILL & GROUT DOWELS, CONSTRUCT ELASTOMERIC CONCRETE HEADERS**  
N.T.S.

NOTE: DETAILS SHOWN ON THIS DRAWING ARE TYPICAL FOR LOCATIONS WITH JOINT HEADER WIDTHS THAT ARE GREATER THAN 11"



**PHASE II - SAWCUT & REMOVE PORTION OF ASPHALT OVERLAY**  
N.T.S.



**PHASE IV - REMOVE TEMPORARY FORMWORK & INSTALL JOINT SEAL**  
N.T.S.

**NOTES:**

- FOR "J" DIMENSIONS, AND "H" VALUES, SEE BRIDGE JOINT TABLES ON DWG'S JD-1 AND JD-2.
- THE JOINT HEADER REPAIR DETAILS SHOWN ON THIS DRAWING ARE BASED ON THE ASSUMPTION THAT THE EXISTING CONCRETE JOINT HEADERS ARE IN SATISFACTORY AND SOUND CONDITION AND FREE OF LOOSE OR DETERIORATED CONCRETE, AS DETERMINED BY THE ENGINEER IN CHARGE.
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- PRIOR TO PLACING THE NEW ASPHALT OVERLAY, THE CONTRACTOR SHALL INSPECT ALL JOINT HEADERS AND CONSULT WITH THE E.I.C. TO DETERMINE WHICH JOINT REPLACEMENT METHOD WILL BE USED AT EACH BRIDGE LOCATION.
- PRIOR TO PLACING THE NEW ASPHALT OVERLAY, THE CONTRACTOR SHALL FIELD MEASURE THE LIMITS OF THE EXISTING JOINT HEADERS, RECORD MEASUREMENTS AND/OR MAKE TEMPORARY REFERENCE MARKS ON THE EXISTING CONCRETE BARRIERS IN ORDER TO EASILY DETERMINE THE LOCATIONS OF THE PAVEMENT SAWCUT LIMITS DURING PHASE II.
- DIMENSIONS SHOWN ON THESE DETAILS PERTAINING TO THE EXISTING BRIDGE GEOMETRY AS WELL AS PROPOSED REPAIRS ARE APPROXIMATE AND BASED ON ORIGINAL DESIGN PLANS & VARIOUS REHABILITATION PROJECTS INFO. THE CONTRACTOR WILL BE RESPONSIBLE FOR FIELD VERIFYING ALL DIMENSIONS AT EACH BRIDGE LOCATION AND ADJUSTING QUANTITIES AS NECESSARY.
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- REFER TO DWG. JD-1 FOR ADDITIONAL NOTES.

ALTERED ON: \_\_\_\_\_  
AFFIXED ON: 5/10/2024  
SIGNATURE: \_\_\_\_\_  
STAMP: \_\_\_\_\_



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



JOINT REPLACEMENT - METHOD A	
TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING BRIDGE JOINT REPAIR DETAILS (AT PIERS) METHOD A	DRAWING NUMBER: JD-4

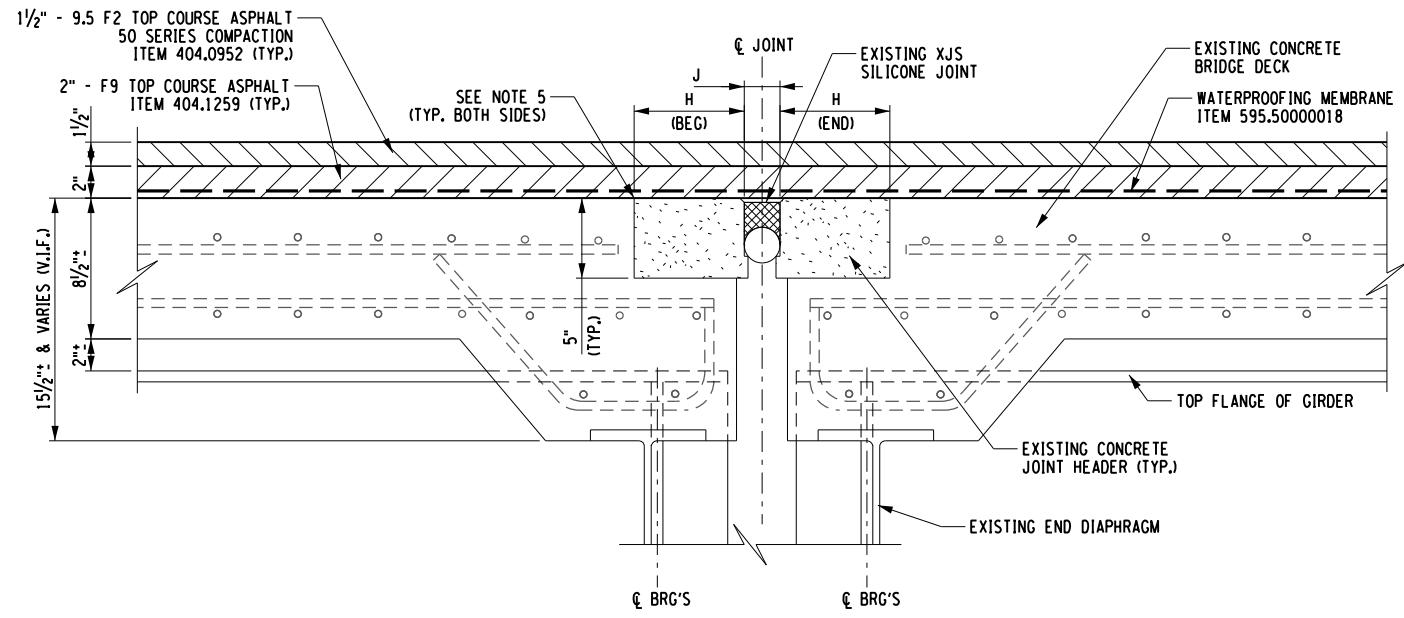
CHECKED BY: Y. DRIHMI

DRAFTED BY: S. SCHUKRAFT

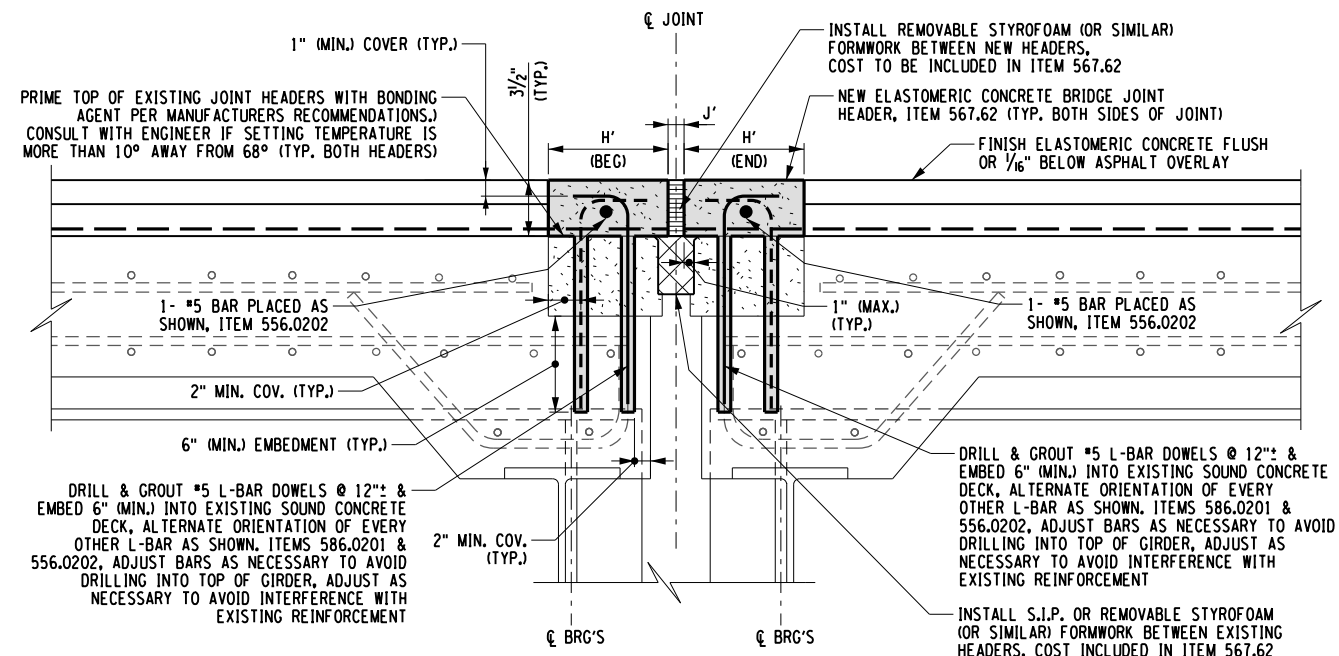
CHECKED BY: Y. DRIHMI

DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH

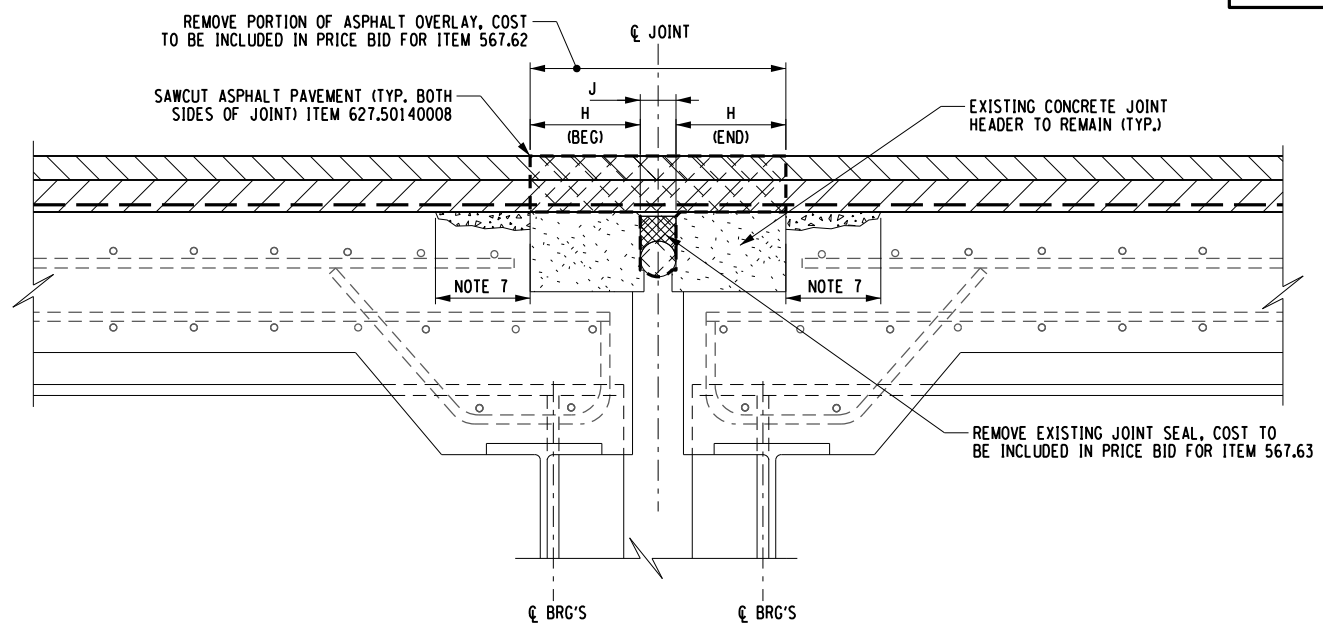


**PHASE I - PLACE ASPHALT OVERLAY OVER EXISTING JOINT**  
N.T.S.

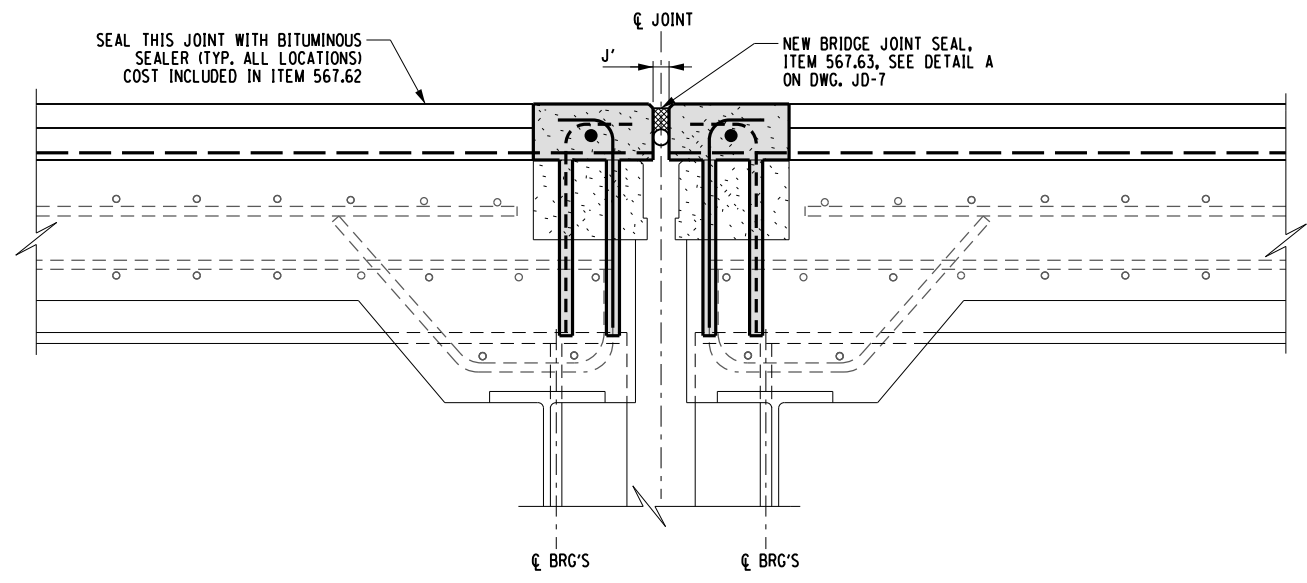


**PHASE III - DRILL & GROUT DOWELS, CONSTRUCT ELASTOMERIC CONCRETE HEADERS**  
N.T.S.

NOTE: DETAILS SHOWN ON THIS DRAWING ARE TYPICAL FOR LOCATIONS WITH JOINT HEADER WIDTHS THAT ARE LESS THAN 11"



**PHASE II - SAWCUT & REMOVE PORTION OF ASPHALT OVERLAY**  
N.T.S.



**PHASE IV - REMOVE TEMPORARY FORMWORK & INSTALL JOINT SEAL**  
N.T.S.

**NOTES:**

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

JOINT REPLACEMENT - METHOD A	
TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING BRIDGE JOINT REPAIR DETAILS (AT PIERS) METHOD A	DRAWING NUMBER: JD-5

ALTERED ON: 5/10/2024

AFFIXED ON: 5/10/2024

SIGNATURE: [Signature]

STAMP: [Professional Engineer Seal for Y. Drihmi, State of New York, License No. 100805]

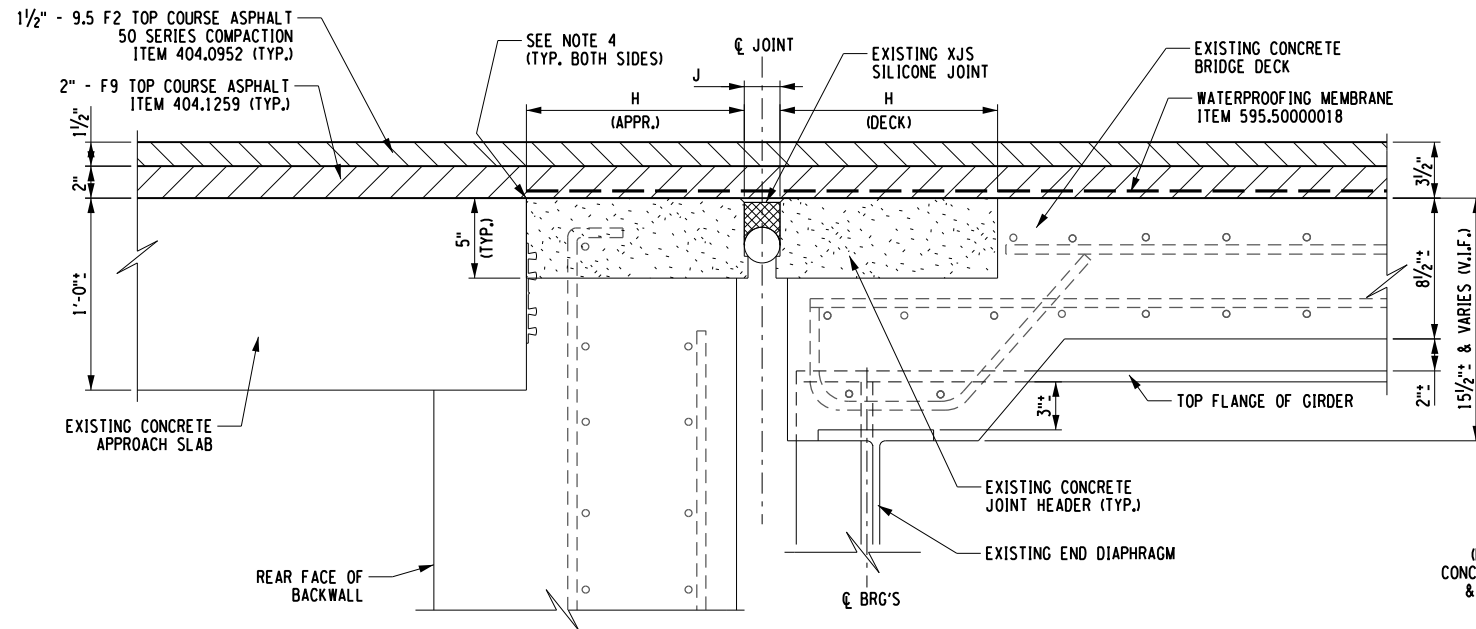
CHECKED BY: Y. DRHMI

DRAFTED BY: S. SCHUKRAFT

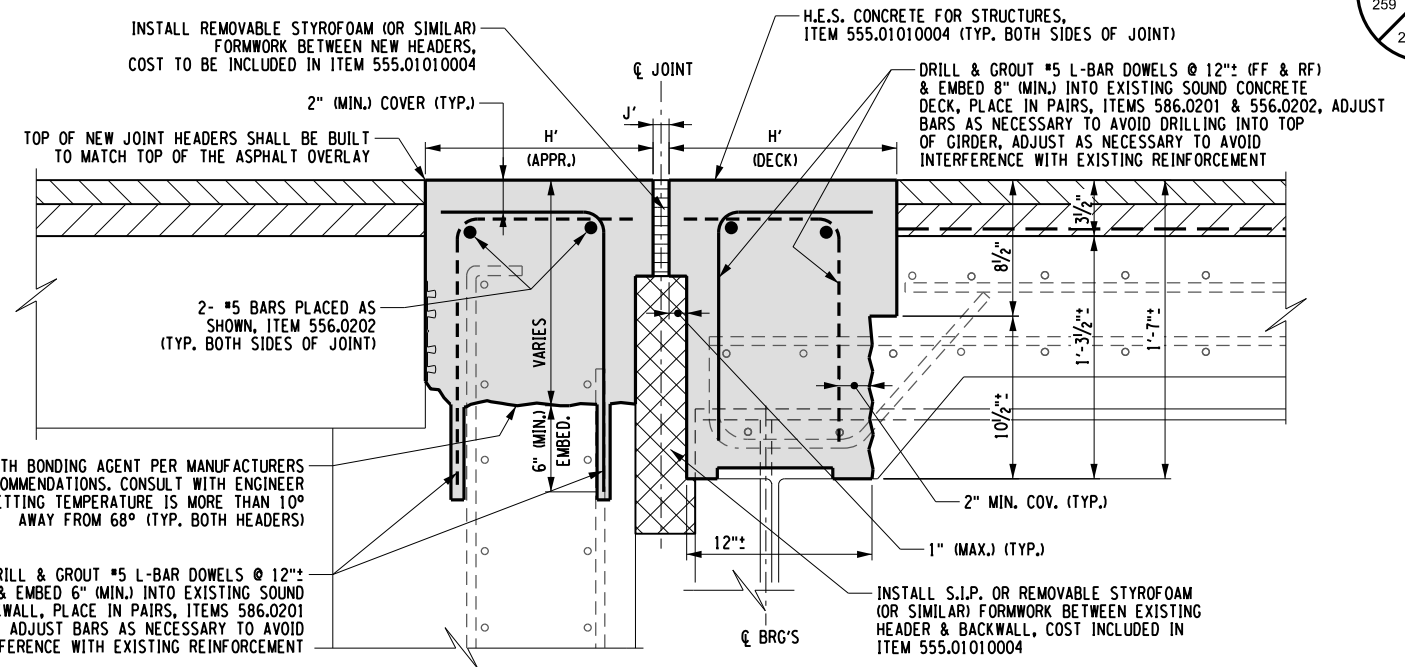
CHECKED BY: Y. DRHMI

DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH

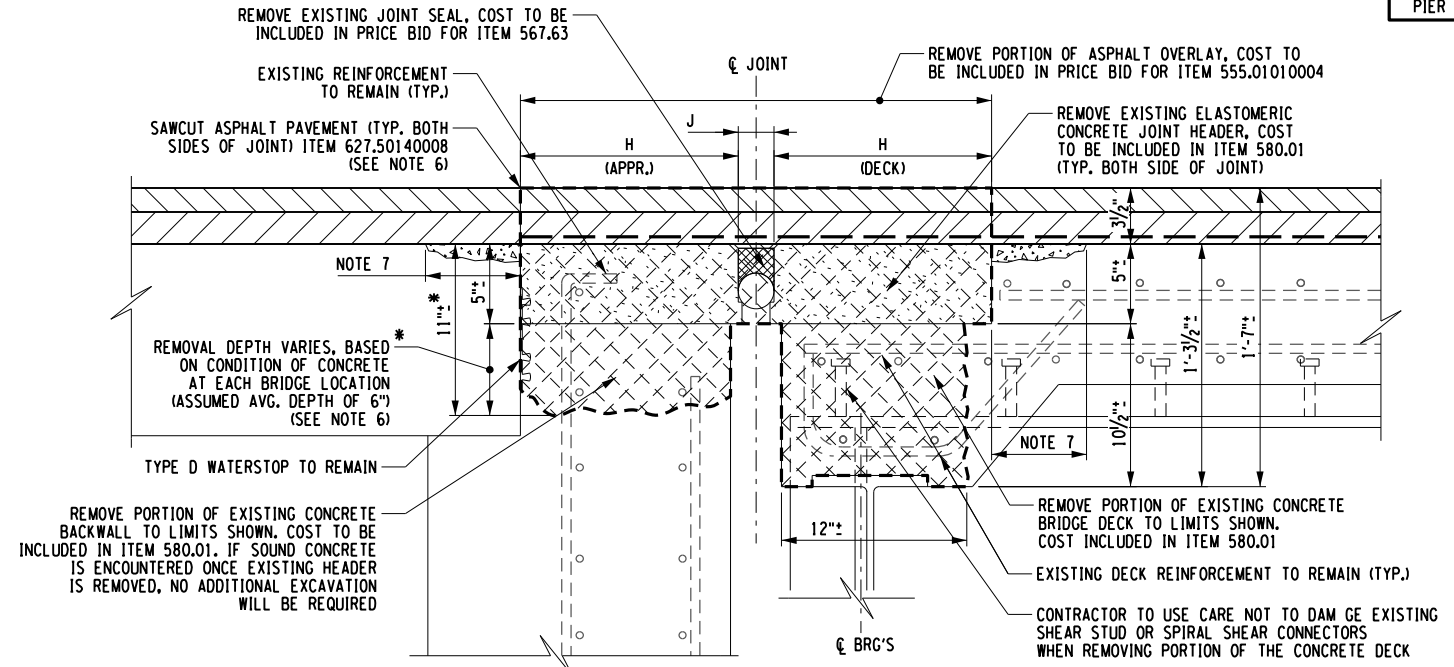


**PHASE I - PLACE ASPHALT OVERLAY OVER EXISTING JOINT**  
N.T.S.

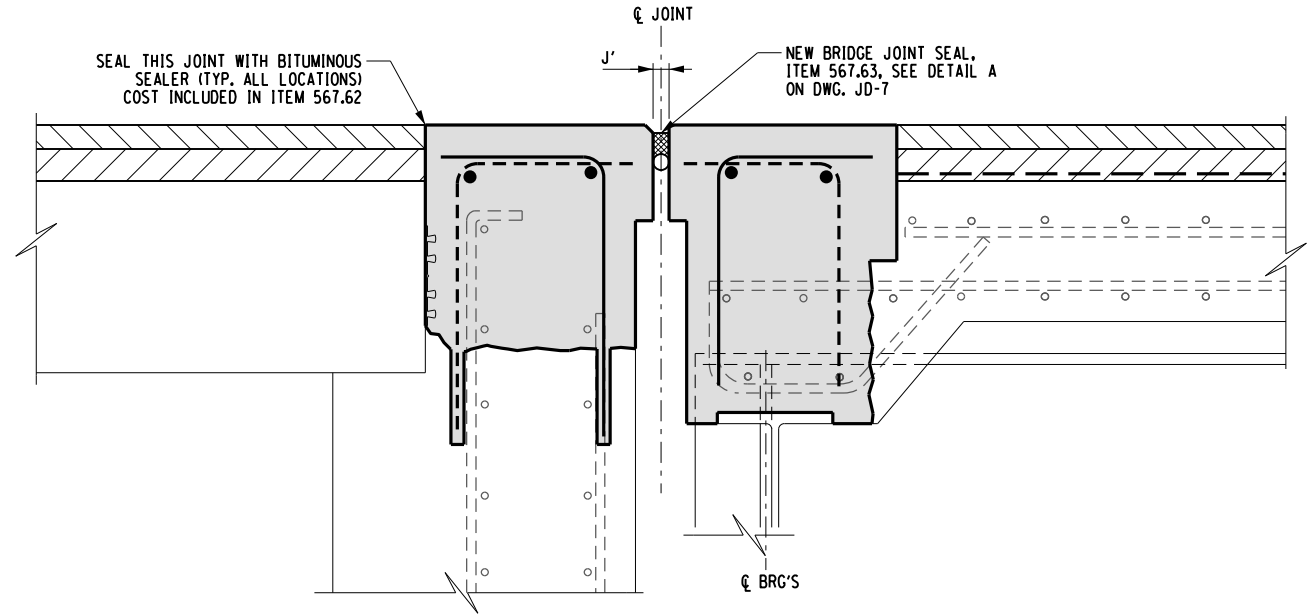


**PHASE III - DRILL & GROUT DOWELS, CONSTRUCT H.E.S. CONCRETE HEADERS**  
N.T.S.

ABUTMENT DETAILS SHOWN  
PIER DETAILS SIMILAR



**PHASE II - SAWCUT & REMOVE PORTION OF ASPHALT OVERLAY, REMOVE EXISTING JOINT HEADERS, BACKWALL & DECK CONCRETE**  
N.T.S.



**PHASE IV - REMOVE TEMPORARY FORMWORK & INSTALL JOINT SEAL**  
N.T.S.

**LEGEND**

REMOVAL LIMITS OF EXISTING ELASTOMERIC CONCRETE HEADERS, BRIDGE DECK & BACKWALL CONCRETE, ITEM 580.01

H.E.S. CONCRETE ITEM 555.01010004

8. REFER TO DWG. JD-1 FOR ADDITIONAL NOTES.

**NOTES:**

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



JOINT REPLACEMENT - METHOD B	
TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING BRIDGE JOINT REPAIR DETAILS (AT ABUTMENTS) METHOD B	DRAWING NUMBER: JD-6

ALTERED ON: 5/10/2024

AFFIXED ON: 5/10/2024

SIGNATURE: [Blank]

STAMP: [Blank]

STATE OF NEW YORK  
YOUNES DRHMI  
LICENSED PROFESSIONAL ENGINEER  
100805  
5/10/24

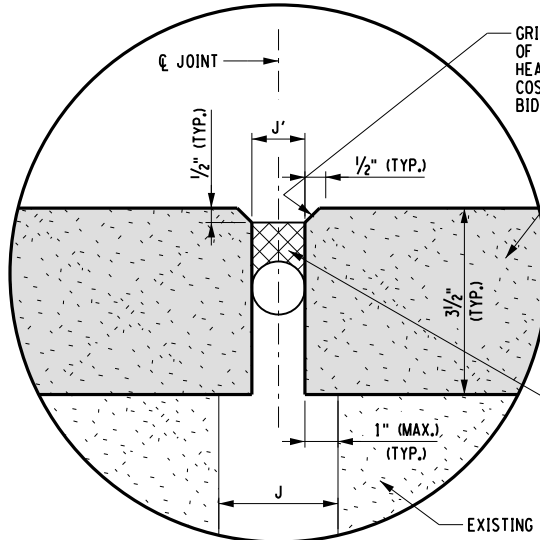
CHECKED BY: Y. DRIHMI

DRAFTED BY: S. SCHUKRAFT

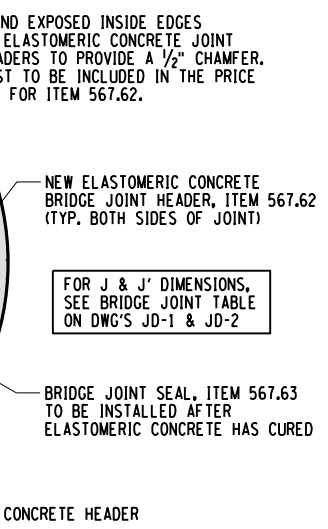
CHECKED BY: Y. DRIHMI

DESIGNED BY: S. SCHUKRAFT

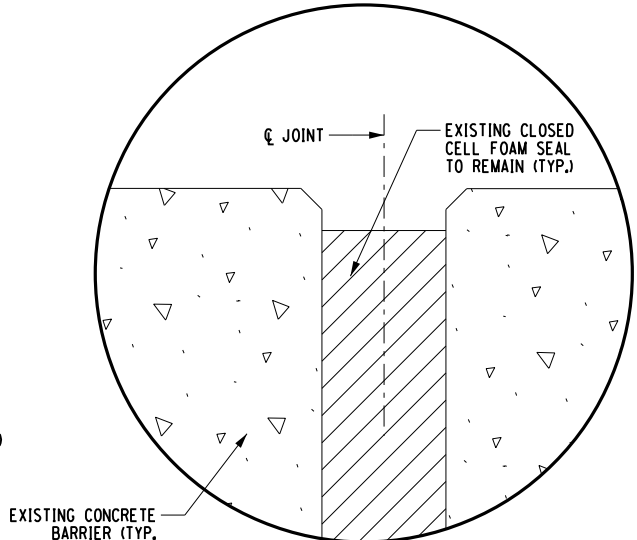
DESIGN SUPERVISOR: D. MARSH



**SECTION A-A**  
**NEW JOINT SEAL AT HEADERS**  
N.T.S.

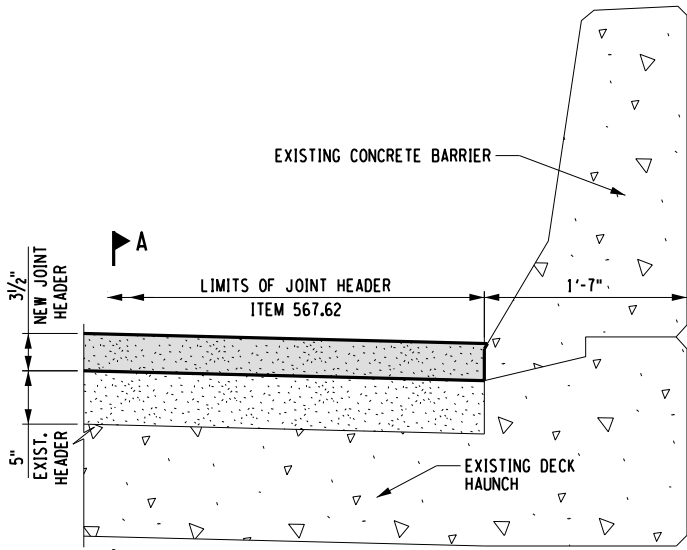


**SECTION B-B**  
**NEW JOINT SEAL AT HEADERS**  
N.T.S.

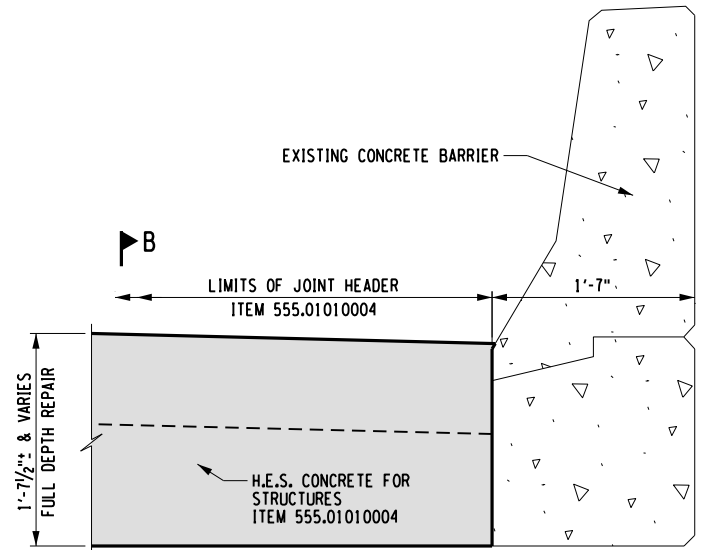


**SECTION C-C**  
**EXISTING JOINT SEAL AT BARRIER**  
N.T.S.

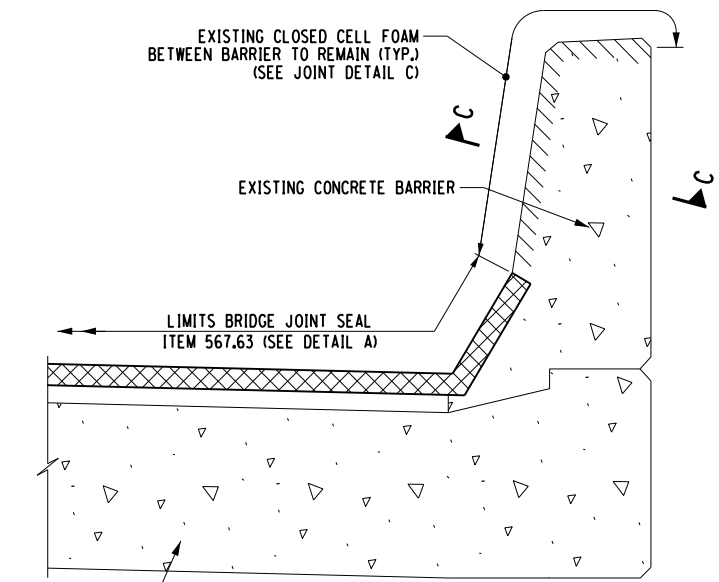
**NOTES:**  
1. A BRIDGE JOINT SEAL WITH FOAM-SUPPORTED SILICONE IN ACCORDANCE WITH MATERIAL SPECIFICATION 705-23 FROM THE DEPARTMENTS APPROVED LIST SHALL BE USED FOR ITEM 567.63 - BRIDGE JOINT SEAL.



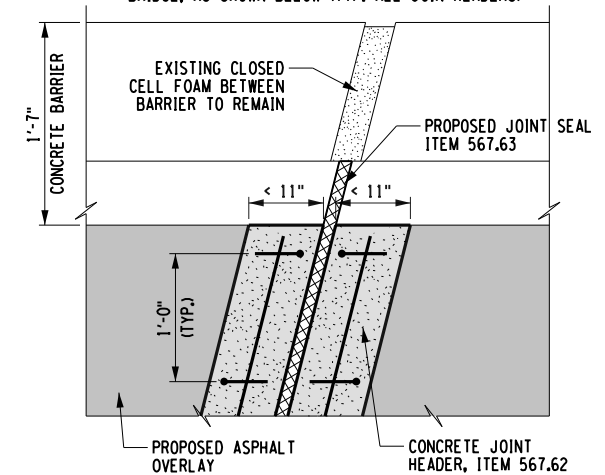
**JOINT HEADER DETAIL - METHOD A**  
N.T.S.



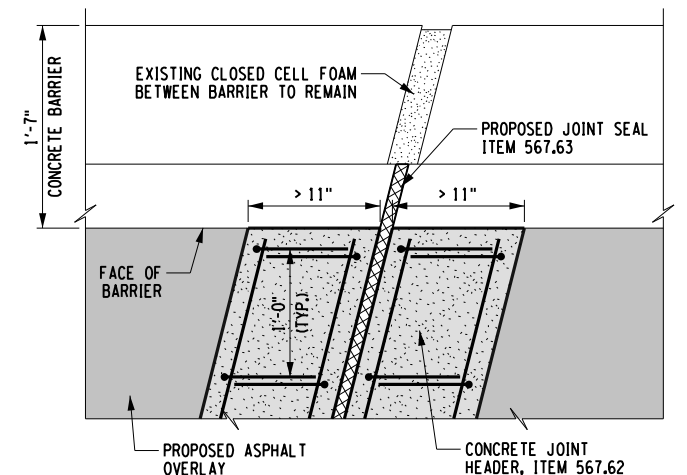
**JOINT HEADER DETAIL - METHOD B**  
N.T.S.



**JOINT SEAL DETAIL**  
N.T.S.

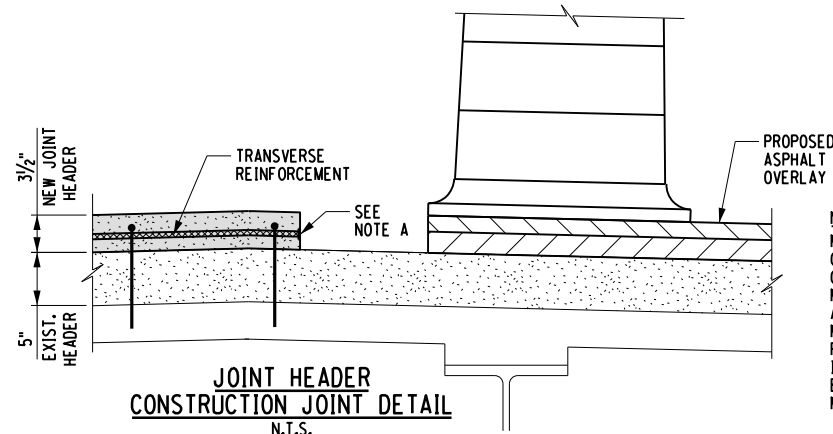


**JOINT HEADER WIDTH'S LESS THAN 11"**



**JOINT HEADER WIDTH'S GREATER THAN 11"**

**TYPICAL PLAN AT CONCRETE BARRIER**  
N.T.S.



**JOINT HEADER CONSTRUCTION JOINT DETAIL**  
N.T.S.

**NOTE 'A'**  
MECHANICAL CONNECTORS SHALL BE PROVIDED AT ALL STAGED CONSTRUCTION JOINTS IN THE ENDS OF JOINT HEADERS. THE MECHANICAL CONNECTORS PROVIDED SHALL BE ON THE NYSDOT APPROVED LIST FOR MECHANICAL CONNECTORS FOR REINFORCING BAR SPLICES (709-10). IN ADDITION, MECHANICAL CONNECTORS SHALL BE ABLE TO DEVELOP AT A MINIMUM OF 125% OF THE YIELD STRENGTH OF THE GRADE 60 REINFORCEMENT. THE COST OF THE MECHANICAL CONNECTORS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES. NO DIRECT PAYMENT WILL BE MADE. (TYP. ALL JOINTS AND ALL BRIDGES).

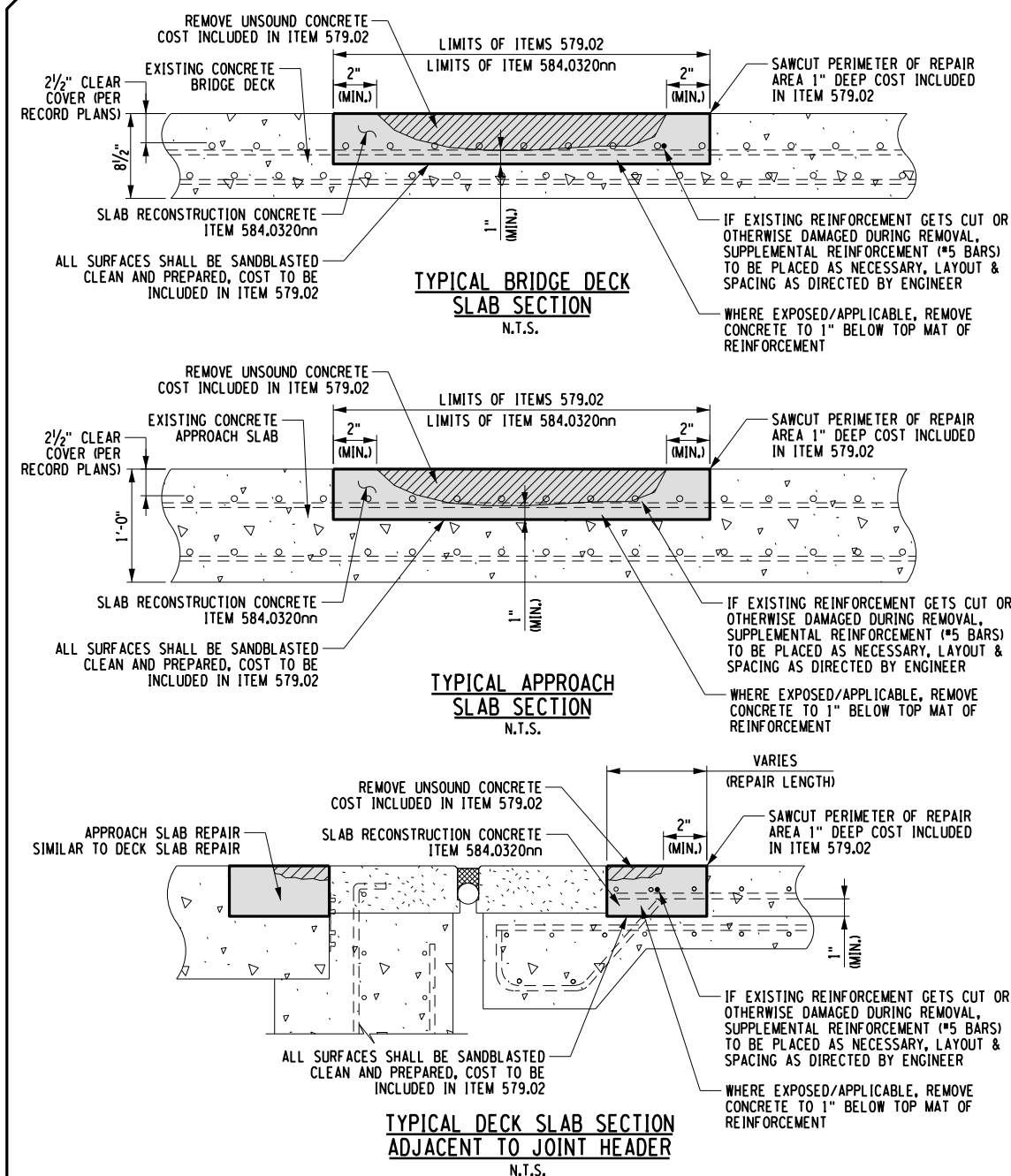
ALTERED ON:	AFFIXED ON: 5/10/2024
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING MISC. BRIDGE JOINT DETAILS	DRAWING NUMBER: JD-7

CHECKED BY: Y. DRIHIM  
DRAFTED BY: S. SCHUKRAFT  
DESIGNED BY: S. SCHUKRAFT  
CHECKED BY: Y. DRIHIM  
DESIGN SUPERVISOR: D. MARSH



**DECK REPAIR NOTES:**

- ALL DECK REPAIRS SHALL BE COMPLETED PRIOR TO THE OVERLAY PLACEMENT.
- THE CONTRACTOR SHALL NOTE THAT THESE ARE TYPICAL REMOVAL DETAILS AND NOT ALL FIELD CONDITIONS MAY BE SHOWN. THE ENGINEER WILL MAKE ANY CHANGES TO REPAIR SIZES AND TYPES AS NEEDED.
- THE CONTRACTOR SHALL MARK OUT CLEARLY THE SAW CUT BOUNDARY LINES. THE ENGINEER WILL INFORM THE CONTRACTOR WHETHER THE MARK OUT IS ACCEPTABLE AND THE CONCRETE REMOVAL CAN BEGIN.
- THE COST FOR ALL SAWCUTTING, ALONG WITH THE REMOVAL AND DISPOSAL OF THE CONCRETE SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 579.02.

ALTERED ON: 5/10/2024

AFFIXED ON: 5/10/2024

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IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

BRIDGE OVER	LOCATION	REINFORCING BAR EXPOSURE			SLAB RECONSTRUCTION CONCRETE		
		ITEM 579.02			ITEM 584.0320nn		
		NB AREA (SF)	SB AREA (SF)	TOTAL (SF)	NB AREA (SY)	SB AREA (SY)	TOTAL (SY)
CLINTON ST M.P. 901.56 BIN 5046459	BEGIN APPROACH SLAB	514	514	4,347	57	57	483
	SPAN 1 DECK	234	234		26	26	
	SPAN 2 DECK	644	644		72	72	
	SPAN 3 DECK	268	268		30	30	
	END APPROACH SLAB	514	514		57	57	
SENECA ST M.P. 902.22 BIN 5011959	BEGIN APPROACH SLAB	307	307	3,845	34	34	427
	SPAN 1 DECK	321	321		36	36	
	SPAN 2 DECK	646	646		72	72	
	SPAN 3 DECK	342	342		38	38	
	END APPROACH SLAB	307	307		34	34	
MILTON ST M.P. 902.44 BIN 5512209	BEGIN APPROACH SLAB	552	401	3,257	61	45	362
	SPAN 1 DECK	210	191		23	21	
	SPAN 2 DECK	299	272		33	30	
	SPAN 3 DECK	199	181		22	20	
	END APPROACH SLAB	552	401		61	45	
BABCOCK ST M.P. 902.63 BIN 5512219	BEGIN APPROACH SLAB	279	279	2,810	31	31	312
	SPAN 1 DECK	184	184		20	20	
	SPAN 2 DECK	353	353		39	39	
	SPAN 3 DECK	188	188		21	21	
	END APPROACH SLAB	401	401		45	45	
PEABODY ST M.P. 902.84 BIN 5512229	BEGIN APPROACH SLAB	373	373	2,755	41	41	306
	SPAN 1 DECK	168	168		19	19	
	SPAN 2 DECK	271	271		30	30	
	SPAN 3 DECK	193	193		21	21	
	END APPROACH SLAB	373	373		41	41	
SMITH ST M.P. 903.14 BIN 5512249	BEGIN APPROACH SLAB	262	262	3,034	29	29	337
	SPAN 1 DECK	252	252		28	28	
	SPAN 2 DECK	475	475		53	53	
	SPAN 3 DECK	266	266		30	30	
	END APPROACH SLAB	262	262		29	29	
ERIE RR. M.P. 903.32 BIN 5512259	BEGIN APPROACH SLAB	614	435	4,032	68	48	448
	SPAN 1 DECK	326	297		36	33	
	SPAN 2 DECK	399	364		44	40	
	SPAN 3 DECK	287	261		32	29	
	END APPROACH SLAB	614	435		68	48	
VAN RENSSELAER ST M.P. 903.48 BIN 5512269	BEGIN APPROACH SLAB	367	367	2,921	41	41	325
	SPAN 1 DECK	247	247		27	27	
	SPAN 2 DECK	270	270		30	30	
	SPAN 3 DECK	210	210		23	23	
	END APPROACH SLAB	367	367		41	41	
CSX RR M.P. 903.66 BIN 5512279	BEGIN APPROACH SLAB	195	195	3,542	22	22	394
	SPAN 1 DECK	318	318		35	35	
	SPAN 2 DECK	304	304		34	34	
	SPAN 3 DECK	460	460		51	51	
	SPAN 4 DECK	299	299		33	33	
HAMBURG ST M.P. 903.79 BIN 5512289	BEGIN APPROACH SLAB	481	361	3,413	53	40	379
	SPAN 1 DECK	252	229		28	25	
	SPAN 2 DECK	376	342		42	38	
	SPAN 3 DECK	278	253		31	28	
	END APPROACH SLAB	481	361		53	40	
		SUB-TOTAL		33,956	SUB-TOTAL		3,773

\* FOR ITEM SERIALIZATION, SEE TABLE THIS SHEET

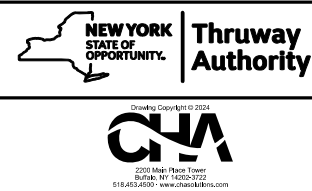
BRIDGE OVER	LOCATION	REINFORCING BAR EXPOSURE			SLAB RECONSTRUCTION CONCRETE		
		ITEM 579.02			ITEM 584.0320nn		
		NB AREA (SF)	SB AREA (SF)	TOTAL (SF)	NB AREA (SY)	SB AREA (SY)	TOTAL (SY)
BAILEY AVE & NS RR M.P. 901.88 BIN 5512199	BEGIN APPROACH SLAB	174	173	5,988	19	19	665
	SPAN 1 DECK	211	184		23	20	
	SPAN 2 DECK	253	226		28	25	
	SPAN 3 DECK	179	186		20	21	
	SPAN 4 DECK	209	190		23	21	
	SPAN 5 DECK	205	0		23	0	
	SPAN 6 DECK	168	176		19	20	
	SPAN 7 DECK	341	313		38	35	
	SPAN 8 DECK	0	197		0	22	
	SPAN 9 DECK	172	200		19	22	
	SPAN 10 DECK	0	197		0	22	
	SPAN 11 DECK	251	201		28	22	
	SPAN 12 DECK	0	331		0	37	
SPAN 13 DECK	596	163	66	18			
END APPROACH SLAB	246	244	27	27			
SELKIRK ST & RR YARD M.P. 902.99 BIN 5512239	BEGIN APPROACH SLAB	130	130	3,462	14	14	385
	SPAN 1 DECK	133	146		15	16	
	SPAN 2 DECK	248	271		28	30	
	SPAN 3 DECK	257	281		29	31	
	SPAN 4 DECK	260	284		29	32	
	SPAN 5 DECK	223	244		25	27	
	SPAN 6 DECK	180	162		20	18	
	SPAN 7 DECK	134	146		15	16	
END APPROACH SLAB	117	117	13	13			
		SUB-TOTAL		9,450	SUB-TOTAL		1,050
		TOTAL		43,406	TOTAL		4,823

\* FOR ITEM SERIALIZATION, SEE TABLE THIS SHEET

- REPAIR QUANTITY NOTES:**
- THE REPAIR AREA QUANTITIES SHOWN FOR ITEMS 579.02 & 584.0320nn ARE APPROXIMATE AND SHOWN TO HELP FACILITATE BIDDING.
  - THE REPAIR QUANTITIES SHOWN ARE BASED ON AN INFORMATION COLLECTED FROM RECENT BRIDGE INSPECTION REPORTS AS WELL AS PRELIMINARY DETERIORATION PERCENTAGES DETECTED FROM THE GPR DATA. THESE REPAIR QUANTITIES MAY VARY SLIGHTLY WHEN THE FINAL MAPPING IS COMPLETE.
  - AS OF PROJECT ADVERTISEMENT, THE GPR DATA IS STILL BEING PROCESSED. THE GPR REPORT INDICATING REPAIR SIZES & LOCATION FOR EACH BRIDGE DECK WILL BE PROVIDED TO THE CONTRACTOR PRIOR TO CONSTRUCTION.

SLAB RECONSTRUCTION CONCRETE ITEM SERIALIZATION			
BRIDGE OVER	BIN	MILEPOST	ITEM NUMBER
CLINTON ST	5046459	M.P. 901.56	584.032001
SENECA ST	5011959	M.P. 902.22	584.032002
MILTON ST	5512209	M.P. 902.44	584.032003
BABCOCK ST	5512219	M.P. 902.63	584.032004
PEABODY ST	5512229	M.P. 902.84	584.032005
SMITH ST	5512249	M.P. 903.14	584.032006
ERIE RR.	5512259	M.P. 903.32	584.032007
VAN RENSSELAER ST	5512269	M.P. 903.48	584.032008
CSX RR	5512279	M.P. 903.66	584.032009
HAMBURG ST	5512289	M.P. 903.79	584.032010
BAILEY AVE	5512199	M.P. 901.88	584.032011
SELKIRK ST	5512239	M.P. 902.99	584.032012

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING BRIDGE DECK & APPROACH SLAB REPAIRS	DRAWING NUMBER: BR-1

CHECKED BY: Y. DRIHIM

DRAFTED BY: S. SCHUKRAFT

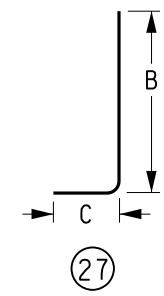
CHECKED BY: Y. DRIHIM

DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER CLINTON STREET - MP 901.56 - NORTHBOUND																	
BEGIN ABUTMENT JOINT (POUR 1)																	
L-BAR DOWELS (APPR.)	5SE1	134	5	27	2' - 10"	396		13"	21"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE10	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE II (APPR.)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (APPR.)	5SE12	2	5	STR.	30' - 2"	63											
L-BAR DOWELS (DECK)	5SE2	134	5	27	2' - 0"	280		13"	11"								
TRANSVERSE BARS - STAGE I (DECK)	5SE10	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE II (DECK)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (DECK)	5SE12	2	5	STR.	30' - 2"	63											
BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL:						952	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
PIER 1 JOINT (POUR 2)																	
L-BAR DOWELS (BEG.)	5SE2	134	5	27	2' - 0"	280		13"	11"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE10	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE II (BEG.)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (BEG.)	5SE12	2	5	STR.	30' - 2"	63											
L-BAR DOWELS (END)	5SE2	134	5	27	2' - 0"	280		13"	11"								
TRANSVERSE BARS - STAGE I (END)	5SE10	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE II (END)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (END)	5SE12	2	5	STR.	30' - 2"	63											
PIER 1 JOINT (POUR 2) SUB-TOTAL:						836	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
PIER 2 JOINT (POUR 3)																	
L-BAR DOWELS (BEG.)	5SE2	134	5	27	2' - 0"	280		13"	11"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE10	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE II (BEG.)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (BEG.)	5SE12	2	5	STR.	30' - 2"	63											
L-BAR DOWELS (END)	5SE3	134	5	27	1' - 8"	233		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE10	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE II (END)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (END)	5SE12	2	5	STR.	30' - 2"	63											
PIER 2 JOINT (POUR 3) SUB-TOTAL:						789	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
END ABUTMENT JOINT (POUR 4)																	
L-BAR DOWELS (DECK)	5SE2	134	5	27	2' - 0"	280		13"	11"								
TRANSVERSE BARS - STAGE I (DECK)	5SE10	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE II (DECK)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (DECK)	5SE12	2	5	STR.	30' - 2"	63											
L-BAR DOWELS (APPR.)	5SE5	134	5	27	3' - 3"	455		18"	21"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE10	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE II (APPR.)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (APPR.)	5SE12	2	5	STR.	30' - 2"	63											
END ABUTMENT JOINT (POUR 4) SUB-TOTAL:						1,011	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
TOTAL WEIGHT ENTIRE STRUCTURE:						3,588	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER CLINTON STREET - MP 901.56 - SOUTHBOUND																	
BEGIN ABUTMENT JOINT (POUR 1)																	
L-BAR DOWELS (APPR.)	5SE1	134	5	27	2' - 10"	396		13"	21"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE10	2	5	STR.	22' - 10"	48											
TRANSVERSE BARS - STAGE II (APPR.)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (APPR.)	5SE12	2	5	STR.	30' - 1"	63											
L-BAR DOWELS (DECK)	5SE2	134	5	27	2' - 0"	280		13"	11"								
TRANSVERSE BARS - STAGE I (DECK)	5SE10	2	5	STR.	22' - 10"	48											
TRANSVERSE BARS - STAGE II (DECK)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (DECK)	5SE12	2	5	STR.	30' - 1"	63											
BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL:						962	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
PIER 1 JOINT (POUR 2)																	
L-BAR DOWELS (BEG.)	5SE2	134	5	27	2' - 0"	280		13"	11"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE10	2	5	STR.	22' - 10"	48											
TRANSVERSE BARS - STAGE II (BEG.)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (BEG.)	5SE12	2	5	STR.	30' - 1"	63											
L-BAR DOWELS (END)	5SE2	134	5	27	2' - 0"	280		13"	11"								
TRANSVERSE BARS - STAGE I (END)	5SE10	2	5	STR.	22' - 10"	48											
TRANSVERSE BARS - STAGE II (END)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (END)	5SE12	2	5	STR.	30' - 1"	63											
PIER 1 JOINT (POUR 2) SUB-TOTAL:						846	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
PIER 2 JOINT (POUR 3)																	
L-BAR DOWELS (BEG.)	5SE3	134	5	27	1' - 8"	233		13"	7"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE10	2	5	STR.	22' - 10"	48											
TRANSVERSE BARS - STAGE II (BEG.)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (BEG.)	5SE12	2	5	STR.	30' - 1"	63											
L-BAR DOWELS (END)	5SE3	134	5	27	1' - 8"	233		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE10	2	5	STR.	22' - 10"	48											
TRANSVERSE BARS - STAGE II (END)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (END)	5SE12	2	5	STR.	30' - 1"	63											
PIER 2 JOINT (POUR 3) SUB-TOTAL:						752	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
END ABUTMENT JOINT (POUR 4)																	
L-BAR DOWELS (DECK)	5SE2	134	5	27	2' - 0"	280		13"	11"								
TRANSVERSE BARS - STAGE I (DECK)	5SE10	2	5	STR.	22' - 10"	48											
TRANSVERSE BARS - STAGE II (DECK)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (DECK)	5SE12	2	5	STR.	30' - 1"	63											
L-BAR DOWELS (APPR.)	5SE1	134	5	27	2' - 10"	396		13"	21"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE10	2	5	STR.	22' - 10"	48											
TRANSVERSE BARS - STAGE II (APPR.)	5SE11	2	5	STR.	15' - 2"	32											
TRANSVERSE BARS - STAGE III (APPR.)	5SE12	2	5	STR.	30' - 1"	63											
END ABUTMENT JOINT (POUR 4) SUB-TOTAL:						962	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
TOTAL WEIGHT ENTIRE STRUCTURE:						3,522	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										



- NOTES:**
- ALL REINFORCEMENT SHOWN IN THE TABLES ABOVE IS INTENDED TO BE USED FOR "METHOD A" JOINT HEADERS ONLY.
  - THE LENGTHS OF ALL STRAIGHT REINFORCING BARS SHOWN IN THE TABLES ABOVE, WERE DETERMINED BASED ON THE WORK ZONE CONSTRUCTION STAGING LIMITS SHOWN ON DWG. CS-1. THESE BAR LENGTHS ARE APPROXIMATE AND TO HELP FACILITATE BIDDING.
  - IF THE CONTRACTOR ELECTS TO USE DIFFERENT CONSTRUCTION STAGING LIMITS AS SHOWN ON DWG. CS-1, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ADJUST THE LENGTH OF ALL STRAIGHT BARS ACCORDINGLY.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

STATE OF NEW YORK  
YOUNES DRIHIM  
LICENSED PROFESSIONAL ENGINEER  
100805  
5/10/24

**BAR BENDING DIAGRAMS**  
N.T.S.

BAR LIST IS FOR INFORMATION ONLY AND IS NOT GUARANTEED. THE CONTRACTOR IS RESPONSIBLE TO VERIFY THE BAR LIST AND REPORT ANY DISCREPANCIES TO THE ENGINEER.

**BAR MARK DESIGNATIONS**  
S = BRIDGE JOINT HEADER  
E = EPOXY COATED

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<p><b>Thruway Authority</b></p>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING JOINT HEADER BAR LIST CLINTON ST - M.P. 901.56	DRAWING NUMBER: BL-1

CHECKED BY: Y. DRHIM

DRAFTED BY: S. SCHUKRAFT

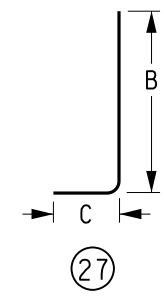
CHECKED BY: Y. DRHIM

DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER SENECA STREET - MP 902.22 - NORTHBOUND																	
BEGIN ABUTMENT JOINT (POUR 1)																	
L-BAR DOWELS (APPR.)	5SE1	180	5	27	2' - 10"	532		13"	21"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (APPR.)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (APPR.)	5SE15	2	5	STR.	41' - 0"	86											
L-BAR DOWELS (DECK)	5SE2	180	5	27	2' - 0"	376		13"	11"								
TRANSVERSE BARS - STAGE I (DECK)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (DECK)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (DECK)	5SE15	2	5	STR.	41' - 0"	86											
BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL:						1,284	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
PIER 1 JOINT (POUR 2)																	
L-BAR DOWELS (BEG.)	5SE2	180	5	27	2' - 0"	376		13"	11"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (BEG.)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (BEG.)	5SE15	2	5	STR.	41' - 0"	86											
L-BAR DOWELS (END)	5SE2	180	5	27	2' - 0"	376		13"	11"								
TRANSVERSE BARS - STAGE I (END)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (END)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (END)	5SE15	2	5	STR.	41' - 0"	86											
PIER 1 JOINT (POUR 2) SUB-TOTAL:						1,128	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
PIER 2 JOINT (POUR 3)																	
L-BAR DOWELS (BEG.)	5SE2	180	5	27	2' - 0"	376		13"	11"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (BEG.)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (BEG.)	5SE15	2	5	STR.	41' - 0"	86											
L-BAR DOWELS (END)	5SE2	180	5	27	2' - 0"	376		13"	11"								
TRANSVERSE BARS - STAGE I (END)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (END)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (END)	5SE15	2	5	STR.	41' - 0"	86											
PIER 2 JOINT (POUR 3) SUB-TOTAL:						1,128	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
END ABUTMENT JOINT (POUR 4)																	
L-BAR DOWELS (DECK)	5SE2	180	5	27	2' - 0"	376		13"	11"								
TRANSVERSE BARS - STAGE I (DECK)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (DECK)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (DECK)	5SE15	2	5	STR.	41' - 0"	86											
L-BAR DOWELS (APPR.)	5SE1	180	5	27	2' - 10"	532		13"	21"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (APPR.)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (APPR.)	5SE15	2	5	STR.	41' - 0"	86											
END ABUTMENT JOINT (POUR 4) SUB-TOTAL:						1,284	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
TOTAL WEIGHT ENTIRE STRUCTURE:						4,824	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER SENECA STREET - MP 902.22 - SOUTHBOUND																	
BEGIN ABUTMENT JOINT (POUR 1)																	
L-BAR DOWELS (APPR.)	5SE1	180	5	27	2' - 10"	532		13"	21"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (APPR.)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (APPR.)	5SE15	2	5	STR.	41' - 0"	86											
L-BAR DOWELS (DECK)	5SE2	180	5	27	2' - 0"	376		13"	11"								
TRANSVERSE BARS - STAGE I (DECK)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (DECK)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (DECK)	5SE15	2	5	STR.	41' - 0"	86											
BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL:						1,284	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
PIER 1 JOINT (POUR 2)																	
L-BAR DOWELS (BEG.)	5SE2	180	5	27	2' - 0"	376		13"	11"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (BEG.)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (BEG.)	5SE15	2	5	STR.	41' - 0"	86											
L-BAR DOWELS (END)	5SE2	180	5	27	2' - 0"	376		13"	11"								
TRANSVERSE BARS - STAGE I (END)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (END)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (END)	5SE15	2	5	STR.	41' - 0"	86											
PIER 1 JOINT (POUR 2) SUB-TOTAL:						1,128	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
PIER 2 JOINT (POUR 3)																	
L-BAR DOWELS (BEG.)	5SE2	180	5	27	2' - 0"	376		13"	11"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (BEG.)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (BEG.)	5SE15	2	5	STR.	41' - 0"	86											
L-BAR DOWELS (END)	5SE2	180	5	27	2' - 0"	376		13"	11"								
TRANSVERSE BARS - STAGE I (END)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (END)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (END)	5SE15	2	5	STR.	41' - 0"	86											
PIER 2 JOINT (POUR 3) SUB-TOTAL:						1,128	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
END ABUTMENT JOINT (POUR 4)																	
L-BAR DOWELS (DECK)	5SE2	180	5	27	2' - 0"	376		13"	11"								
TRANSVERSE BARS - STAGE I (DECK)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (DECK)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (DECK)	5SE15	2	5	STR.	41' - 0"	86											
L-BAR DOWELS (APPR.)	5SE1	180	5	27	2' - 10"	532		13"	21"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE13	2	5	STR.	28' - 1"	59											
TRANSVERSE BARS - STAGE II (APPR.)	5SE14	2	5	STR.	20' - 7"	43											
TRANSVERSE BARS - STAGE III (APPR.)	5SE15	2	5	STR.	41' - 0"	86											
END ABUTMENT JOINT (POUR 4) SUB-TOTAL:						1,284	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
TOTAL WEIGHT ENTIRE STRUCTURE:						4,824	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										



- NOTES:**
1. ALL REINFORCEMENT SHOWN IN THE TABLES ABOVE IS INTENDED TO BE USED FOR "METHOD A" JOINT HEADERS ONLY.
  2. THE LENGTHS OF ALL STRAIGHT REINFORCING BARS SHOWN IN THE TABLES ABOVE, WERE DETERMINED BASED ON THE WORK ZONE CONSTRUCTION STAGING LIMITS SHOWN ON DWG. CS-1. THESE BAR LENGTHS ARE APPROXIMATE AND TO HELP FACILITATE BIDDING.
  3. IF THE CONTRACTOR ELECTS TO USE DIFFERENT CONSTRUCTION STAGING LIMITS AS SHOWN ON DWG. CS-1, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ADJUST THE LENGTH OF ALL STRAIGHT BARS ACCORDINGLY.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

**BAR BENDING DIAGRAMS**  
N.T.S.

BAR LIST IS FOR INFORMATION ONLY AND IS NOT GUARANTEED. THE CONTRACTOR IS RESPONSIBLE TO VERIFY THE BAR LIST AND REPORT ANY DISCREPANCIES TO THE ENGINEER.

**BAR MARK DESIGNATIONS**  
S = BRIDGE JOINT HEADER  
E = EPOXY COATED

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING JOINT HEADER BAR LIST SENECA ST - M.P. 902.22	DRAWING NUMBER: BL-2

CHECKED BY: Y. DRIHIM

DRAFTED BY: S. SCHUKRAFT

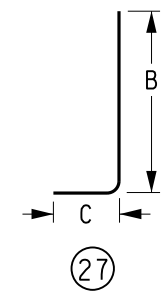
CHECKED BY: Y. DRIHIM

DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER MILTON STREET - MP 902.44 - NORTHBOUND																	
BEGIN ABUTMENT JOINT (POUR 1)																	
L-BAR DOWELS (APPR.)	5SE2	110	5	27	2' - 0"	230		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE16	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE17	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (APPR.)	5SE18	2	5	STR.	27' - 4"	58											
L-BAR DOWELS (DECK)	5SE3	110	5	27	1' - 8"	192		13"	7"								
TRANSVERSE BARS - STAGE I (DECK)	5SE16	2	5	STR.	16' - 5"	35											
TRANSVERSE BARS - STAGE II (DECK)	5SE17	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (DECK)	5SE18	2	5	STR.	26' - 3"	55											
BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL:						650	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
PIER 1 JOINT (POUR 2)																	
L-BAR DOWELS (BEG.)	5SE3	110	5	27	1' - 8"	192		13"	7"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE16	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (BEG.)	5SE17	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (BEG.)	5SE18	2	5	STR.	27' - 4"	58											
L-BAR DOWELS (END)	5SE3	110	5	27	1' - 8"	192		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE16	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (END)	5SE17	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (END)	5SE18	2	5	STR.	27' - 4"	58											
PIER 1 JOINT (POUR 2) SUB-TOTAL:						612	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
PIER 2 JOINT (POUR 3)																	
L-BAR DOWELS (BEG.)	5SE3	110	5	27	1' - 8"	192		13"	7"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE16	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (BEG.)	5SE17	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (BEG.)	5SE18	2	5	STR.	27' - 4"	58											
L-BAR DOWELS (END)	5SE3	110	5	27	1' - 8"	192		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE16	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (END)	5SE17	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (END)	5SE18	2	5	STR.	27' - 4"	58											
PIER 2 JOINT (POUR 3) SUB-TOTAL:						612	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
END ABUTMENT JOINT (POUR 4)																	
L-BAR DOWELS (DECK)	5SE3	110	5	27	1' - 8"	192		13"	7"								
TRANSVERSE BARS - STAGE I (DECK)	5SE36	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (DECK)	5SE37	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (DECK)	5SE38	2	5	STR.	27' - 4"	58											
L-BAR DOWELS (APPR.)	5SE2	110	5	27	2' - 0"	230		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE36	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE37	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (APPR.)	5SE38	2	5	STR.	27' - 4"	58											
END ABUTMENT JOINT (POUR 4) SUB-TOTAL:						650	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
TOTAL WEIGHT ENTIRE STRUCTURE:						2,524	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER MILTON STREET - MP 902.44 - SOUTHBOUND																	
BEGIN ABUTMENT JOINT (POUR 1)																	
L-BAR DOWELS (APPR.)	5SE2	100	5	27	2' - 0"	209		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE19	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE20	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (APPR.)	5SE21	2	5	STR.	22' - 6"	47											
L-BAR DOWELS (DECK)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (DECK)	5SE19	2	5	STR.	16' - 5"	35											
TRANSVERSE BARS - STAGE II (DECK)	5SE20	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (DECK)	5SE21	2	5	STR.	21' - 6"	45											
BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL:						590	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
PIER 1 JOINT (POUR 2)																	
L-BAR DOWELS (BEG.)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE19	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (BEG.)	5SE20	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (BEG.)	5SE21	2	5	STR.	22' - 6"	47											
L-BAR DOWELS (END)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE19	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (END)	5SE20	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (END)	5SE21	2	5	STR.	22' - 6"	47											
PIER 1 JOINT (POUR 2) SUB-TOTAL:						554	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
PIER 2 JOINT (POUR 3)																	
L-BAR DOWELS (BEG.)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE19	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (BEG.)	5SE20	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (BEG.)	5SE21	2	5	STR.	22' - 6"	47											
L-BAR DOWELS (END)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE19	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (END)	5SE20	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (END)	5SE21	2	5	STR.	22' - 6"	47											
PIER 2 JOINT (POUR 3) SUB-TOTAL:						554	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
END ABUTMENT JOINT (POUR 4)																	
L-BAR DOWELS (DECK)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (DECK)	5SE19	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (DECK)	5SE20	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (DECK)	5SE21	2	5	STR.	22' - 6"	47											
L-BAR DOWELS (APPR.)	5SE2	100	5	27	2' - 0"	209		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE19	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE20	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (APPR.)	5SE21	2	5	STR.	22' - 6"	47											
END ABUTMENT JOINT (POUR 4) SUB-TOTAL:						589	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
TOTAL WEIGHT ENTIRE STRUCTURE:						2,287	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										



- NOTES:**
- ALL REINFORCEMENT SHOWN IN THE TABLES ABOVE IS INTENDED TO BE USED FOR "METHOD A" JOINT HEADERS ONLY.
  - THE LENGTHS OF ALL STRAIGHT REINFORCING BARS SHOWN IN THE TABLES ABOVE, WERE DETERMINED BASED ON THE WORK ZONE CONSTRUCTION STAGING LIMITS SHOWN ON DWG. CS-1. THESE BAR LENGTHS ARE APPROXIMATE AND TO HELP FACILITATE BIDDING.
  - IF THE CONTRACTOR ELECTS TO USE DIFFERENT CONSTRUCTION STAGING LIMITS AS SHOWN ON DWG. CS-1, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ADJUST THE LENGTH OF ALL STRAIGHT BARS ACCORDINGLY.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP:

STATE OF NEW YORK  
YOUNES DRIHIM  
LICENSED PROFESSIONAL ENGINEER  
100805  
5/10/24

**BAR BENDING DIAGRAMS**  
N.T.S.

BAR LIST IS FOR INFORMATION ONLY AND IS NOT GUARANTEED. THE CONTRACTOR IS RESPONSIBLE TO VERIFY THE BAR LIST AND REPORT ANY DISCREPANCIES TO THE ENGINEER.

**BAR MARK DESIGNATIONS**  
S = BRIDGE JOINT HEADER  
E = EPOXY COATED

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

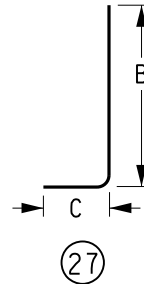
REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<p><b>Thruway Authority</b></p>	TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
	LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
	TITLE OF DRAWING JOINT HEADER BAR LIST MILTON ST - M.P. 902.44	DRAWING NUMBER: BL-3



REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER BABCOCK STREET - MP 902.63 - NORTHBOUND																	
<b>BEGIN ABUTMENT JOINT (POUR 1)</b>																	
L-BAR DOWELS (APPR.)	5SE2	100	5	27	2' - 0"	209		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (APPR.)	5SE24	2	5	STR.	22' - 6"	47											
L-BAR DOWELS (DECK)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (DECK)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (DECK)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (DECK)	5SE24	2	5	STR.	22' - 6"	47											
<b>BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL: 589 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>																	
<b>PIER 1 JOINT (POUR 2)</b>																	
L-BAR DOWELS (BEG.)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (BEG.)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (BEG.)	5SE24	2	5	STR.	22' - 6"	47											
L-BAR DOWELS (END)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (END)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (END)	5SE24	2	5	STR.	22' - 6"	47											
<b>PIER 1 JOINT (POUR 2) SUB-TOTAL: 554 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>																	
<b>PIER 2 JOINT (POUR 3)</b>																	
L-BAR DOWELS (BEG.)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (BEG.)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (BEG.)	5SE24	2	5	STR.	22' - 6"	47											
L-BAR DOWELS (END)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (END)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (END)	5SE24	2	5	STR.	22' - 6"	47											
<b>PIER 2 JOINT (POUR 3) SUB-TOTAL: 554 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>																	
<b>END ABUTMENT JOINT (POUR 4)</b>																	
L-BAR DOWELS (DECK)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (DECK)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (DECK)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (DECK)	5SE24	2	5	STR.	22' - 6"	47											
L-BAR DOWELS (APPR.)	5SE2	100	5	27	2' - 0"	209		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (APPR.)	5SE24	2	5	STR.	22' - 6"	47											
<b>END ABUTMENT JOINT (POUR 4) SUB-TOTAL: 589 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>																	
<b>TOTAL WEIGHT ENTIRE STRUCTURE: 2,286 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>																	

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER BABCOCK STREET - MP 902.63 - SOUTHBOUND																	
<b>BEGIN ABUTMENT JOINT (POUR 1)</b>																	
L-BAR DOWELS (APPR.)	5SE2	100	5	27	2' - 0"	209		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (APPR.)	5SE24	2	5	STR.	22' - 6"	47											
L-BAR DOWELS (DECK)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (DECK)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (DECK)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (DECK)	5SE24	2	5	STR.	22' - 6"	47											
<b>BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL: 589 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>																	
<b>PIER 1 JOINT (POUR 2)</b>																	
L-BAR DOWELS (BEG.)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (BEG.)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (BEG.)	5SE24	2	5	STR.	22' - 6"	47											
L-BAR DOWELS (END)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (END)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (END)	5SE24	2	5	STR.	22' - 6"	47											
<b>PIER 1 JOINT (POUR 2) SUB-TOTAL: 554 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>																	
<b>PIER 2 JOINT (POUR 3)</b>																	
L-BAR DOWELS (BEG.)	5SE4	50	5	27	1' - 8"	87		13"	7"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE22	1	5	STR.	15' - 4"	16											
TRANSVERSE BARS - STAGE II (BEG.)	5SE23	1	5	STR.	11' - 4"	12											
TRANSVERSE BARS - STAGE III (BEG.)	5SE24	1	5	STR.	22' - 6"	24											
L-BAR DOWELS (END)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (END)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (END)	5SE24	2	5	STR.	22' - 6"	47											
<b>PIER 2 JOINT (POUR 3) SUB-TOTAL: 416 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>																	
<b>END ABUTMENT JOINT (POUR 4)</b>																	
L-BAR DOWELS (DECK)	5SE3	100	5	27	1' - 8"	174		13"	7"								
TRANSVERSE BARS - STAGE I (DECK)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (DECK)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (DECK)	5SE24	2	5	STR.	22' - 6"	47											
L-BAR DOWELS (APPR.)	5SE2	100	5	27	2' - 0"	209		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE22	2	5	STR.	15' - 4"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE23	2	5	STR.	11' - 4"	24											
TRANSVERSE BARS - STAGE III (APPR.)	5SE24	2	5	STR.	22' - 6"	47											
<b>END ABUTMENT JOINT (POUR 4) SUB-TOTAL: 589 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>																	
<b>TOTAL WEIGHT ENTIRE STRUCTURE: 2,148 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>																	



ALTERED ON: \_\_\_\_\_

AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_

**BAR BENDING DIAGRAMS**  
N.T.S.

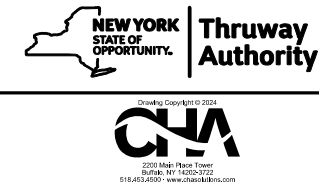
BAR LIST IS FOR INFORMATION ONLY AND IS NOT GUARANTEED. THE CONTRACTOR IS RESPONSIBLE TO VERIFY THE BAR LIST AND REPORT ANY DISCREPANCIES TO THE ENGINEER.

**BAR MARK DESIGNATIONS**

S = BRIDGE JOINT HEADER  
E = EPOXY COATED

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



**NOTES:**

- ALL REINFORCEMENT SHOWN IN THE TABLES ABOVE IS INTENDED TO BE USED FOR "METHOD A" JOINT HEADERS ONLY.
- THE LENGTHS OF ALL STRAIGHT REINFORCING BARS SHOWN IN THE TABLES ABOVE, WERE DETERMINED BASED ON THE WORK ZONE CONSTRUCTION STAGING LIMITS SHOWN ON DWG. CS-1. THESE BAR LENGTHS ARE APPROXIMATE AND TO HELP FACILITATE BIDDING.
- IF THE CONTRACTOR ELECTS TO USE DIFFERENT CONSTRUCTION STAGING LIMITS AS SHOWN ON DWG. CS-1, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ADJUST THE LENGTH OF ALL STRAIGHT BARS ACCORDINGLY.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING JOINT HEADER BAR LIST BABCOCK ST - M.P. 902.63	DRAWING NUMBER: BL-4

CHECKED BY: Y. DRHMI  
DRAFTED BY: S. SCHUKRAFT  
DESIGNED BY: S. SCHUKRAFT  
DESIGN SUPERVISOR: D. MARSH

CHECKED BY: Y. DRHMI

DRAFTED BY: S. SCHUKRAFT

CHECKED BY: Y. DRHMI

DESIGNED BY: S. SCHUKRAFT

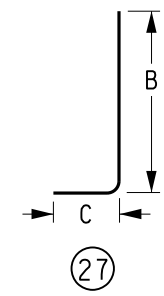
DESIGN SUPERVISOR: D. MARSH

**REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS**

LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER PEABODY STREET - MP 902.84 - NORTHBOUND																	
<b>BEGIN ABUTMENT JOINT (POUR 1)</b>																	
L-BAR DOWELS (APPR.)	5SE2	98	5	27	2' - 0"	205		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (APPR.)	5SE27	2	5	STR.	21' - 11"	46											
L-BAR DOWELS (DECK)	5SE7	98	5	27	1' - 10"	188		15"	7"								
TRANSVERSE BARS - STAGE I (DECK)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (DECK)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (DECK)	5SE27	2	5	STR.	21' - 11"	46											
BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL:						597	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
<b>PIER 1 JOINT (POUR 2)</b>																	
L-BAR DOWELS (BEG.)	5SE3	98	5	27	1' - 8"	171		13"	7"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (BEG.)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (BEG.)	5SE27	2	5	STR.	21' - 11"	46											
L-BAR DOWELS (END)	5SE3	98	5	27	1' - 8"	171		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (END)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (END)	5SE27	2	5	STR.	21' - 11"	46											
PIER 1 JOINT (POUR 2) SUB-TOTAL:						546	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
<b>PIER 2 JOINT (POUR 3)</b>																	
L-BAR DOWELS (BEG.)	5SE3	98	5	27	1' - 8"	171		13"	7"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (BEG.)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (BEG.)	5SE27	2	5	STR.	21' - 11"	46											
L-BAR DOWELS (END)	5SE3	98	5	27	1' - 8"	171		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (END)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (END)	5SE27	2	5	STR.	21' - 11"	46											
PIER 2 JOINT (POUR 3) SUB-TOTAL:						546	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
<b>END ABUTMENT JOINT (POUR 4)</b>																	
L-BAR DOWELS (DECK)	5SE3	98	5	27	1' - 8"	171		13"	7"								
TRANSVERSE BARS - STAGE I (DECK)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (DECK)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (DECK)	5SE27	2	5	STR.	21' - 11"	46											
L-BAR DOWELS (APPR.)	5SE6	98	5	27	2' - 5"	248		18"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (APPR.)	5SE27	2	5	STR.	21' - 11"	46											
END ABUTMENT JOINT (POUR 4) SUB-TOTAL:						623	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
<b>TOTAL WEIGHT ENTIRE STRUCTURE:</b>						<b>2,312</b>	<b>LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>										

**REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS**

LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER PEABODY STREET - MP 902.84 - SOUTHBOUND																	
<b>BEGIN ABUTMENT JOINT (POUR 1)</b>																	
L-BAR DOWELS (APPR.)	5SE2	98	5	27	2' - 0"	205		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (APPR.)	5SE27	2	5	STR.	21' - 11"	46											
L-BAR DOWELS (DECK)	5SE7	98	5	27	1' - 10"	188		15"	7"								
TRANSVERSE BARS - STAGE I (DECK)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (DECK)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (DECK)	5SE27	2	5	STR.	21' - 11"	46											
BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL:						597	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
<b>PIER 1 JOINT (POUR 2)</b>																	
L-BAR DOWELS (BEG.)	5SE3	98	5	27	1' - 8"	171		13"	7"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (BEG.)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (BEG.)	5SE27	2	5	STR.	21' - 11"	46											
L-BAR DOWELS (END)	5SE3	98	5	27	1' - 8"	171		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (END)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (END)	5SE27	2	5	STR.	21' - 11"	46											
PIER 1 JOINT (POUR 2) SUB-TOTAL:						546	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
<b>PIER 2 JOINT (POUR 3)</b>																	
L-BAR DOWELS (BEG.)	5SE3	98	5	27	1' - 8"	171		13"	7"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (BEG.)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (BEG.)	5SE27	2	5	STR.	21' - 11"	46											
L-BAR DOWELS (END)	5SE3	98	5	27	1' - 8"	171		13"	7"								
TRANSVERSE BARS - STAGE I (END)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (END)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (END)	5SE27	2	5	STR.	21' - 11"	46											
PIER 2 JOINT (POUR 3) SUB-TOTAL:						546	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
<b>END ABUTMENT JOINT (POUR 4)</b>																	
L-BAR DOWELS (DECK)	5SE3	98	5	27	1' - 8"	171		13"	7"								
TRANSVERSE BARS - STAGE I (DECK)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (DECK)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (DECK)	5SE27	2	5	STR.	21' - 11"	46											
L-BAR DOWELS (APPR.)	5SE6	98	5	27	2' - 5"	248		18"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE25	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE26	2	5	STR.	11' - 1"	24											
TRANSVERSE BARS - STAGE III (APPR.)	5SE27	2	5	STR.	21' - 11"	46											
END ABUTMENT JOINT (POUR 4) SUB-TOTAL:						623	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES										
<b>TOTAL WEIGHT ENTIRE STRUCTURE:</b>						<b>2,312</b>	<b>LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>										



**NOTES:**

- 1. ALL REINFORCEMENT SHOWN IN THE TABLES ABOVE IS INTENDED TO BE USED FOR "METHOD A" JOINT HEADERS ONLY.
- 2. THE LENGTHS OF ALL STRAIGHT REINFORCING BARS SHOWN IN THE TABLES ABOVE, WERE DETERMINED BASED ON THE WORK ZONE CONSTRUCTION STAGING LIMITS SHOWN ON DWG. CS-1. THESE BAR LENGTHS ARE APPROXIMATE AND TO HELP FACILITATE BIDDING.
- 3. IF THE CONTRACTOR ELECTS TO USE DIFFERENT CONSTRUCTION STAGING LIMITS AS SHOWN ON DWG. CS-1, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ADJUST THE LENGTH OF ALL STRAIGHT BARS ACCORDINGLY.

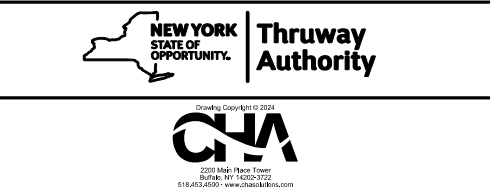
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SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_  
SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_  
**STATE OF NEW YORK**  
**YOUNES DRHMI**  
**EXCELSIOR**  
**100805**  
**LICENSED PROFESSIONAL ENGINEER**  
5/10/24

**BAR BENDING DIAGRAMS**  
N.T.S.  
BAR LIST IS FOR INFORMATION ONLY AND IS NOT GUARANTEED. THE CONTRACTOR IS RESPONSIBLE TO VERIFY THE BAR LIST AND REPORT ANY DISCREPANCIES TO THE ENGINEER.

**BAR MARK DESIGNATIONS**  
S = BRIDGE JOINT HEADER  
E = EPOXY COATED

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT	I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER:	TAN 24-12
LOCATION OF PROJECT	I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE:	MAY 2024
TITLE OF DRAWING	JOINT HEADER BAR LIST PEABODY ST - M.P. 902.84	DRAWING NUMBER:	BL-5

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS

LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER SMITH STREET - MP 903.14 - NORTHBOUND																	
BEGIN ABUTMENT JOINT (POUR 1)																	
L-BAR DOWELS (APPR.)	5SE2	106	5	27	2' - 0"	222		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE28	2	5	STR.	15' - 0"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE29	2	5	STR.	11' - 0"	23											
TRANSVERSE BARS - STAGE III (APPR.)	5SE30	2	5	STR.	26' - 8"	56											
L-BAR DOWELS (DECK)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (DECK)	5SE28	1	5	STR.	15' - 0"	16											
TRANSVERSE BARS - STAGE II (DECK)	5SE29	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (DECK)	5SE30	1	5	STR.	26' - 8"	28											
BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL: 463 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
PIER 1 JOINT (POUR 2)																	
L-BAR DOWELS (BEG.)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE28	1	5	STR.	15' - 0"	16											
TRANSVERSE BARS - STAGE II (BEG.)	5SE29	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (BEG.)	5SE30	1	5	STR.	26' - 8"	28											
L-BAR DOWELS (END)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE28	1	5	STR.	15' - 0"	16											
TRANSVERSE BARS - STAGE II (END)	5SE29	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (END)	5SE30	1	5	STR.	26' - 8"	28											
PIER 1 JOINT (POUR 2) SUB-TOTAL: 260 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
PIER 2 JOINT (POUR 3)																	
L-BAR DOWELS (BEG.)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE28	1	5	STR.	15' - 0"	16											
TRANSVERSE BARS - STAGE II (BEG.)	5SE29	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (BEG.)	5SE30	1	5	STR.	26' - 8"	28											
L-BAR DOWELS (END)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE28	1	5	STR.	15' - 0"	16											
TRANSVERSE BARS - STAGE II (END)	5SE29	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (END)	5SE30	1	5	STR.	26' - 8"	28											
PIER 2 JOINT (POUR 3) SUB-TOTAL: 260 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
TOTAL WEIGHT ENTIRE STRUCTURE: 983 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS

LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER SMITH STREET - MP 903.14 - SOUTHBOUND																	
BEGIN ABUTMENT JOINT (POUR 1)																	
L-BAR DOWELS (APPR.)	5SE2	106	5	27	2' - 0"	222		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE28	2	5	STR.	15' - 0"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE29	2	5	STR.	11' - 0"	23											
TRANSVERSE BARS - STAGE III (APPR.)	5SE30	2	5	STR.	26' - 8"	56											
L-BAR DOWELS (DECK)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (DECK)	5SE28	1	5	STR.	15' - 0"	16											
TRANSVERSE BARS - STAGE II (DECK)	5SE29	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (DECK)	5SE30	1	5	STR.	26' - 8"	28											
BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL: 463 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
PIER 1 JOINT (POUR 2)																	
L-BAR DOWELS (BEG.)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE28	1	5	STR.	15' - 0"	16											
TRANSVERSE BARS - STAGE II (BEG.)	5SE29	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (BEG.)	5SE30	1	5	STR.	26' - 8"	28											
L-BAR DOWELS (END)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE28	1	5	STR.	15' - 0"	16											
TRANSVERSE BARS - STAGE II (END)	5SE29	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (END)	5SE30	1	5	STR.	26' - 8"	28											
PIER 1 JOINT (POUR 2) SUB-TOTAL: 260 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
PIER 2 JOINT (POUR 3)																	
L-BAR DOWELS (BEG.)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE28	1	5	STR.	15' - 0"	16											
TRANSVERSE BARS - STAGE II (BEG.)	5SE29	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (BEG.)	5SE30	1	5	STR.	26' - 8"	28											
L-BAR DOWELS (END)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE28	1	5	STR.	15' - 0"	16											
TRANSVERSE BARS - STAGE II (END)	5SE29	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (END)	5SE30	1	5	STR.	26' - 8"	28											
PIER 2 JOINT (POUR 3) SUB-TOTAL: 260 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
TOTAL WEIGHT ENTIRE STRUCTURE: 983 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	

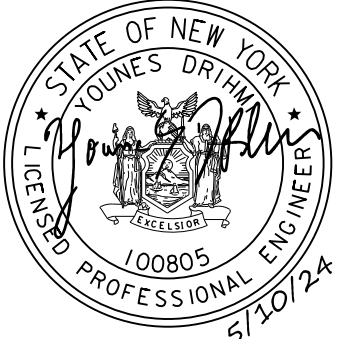
CHECKED BY: Y. DRIHIM

DRAFTED BY: S. SCHUKRAFT

CHECKED BY: Y. DRIHIM

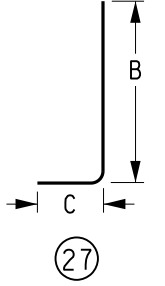
DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

**BAR BENDING DIAGRAMS**  
N.T.S.  
BAR LIST IS FOR INFORMATION ONLY AND IS NOT GUARANTEED. THE CONTRACTOR IS RESPONSIBLE TO VERIFY THE BAR LIST AND REPORT ANY DISCREPANCIES TO THE ENGINEER.

**BAR MARK DESIGNATIONS**  
S = BRIDGE JOINT HEADER  
E = EPOXY COATED



**NOTES:**

- ALL REINFORCEMENT SHOWN IN THE TABLES ABOVE IS INTENDED TO BE USED FOR "METHOD A" JOINT HEADERS ONLY.
- THE LENGTHS OF ALL STRAIGHT REINFORCING BARS SHOWN IN THE TABLES ABOVE, WERE DETERMINED BASED ON THE WORK ZONE CONSTRUCTION STAGING LIMITS SHOWN ON DWG. CS-1. THESE BAR LENGTHS ARE APPROXIMATE AND TO HELP FACILITATE BIDDING.
- IF THE CONTRACTOR ELECTS TO USE DIFFERENT CONSTRUCTION STAGING LIMITS AS SHOWN ON DWG. CS-1, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ADJUST THE LENGTH OF ALL STRAIGHT BARS ACCORDINGLY.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING JOINT HEADER BAR LIST SMITH ST - M.P. 903.14	DRAWING NUMBER: BL-6

CHECKED BY: Y. DRHMI

DRAFTED BY: S. SCHUKRAFT

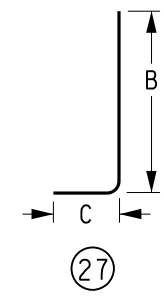
CHECKED BY: Y. DRHMI

DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER ERIE R.R. - MP 903.32 - NORTHBOUND																	
<b>PIER 1 JOINT (POUR 1)</b>																	
L-BAR DOWELS (BEG.)	5SE4	59	5	27	1' - 4"	83		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE31	1	5	STR.	16' - 5"	18											
TRANSVERSE BARS - STAGE II (BEG.)	5SE32	1	5	STR.	12' - 2"	13											
TRANSVERSE BARS - STAGE III (BEG.)	5SE33	1	5	STR.	29' - 3"	31											
L-BAR DOWELS (END)	5SE4	59	5	27	1' - 4"	83		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE31	1	5	STR.	16' - 5"	18											
TRANSVERSE BARS - STAGE II (END)	5SE32	1	5	STR.	12' - 2"	13											
TRANSVERSE BARS - STAGE III (END)	5SE33	1	5	STR.	29' - 3"	31											
PIER 1 JOINT (POUR 1) SUB-TOTAL: 290 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
<b>PIER 2 JOINT (POUR 2)</b>																	
L-BAR DOWELS (BEG.)	5SE4	59	5	27	1' - 4"	83		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE31	1	5	STR.	16' - 5"	18											
TRANSVERSE BARS - STAGE II (BEG.)	5SE32	1	5	STR.	12' - 2"	13											
TRANSVERSE BARS - STAGE III (BEG.)	5SE33	1	5	STR.	29' - 3"	31											
L-BAR DOWELS (END)	5SE4	59	5	27	1' - 4"	83		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE31	1	5	STR.	16' - 5"	18											
TRANSVERSE BARS - STAGE II (END)	5SE32	1	5	STR.	12' - 2"	13											
TRANSVERSE BARS - STAGE III (END)	5SE33	1	5	STR.	29' - 3"	31											
PIER 2 JOINT (POUR 2) SUB-TOTAL: 290 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
TOTAL WEIGHT ENTIRE STRUCTURE: 580 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER ERIE R.R. - MP 903.32 - SOUTHBOUND																	
<b>PIER 1 JOINT (POUR 1)</b>																	
L-BAR DOWELS (BEG.)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE34	1	5	STR.	16' - 5"	18											
TRANSVERSE BARS - STAGE II (BEG.)	5SE35	1	5	STR.	12' - 2"	13											
TRANSVERSE BARS - STAGE III (BEG.)	5SE36	1	5	STR.	24' - 1"	26											
L-BAR DOWELS (END)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE34	1	5	STR.	16' - 5"	18											
TRANSVERSE BARS - STAGE II (END)	5SE35	1	5	STR.	12' - 2"	13											
TRANSVERSE BARS - STAGE III (END)	5SE36	1	5	STR.	24' - 1"	26											
PIER 1 JOINT (POUR 1) SUB-TOTAL: 262 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
<b>PIER 2 JOINT (POUR 2)</b>																	
L-BAR DOWELS (BEG.)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE34	1	5	STR.	16' - 5"	18											
TRANSVERSE BARS - STAGE II (BEG.)	5SE35	1	5	STR.	12' - 2"	13											
TRANSVERSE BARS - STAGE III (BEG.)	5SE36	1	5	STR.	24' - 1"	26											
L-BAR DOWELS (END)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE34	1	5	STR.	16' - 5"	18											
TRANSVERSE BARS - STAGE II (END)	5SE35	1	5	STR.	12' - 2"	13											
TRANSVERSE BARS - STAGE III (END)	5SE36	1	5	STR.	24' - 1"	26											
PIER 2 JOINT (POUR 2) SUB-TOTAL: 262 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
TOTAL WEIGHT ENTIRE STRUCTURE: 524 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	



**NOTES:**

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- IF THE CONTRACTOR ELECTS TO USE DIFFERENT CONSTRUCTION STAGING LIMITS AS SHOWN ON DWG. CS-1, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ADJUST THE LENGTH OF ALL STRAIGHT BARS ACCORDINGLY.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

**BAR BENDING DIAGRAMS**  
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**BAR MARK DESIGNATIONS**  
S = BRIDGE JOINT HEADER  
E = EPOXY COATED

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING JOINT HEADER BAR LIST ERIE RR - M.P. 903.32	DRAWING NUMBER: BL-7

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER VAN RENSSELAER STREET - MP 903.48 - NORTHBOUND																	
<b>PIER 1 JOINT (POUR 1)</b>																	
L-BAR DOWELS (BEG.)	5SE4	49	5	27	1' - 4"	69		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE37	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (BEG.)	5SE38	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (BEG.)	5SE39	1	5	STR.	21' - 10"	23											
L-BAR DOWELS (END)	5SE4	49	5	27	1' - 4"	69		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE37	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (END)	5SE38	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (END)	5SE39	1	5	STR.	21' - 10"	23											
PIER 1 JOINT (POUR 1) SUB-TOTAL: 240 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
<b>PIER 2 JOINT (POUR 2)</b>																	
L-BAR DOWELS (BEG.)	5SE4	49	5	27	1' - 4"	69		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE37	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (BEG.)	5SE38	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (BEG.)	5SE39	1	5	STR.	21' - 10"	23											
L-BAR DOWELS (END)	5SE4	49	5	27	1' - 4"	69		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE37	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (END)	5SE38	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (END)	5SE39	1	5	STR.	21' - 10"	23											
PIER 2 JOINT (POUR 2) SUB-TOTAL: 240 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
<b>END ABUTMENT JOINT (POUR 3)</b>																	
L-BAR DOWELS (DECK)	5SE4	49	5	27	1' - 4"	69		13"	3"								
TRANSVERSE BARS - STAGE I (DECK)	5SE37	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (DECK)	5SE38	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (DECK)	5SE39	1	5	STR.	21' - 10"	23											
L-BAR DOWELS (APPR.)	5SE6	98	5	27	2' - 5"	248		18"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE37	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE38	2	5	STR.	11' - 0"	23											
TRANSVERSE BARS - STAGE III (APPR.)	5SE39	2	5	STR.	21' - 10"	46											
END ABUTMENT JOINT (POUR 3) SUB-TOTAL: 469 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
<b>TOTAL WEIGHT ENTIRE STRUCTURE: 949 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>																	

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER VAN RENSSELAER STREET - MP 903.48 - SOUTHBOUND																	
<b>PIER 1 JOINT (POUR 1)</b>																	
L-BAR DOWELS (BEG.)	5SE4	49	5	27	1' - 4"	69		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE37	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (BEG.)	5SE38	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (BEG.)	5SE39	1	5	STR.	21' - 10"	23											
L-BAR DOWELS (END)	5SE4	49	5	27	1' - 4"	69		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE37	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (END)	5SE38	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (END)	5SE39	1	5	STR.	21' - 10"	23											
PIER 1 JOINT (POUR 1) SUB-TOTAL: 240 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
<b>PIER 2 JOINT (POUR 2)</b>																	
L-BAR DOWELS (BEG.)	5SE4	49	5	27	1' - 4"	69		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE37	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (BEG.)	5SE38	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (BEG.)	5SE39	1	5	STR.	21' - 10"	23											
L-BAR DOWELS (END)	5SE4	49	5	27	1' - 4"	69		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE37	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (END)	5SE38	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (END)	5SE39	1	5	STR.	21' - 10"	23											
PIER 2 JOINT (POUR 2) SUB-TOTAL: 240 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
<b>END ABUTMENT JOINT (POUR 3)</b>																	
L-BAR DOWELS (DECK)	5SE4	49	5	27	1' - 4"	69		13"	3"								
TRANSVERSE BARS - STAGE I (DECK)	5SE37	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (DECK)	5SE38	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (DECK)	5SE39	1	5	STR.	21' - 10"	23											
L-BAR DOWELS (APPR.)	5SE6	98	5	27	2' - 5"	248		18"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE37	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE38	2	5	STR.	11' - 0"	23											
TRANSVERSE BARS - STAGE III (APPR.)	5SE39	2	5	STR.	21' - 10"	46											
END ABUTMENT JOINT (POUR 3) SUB-TOTAL: 469 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
<b>TOTAL WEIGHT ENTIRE STRUCTURE: 949 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES</b>																	


CHECKED BY: Y.DRIHIM

DRAFTED BY: S.SCHUKRAFT

CHECKED BY: Y.DRIHIM

DESIGNED BY: S.SCHUKRAFT

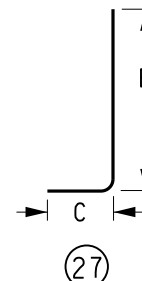
DESIGN SUPERVISOR: D.MARSH

ALTERED ON:	AFFIXED ON: 5/10/2024
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**BAR BENDING DIAGRAMS**  
N.T.S.

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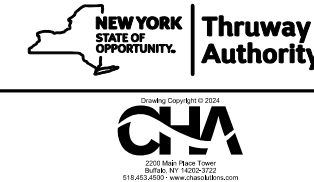
**BAR MARK DESIGNATIONS**  
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E = EPOXY COATED



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- IF THE CONTRACTOR ELECTS TO USE DIFFERENT CONSTRUCTION STAGING LIMITS AS SHOWN ON DWG. CS-1, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ADJUST THE LENGTH OF ALL STRAIGHT BARS ACCORDINGLY.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING JOINT HEADER BAR LIST VAN RENSSELAER ST - M.P. 903.48	DRAWING NUMBER: BL-8

CHECKED BY: Y. DRIHIM

DRAFTED BY: S. SCHUKRAFT

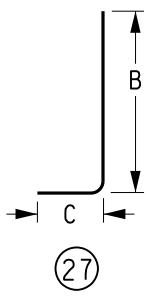
CHECKED BY: Y. DRIHIM

DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER CSX R.R. - MP 903.66 - NORTHBOUND																	
PIER 1 JOINT (POUR 1)																	
L-BAR DOWELS (BEG.)	5SE4	54	5	27	1' - 4"	76		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE40	1	5	STR.	16' - 8"	18											
TRANSVERSE BARS - STAGE II (BEG.)	5SE41	1	5	STR.	12' - 4"	13											
TRANSVERSE BARS - STAGE III (BEG.)	5SE42	1	5	STR.	24' - 5"	26											
L-BAR DOWELS (END)	5SE4	54	5	27	1' - 4"	76		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE40	1	5	STR.	16' - 8"	18											
TRANSVERSE BARS - STAGE II (END)	5SE41	1	5	STR.	12' - 4"	13											
TRANSVERSE BARS - STAGE III (END)	5SE42	1	5	STR.	24' - 5"	26											
PIER 1 JOINT (POUR 1) SUB-TOTAL: 266 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
PIER 2 JOINT (POUR 2)																	
L-BAR DOWELS (BEG.)	5SE4	54	5	27	1' - 4"	76		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE40	1	5	STR.	16' - 8"	18											
TRANSVERSE BARS - STAGE II (BEG.)	5SE41	1	5	STR.	12' - 4"	13											
TRANSVERSE BARS - STAGE III (BEG.)	5SE42	1	5	STR.	24' - 5"	26											
L-BAR DOWELS (END)	5SE4	54	5	27	1' - 4"	76		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE40	1	5	STR.	16' - 8"	18											
TRANSVERSE BARS - STAGE II (END)	5SE41	1	5	STR.	12' - 4"	13											
TRANSVERSE BARS - STAGE III (END)	5SE42	1	5	STR.	24' - 5"	26											
PIER 2 JOINT (POUR 2) SUB-TOTAL: 266 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
PIER 3 JOINT (POUR 3)																	
L-BAR DOWELS (BEG.)	5SE4	54	5	27	1' - 4"	76		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE40	1	5	STR.	16' - 8"	18											
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L-BAR DOWELS (END)	5SE4	54	5	27	1' - 4"	76		13"	3"								
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TRANSVERSE BARS - STAGE III (END)	5SE42	1	5	STR.	24' - 5"	26											
PIER 3 JOINT (POUR 3) SUB-TOTAL: 266 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
TOTAL WEIGHT ENTIRE STRUCTURE: 798 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER CSX R.R. - MP 903.66 - SOUTHBOUND																	
PIER 1 JOINT (POUR 1)																	
L-BAR DOWELS (BEG.)	5SE4	54	5	27	1' - 4"	76		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE40	1	5	STR.	16' - 8"	18											
TRANSVERSE BARS - STAGE II (BEG.)	5SE41	1	5	STR.	12' - 4"	13											
TRANSVERSE BARS - STAGE III (BEG.)	5SE42	1	5	STR.	24' - 5"	26											
L-BAR DOWELS (END)	5SE4	54	5	27	1' - 4"	76		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE40	1	5	STR.	16' - 8"	18											
TRANSVERSE BARS - STAGE II (END)	5SE41	1	5	STR.	12' - 4"	13											
TRANSVERSE BARS - STAGE III (END)	5SE42	1	5	STR.	24' - 5"	26											
PIER 1 JOINT (POUR 1) SUB-TOTAL: 266 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
PIER 2 JOINT (POUR 2)																	
L-BAR DOWELS (BEG.)	5SE4	54	5	27	1' - 4"	76		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE40	1	5	STR.	16' - 8"	18											
TRANSVERSE BARS - STAGE II (BEG.)	5SE41	1	5	STR.	12' - 4"	13											
TRANSVERSE BARS - STAGE III (BEG.)	5SE42	1	5	STR.	24' - 5"	26											
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TRANSVERSE BARS - STAGE I (END)	5SE40	1	5	STR.	16' - 8"	18											
TRANSVERSE BARS - STAGE II (END)	5SE41	1	5	STR.	12' - 4"	13											
TRANSVERSE BARS - STAGE III (END)	5SE42	1	5	STR.	24' - 5"	26											
PIER 2 JOINT (POUR 2) SUB-TOTAL: 266 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
PIER 3 JOINT (POUR 3)																	
L-BAR DOWELS (BEG.)	5SE4	54	5	27	1' - 4"	76		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE40	1	5	STR.	16' - 8"	18											
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L-BAR DOWELS (END)	5SE4	54	5	27	1' - 4"	76		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE40	1	5	STR.	16' - 8"	18											
TRANSVERSE BARS - STAGE II (END)	5SE41	1	5	STR.	12' - 4"	13											
TRANSVERSE BARS - STAGE III (END)	5SE42	1	5	STR.	24' - 5"	26											
PIER 3 JOINT (POUR 3) SUB-TOTAL: 266 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	
TOTAL WEIGHT ENTIRE STRUCTURE: 798 LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES																	



- NOTES:**
1. ALL REINFORCEMENT SHOWN IN THE TABLES ABOVE IS INTENDED TO BE USED FOR "METHOD A" JOINT HEADERS ONLY.
  2. THE LENGTHS OF ALL STRAIGHT REINFORCING BARS SHOWN IN THE TABLES ABOVE, WERE DETERMINED BASED ON THE WORK ZONE CONSTRUCTION STAGING LIMITS SHOWN ON DWG. CS-1. THESE BAR LENGTHS ARE APPROXIMATE AND TO HELP FACILITATE BIDDING.
  3. IF THE CONTRACTOR ELECTS TO USE DIFFERENT CONSTRUCTION STAGING LIMITS AS SHOWN ON DWG. CS-1, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ADJUST THE LENGTH OF ALL STRAIGHT BARS ACCORDINGLY.

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: [Blank]	SIGNATURE: [Blank]

**BAR BENDING DIAGRAMS**  
N.T.S.

BAR LIST IS FOR INFORMATION ONLY AND IS NOT GUARANTEED. THE CONTRACTOR IS RESPONSIBLE TO VERIFY THE BAR LIST AND REPORT ANY DISCREPANCIES TO THE ENGINEER.

**BAR MARK DESIGNATIONS**  
S = BRIDGE JOINT HEADER  
E = EPOXY COATED

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT	I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER:	TAN 24-12
LOCATION OF PROJECT	I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE:	MAY 2024
TITLE OF DRAWING	JOINT HEADER BAR LIST CSX RR - M.P. 903.66	DRAWING NUMBER:	BL-9

CHECKED BY: Y.DRHHM

DRAFTED BY: S.SCHUKRAFT

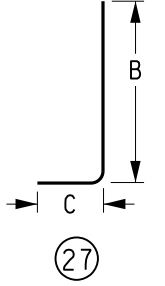
CHECKED BY: Y.DRHHM

DESIGNED BY: S.SCHUKRAFT

DESIGN SUPERVISOR: D.MARSH

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER HAMBURG STREET - MP 903.79 - NORTHBOUND																	
<b>BEGIN ABUTMENT JOINT (POUR 1)</b>																	
L-BAR DOWELS (APPR.)	5SE2	106	5	27	2' - 0"	222		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE43	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE44	2	5	STR.	11' - 0"	23											
TRANSVERSE BARS - STAGE III (APPR.)	5SE45	2	5	STR.	26' - 6"	56											
L-BAR DOWELS (DECK)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (DECK)	5SE43	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (DECK)	5SE44	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (DECK)	5SE45	1	5	STR.	26' - 6"	28											
<b>BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL:</b>							463	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES									
<b>PIER 1 JOINT (POUR 2)</b>																	
L-BAR DOWELS (BEG.)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE43	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (BEG.)	5SE44	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (BEG.)	5SE45	1	5	STR.	26' - 6"	28											
L-BAR DOWELS (END)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE43	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (END)	5SE44	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (END)	5SE45	1	5	STR.	26' - 6"	28											
<b>PIER 1 JOINT (POUR 2) SUB-TOTAL:</b>							260	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES									
<b>PIER 2 JOINT (POUR 3)</b>																	
L-BAR DOWELS (BEG.)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE43	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (BEG.)	5SE44	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (BEG.)	5SE45	1	5	STR.	26' - 6"	28											
L-BAR DOWELS (END)	5SE4	53	5	27	1' - 4"	74		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE43	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (END)	5SE44	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (END)	5SE45	1	5	STR.	26' - 6"	28											
<b>PIER 2 JOINT (POUR 3) SUB-TOTAL:</b>							260	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES									
<b>TOTAL WEIGHT ENTIRE STRUCTURE:</b>							983	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES									

REINFORCING STEEL BAR SCHEDULE - JOINT HEADERS																	
LOCATION	MARK	QTY	SIZE	TYPE	LENGTH	WEIGHT	A	B	C	D	E	G	H	J	K	O	R
BRIDGE OVER HAMBURG STREET - MP 903.79 - SOUTHBOUND																	
<b>BEGIN ABUTMENT JOINT (POUR 1)</b>																	
L-BAR DOWELS (APPR.)	5SE2	96	5	27	2' - 0"	201		13"	11"								
TRANSVERSE BARS - STAGE I (APPR.)	5SE46	2	5	STR.	14' - 11"	32											
TRANSVERSE BARS - STAGE II (APPR.)	5SE47	2	5	STR.	11' - 0"	23											
TRANSVERSE BARS - STAGE III (APPR.)	5SE48	2	5	STR.	21' - 10"	46											
L-BAR DOWELS (DECK)	5SE4	48	5	27	1' - 4"	67		13"	3"								
TRANSVERSE BARS - STAGE I (DECK)	5SE46	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (DECK)	5SE47	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (DECK)	5SE48	1	5	STR.	21' - 10"	23											
<b>BEGIN ABUTMENT JOINT (POUR 1) SUB-TOTAL:</b>							420	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES									
<b>PIER 1 JOINT (POUR 2)</b>																	
L-BAR DOWELS (BEG.)	5SE4	48	5	27	1' - 4"	67		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE46	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (BEG.)	5SE47	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (BEG.)	5SE48	1	5	STR.	21' - 10"	23											
L-BAR DOWELS (END)	5SE4	48	5	27	1' - 4"	67		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE46	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (END)	5SE47	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (END)	5SE48	1	5	STR.	21' - 10"	23											
<b>PIER 1 JOINT (POUR 2) SUB-TOTAL:</b>							236	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES									
<b>PIER 2 JOINT (POUR 3)</b>																	
L-BAR DOWELS (BEG.)	5SE4	48	5	27	1' - 4"	67		13"	3"								
TRANSVERSE BARS - STAGE I (BEG.)	5SE46	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (BEG.)	5SE47	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (BEG.)	5SE48	1	5	STR.	21' - 10"	23											
L-BAR DOWELS (END)	5SE4	48	5	27	1' - 4"	67		13"	3"								
TRANSVERSE BARS - STAGE I (END)	5SE46	1	5	STR.	14' - 11"	16											
TRANSVERSE BARS - STAGE II (END)	5SE47	1	5	STR.	11' - 0"	12											
TRANSVERSE BARS - STAGE III (END)	5SE48	1	5	STR.	21' - 10"	23											
<b>PIER 2 JOINT (POUR 3) SUB-TOTAL:</b>							236	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES									
<b>TOTAL WEIGHT ENTIRE STRUCTURE:</b>							892	LBS - ITEM 556.0202 - EPOXY-COATED BAR REINFORCEMENT FOR STRUCTURES									



- NOTES:**
- ALL REINFORCEMENT SHOWN IN THE TABLES ABOVE IS INTENDED TO BE USED FOR "METHOD A" JOINT HEADERS ONLY.
  - THE LENGTHS OF ALL STRAIGHT REINFORCING BARS SHOWN IN THE TABLES ABOVE, WERE DETERMINED BASED ON THE WORK ZONE CONSTRUCTION STAGING LIMITS SHOWN ON DWG. CS-1. THESE BAR LENGTHS ARE APPROXIMATE AND TO HELP FACILITATE BIDDING.
  - IF THE CONTRACTOR ELECTS TO USE DIFFERENT CONSTRUCTION STAGING LIMITS AS SHOWN ON DWG. CS-1, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO ADJUST THE LENGTH OF ALL STRAIGHT BARS ACCORDINGLY.

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

**BAR BENDING DIAGRAMS**  
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**BAR MARK DESIGNATIONS**  
S = BRIDGE JOINT HEADER  
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IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 	TITLE OF PROJECT	CONTRACT NUMBER:
	I-190 HIGHWAY REHABILITATION	TAN 24-12
	LOCATION OF PROJECT	DATE:
	I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	MAY 2024
	TITLE OF DRAWING	DRAWING NUMBER:
JOINT HEADER BAR LIST HAMBURG ST - M.P. 903.79		BL-10

CHECKED BY: Y. DRIHIM

DRAFTED BY: S. SCHUKRAFT

CHECKED BY: Y. DRIHIM

DESIGNED BY: S. SCHUKRAFT

DESIGN SUPERVISOR: D. MARSH

STEEL REPAIR NOTES:

- THE INTENT OF THESE STRUCTURAL STEEL REPAIRS IS TO SUPPORT THE ADDITIONAL SUPERIMPOSED DEAD LOAD RESULTING FROM THE APPLICATION OF THE 3/2" ASPHALT OVERLAY.
- ALL STRUCTURAL STEEL WORK, INCLUDING, BUT NOT LIMITED TO, FABRICATION, INSPECTION, TRANSPORTATION, BOLT INSTALLATION, AND ERECTION SHALL BE DONE IN ACCORDANCE WITH THE LATEST EDITION OF THE NYS DOT STEEL CONSTRUCTION MANUAL (NYSSCM).
- FABRICATION, FURNISHMENT, AND INSTALLATION OF STEEL FOR THE GIRDER END WEB PLATES REPAIRS SHALL BE PAID FOR UNDER ITEM 564.510001 AND THE GIRDER BOTTOM FLANGE COVER PLATE REPAIRS SHALL BE PAID FOR UNDER ITEM 564.510002.
- THE CONTRACTOR SHALL INCLUDE IN THE PRICE BID FOR THIS ITEM THE COST OF TEMPORARY SUPPORT & CLEANING OF FAYING SURFACES. PROPOSED WEB PLATES AND COVER PLATES SHALL BE FURNISHED TO MINIMUM CHARPY V-NOTCH TOUGHNESS REQUIREMENTS AS DESCRIBED IN MATERIAL SPECIFICATION 715-01 OF THE STANDARD SPECIFICATIONS.
- FIELD DRILLING OF BOLT HOLES SHALL BE PERFORMED UNDER ITEM 586.10. ALL HOLES SHALL BE DRILLED IN ACCORDANCE WITH REQUIREMENTS OF SECTION 613, BOLT HOLES IN STEEL MEMBERS" OF THE LATEST EDITION OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL (NYSSCM).
- ALL NEW STRUCTURAL STEEL SHALL BE ASTM A709 Gr. 50 ksi (AASHTO M270 GR 50).
- ALL BOLTS SHALL BE ASTM F3125 GR A325 TYPE 1 (HDG) WITH ASTM A563 HEAVY HEX NUTS & F436 HARDENED WASHERS. ALL NEW H.S BOLTS SHALL BE 1/2", EXCEPT AS NOTED. WASHERS SHALL BE PLACED UNDER BOTH THE BOLT HEAD AND NUT (2 WASHERS PER BOLT).
- ALL FASTENERS, INCLUDING ANCHOR BOLTS, SHALL BE HOT DIPPED GALVANIZED IN ACCORDANCE WITH THE SPECIFICATIONS.
- ALL STEEL REPAIR PARTS WILL REQUIRE SHOP DRAWINGS. SHOP DRAWINGS SHALL BE PREPARED IN ENGLISH UNITS AND SUBMITTED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL AS MODIFIED BY CONTRACT DOCUMENTS, AND THE PROCEDURE FOR PROCESSING ENGINEERING SUBMITTALS (430-0-10).
- THE WEIGHTS OF ALL COMPONENTS SHALL BE INDICATED IN THE BILL OF MATERIALS WITHIN THE SHOP DRAWINGS.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM ALL PERTINENT DIMENSIONS PRIOR TO FABRICATION TO ENSURE PROPER FIT-UP OF THE NEW PLATES.

REPAIR PROCEDURE NOTES:

- INSTALL ENVIRONMENTAL GROUND PROTECTION.
- PRIOR TO INSTALLATION OF NEW STEEL, THE PAINT SHALL BE REMOVED FOR A MINIMUM OF 6" ON EACH SIDE OF THE CENTERLINE OF BOLT ROW, AS APPLICABLE.
- REMOVE ALL REMAINING PAINT, RUST, RUST SCALE AND MILL SCALE TO SSPC SP-10 STANDARDS TO THE LIMITS OF THE PROPOSED WEB REPAIR PLATES AND BOTTOM FLANGE COVER PLATES AS SHOWN IN THE DETAILS INCLUDING 6 INCHES ON EACH SIDE OF PLATE (WHERE APPLICABLE).
- FURNISH AND INSTALL NEW 1/2" THICK STEEL REPAIR PLATES ON EACH SIDE OF THE GIRDER WEB AS DETAILED. GRIND EDGES AS NECESSARY TO OBTAIN TIGHT FIT TO THE BOTTOM FLANGES. FAYING SURFACES OF STEEL SHALL BE CLEANED. FIELD DRILL 1 5/16" HOLES TO ACCOMMODATE 1/2" BOLTS TO BE INSTALLED.
- FURNISH AND INSTALL NEW 1 1/4" THICK STEEL COVER PLATE ON THE UNDERSIDE OF THE GIRDER BOTTOM FLANGE AS DETAILED. FAYING SURFACES OF STEEL SHALL BE CLEANED. FIELD DRILL 1 5/16" HOLES TO ACCOMMODATE 1/2" BOLTS TO BE INSTALLED.
- PREPARE SURFACES FOR PAINTING IN ACCORDANCE WITH SECTION 574-3.03 OF THE STANDARD SPECIFICATIONS. PRIME AND PAINT EXPOSED STEEL AREAS FROM THE ABOVE WORK AS DIRECTED BY THE ENGINEER USING AN APPROVED PAINT SYSTEM FROM THE AUTHORITY'S APPROVED LIST.

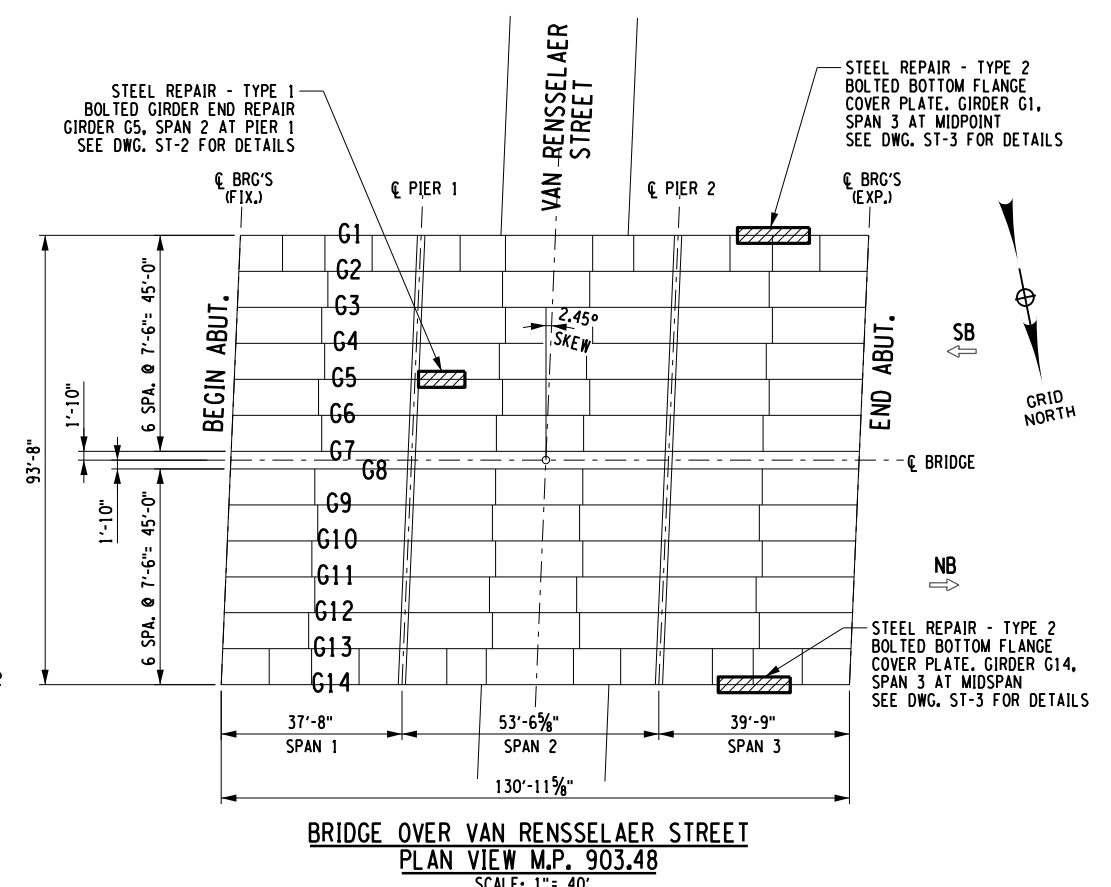
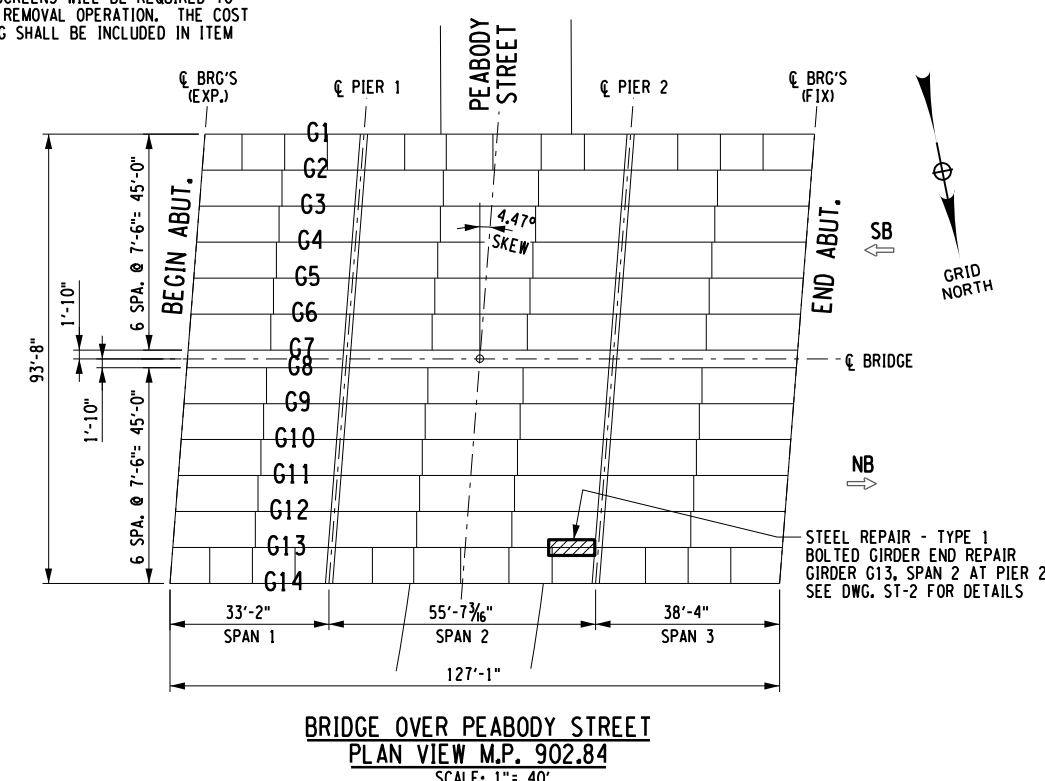
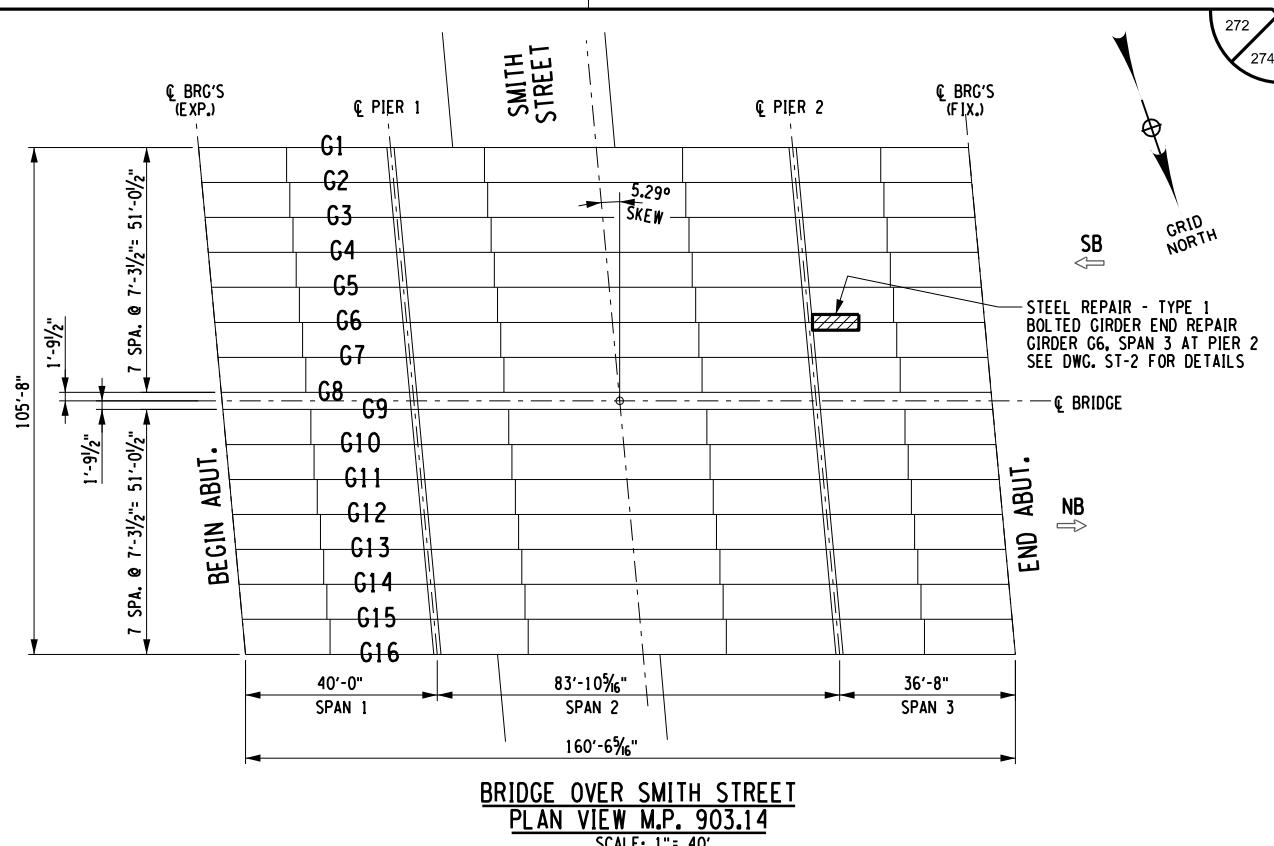
PAINTING NOTES:

- ASIDE FROM PAINTING REQUIRED FOR THE STEEL REPAIRS, AND AREAS WITHIN AND IMMEDIATELY ADJACENT TO THE STEEL REPAIRS, NO OTHER CLEANING OR PAINTING OF THE STRUCTURAL STEEL IS ANTICIPATED.
- PRIOR TO SHIPPING TO THE FIELD, ALL NEW STRUCTURAL STEEL SHALL BE ABRASIVELY BLAST CLEANED IN ACCORDANCE WITH SSPC SP 10, NEAR WHITE METAL BLAST AND SHALL BE PRE-PRIMED, AFTER SHOP FABRICATION, WITH AN INORGANIC ZINC-RICH PRIMER. ALL DIRT, GREASE AND FOREIGN MATTER SHALL BE CLEANED FROM THE NEW STEEL PRIOR TO APPLICATION OF THE SHOP OR FIELD APPLIED INTERMEDIATE COAT, AND FINISH COAT.
- FAYING SURFACES FOR BOLTED CONNECTIONS SHALL BE PRIMED WITH SLIP B APPROVED ORGANIC ZINC RICH PRIMERS. PRIMED FAYING SURFACES OF ALL PRIMED BOLTED CONNECTIONS SHALL BE MASKED WITHIN 3 INCHES (75 MM) OF ALL OPEN HOLES PRIOR TO APPLICATION OF THE INTERMEDIATE AND FINAL COATS OF PAINT. TOUCH-UP FIELD PAINTING OF THE INTERMEDIATE AND FINAL COATS SHALL BE APPLIED AFTER ASSEMBLY OF THE CONNECTION.
- FIELD PAINTING OPERATIONS SHALL BE PERFORMED UNDER ITEM 574.030003 - STRUCTURAL STEEL PAINTING: LOCALIZED. THE CONTRACTOR SHALL REMOVE PAINT TO THE LIMITS NOTED AND PAINT AREAS AS SPECIFIED ON THE REPAIR PROCEDURE. FAYING SURFACES SHALL BE MASKED AND UNPAINTED OR PRIMED.
- SHOP PAINTING OPERATIONS SHALL BE PERFORMED UNDER ITEM 572.010003 - STRUCTURAL STEEL PAINT SYSTEM: SHOP APPLIED. FAYING SURFACES SHALL BE MASKED AND UNPAINTED OR PRIMED WITH AN APPROVED PRIMER IN ACCORDANCE WITH SECTION 1303, BOLTED SPLICES/CONNECTIONS" OF THE NYSSCM.
- THE NEW GIRDER WEB REPAIR PLATES AND GIRDER BOTTOM FLANGE COVER PLATES SHALL BE 3-COAT PAINTED IN THE SHOP (IN ACCORDANCE WITH SECTION 572), WITH THEIR CONTACT SURFACES MASKED & PRIMED ONLY. COST TO BE INCLUDED IN ITEM 572.010003.
- FIELD TOUCH-UP OF DAMAGED AREAS OF PRIMER COAT SHALL BE AS DIRECTED BY THE ENGINEER, USING A COMPATIBLE, ORGANIC ZINC TOUCH-UP PRIMER.
- GALVANIZED BOLTS USED FOR THE STRUCTURAL STEEL REPAIRS WILL NOT BE REQUIRED TO BE PAINTED.
- WHERE NEW MATERIAL IS TO BE BOLTED TO EXISTING MATERIAL, THE EXISTING SURFACE SHALL BE CLEANED PER THE REQUIREMENTS OF SSPC SP 11 "POWER TOOL CLEANING TO BARE METAL." THE CONTACT SURFACES SHALL BE COATED WITH THE ORGANIC ZINC-RICH PRIMER PRIOR TO INSTALLATION OF NEW MATERIAL.
- WHERE NECESSARY, TARPS AND SCREENS WILL BE REQUIRED TO REMAIN IN PLACE DURING PAINT REMOVAL OPERATION. THE COST FOR THE CLEANING AND PAINTING SHALL BE INCLUDED IN ITEM 574.030003.
- FINISH COLOR: SAGE GREEN, MUNSELL NUMBER/FEDERAL STANDARD 7/5 CY 5/4.

STRUCTURAL STEEL PAINT SYSTEM (SHOP APPLIED)				
ITEM 572.010003				
BRIDGE OVER	LOCATION	NB (SF)	SB (SF)	TOTAL (SF)
PEABODY ST	SPAN 2	10	-	10
SMITH ST	SPAN 3	-	10	10
VAN RENSSELAER ST	SPAN 2	-	10	10
	SPAN 3	15	15	30
TOTAL				60

STRUCTURAL STEEL PAINT SYSTEM (LOCALIZED)				
ITEM 574.030003				
BRIDGE OVER	LOCATION	NB (SF)	SB (SF)	TOTAL (SF)
PEABODY ST	SPAN 2	5	-	5
SMITH ST	SPAN 3	-	5	5
VAN RENSSELAER ST	SPAN 2	-	5	5
	SPAN 3	17	17	34
TOTAL				49

DISPOSAL OF NON-HAZARDOUS INDUSTRIAL SOLID PAINT WASTE				
ITEM 571.04				
BRIDGE OVER	LOCATION	NB (LB)	SB (LB)	TOTAL (LB)
PEABODY ST	SPAN 2	200	-	200
SMITH ST	SPAN 3	-	200	200
VAN RENSSELAER ST	SPAN 2	-	50	50
	SPAN 3	200	50	250
TOTAL				700



ALTERED ON: \_\_\_\_\_ AFFIXED ON: 5/10/2024

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY** | **Thruway Authority**

TITLE OF PROJECT: I-190 HIGHWAY REHABILITATION  
 LOCATION OF PROJECT: I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2  
 CONTRACT NUMBER: TAN 24-12  
 DATE: MAY 2024  
 TITLE OF DRAWING: STRUCTURAL STEEL REPAIRS FRAMING PLANS  
 DRAWING NUMBER: ST-1



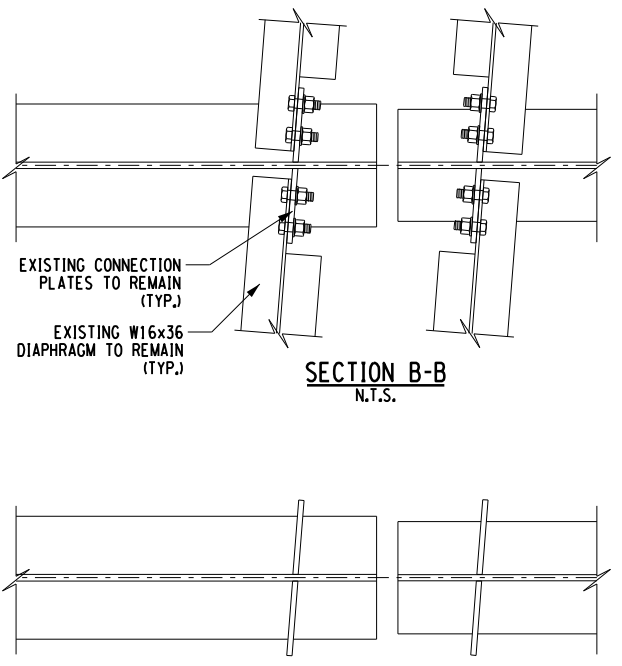
CHECKED BY: Y. DRIHMI

DRAFTED BY: S. SCHUKRAFT

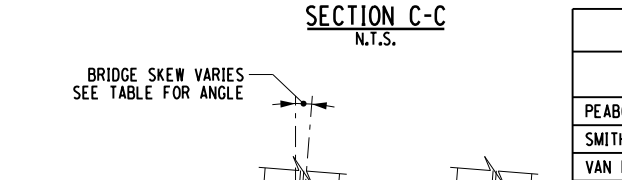
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DESIGNED BY: S. SCHUKRAFT

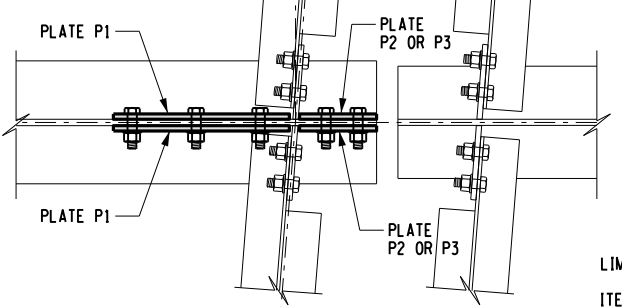
DESIGN SUPERVISOR: D. MARSH



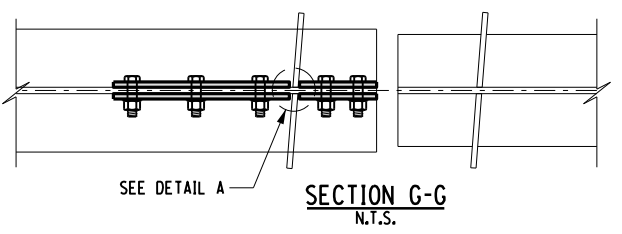
SECTION B-B  
N.T.S.



SECTION C-C  
N.T.S.



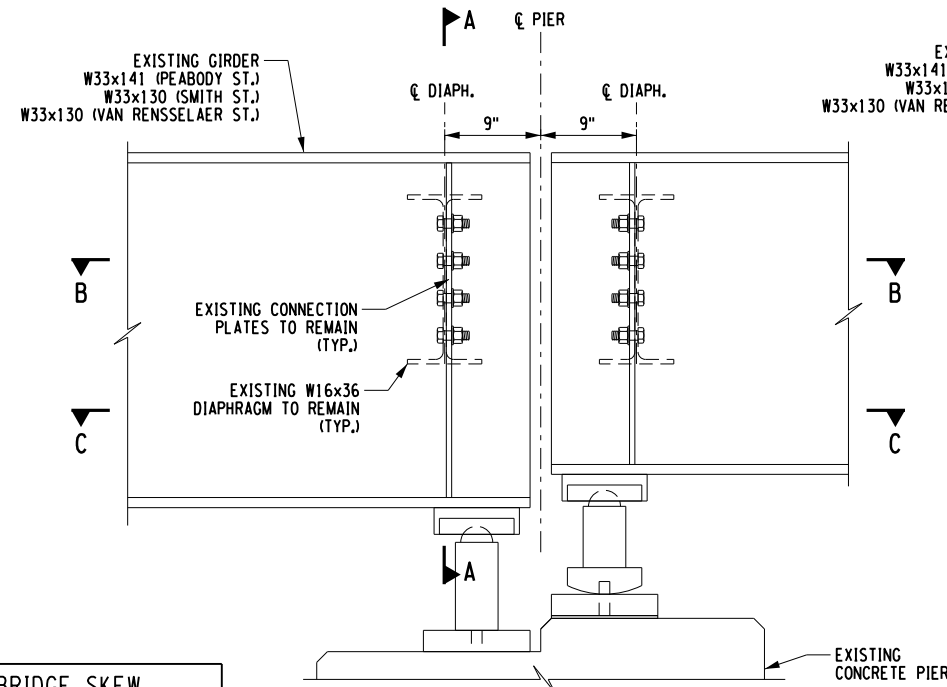
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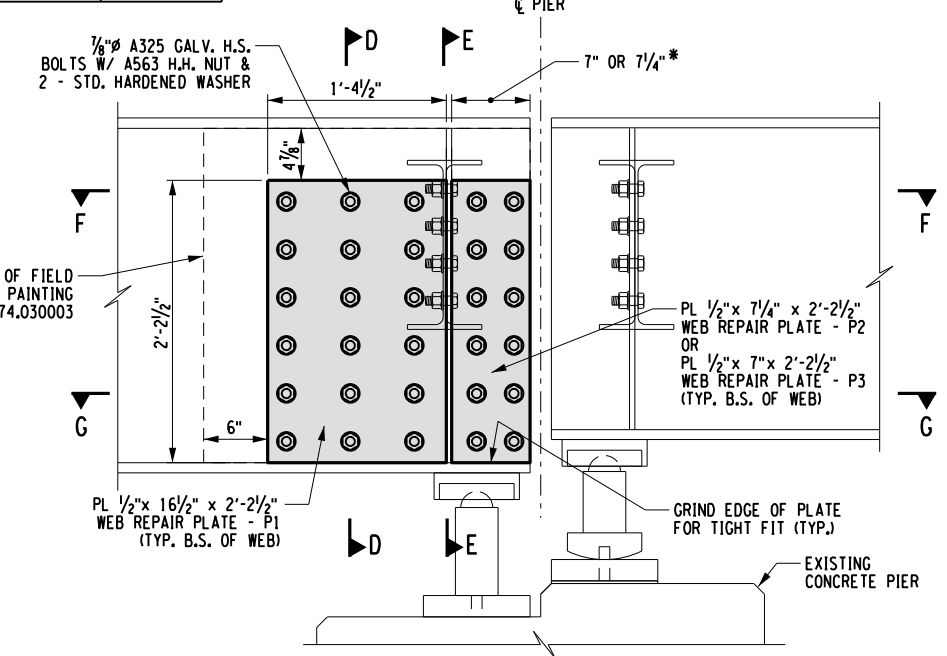
SECTION G-G  
N.T.S.

BRIDGE SKEW

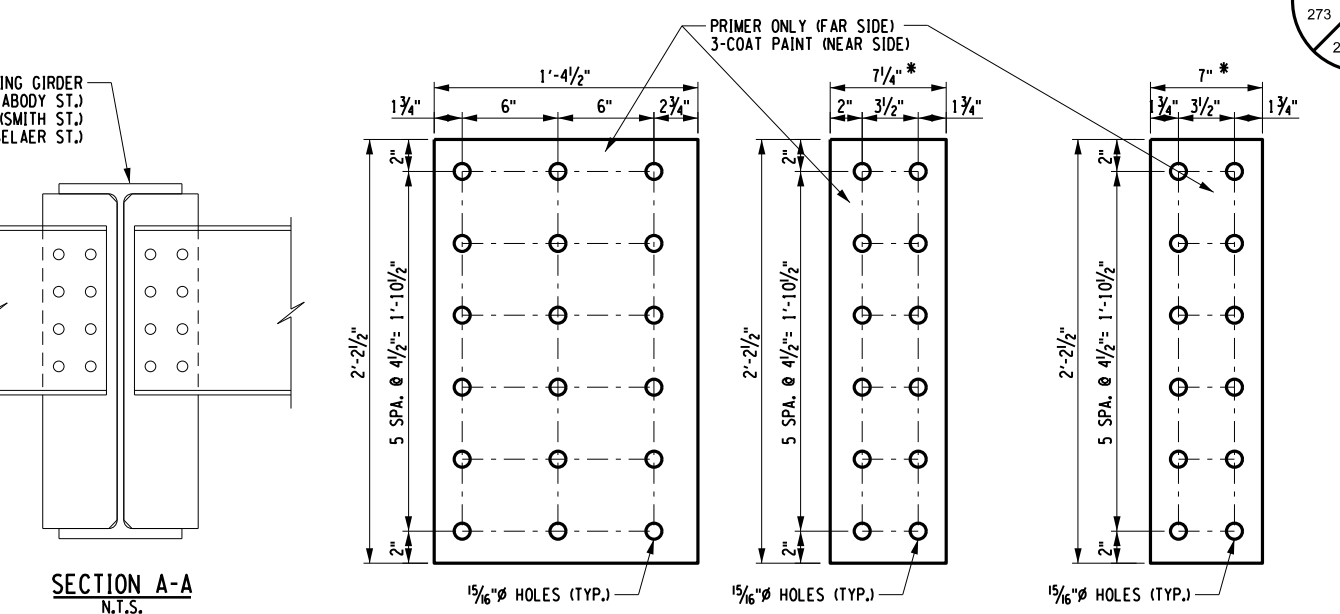
LOCATION	SKEW ANGLE
PEABODY ST	4.47°
SMITH ST	5.29°
VAN RENSSELAER ST	2.45°



EXISTING ELEVATION  
GIRDER END WEB REPAIR  
(LOOKING SOUTH)



PROPOSED ELEVATION  
GIRDER END WEB REPAIR  
(LOOKING SOUTH)



WEB REPAIR PLATE - P1  
PL 1/2" x 16 1/2" x 2'-2 1/2"  
(2 REQ'D AT PEABODY ST)  
(2 REQ'D AT SMITH ST)  
(2 REQ'D AT VAN RENSSELAER ST)  
(6 REQ'D TOTAL) (62 LB./EA.)

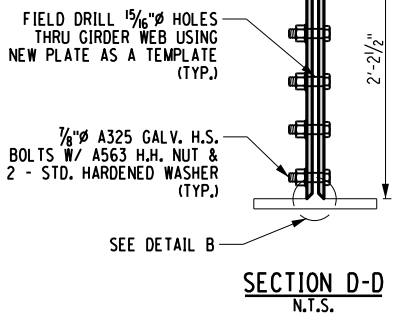
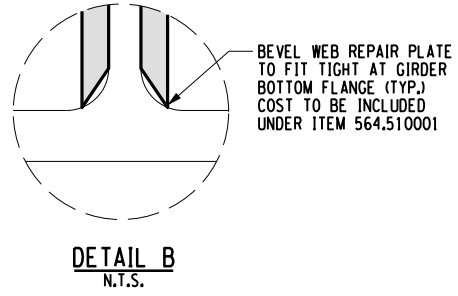
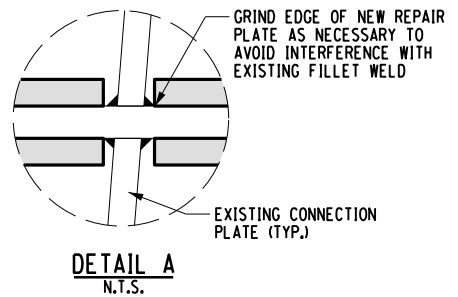
WEB REPAIR PLATE - P2  
PL 1/2" x 7 1/4" x 2'-2 1/2"  
(2 REQ'D AT PEABODY ST ONLY)  
(2 REQ'D TOTAL) (27 LB./EA.)

WEB REPAIR PLATE - P3  
PL 1/2" x 7" x 2'-2 1/2"  
(2 REQ'D AT SMITH ST)  
(2 REQ'D AT VAN RENSSELAER ST)  
(4 REQ'D TOTAL) (26 LB./EA.)

LEGEND

[Hatched Box]	STRUCTURAL STEEL ITEM 564.510001
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\* CONTRACTOR TO FIELD VERIFY  
THE PLATE WIDTH DIMENSIONS  
PRIOR TO FABRICATION



SECTION E-E  
N.T.S.

SECTION D-D  
N.T.S.

STRUCTURAL STEEL  
ITEM 564.510001

BRIDGE OVER	LOCATION	NB (LB)	SB (LB)	TOTAL (LB)
PEABODY ST	SPAN 2	179	-	179
SMITH ST	SPAN 3	-	177	177
VAN RENSSELAER ST	SPAN 2	-	177	177
	SPAN 3	-	-	0
	TOTAL			533

H.S. BOLTS REQUIRED  
INCLUDED IN ITEM 564.510001

BRIDGE OVER	LOCATION	NB (EA)	SB (EA)	TOTAL (EA)
PEABODY ST	SPAN 2	30	-	30
SMITH ST	SPAN 3	-	30	30
VAN RENSSELAER ST	SPAN 2	-	30	30
	SPAN 3	-	-	0
	TOTAL			90

FIELD DRILL HOLES IN EXISTING STEEL  
ITEM 586.10

BRIDGE OVER	LOCATION	NB (SF)	SB (SF)	TOTAL (SF)
PEABODY ST	SPAN 2	30	-	30
SMITH ST	SPAN 3	-	30	30
VAN RENSSELAER ST	SPAN 2	-	30	30
	SPAN 3	60	-	60
	TOTAL			210

NOTES:  
1. REFER TO DWG. ST-1 FOR STEEL REPAIR  
LOCATIONS AND NOTES.

NOTE: DETAILS SHOWN ARE FOR  
PEABODY STREET, SMITH STREET &  
VAN RENSSELAER STREET SIMILAR

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REVISIONS

DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY

Thruway Authority

2024

QMA

2024

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING STEEL REPAIR TYPE 1 GIRDER WEB REPAIR PLATE DETAILS	DRAWING NUMBER: ST-2



ALTERED ON: AFFIXED ON: 5/10/2024

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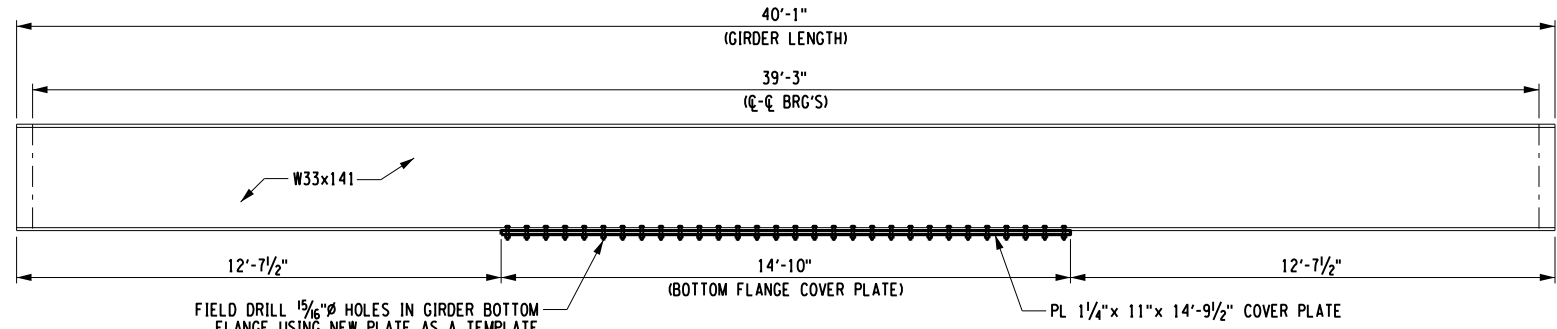
CHECKED BY: Y. DRIHMI

DRAFTED BY: S. SCHUKRAFT

CHECKED BY: Y. DRIHMI

DESIGNED BY: S. SCHUKRAFT

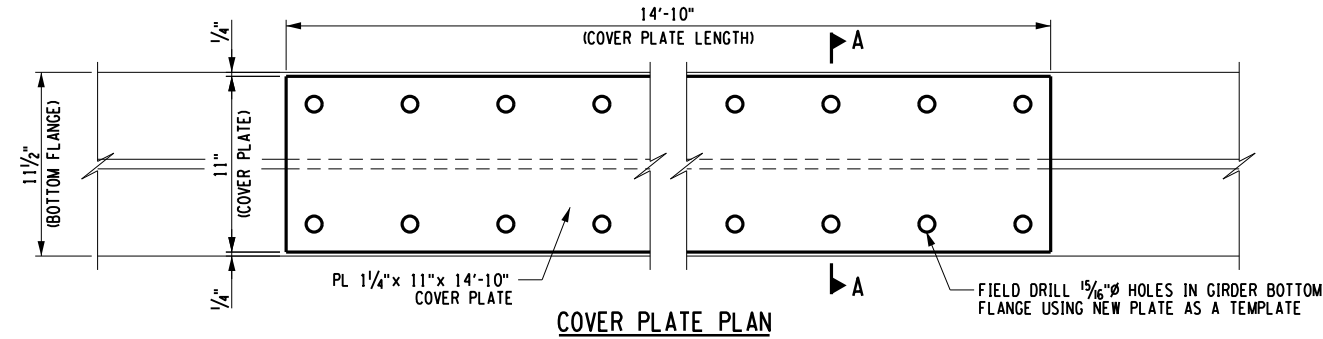
DESIGN SUPERVISOR: D. MARSH



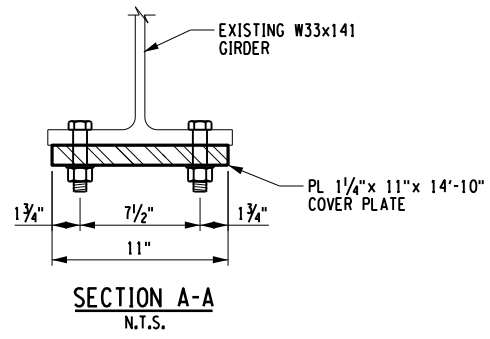
GIRDER ELEVATION

LEGEND	
	STRUCTURAL STEEL ITEM 564.510002

H.S. BOLTS REQUIRED INCLUDED IN ITEM 564.510002				
BRIDGE OVER	LOCATION	NB (EA)	SB (EA)	TOTAL (EA)
PEABODY ST	SPAN 2	-	-	0
SMITH ST	SPAN 3	-	-	0
VAN RENSSELAER ST	SPAN 2	-	-	0
	SPAN 3	60	60	120
TOTAL				120

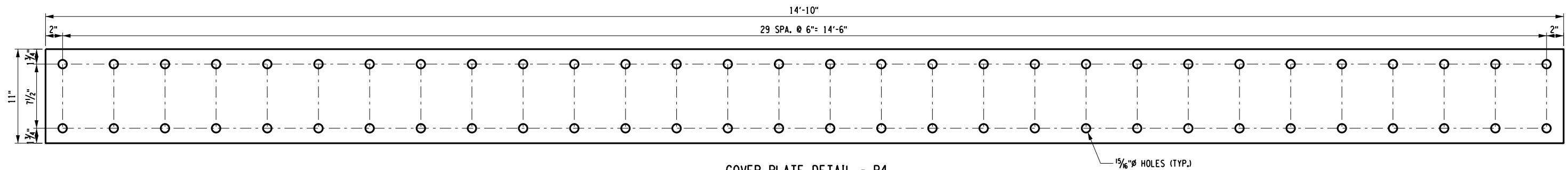


COVER PLATE PLAN



SECTION A-A  
N.T.S.

STRUCTURAL STEEL ITEM 564.510002				
BRIDGE OVER	LOCATION	NB (LB)	SB (LB)	TOTAL (LB)
PEABODY ST	SPAN 2	-	-	0
SMITH ST	SPAN 3	-	-	0
VAN RENSSELAER ST	SPAN 2	-	-	0
	SPAN 3	694	694	1,388
TOTAL				1,388



COVER PLATE DETAIL - P4  
PL 1 1/4" x 11" x 14'-10"  
(2 REQ'D) (694 LB./EA.)

ALTERED ON:	AFFIXED ON: 5/10/2024
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYMBOL

TITLE OF PROJECT I-190 HIGHWAY REHABILITATION	CONTRACT NUMBER: TAN 24-12
LOCATION OF PROJECT I-190 NIAGARA THRUWAY MP 900.7 TO MP 904.2	DATE: MAY 2024
TITLE OF DRAWING STEEL REPAIR TYPE 2 GIRDER BOTTOM FLANGE COVER PLATE DETAILS	DRAWING NUMBER: ST-3

NOTES:  
1. REFER TO DWG. ST-1 FOR STEEL REPAIR LOCATIONS & NOTES.