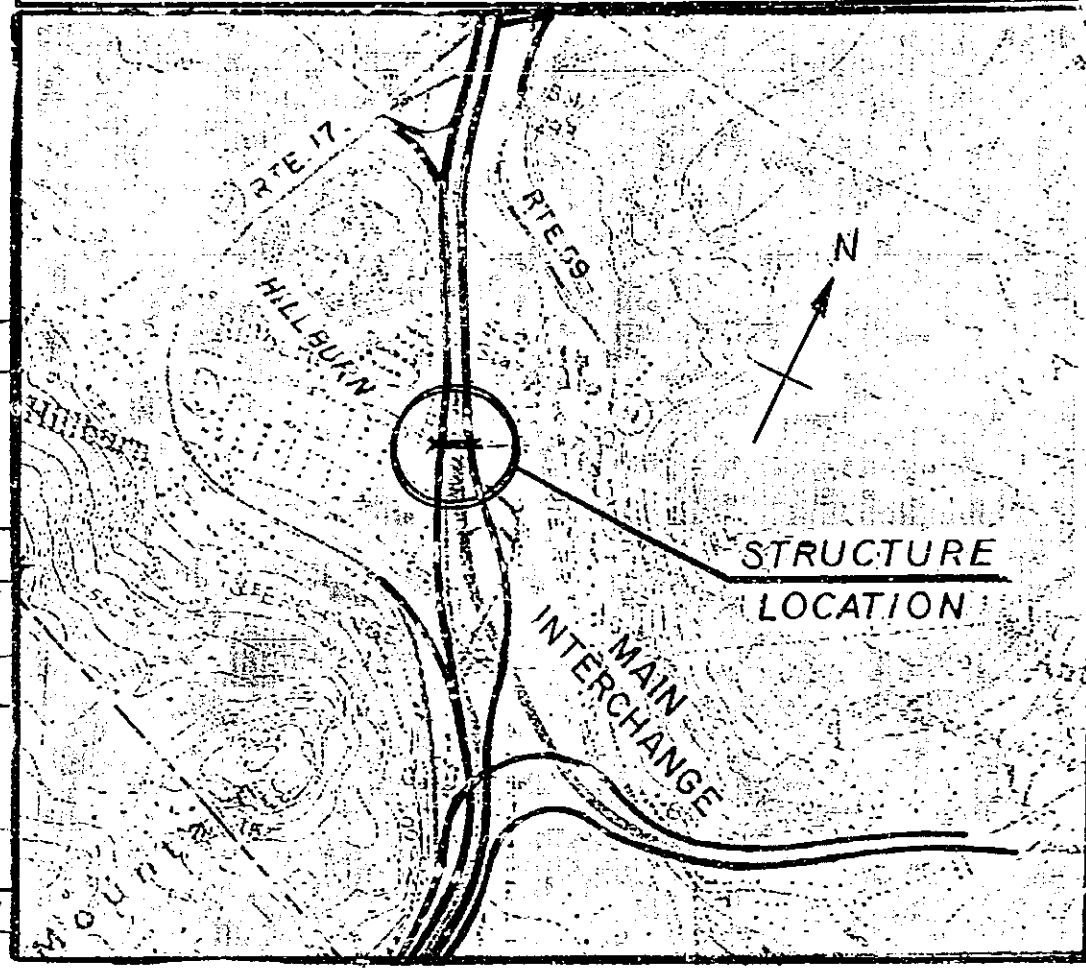


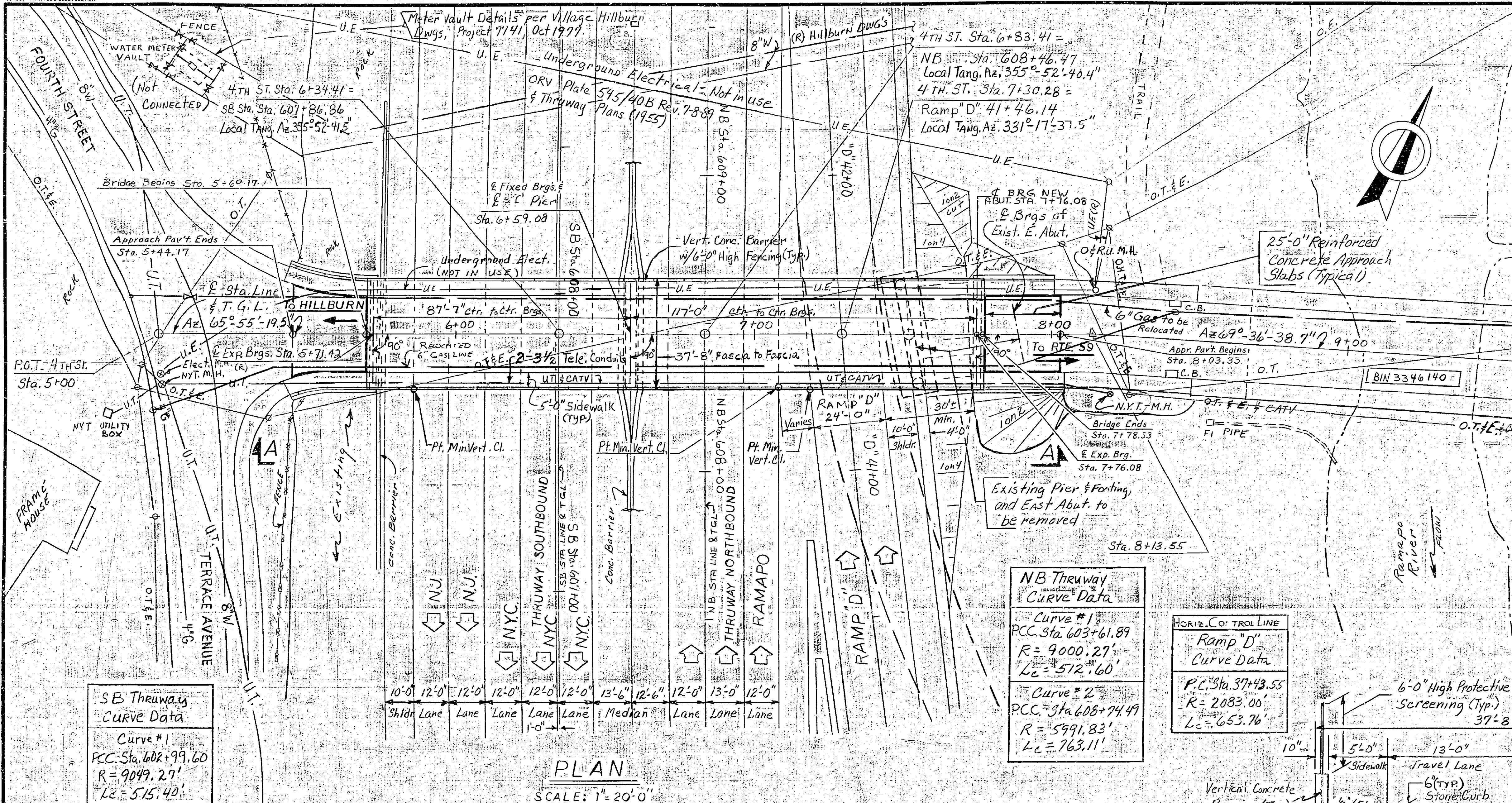
FED. RD. REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK		726	970

I-287/THRUWAY/ROUTE 17 INTERCHANGE
ROCKLAND COUNTY
CAPITAL PROJECT IDENTIFICATION NO. 8128.00



LOCATION MAP
SCALE: 1"=1500'
SLOATSBURG-RAMSEY QUADRANGLES

(R) = UTILITY DATA FROM RECORDED PLANS



PLAN
SCALE: 1"=20'-0"

SB Thruway Curve Data

Curve #1
P.C. Sta. 602+99.60
R = 9049.21'
Lc = 515.40'

Curve #2
P.C. Sta. 608+15.00
R = 6040.83'
Lc = 769.35'

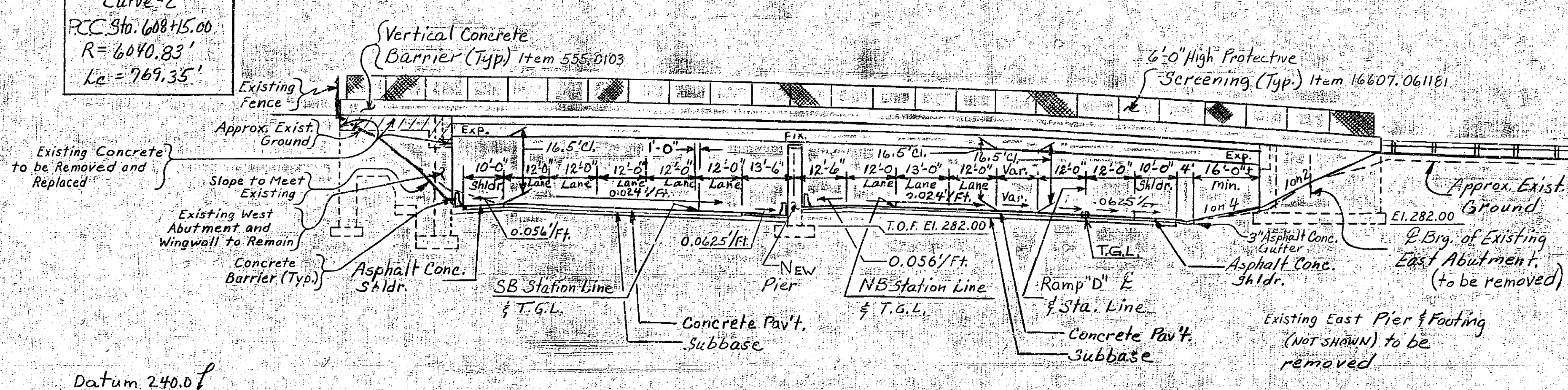
NB Thruway Curve Data

Curve #1
P.C. Sta. 603+61.89
R = 9000.21'
Lc = 512.60'

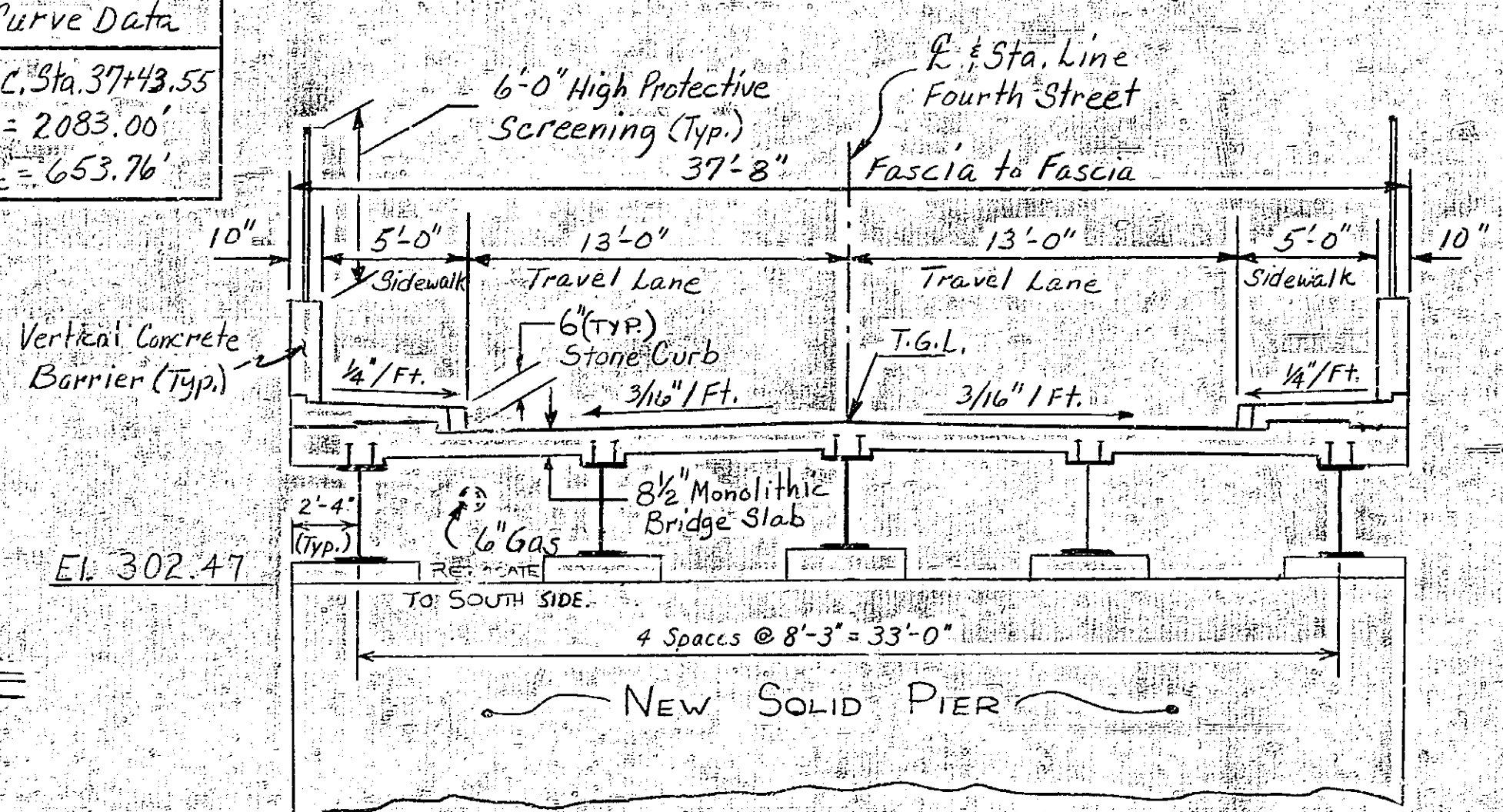
Curve #2
P.C. Sta. 608+74.49
R = 5991.83'
Lc = 763.11'

Ramp "D" Curve Data

P.C. Sta. 37+43.53
R = 2083.00'
Lc = 653.76'



ELEVATION A-A
SCALE: 1"=20'-0"



TYPICAL BRIDGE SECTION
SCALE: 1"=5'-0" (B.I.N. 5514160)

LOAD RATING

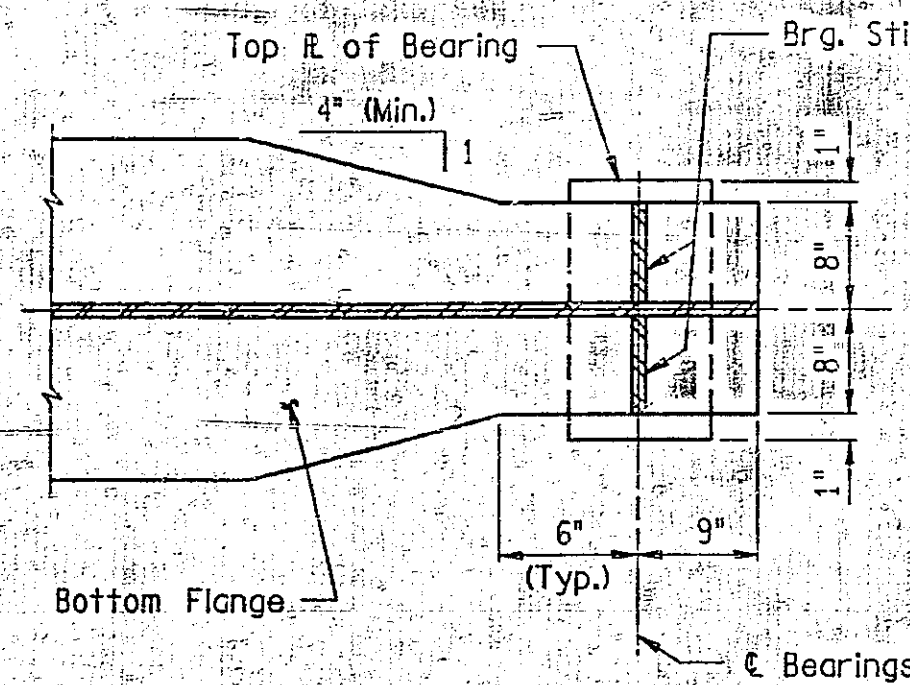
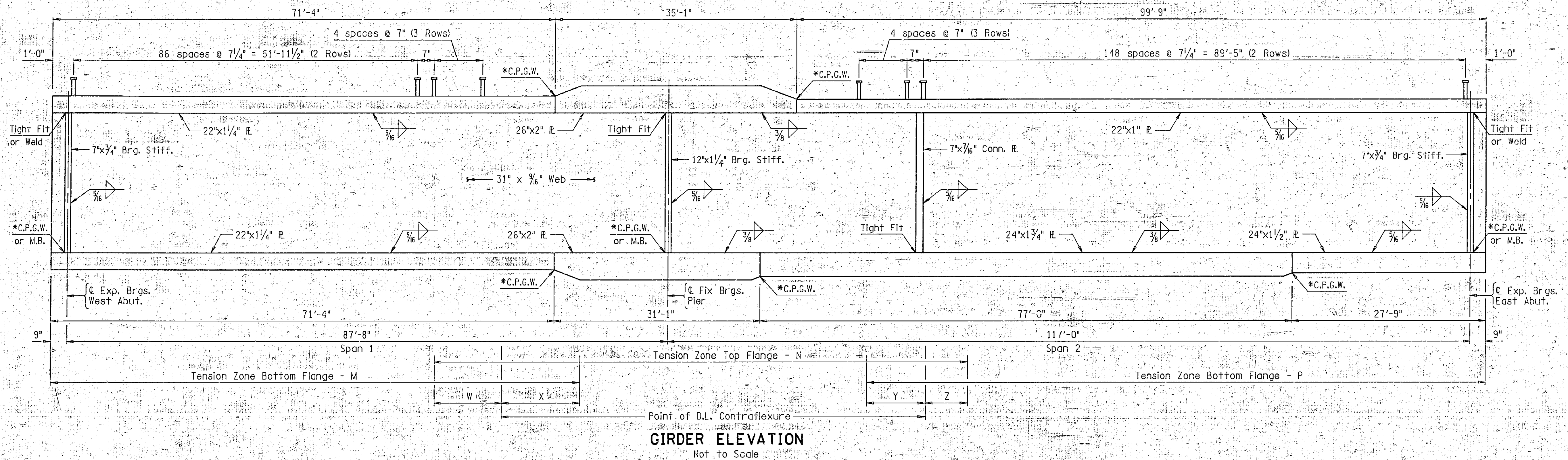
Inventory	H.S. 20 (36 Ton) W.S.
Operating	H.S. 39 (70 Ton) W.S.

STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN AND CONSTRUCTION DIVISION	
Fourth Street over Northbound & Southbound I-87 Mainline	
GENERAL PLAN AND ELEVATION	
PROJ. ENG. A.P. Yannotti	DATE MADE 5/91
SQUAD R.W. Laime	DRAWING NO. 1 OF 29

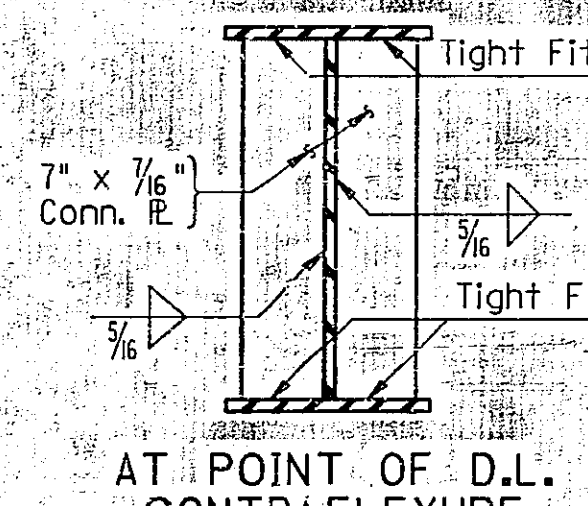
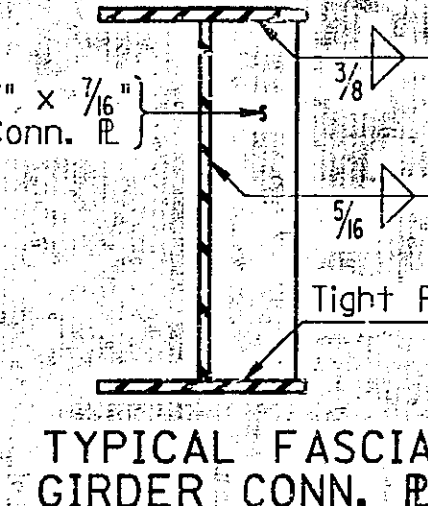
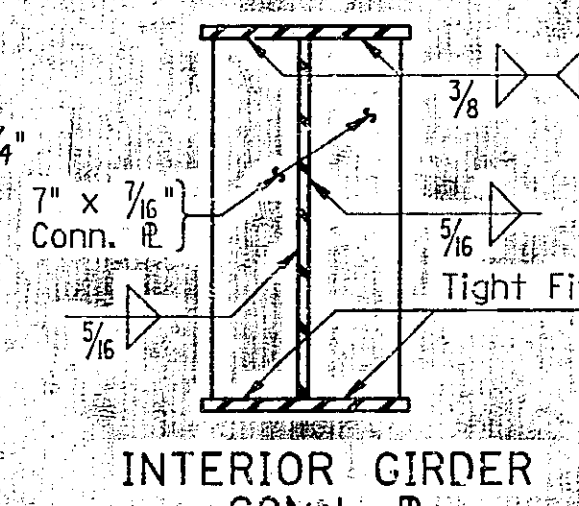
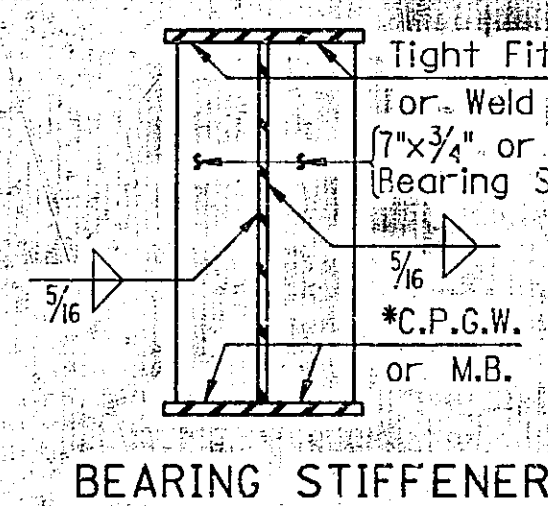
PRELIMINARY PLAN RECOMMENDED BY J.D. MOORE DATE 4/91
DESIGNED BY J.D. MOORE
DETAIL CHECKED BY J.D. MOORE
DESIGN CHECKED BY J.D. MOORE

MARK	TOTAL LENGTH	TENSION ZONES (ft.)							TOTAL STUDS/GIRDER
		M	W	X	N	Y	Z	P	
G1-G5	206'-2"	64.3	9.7	11.5	77.7	8.5	6.0	99.8	502

FED. RD. REG. NO.	STATE	FEDERAL AID PROJECT NO.	SHEET NO.	TOTAL SHEETS
	NEW YORK		743	970
I-287/THRUWAY/ROUTE 17 INTERCHANGE ROCKLAND COUNTY				
CAPITAL PROJECT IDENTIFICATION NO. 8128.00				



BOTTOM FLANGE TAPER
Not to Scale



GIRDER SECTIONS
Not to Scale

Welding Note "A"
No welding of stiffeners or connection plates to tension flanges. Plates shall be "tight fit."

Welding Note "B"
Intermediate stiffeners and connection plates used in pairs may be placed point tight against both flanges and not welded to either flange. As an alternate, at locations other than points of contraflexure, stiffeners and connection plates may be cut 1/4 inch short and then placed point tight against the tension flange and welded to the compression flange. The weld size at the compression flange shall be increased by the amount of the gap as described in Article 1101.1 of the STEEL CONSTRUCTION MANUAL. Fitted stiffeners and connection plates, shall not be driven in place with sufficient force to distort the flange, web, stiffener or connection plates.

Welding Note "C"
Weld size is determined by the thicker of the two parts joined unless a larger size is required by calculated stress. The weld size need not exceed the thickness of the thinner part joined. See table below.

Welding Notes
*C.P.G.W.-Complete Penetration Groove Weld.
M.B.-Mill to Bear.

Note to the Contractor
Within the tension zones delineated, there shall be no welding permitted other than what is detailed on the plans. Welding for the attachment of forms, ties, etc. shall not be permitted.

STATE OF NEW YORK DEPARTMENT OF TRANSPORTATION STRUCTURES DESIGN AND CONSTRUCTION DIVISION	
Fourth Street over Northbound & Southbound I-87 Mainline	
GIRDER ELEVATION & DETAILS	
PROJ. ENG. A. P. YANNOTTI	DATE MADE 5/91
SQUAD R. LAIME	DRAWING NO. 18 OF 29

DESIGNED BY *R. L. Snyder*
CHECKED BY *J. L. Snyder*

DESIGNED BY *R. L. Snyder*
CHECKED BY *J. L. Snyder*

DESIGNED BY *R. L. Snyder*
CHECKED BY *J. L. Snyder*

DESIGNED BY *R. L. Snyder*
CHECKED BY *J. L. Snyder*