

NEW YORK STATE THRUWAY AUTHORITY
DEPARTMENT OF ENGINEERING

ALBANY, NY
JANUARY 21, 2025

AMENDMENT NO. 1
TO

CONTRACT TAA 25-6B
D215009
REHABILITATION OF NYS THRUWAY (I-90)
BRIDGE OVER NYS ROUTE 5S / ERIE
CANAL RAIL TRAIL
AT MILEPOST 178.38
IN THE ALBANY DIVISION
IN MONTGOMERY COUNTY

IN THE LETTING OF
JANUARY 29, 2025

NOTE: This amendment shall be attached to and become a part of the Proposal for Contract **TAA 25-6B**.

NOTICE

For Electronic Bidders, the Project's amended EBSX file will automatically account for any necessary item changes (deletions, changes in quantities, or additions) that this Amendment may describe as being required regarding the project's estimated cost. Instructions to make physical changes to the Project Proposal's bid sheets are intended for "paper" Bidders who choose to submit bids via paper.

Questions and Answers for this project are available from the Authority's website, specifically at: http://www.thruway.ny.gov/netdata/contractors/documents/d215009_taa25-6b_questions-and-answers-01-21-2025.pdf. This document may be updated periodically without formal issuance of an Amendment. Prospective bidders are advised to revisit this link weekly and before proposals are due, for any possible additional questions and answers information.

PROPOSAL

1. **DELETE** Contract Proposal page **67** and **SUBSTITUTE** with the attached page **67-A1**. The Embodied Carbon Reporting and Environmental Product Declarations (EPD's) minimum quantities for reporting have been updated, and identified as for "Transportation Projects", similar to how the New York State Department of Transportation has agreed with the Office of General Services to show for their transportation capital projects.

2. **DELETE** the following Items:

| <u>Contract Proposal Page</u> | <u>Item No.</u> | <u>Description</u> | <u>Unit</u> | <u>Estimated Quantity</u> |
|-------------------------------|-----------------|---|-------------|---------------------------|
| 177 | 582.0061 | Removal of Structural Concrete - Replacement with Vertical and Overhead Patching Material | SF | 1,570 |
| 179 | 586.02 25 | Drilling and Grouting Bolts or Reinforcing Bars Concrete Grout | EA | 1,790 |
| 182 | 645.51000125 | Install Ground-Mounted Sign Panels (Authority Supplied) | SF | 3 |

The Bidder shall **CROSS OUT** and not enter a bid for these items in the Contract Proposal. If the Bidder fails to make these changes, the Authority will do so and adjust the TOTAL BID accordingly.

DELETE all other references to the above item in the Contract Plans and Contract Proposal.

3. **CHANGE** the following Items:

| <u>Contract Proposal Page</u> | <u>Item No.</u> | <u>Description</u> | <u>Unit</u> | <u>Initial Estimated Quantity</u> | <u>Revised Estimated Quantity</u> |
|-------------------------------|-----------------|--|-------------|-----------------------------------|-----------------------------------|
| 174 | 404.0001 | Plant Production Quality Adjustment to Asphalt Items | QU | 110 | 116 |
| 180 | 619.0803 | Cover Existing Pavement Marking Stripes (Removable Tape) | LF | 9,800 | 11,800 |
| 181 | 619.1713 | Temporary Positive Barrier – Category 3 (Pinning Prohibited) | LF | 390 | 1,230 |
| 181 | 619.1716 | Temporary Positive Barrier – Category 6 (Pinning Required) | LF | 2,700 | 9,000 |

The “paper” Bidder shall **CROSS OUT** the estimated quantity shown for these items in the Contract Proposal, **WRITE IN** the revised estimated quantity, and submit its bid based on the noted changes. If the Bidder fails to make these changes, the Authority will do so and adjust the BID AMOUNT for the items and TOTAL BID, accordingly.

4. **ADD** the following Items:

| <u>Contract Proposal Page</u> | <u>Item No.</u> | <u>Description</u> | <u>Unit</u> | <u>Estimated Quantity</u> |
|-------------------------------|-----------------|--|-------------|---------------------------|
| 182A-A1 | 404.0189 | Truing & Leveling F9, Asphalt, 80 Series Compaction | TON | 60 |
| 182A-A1 | 404.1979 | 19 F9 Binder Course Asphalt, 70 Series Compaction | TON | 30 |
| 182A-A1 | 404.3779 | 37.5 F9 Base Course Asphalt, 70 Series Compaction | TON | 40 |
| 182A-A1 | 586.0201 | Drilling and Grouting Anchor Bolts or Reinforcing Bars | EA | 1,790 |

| Contract Proposal Page | Item No. | Description | Unit | Estimated Quantity |
|------------------------|----------|--|------|--------------------|
| 182A-A1 | 619.1711 | Temporary Positive Barrier - Category 1 (Pinning Prohibited) | LF | 390 |

The Bidder must **RETURN THE ATTACHED PAGE 182A-A1** with its bid for this Contract.

PLANS

5. **DELETE** Drawing Number GNN-1, Sheet 6 of 95 and **SUBSTITUTE** with the attached Drawing Number GNN-1, Sheet 6-A1 of 95. Note 16 was removed and Note 23 was modified to clarify the Contractor's responsibility to prepare erection drawings per the NYS Steel Construction Manual.
6. **DELETE** Drawing Number GNN-2, Sheet 7 of 95 and **SUBSTITUTE** with the attached Drawing Number GNN-2, Sheet 7-A1 of 95. Painting Note 11 a. was removed to eliminate the finish paint coat requirement on new galvanized steel and Note 11 b. was modified to not limit the definition of "Critical Area" and reference a typical girder painting detail on ST-29.
7. **DELETE** Drawing Number TYP-01, sheet 8 of 95, and **SUBSTITUTE** with the attached Drawing Number TYP-01, Sheet 8-A1 of 95. Item 404.0189 Truing & Leveling F9 Asphalt, 80 Series Compaction has been added to the contract to fill in voids and establish proper profile and grade.
8. **DELETE** Drawing Number TYP-02, sheet 9 of 95, and **SUBSTITUTE** with the attached Drawing Number TYP-02, Sheet 9-A1 of 95. Item 404.0189 Truing & Leveling F9 Asphalt, 80 Series Compaction has been added to the contract to fill in voids and establish proper profile and grade.
9. **DELETE** Drawing Number TYP-03, sheet 10 of 95, and **SUBSTITUTE** with the attached Drawing Number TYP-03, Sheet 10-A1 of 95. Item 404.0189 Truing & Leveling F9 Asphalt, 80 Series Compaction has been added to the contract to fill in voids and establish proper profile and grade.
10. **DELETE** Drawing Number TCN-1, sheet 12 of 95, and **SUBSTITUTE** with the attached Drawing Number TCN-1, Sheet 12-A1 of 95. Stage 3 Sequence of Operations was modified to incorporate Winter Shutdown and associated sheets into the sequence of operations.
11. **DELETE** Drawing Number WZSD-1, sheet 13 of 95, and **SUBSTITUTE** with the attached Drawing Number WZSD-1, Sheet 13-A1 of 95. The "Road Work Ahead" sign has been added to the table.
12. **ADD** Drawing Number TC-6A, sheet 19A-A1 of 95. This sheet was added to provide a Work Zone Traffic Control Layout for the Winter Shutdown.
13. **ADD** Drawing Number TC-6B, sheet 19BA-A1 of 95. This sheet was added to provide a Work Zone Traffic Control Layout for the Winter Shutdown.
14. **ADD** Drawing Number TC-6C, sheet 19CA-A1 of 95. This sheet was added to provide a Work Zone Traffic Control Layout for the Winter Shutdown.
15. **DELETE** Drawing Number TC-9, sheet 22 of 95, and **SUBSTITUTE** with the attached Drawing Number TC-9, Sheet 22-A1 of 95. Removed notation indicating existing pavement markings on the Mainline Eastbound lanes were previously covered in Stage 2.

16. **DELETE** Drawing Number TC-11, sheet 24 of 95, and **SUBSTITUTE** with the attached Drawing Number TC-11, Sheet 24-A1 of 95. Removed notation indicating existing pavement markings on the Mainline Eastbound lanes were previously covered in Stage 2.
17. **DELETE** Drawing Number GNP-02, sheet 32 of 95, and **SUBSTITUTE** with the attached Drawing Number GNP-02, Sheet 32-A1 of 95. Note 3 was added to refer the reader to Drawing ST-42 for pavement reconstruction adjacent to the approach slab.
18. **DELETE** Drawing Number ST-9, sheet 49 of 95, and **SUBSTITUTE** with the attached Drawing Number ST-9, Sheet 49-A1 of 95. Note 5 was modified to reflect the use of standard item number 586.0201 for drilling and grouting reinforcing bars.
19. **DELETE** Drawing Number ST-16, sheet 56 of 95, and **SUBSTITUTE** with the attached Drawing Number ST-16, Sheet 56-A1 of 95. Previous references to Item 582.0061 have been replaced with the correct Item 582.0051, instead. Also, a previous callout has been corrected. The existing pedestals are to be removed under Item 580.01, and not under (incorrect) Item 582.0051.
20. **DELETE** Drawing Number ST-19, sheet 59 of 95, and **SUBSTITUTE** with the attached Drawing Number ST-19, Sheet 59-A1 of 95. Note 5 was added to reflect the use of standard item number 586.0201 for drilling and grouting reinforcing bars.
21. **DELETE** Drawing Number ST-21, sheet 61 of 95, and **SUBSTITUTE** with the attached Drawing Number ST-21, Sheet 61-A1 of 95. Note 4 was added to reflect the use of standard item number 586.0201 for drilling and grouting reinforcing bars.
22. **DELETE** Drawing Number ST-29, sheet 69 of 95, and **SUBSTITUTE** with the attached Drawing Number ST-29, Sheet 69-A1 of 95. A Typical Girder Painting Limits detail was added to clarify the extent of field painting to be performed under item 574.9403--25.
23. **DELETE** Drawing Number ST-42, sheet 82 of 95, and **SUBSTITUTE** with the attached Drawing Number ST-42, Sheet 82-A1 of 95. An approximate 3 ft. wide area of full depth pavement reconstruction was added to Section A-A to allow for additional excavation to construct the proposed approach slab.

The Bidder **MUST** complete **Page 197** of the Proposal acknowledging receipt of this amendment. If the Bidder fails to complete the “Amendment Acknowledgement” sheet, his bid could be declared informal thereby delaying award of the contract.

PLEASE BE GOVERNED ACCORDINGLY WHEN SUBMITTING BIDS.

Brent E. Howard, P.E.
Chief Engineer

SPECIAL NOTE**EMBODIED CARBON REPORTING and ENVIRONMENTAL PRODUCT DECLARATIONS (EPDs)**

Projects must comply with all applicable New York State (NYS) laws and policies to reduce the embodied carbon in building and infrastructure projects, including New York State Executive Order 22 (2022).

These laws and policies require Contractors to submit Environmental Product Declarations (EPDs) for certain building and construction materials supplied to State contracts. EPDs must be Product-specific Type III (Third Party Reviewed), in adherence with ISO 14025 *Environmental Labels and Declarations*, ISO 14044 *Environmental Management – Life Cycle Assessment*, and ISO 21930 *Core Rules for Environmental Product Declarations of Construction Products and Services*. Supply Chain-specific data should be used when available.

For permanently incorporated and temporary use construction materials manufactured and supplied for projects let after December 18, 2024, the Contractor shall ensure that all available EPDs are collected and submitted for asphalt mixtures, concrete mixtures (excluding precast), glass, and steel items when the quantity supplied to the project exceeds the values listed below. Units that are given in this document may differ from units listed in pay items and therefore, the Contractor must perform the necessary quantity conversion(s).

| Construction Material | Minimum Quantity for Reporting (for Transportation Projects) |
|--|---|
| Concrete mixes | 200 cubic yards or more (per mix design) |
| Asphalt mixes | 8,000 short tons or more (per mix design) |
| Steel i. Rebar ii. Hollow Structural Sections iii. Fabricated Steel Plate iv. Hot-Rolled Sections v. Cold-Formed & Galvanized | 20,000 pounds or more for rebar (category i.), per fabrication location 15,000 pounds or more for all others (categories ii – v), per fabrication location |
| Glass i. Flat Glass ii. Processed Glass iii. Insulated Glazing Units | 2,000 square feet or more |

The Contractor is not required to develop an EPD if one has not been developed by or for the manufacturer or supplier.

For projects that are active during the State Fiscal Year (April 1 - March 31) and are expected or known to meet the minimum quantities for reporting over the duration of the entire project, the Contractor shall complete and submit a “Contractor EPD Material Reporting Form” (Excel .xlsx format). The form is available on the Thruway Authority website at <https://www.thruway.ny.gov/business/contractors/forms/index.html#contractor>. The Contractor must submit the Contractor EPD Material Reporting form and all EPD(s) collected for the project to the Authority by emailing them to CarbonReports@thruway.ny.gov, with the Contract D number in the subject line of all e-mails. Questions about completing the form can be directed to the same e-mail address.



Thruway Authority

01/21/2025 12:00:24 PM

AASHTOWare Project™ Version 5.00 Revision 031

Proposal Schedule of Items

Proposal ID: D215009

Project(s): A52898

Contractor: _____

SECTION:

Alt Set ID:

Alt Mbr ID:

| Proposal Line Number | Item ID Description | Approximate Quantity and Units | Unit Price | | Bid Amount | |
|----------------------|--|--------------------------------|---------------|-------|------------|-------|
| | | | Dollars | Cents | Dollars | Cents |
| 0300 | 404.0189 TRUING AND LEVELING F9, ASPHALT, 80 SERIES COMPACTION | 60.000 TON | _____ | _____ | _____ | _____ |
| 0302 | 404.1979 19 F9 BINDER COURSE ASPHALT, 70 SERIES COMPACTION | 30.000 TON | _____ | _____ | _____ | _____ |
| 0304 | 404.3779 37.5 F9 BASE COURSE ASPHALT, 70 SERIES COMPACTION | 40.000 TON | _____ | _____ | _____ | _____ |
| 0306 | 586.0201 DRILLING AND GROUTING BOLTS OR REINFORCEMENT BARS | 1,790.000 EACH | _____ | _____ | _____ | _____ |
| 0308 | 619.1711 TEMPORARY POSITIVE BARRIER - CATEGORY 1 (PINNING PROHIBITED) | 390.000 LF | _____ | _____ | _____ | _____ |
| Section: | | | Total: | | _____ | |

PW: D. LEVINE
 CHECKED BY: D. LEVINE
 DRAFTED BY: J.P. O'LOUGHLIN
 CHECKED BY: D. LEVINE
 DESIGNED BY: J.P. O'LOUGHLIN
 DESIGN SUPERVISOR: D. LEVINE

GENERAL NOTES

THIS SHEET SUPERSEDES SHEET 6

6A1
95

1. DESIGN SPECIFICATIONS: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS (9TH EDITION) 2020, AND AS AMENDED BY NEW YORK STATE DEPARTMENT OF TRANSPORTATION LRFD BLUE PAGES, THE NEW YORK STATE THRUWAY AUTHORITY ORANGE PAGES AND THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION BRIDGE MANUAL, 2021 U.S. CUSTOMARY EDITION.
2. CONSTRUCTION AND MATERIAL SPECIFICATION: ALL WORK UNDER THIS CONTRACT AS SHOWN IN THESE PLANS AND/OR SPECIFICATIONS AND THE ACCOMPANYING PROPOSAL IS TO BE IN CONFORMITY WITH "STANDARD SPECIFICATIONS" (U.S. CUSTOMARY UNITS), NEW YORK STATE DEPARTMENT OF TRANSPORTATION OFFICE OF ENGINEERING, CURRENT VERSION, WITH ALL CURRENT ADDENDA, & ALL REVISIONS ISSUED BY NYS DOT ENGINEERING BULLETINS, DIRECTIVES OR INSTRUCTIONS BEFORE THE FIRST PUBLISH DATE OF THE ADVERTISEMENT FOR THE CONTRACT, EXCEPT AS AMENDED ON THE PLANS AND/OR IN THE SPECIFICATIONS IN THE PROJECT PROPOSAL.
3. DESIGN LIVE LOAD: AASHTO HS-20

LOAD RATINGS:

LFR:INVENTORY: HS-21 OPERATING: HS-34

HS LOAD RATINGS WERE COMPUTED IN ACCORDANCE WITH THE AASHTO MANUAL FOR BRIDGE EVALUATION 3RD EDITION WITH 2019 AND 2020 INTERIM REVISIONS. ALL RATINGS PROVIDE FOR A 20 PSF FUTURE WEARING SURFACE.
4. CONCRETE:
THE MINIMUM CONCRETE COMPRESSIVE STRENGTH SHALL BE 4,000 PSI AT 28 DAYS.
5. REINFORCEMENT:
UNLESS OTHERWISE NOTED, ALL BAR REINFORCEMENT SHALL BE ASTM A615 GRADE 60 AND BE SUPPLIED GALVANIZED IN ACCORDANCE WITH ASTM A767, AND MEET THE REQUIREMENTS OF NYS DOT MATERIAL SPECIFICATION 709-11. ALL WIRE FABRIC REINFORCEMENT SHALL BE ASTM A1064. NO SUBSTITUTIONS WILL BE ALLOWED. WIRE FABRIC SHALL BE GALVANIZED AND FABRICATED IN ACCORDANCE WITH ASTM A767 AND ASTM A1234 AND MEET THE REQUIREMENTS OF NYS DOT MATERIAL SPECIFICATION 709-02. THE REQUIREMENTS OF ASTM A767 SECTION 4.3 CHROMATING WILL NOT BE WAIVED FOR BAR REINFORCEMENT AND/OR WIRE FABRIC REINFORCEMENT. REINFORCEMENT FOR REINFORCED CONCRETE PIPE AND RELATED DRAINAGE STRUCTURES SHALL MEET THE REQUIREMENTS OF NYS DOT MATERIAL SPECIFICATION "SECTION 706 - CONCRETE, CLAY, AND PLASTIC PIPE".
6. THE CONTRACTOR SHALL VISIT THE SITE BEFORE BIDDING TO FAMILIARIZE THEMSELVES WITH THE PRESENT CONDITIONS AND TO JUDGE FOR THEMSELVES THE EXTENT AND NATURE OF THE WORK TO BE PERFORMED UNDER THIS CONTRACT. NO EXTRA COMPENSATION WILL BE ALLOWED BECAUSE OF THEIR FAILURE TO INCLUDE IN THE BID, ALL ITEMS AND MATERIALS WHICH THEY ARE REQUIRED TO FURNISH IN ACCORDANCE WITH CONTRACT DOCUMENTS.
7. THE CONTRACTOR SHALL EXAMINE AND VERIFY IN THE FIELD, ALL EXISTING CONDITIONS AND DIMENSIONS SHOWN ON THE PLANS. IF FIELD CONDITIONS AND DIMENSIONS DIFFER FROM THOSE ON THE PLANS, THE CONTRACTOR SHALL USE THE FIELD CONDITIONS AND DIMENSIONS TO MAKE THE APPROPRIATE CHANGES TO THOSE SHOWN ON THE PLANS AS APPROVED BY THE ENGINEER.
8. THE CONTRACTOR SHOULD NOTE THAT ADDITIONAL WORK MAY BE REQUIRED AS THE CONTRACT PROGRESSES WHICH IS NOT SHOWN OR NOTED ON THE PLANS. THIS WORK SHALL BE PERFORMED BY THE CONTRACTOR AS ORDERED BY THE ENGINEER AND PAYMENT SHALL BE MADE AT THE UNIT BID PRICE FOR THE APPROPRIATE ITEMS.
9. THE CONTRACTOR SHALL EXERCISE CAUTION AND PERFORM ALL WORK WITH CARE SO THAT ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR BE RE-USED, OR WHICH ARE TO REMAIN THE PROPERTY OF THE THRUWAY AUTHORITY WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE THRUWAY AUTHORITY, THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR, WITH NO ADDITIONAL COST TO THE THRUWAY AUTHORITY.
10. THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ALL DAMAGES CAUSED BY THEIR OPERATIONS TO THE EXISTING ROADWAYS WHICH ARE NOT INCLUDED AS PART OF THE INTENDED WORK. ALL DAMAGE TO EXISTING ROADWAYS WHICH ARE NOT PART OF THE INTENDED WORK SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER, WITHOUT COST TO THE THRUWAY AUTHORITY.
11. ALL WORK SHALL BE LIMITED TO WITHIN THE RIGHTS OF WAY, EASEMENTS, OR LIMITS OF RELEASE.
12. WHENEVER ITEMS IN THE CONTRACT REQUIRE MATERIALS TO BE REMOVED AND DISPOSED OF, THE COST OF SUPPLYING A DISPOSAL AREA AND TRANSPORTATION TO THAT AREA SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THOSE ITEMS.
13. THIS BRIDGE SHALL BE MAINTAINED IN ACCORDANCE WITH THE GUIDELINES CONTAINED IN THE CURRENT EDITION OF THE AASHTO MAINTENANCE MANUAL FOR ROADWAYS AND BRIDGES.
14. HAZARDOUS MATERIALS CONTAINING LEAD AND ASBESTOS ARE BELIEVED TO EXIST AT VARIOUS LOCATIONS ON OR IN CERTAIN STRUCTURES CONTAINED IN THE CONTRACT. THESE MATERIALS WERE NOTED ON THE ORIGINAL CONTRACT PLANS OF THE STRUCTURES AND/OR DURING FIELD INSPECTIONS AND ARE PRESENTED IN THE TABLE BELOW. SEE PRELIMINARY ASBESTOS SCREENING: PRELIMINARY INVESTIGATION AND HAZARDOUS MATERIALS WORKPLAN PREPARED BY HUNT EAS DATED MAY 2, 2024 AND REVISED JUNE 7, 2024. ALSO REFERENCE THE LIMITED ASBESTOS SURVEY PREPARED BY ATLANTIC TESTING LABORATORIES DATED OCTOBER 24, 2024.
15. UNIT BID PRICES SHALL INCLUDE ANY ADDITIONAL COSTS INCURRED TO MEET U.S. ARMY CORP. OF ENGINEERS, NYSDEC, NYS DOT, US EPA, AND OSHA REGULATIONS.
16. NOT USED.
17. RECORD PLANS FOR THE EXISTING BRIDGE WILL BE AVAILABLE AS PROJECT SPECIFIC SUPPLEMENTAL INFORMATION.
18. NO DIRECT PAYMENT SHALL BE MADE FOR WORK INDICATED ON THE DRAWINGS OR WITHIN THE SPECIFICATIONS FOR WHICH NO ITEM NUMBER IS INDICATED. THE COST FOR SUCH WORK SHALL BE INCLUDED IN THE VARIOUS ITEMS OF THE CONTRACT.
19. THE CONTRACTOR IS ADVISED THAT ADDITIONAL "NOTES" WILL BE FOUND ON SUBSEQUENT SHEETS OF THE CONTRACT PLANS AND SUCH "NOTES", WHILE PERTAINING TO THE SPECIFIC DRAWINGS THEY ARE PLACED ON, ALSO SUPPLEMENT THE GENERAL NOTES LISTED HEREIN.
20. THE CONTRACTOR IS REQUIRED TO PROTECT THEIR WORKERS IN ACCORDANCE WITH SECTION 107-05 OF THE NYS DOT STANDARD SPECIFICATIONS AND SHALL SUBMIT A HEALTH AND SAFETY PLAN TO THE ENGINEER PRIOR TO COMMENCING WORK.
21. SUBSURFACE EXPLORATIONS HAVE NOT BEEN MADE FOR THIS PROJECT. SEE EXISTING DRAWINGS FOR RECORD BORING INFORMATION.
22. THERE IS NO IDENTIFIED SPOIL AREA WITHIN THE THRUWAY AUTHORITY'S RIGHT-OF-WAY FOR THIS CONTRACT. THEREFORE, THE CONTRACTOR SHALL REMOVE ALL SURPLUS MATERIAL AND WASTE FROM THE AUTHORITY'S PROPERTY. THE CONTRACTOR SHALL BID ACCORDINGLY FOR THIS CONDITION.
23. THE CONTRACTOR SHALL PROVIDE FOR THE STABILITY OF THE STRUCTURAL STEEL DURING ALL PHASES OF ERECTION AND CONSTRUCTION, IN ACCORDANCE WITH SECTION 2, SUBSECTION 204 OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL (SCM). THE GIRDERS FOR THIS BRIDGE SHALL BE STABILIZED DURING ERECTION BY USE OF FALSEWORK, TEMPORARY BRACING, COMPRESSION FLANGE STIFFENING TRUSSES, CHOOSING ALTERNATE PICKING POINTS, OR BY USE OF A HOLDING CRANE UNTIL A SUFFICIENT NUMBER OF GIRDERS HAVE BEEN ERRECTED AND CROSS FRAMES INSTALLED. THE MEANS AND METHODS USED BY THE CONTRACTOR SHALL BE DOCUMENTED ON THE ERECTION DRAWINGS AND INCLUDED IN THE SUPPORTING STABILITY CALCULATIONS. THE ERECTION DRAWINGS AND STABILITY CALCULATIONS SHALL BE SEALED BY A REGISTERED NEW YORK STATE PROFESSIONAL ENGINEER AND SUBMITTED TO THE AUTHORITY IN ACCORDANCE WITH THE SCM.
24. ALL METAL REINFORCING BAR CHAIRS AND SUPPORTS SHALL HAVE PLASTIC SHOES.

BRIDGE REMOVAL, EXCAVATION & BACKFILL NOTES:

1. BOTH EXISTING ABUTMENTS AND PIER COLUMNS SHALL BE EXCAVATED TO THE LIMITS INDICATED ON THE PLANS UNDER ITEM 206.01
2. THE EXISTING SUPERSTRUCTURE SLAB SHALL BE REMOVED UNDER ITEM 202.2202.
3. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE REQUIREMENTS OF SUBSECTION 202-3.01 GENERAL SAFETY REQUIREMENTS. A SUPERSTRUCTURE SLAB REMOVAL PLAN SHALL BE SUBMITTED TO THE ENGINEER PRIOR TO BEGINNING THE DEMOLITION.
4. DURING REMOVAL OPERATIONS, THE CONTRACTOR SHALL NOT DROP WASTE CONCRETE, DEBRIS OR OTHER MATERIAL ONTO NYSS & ERIE CANAL TRAILWAY BELOW THE BRIDGE EXCEPT WHERE THE PLANS OR SPECIFICATIONS SPECIFICALLY PERMIT THE DROPPING OF MATERIALS. PLATFORMS, NETS, SCREENS OR OTHER PROTECTIVE DEVICES SHALL BE USED TO CATCH THE MATERIAL. IF THE ENGINEER DETERMINES THAT ADEQUATE PROTECTIVE DEVICES ARE NOT BEING EMPLOYED, THE WORK SHALL BE SUSPENDED UNTIL ADEQUATE PROTECTION IS PROVIDED. IF MATERIAL FALLS ON THE AREA BELOW AND ADJACENT TO THE BRIDGE, IT SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR ON A DAILY BASIS AT NO COST TO THE THRUWAY AUTHORITY.

5. THE COST OF FURNISHING, INSTALLING, MAINTAINING, REMOVING AND DISPOSING OF ALL PLATFORMS, NETS, SCREENS OR OTHER PROTECTIVE DEVICES SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THE APPROPRIATE ITEMS OF THE CONTRACT.
6. LIMITS AND METHODS FOR REMOVAL OF PAINT AT LOCATIONS OF FASTENER REMOVAL OR FLAME CUTTING SHALL BE AS DESCRIBED IN SUBSECTIONS 202-3.01 AND 574 OF THE STANDARD SPECIFICATIONS. THE COST OF ANY PAINT REMOVAL OR PAINT STRIPPING REQUIRED SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR THE SUPERSTRUCTURE REMOVAL ITEM. PAINT WASTE SHALL BE COLLECTED FOLLOWING THE REQUIREMENTS OF THE ENVIRONMENTAL GROUND PROTECTION ITEM. PAINT WASTE SHALL BE DISPOSED OF FOLLOWING THE REQUIREMENTS OF NYS DOT STANDARD SPECIFICATIONS SECTION 571 DISPOSAL OF PAINT REMOVAL WASTE.
7. IN ADDITION TO THE EXCAVATION PROTECTION SYSTEMS SHOWN IN THE PLANS, (I.E. LAYBACKS), THE CONTRACTOR SHALL PROVIDE ALL ADDITIONAL TEMPORARY SUPPORTS, BRACING, AND OTHER DEVICES REQUIRED OR DIRECTED BY THE ENGINEER TO PROTECT THE SAFETY OF WORKERS, ADJACENT STRUCTURES, ROADWAYS AND UTILITIES. ALL COSTS FOR THIS WORK SHALL BE INCLUDED IN VARIOUS ITEMS IN THE CONTRACT.
8. ALL PLACEMENTS OF SELECT STRUCTURAL FILL, ITEM 203.21 SHALL BE COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY. THE CONTRACTOR SHALL ADD WATER AS REQUIRED TO OBTAIN THE SPECIFIED COMPACTION & DENSITY. THE CONTRACTOR SHALL PROVIDE A STANDARD PROCTOR GRAPH FOR THE SELECT MATERIAL USED.
9. THE COST OF WATER USED FOR COMPACTION OF VARIOUS ITEMS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE RESPECTIVE ITEM. NO SEPARATE PAYMENT SHALL BE MADE.
10. IN THE EVENT THAT WORK IMPACTS EXISTING ROADWAY NYSS EITHER DURING EXCAVATION OR OTHER OPERATIONS, THE ROADWAY SHALL BE RESTORED TO ITS ORIGINAL CONDITION.

SUBSTRUCTURE NOTES:

1. THE COST OF ALL JOINT MATERIALS AND WATERSTOPS IS TO BE INCLUDED IN THE UNIT PRICES BID FOR THE VARIOUS ITEMS OF THE CONTRACT UNLESS OTHERWISE SPECIFIED ON THE PLANS.
2. ALL EXPOSED EDGES OF ABUTMENTS AND WINGWALLS SHALL HAVE A MINIMUM CHAMFER OF 1", EXCEPT WHERE OTHERWISE NOTED ON THE PLANS.
3. REINFORCEMENT BAR SCHEDULES ARE FOR INFORMATION ONLY. ALL BAR SCHEDULES MUST BE VERIFIED BY THE CONTRACTOR PRIOR TO FABRICATION.

SUPERSTRUCTURE NOTES:

1. ALL STEEL FABRICATION IS GOVERNED BY, AND SHALL MEET THE REQUIREMENTS OF, THE NEW YORK STATE STEEL CONSTRUCTION MANUAL, 4TH EDITION (JANUARY 2018), EXCEPT AS DIRECTED WITHIN THESE PLANS OR AS ORDERED BY THE ENGINEER.
2. STRUCTURAL STEEL:

ALL STRUCTURAL STEEL SHALL BE ASTM A709 GR. 50 UNLESS OTHERWISE NOTED.
3. ALL BOLTS SHALL BE ASTM F3125 GRADE A325, TYPE 1 H.S. BOLTS WITH ASTM A563 DH HEAVY HEX NUTS & F436 TYPE 1 HARDENED WASHERS. ALL NUTS, BOLTS, & WASHERS SHALL BE HOT DIPPED GALVANIZED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL. ALL NEW H.S. BOLTS SHALL BE 7/8" DIA. EXCEPT AS NOTED.
4. WELDING:
ALL WELDING SHALL CONFORM TO THE LATEST VERSION OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL UNLESS OTHERWISE NOTED.
5. FOR THE VARIOUS LUMP SUM STRUCTURAL STEEL ITEMS IN THE CONTRACT, THE "TOTAL WEIGHT FOR PROGRESS PAYMENT" IS AS FOLLOWS:

| ITEM | TOTAL WEIGHT FOR PROGRESS PAYMENT |
|------------|-----------------------------------|
| 564.510001 | 56,000 LB |


• WEIGHT OF WESTBOUND BRIDGE REPAIR STEEL

THESE WEIGHTS SHALL BE USED IN DETERMINING PARTIAL PROGRESS PAYMENTS. UNDER NO CIRCUMSTANCES SHALL THE "TOTAL WEIGHT FOR PROGRESS PAYMENTS" BE USED FOR FINAL PAYMENT PURPOSES. THE CONTRACTOR IS ADVISED NOT TO USE THE TOTAL WEIGHT FOR PROGRESS PAYMENT AS A BIDDING TOOL. DISCREPANCIES BETWEEN THE TOTAL WEIGHT SHIPPED AND TOTAL WEIGHT FOR PROGRESS PAYMENT SHALL NOT BE A BASIS FOR ADDITIONAL COMPENSATION.

6. SHOP DRAWINGS SHALL BE PREPARED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL AND SHALL BE SUBMITTED IN ACCORDANCE WITH THE NYSTA PROCEDURES. SHOP DRAWINGS SHALL BE SUBMITTED IN ENGLISH UNITS OF MEASURE (U.S. CUSTOMARY UNITS). THE WEIGHTS OF ALL COMPONENTS SHALL BE INDICATED ON THE BILL OF MATERIAL WITHIN THE SHOP DRAWINGS.
7. THE CONTRACTOR SHALL PROVIDE FOR THE STABILITY OF STRUCTURAL STEEL DURING ALL PHASES OF ERECTION AND CONSTRUCTION, AS PROVIDED IN SUBSECTION 204 OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL (SCM). THE METHODS USED BY THE CONTRACTOR SHALL BE DOCUMENTED ON THE ERECTION DRAWINGS WITH ALL SUPPORTING STABILITY CALCULATIONS SUBMITTED AND STAMPED BY A LICENSED AND REGISTERED NEW YORK STATE PROFESSIONAL ENGINEER AND SUBMITTED TO THE ENGINEER IN ACCORDANCE WITH THE SCM.



| SAMPLE IDENTIFICATION | MATERIAL | LOCATION | APPROX QUANTITY | ITEM NO. |
|----------------------------|------------------------|--|-----------------|------------|
| AT5796AI03A AT5796AI03B | BLACK SHEET PACKING | BETWEEN ABUTMENT BACKWALL AND SUPERSTRUCTURE SLAB | 800 SF | 210.481201 |

• SEE GENERAL NOTE 14

| | |
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| ALTERED ON: | AFFKED ON: 01/16/2025 |
| SIGNATURE: STAMP: | SIGNATURE: DANIEL A. LEVINE STAMP:  |

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

| REVISIONS | | | |
|-----------|-------------|----|------|
| DATE | DESCRIPTION | BY | SYM. |
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|   | TITLE OF PROJECT REHABILITATION OF I-90 BRIDGE OVER NY 55 & ERIE CANAL TRAIL | CONTRACT NUMBER: TAA 25-06B |
| | LOCATION OF PROJECT ALBANY DIVISION MP 178.38 | DATE: 12/11/2024 |
| TITLE OF DRAWING GENERAL NOTES (SHEET 1 OF 2) | DRAWING NUMBER: GNN-1 | |

GENERAL NOTES (CONT'D)

THIS SHEET SUPERSEDES SHEET 7

7A1
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SUPERSTRUCTURE NOTES:

8. DIAPHRAGMS SHALL BE FABRICATED TO FIT GIRDERS IN-SITU. THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS PRIOR TO THE PREPARATION OF SHOP DRAWINGS.

GALVANIZING NOTES:

ALL STRUCTURAL STEEL SHALL BE HOT-DIP GALVANIZED. THE HOT-DIP GALVANIZING SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 564.20010008 - HOT-DIP GALVANIZING OF STRUCTURAL STEEL.

a. ALL BOLTS, NUTS, AND WASHERS SHALL BE GALVANIZED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL.

b. ALL EXPOSED STEEL SURFACES ON DIAPHRAGMS, UTILITY SUPPORTS, AND SHIMS SHALL BE GALVANIZED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL.

c. REASONABLE ACCOMODATIONS FOR THE PREVENTION OF WET STORAGE STAINING (WHITE RUST) OF HOT-DIPPED GALVANIZED (HDG) MATERIALS SHALL BE PROVIDED AT ALL TIMES. STORAGE OF HDG MATERIALS OUTDOORS SHOULD BE AVOIDED IF POSSIBLE. IF OUTDOOR STORAGE IS UNAVOIDABLE, EXAMPLES OF REASONABLE ACCOMMODATIONS ARE AS FOLLOWS: STORAGE MATERIALS OFF OF THE GROUND AWAY FROM ALL VEGETATION, NON-RESINOUS WOODEN SPACES TO ALLOW VENTILATION AND AVOID MOISTURE BUILD UP, INCLINE MEMBERS TO ALLOW DRAINAGE, EXAMPLES OF NON-RESINOUS WOODS ARE, POPLAR ASH, AND SPRUCE. WHITE RUST THAT IS DETERMINED TO BE DETRIMENTAL TO THE INTENDED USE OF THE MEMBER HAS A NEGATIVE VISUAL IMPACT ON THE STRUCTURE SHALL BE REPAIRED IN ACCORDANCE WITH THE NYS STEEL CONSTRUCTION MANUAL. WHITE RUST THAT IS DETERMINED TO BE CAUSED BY IMPROPER STORAGE OR SHIPPING OF HDG MATERIALS SHALL BE REPAIRED AT NO COST TO THE THRUWAY AUTHORITY.

d. STUD SHEAR CONNECTORS SHALL BE INSTALLED PRIOR TO GALVANIZING. THE CONTRACTOR'S WORKER SAFETY PLAN SHALL SPECIFY THE TYPE OF WALKING/WORKING PRECAUTIONS TO BE IMPLEMENTED TO PROTECT THE WORKERS.

PAINTING NOTES:

a. NOT USED.

b. EXISTING STEEL WITHIN THE "CRITICAL AREA" AS DEFINED IN THE SPECIFICATION ARE TO BE CLEANED AND PAINTED UNDER ITEM 574.9403--25. FINISH COAT COLOR SHALL MATCH THE EXISTING BRIDGE PAINT. VIEWING SHALL BE DONE UNDER NORTHERN STANDARD DAYLIGHT. SEE DWG. NO. ST-29 FOR A DETAIL OF THE TYPICAL GIRDER PAINTING LIMITS.

12. THE COST FOR ALL BOLTS, NUTS, WASHERS, WELDS AND DIAPHRAGM CONNECTION SHIM PLATES SHALL BE INCLUDED IN THE PRICE BID FOR THE STRUCTURAL STEEL ITEM. NO SPECIAL OR DIRECT PAYMENT WILL BE MADE FOR THESE MATERIALS.

SUPERSTRUCTURE SLAB NOTES:

1. DECK FORMS SHALL BE STAY-IN-PLACE (SIP) FORMS BETWEEN ALL GIRDERS AND SHALL BE REMOVABLE FORMS FOR BOTH FASCIA OVERHANGS. SIP FORMS AND SUPPORTS SHALL CONFORM TO ASTM A653, COATING DESIGNATION G235. FABRICATION SHALL BE IN CONFORMANCE WITH ASTM A924.

2. IN ORDER TO PREVENT MOVEMENT OF THE BRIDGE OVERHANG BRACKET DURING CONCRETE PLACEMENT, AS WELL AS TO PREVENT LATERAL DISTORTION OF THE GIRDER WEB, AN OVERHANG BRACKET THAT IS BRACED BY THE BOTTOM FLANGE SHALL BE USED.

3. THE SURFACES OF GIRDERS SHALL BE PROTECTED FROM DECK OVERHANG FORM SUPPORTS TO PREVENT DAMAGE TO THE PAINTED SURFACES.

4. ALL LONGITUDINAL AND TRANSVERSE TOP MAT DECK REINFORCING SHALL BE STAGGERED 1/2 THE SPACING ABOVE THE BOTTOM MAT DECK REINFORCING STEEL UNLESS OTHERWISE INDICATED IN THE PLANS.

5. THE DETAILS FOR THE BARRIER REINFORCEMENT ARE FOR THE SLIP-FORMED OR CAST-IN-PLACE OPTION ONLY. COST OF BARRIER AND ANCHORAGE REINFORCEMENT ORIGINATING IN THE SLAB SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE SINGLE SLOPE (HALF SECTION) CONCRETE BRIDGE BARRIER, ITEM 569.04.

6. PROTECTIVE SEALER, ITEM 559.01, SHALL BE APPLIED TO ALL EXPOSED CONCRETE SURFACES OF THE STRUCTURAL DECK SLAB, BARRIERS, APPROACH SLABS, AND EXPOSED TOP SURFACES OF SLEEPER SLABS. ONLY PENETRATING TYPE SEALER AS INDICATED IN THE SPECIFICATION SHALL BE USED.

WORK ZONE TRAFFIC CONTROL NOTES:

1. PROTECTION OF THE PUBLIC: CONTRACTOR SHALL MAINTAIN AND PROTECT THRUWAY TRAFFIC IN ACCORDANCE WITH SECTION 619, THE NYSTA ADDENDUM TO THE STANDARD SPECIFICATIONS, THE TRAFFIC CONTROL PLANS, THE MUTCD, AND THE NEW YORK STATE SUPPLEMENT TO THE MUTCD. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE REQUIREMENTS OF SECTION 107, LEGAL RELATIONS AND RESPONSIBILITY TO THE PUBLIC, OF THE CURRENT STANDARD SPECIFICATIONS.

2. FOR ADDITIONAL WORK ZONE TRAFFIC CONTROL NOTES, SEE DWG. TCN-1.

EROSION & SEDIMENT CONTROL NOTES:

1. EROSION CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 209 OF THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION SPECIFICATIONS AND STANDARD SHEETS. ADDITIONAL GUIDANCE AND SIZING CRITERIA CAN BE FOUND IN THE MOST CURRENT EDITION OF THE NEW YORK STATE STANDARDS AND SPECIFICATIONS FOR EROSION AND SEDIMENT CONTROL (THE BLUE BOOK) PUBLISHED BY THE NYS DEPARTMENT OF ENVIRONMENTAL CONSERVATION. ADDITIONAL SOIL EROSION AND SEDIMENT CONTROL MEASURES MAY BE REQUIRED AS PER SECTION 107-12 OF THE STANDARD SPECIFICATIONS.

2. THE CONTRACTOR WILL BE REQUIRED TO PERFORM ALL CONSTRUCTION OPERATIONS IN A MANNER SO AS TO MINIMIZE SOIL EROSION AND ENSURE SEDIMENT CONTROL.

3. ALL NECESSARY PRECAUTIONS SHALL BE TAKEN TO PREVENT DIRECT OR INDIRECT CONTAMINATION OF ALL WATER BODIES (INCLUDING WETLANDS) BY SILT, SEDIMENT, FUELS, SOLVENTS, LUBRICANTS, EPOXY COATINGS, CONCRETE LEACHATE, OR ANY OTHER POLLUTANT ASSOCIATED CONSTRUCTION. DURING CONSTRUCTION, NO WET OR FRESH CONCRETE OR LEACHATE SHALL BE ALLOWED TO ESCAPE DIRECTLY OR INDIRECTLY INTO ANY GROUND SURFACES OR WATER BODIES (INCLUDING WETLANDS), NOR SHALL WASHINGS FROM CONCRETE TRUCKS, MIXERS, OR OTHER DEVICES BE ALLOWED TO ESCAPE DIRECTLY OR INDIRECTLY INTO ANY GROUND SURFACES OR WATER BODIES (INCLUDING WETLANDS). CONTRACTOR SHALL PROVIDE A CONCRETE WASHOUT PIT IN ACCORDANCE WITH STANDARD SPECIFICATIONS SHOWN WITHIN NYSDEC'S 2016 BLUE BOOK (SECTION 2 RESOURCE PLANNING). COST SHALL BE INCLUDED IN THE VARIOUS 209 ITEMS.

4. ANY DEBRIS OR EXCESS MATERIAL FROM CONSTRUCTION OF THIS PROJECT SHALL BE IMMEDIATELY AND COMPLETELY REMOVED FROM THE BED AND BANKS OF ALL WATER BODIES (INCLUDING WETLANDS) AND SHALL BE DISPOSED OF AWAY FROM WETLANDS, WATER COURSES, OR OTHER BODIES OF WATER.

5. ALL EXCAVATED MATERIAL SHALL BE DISPOSED OF AND BE PROTECTED SO THAT IT CAN NOT DIRECTLY OR INDIRECTLY RE-ENTER ANY WATER BODY OR WETLAND AREA.

6. TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED AS PER SPECIFICATIONS AND IN ACCORDANCE WITH NYS DOT STANDARD SHEETS, SECTION 209 AND WITHIN THE CONTRACT DOCUMENTS. THE COST OF MAINTAINING AND REMOVING TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INCLUDED IN THE BID PRICE OF THE APPROPRIATE ITEM USED FOR THE INSTALLATION OF THE MEASURE. ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSPECTED BY THE CONTRACTOR DAILY AFTER EACH STORM EVENT THAT GENERATES RUNOFF, NOTED DEFICIENCIES SHALL BE CORRECTED WITHIN ONE BUSINESS DAY.

7. PROVIDE TEMPORARY STABILIZATION (SEED AND MULCH-TEMPORARY, ITEM 209.1003) FOR ALL DISTURBED AREAS AS SOON AS PRACTICABLE AND WITHIN 14-DAYS OF WHEN THE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED.

ITEM 698.06 STEEL/IRON PRICE ADJUSTMENT NOTES:

1. THE STEEL/IRON PRICE ADJUSTMENT ITEM IS INCLUDED IN THE CONTRACT TO PROTECT THE CONTRACTOR AND AUTHORITY FROM WIDE VARIATIONS IN THE COST OF STEEL DURING THE COURSE OF THIS CONTRACT.

WARNING: UNDERGROUND FIBER OPTIC CONDUIT:

1. THE THRUWAY'S FIBER OPTIC "BACKBONE" IS LOCATED WITHIN THE WORK LIMITS OF THIS PROJECT. THE APPROXIMATE LOCATION IS SHOWN ON THE DRAWINGS. THE CONTRACTOR IS ADVISED TO CONTACT UDIG NEW YORK AT 1-800-962-7962 (OR 811) PRIOR TO ANY EXCAVATION. FURTHERMORE, PURSUANT TO N.Y.S. CODE RULE 753, THE CONTRACTOR MUST BE PREPARED TO VERIFY THE LOCATION OF THE FIBER OPTIC LINE THROUGH HAND DUG TEST HOLES AT ONE OR MORE LOCATIONS WITHIN THE WORK AREA PRIOR TO ANY EXCAVATION. HAND DUG TEST HOLES SHALL BE PAID FOR UNDER ITEM 206.05 - TEST PIT EXCAVATION (EACH) AS REQUIRED.

UTILITY NOTES:

1. LOCATION OF EXISTING UTILITIES, PUBLIC AND/OR PRIVATE, AS SHOWN IN THE PLANS OR INDICATED IN THE PROPOSAL ARE APPROXIMATE ONLY. THE EXACT LOCATION OF EACH UTILITY SHALL BE DETERMINED IN THE FIELD. ADDITIONAL UTILITY LINES NOT SHOWN ON THE PLANS, WHETHER ABANDONED OR IN SERVICE, MAY EXIST. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONDUCT THEIR OPERATIONS AND TAKE NECESSARY PRECAUTIONS TO PREVENT INTERFERENCE WITH OR DAMAGE TO THESE OR OTHER FACILITIES DURING THE COURSE OF CONSTRUCTION. THE CONTRACTOR IS ADVISED TO CONTACT UDIG NEW YORK AT 1-800-962-7962 (OR 811) PRIOR TO ANY EXCAVATION.

2. IN THE EVENT THE CONTRACTOR DAMAGES AN EXISTING UTILITY SERVICE CAUSING AN INTERRUPTION IN SAID SERVICE, THEY SHALL IMMEDIATELY COMMENCE WORK TO RESTORE SERVICE AND MAY NOT CEASE THEIR WORK OPERATION UNTIL SERVICE IS RESTORED.

3. THE METHOD OF REMOVAL OF EXISTING ROADWAY OR SHOULDER PAVEMENT IN THE IMMEDIATE VICINITY OF UNDERGROUND UTILITIES SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.

COORDINATION NOTES:

1. THE CONTRACTOR MAY BE REQUIRED TO COORDINATE THEIR WORK WITH OTHER CONTRACTORS AND AUTHORITY MAINTENANCE FORCES. THE CONTRACTOR SHALL SCHEDULE THEIR OPERATIONS SO AS TO CAUSE MINIMAL DISRUPTION TO TRAFFIC.

WORK TO BE DONE:

THE FOLLOWING IS A GENERAL DESCRIPTION OF WORK TO BE DONE UNDER THIS CONTRACT. THIS LIST IS INTENDED TO GIVE THE CONTRACTOR A GENERAL DESCRIPTION OF THE WORK INVOLVED AND IS NOT A COMPLETE LISTING OF ALL WORK TO BE DONE. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS EVEN THOUGH NOT SPECIFICALLY MENTIONED IN THIS LIST.


REHABILITATION OF WESTBOUND STRUCTURE:

1. ESTABLISH AND PERFORM WORKZONE TRAFFIC CONTROL TO SET-UP CROSS-OVER.
2. ESTABLISH AND PERFORM WORKZONE TRAFFIC CONTROL TO SHIFT TRAFFIC TO EASTBOUND STRUCTURE.
3. INSTALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES.
4. INSTALL WZTC MEASURES ALONG NY55 AND PROTECTIVE SHIELDING OVER THE ERIE CANALWAY TRAIL.
5. REMOVE AND DISPOSE OF EXISTING SUPERSTRUCTURE SLAB, END DIAPHRAGMS, BEARINGS, PEDESTALS, AND PORTIONS OF THE SUBSTRUCTURE AS INDICATED. REMOVAL OF DECK IS TO UTILIZE A HOLD AND RELEASE (TRAFFIC CONTROL) METHOD ON NY ROUTE 55 IN COORDINATION WITH LOCAL AUTHORITIES.
6. EXCAVATE FOR PROPOSED SUBSTRUCTURE REPAIRS.
7. PERFORM SUBSTRUCTURE REPAIRS.
8. INSTALL NEW GIRDER END DIAPHRAGMS.
9. INSTALL NEW GIRDER BEARINGS.
10. CONSTRUCT BRIDGE DECK AND TOPSIDE FEATURES.
11. CONSTRUCT BEGIN AND END APPROACH SLAB.
12. RECONSTRUCT BEGIN AND END APPROACHES.
13. TIE-IN PROPOSED WORK WITH EXISTING. CUT AND MILL PORTION OF EXISTING PAVEMENT TO COMPLETE TIE-IN.

REHABILITATION OF EASTBOUND STRUCTURE:


14. ESTABLISH AND PERFORM WORKZONE TRAFFIC CONTROL TO SHIFT TRAFFIC TO WESTBOUND STRUCTURE.
15. REPEAT STEPS 4 THROUGH 13 FOR THE EASTBOUND STRUCTURE.
16. USING SHORT-DURATION LANE CLOSURES, INSTALL NEW PAVEMENT MARKINGS AND SIGNS AS SHOWN ON THE CONTRACT PLANS. REMOVE WORKZONE TRAFFIC CONTROL AND SPLIT TRAFFIC TO FINAL EASTBOUND AND WESTBOUND LAYOUT.
17. REMOVE TEMPORARY EROSION CONTROL MEASURES AFTER TURF ESTABLISHMENT.

PW/ CHECKED BY: D. LEVINE DRAFTED BY: J.P. O'LOUGHLIN CHECKED BY: D. LEVINE DESIGNED BY: J.P. O'LOUGHLIN DESIGN SUPERVISOR: D. LEVINE

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| ALTERED ON: | AFFIXED ON: 01/16/2025 |
| SIGNATURE: STAMP: | SIGNATURE: DANIEL A. LEVINE STAMP:  |

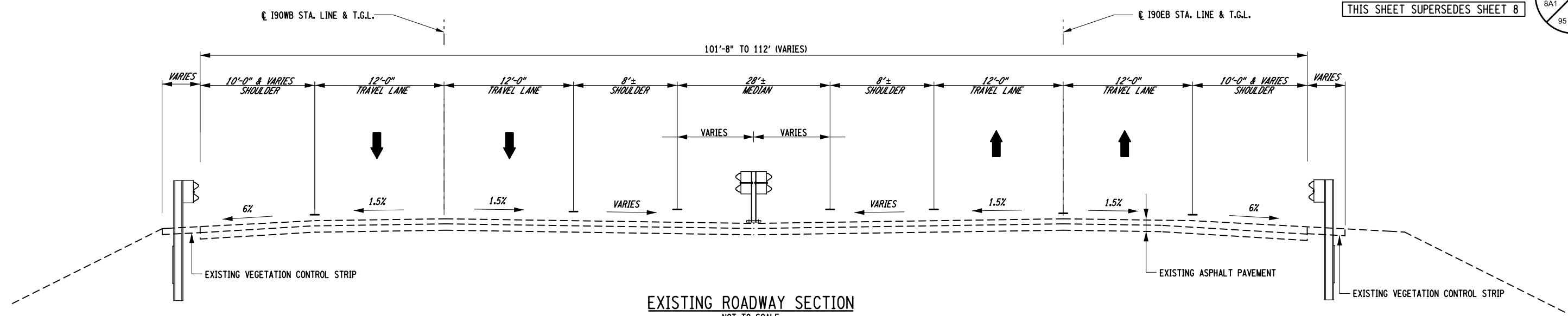
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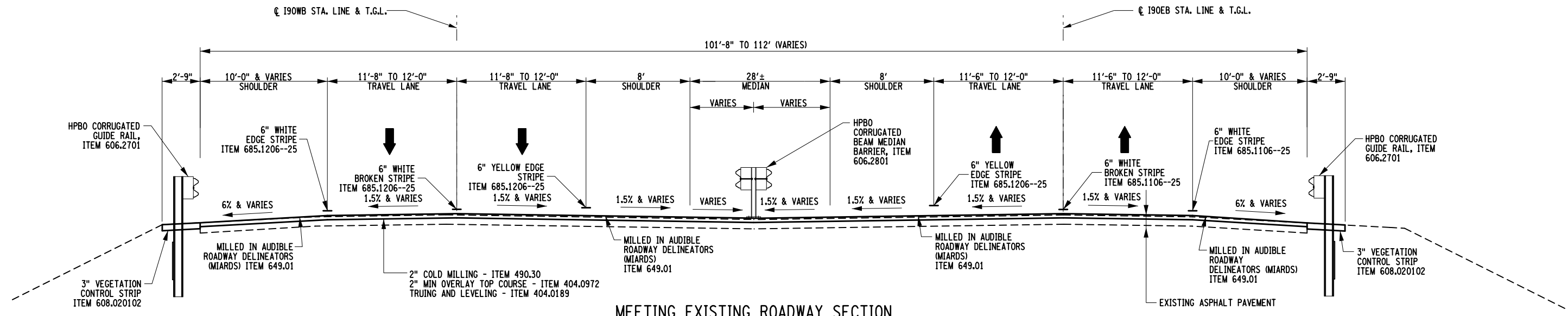
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|  PARSONS | TITLE OF PROJECT REHABILITATION OF I-90 BRIDGE OVER NY 55 & ERIE CANAL TRAIL | CONTRACT NUMBER: TAA 25-06B |
| | LOCATION OF PROJECT ALBANY DIVISION MP 178.38 | DATE: 12/11/2024 |
| TITLE OF DRAWING GENERAL NOTES (SHEET 2 OF 2) | DRAWING NUMBER: GNN-2 | |

PW/ CHECKED BY: D. LEVINE
 DRAFTED BY: J.P. O'LOUGHLIN
 CHECKED BY: D. LEVINE
 DESIGNED BY: J.P. O'LOUGHLIN
 DESIGN SUPERVISOR: D. LEVINE

THIS SHEET SUPERSEDES SHEET 8 8A1
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EXISTING ROADWAY SECTION
 NOT TO SCALE
 190WB STA. 11+47.87 TO 190WB STA. 16+18.96
 190EB STA. 11+24.92 TO 190EB STA. 15+46.13
 APPROXIMATELY MILE 178.23 TO MILE 178.48



MEETING EXISTING ROADWAY SECTION
 NOT TO SCALE
 190WB STA. 11+47.87 TO 190WB STA. 11+97.87
 190EB STA. 11+24.92 TO 190EB STA. 11+74.92
 APPROXIMATELY MILE 178.48

- NOTES:
- EXISTING STRIPED TRAVEL LANE AND SHOULDER WIDTHS VARY.
 - STRAIGHT TACK COAT SHALL BE APPLIED BETWEEN ALL PAVEMENT COURSES AND ON THE MILLED SURFACE.
 - PAVEMENT JOINT ADHESIVE SHALL BE APPLIED AT ALL PAVEMENT JOINTS IN THE TOP COURSE.
 - REFER TO GNP-01 TO GNP-03 FOR LOCATION OF GUIDE RAILS.

ALTERED ON: _____ AFFIKED ON: 01/16/2025

SIGNATURE: _____ STAMP: _____

SIGNATURE: DANIEL A. LEVINE
 STAMP: _____

| ITEM | DESCRIPTION | UNIT | ITEM | DESCRIPTION | UNIT |
|------------|---|------|--------------|---|------|
| 404.0189 | TRUING & LEVELING F9, ASPHALT, 80 SERIES COMPACTION | TON | 649.01 | MILLED-IN AUDIBLE ROADWAY DELINEATORS (MIARDS) | LF |
| 404.0972 | 9.5 F2 TOP COURSE HMA, 70 SERIES COMPACTION | TON | 685.1106--25 | WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 20 MIL | LF |
| 407.0103 | STRAIGHT TACK COAT | GAL | 685.1206--25 | YELLOW REFLECTORIZED PAVEMENT STRIPES - 20 MIL | LF |
| 418.7603 | ASPHALT PAVEMENT JOINT ADHESIVE | LF | | | |
| 490.30 | MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE | SY | | | |
| 606.2701 | HPBO (MOD.) CORRUGATED BEAM GUIDE RAILING | LF | | | |
| 606.2801 | HPBO (MOD.) CORRUGATED BEAM MEDIAN BARRIER | LF | | | |
| 608.020102 | VEGETATION CONTROL STRIP | TON | | | |

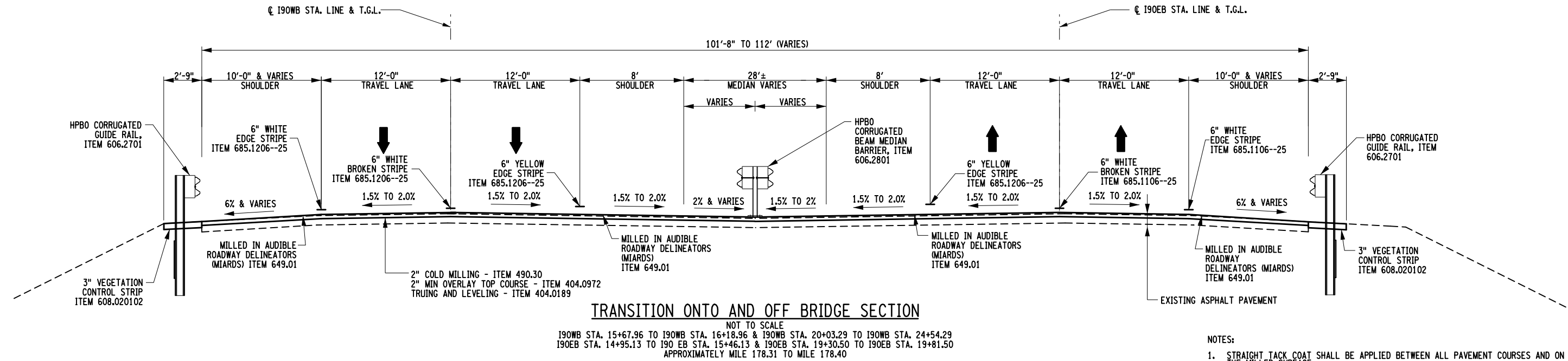
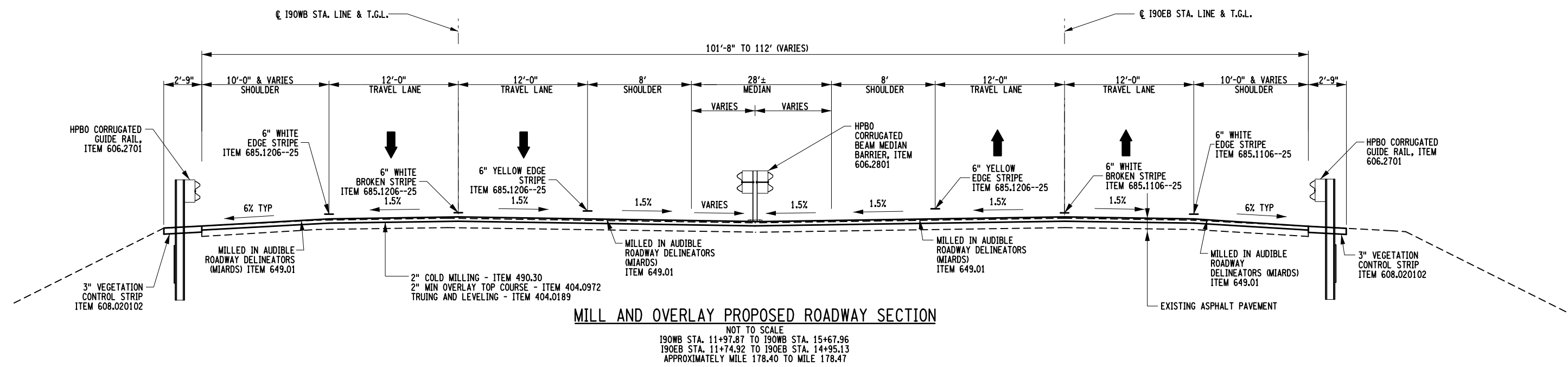
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| TITLE OF PROJECT REHABILITATION OF I-90 BRIDGE OVER NY 55 & ERIE CANAL TRAIL | CONTRACT NUMBER: TAA 25-06B |
| LOCATION OF PROJECT ALBANY DIVISION MP 178.38 | DATE: 12/11/2024 |
| TITLE OF DRAWING TYPICAL SECTION (SHEET 1 OF 3) | DRAWING NUMBER: TYP-01 |

PW/ CHECKED BY: D. LEVINE
 DRAFTED BY: J.P. O'LOUGHLIN
 CHECKED BY: D. LEVINE
 DESIGNED BY: J.P. O'LOUGHLIN
 DESIGN SUPERVISOR: D. LEVINE




- NOTES:
1. STRAIGHT TACK COAT SHALL BE APPLIED BETWEEN ALL PAVEMENT COURSES AND ON THE MILLED SURFACE.
 2. PAVEMENT JOINT ADHESIVE SHALL BE APPLIED AT ALL PAVEMENT JOINTS IN THE TOP COURSE.
 3. REFER TO GNP-01 TO GNP-03 FOR LOCATION OF GUIDE RAILS.
 4. SAFETY EDGE TAPER IS TO BE 35 DEGREES FROM FINISHED GRADE.
 5. SEE PLAN FOR SHOULDER WIDTH AND LANE TRANSITIONS.

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| ALTERED ON: | AFFKED ON: 01/16/2025 |
| SIGNATURE: STAMP: | SIGNATURE: DANIEL A. LEVINE STAMP:  |

| ITEM | DESCRIPTION | UNIT | ITEM | DESCRIPTION | UNIT |
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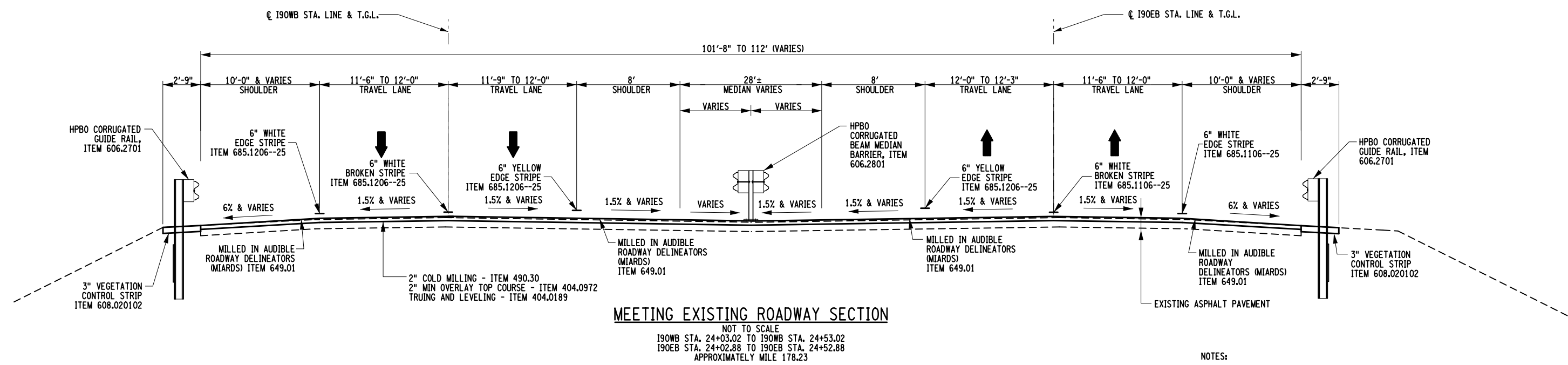
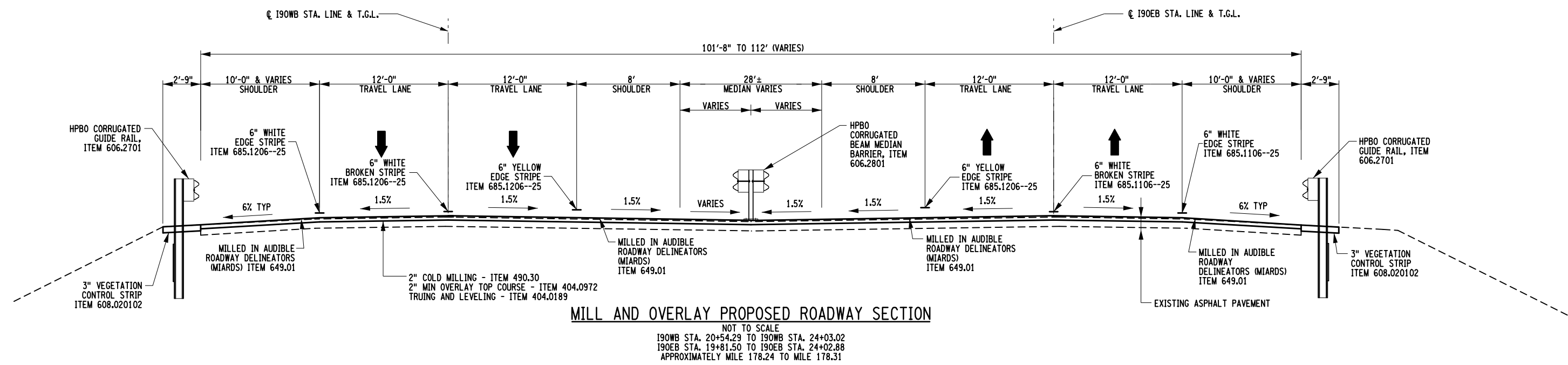
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| DATE | DESCRIPTION | BY | SYM. |
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| LOCATION OF PROJECT ALBANY DIVISION MP 178.38 | DATE: 12/11/2024 |
| TITLE OF DRAWING TYPICAL SECTION (SHEET 2 OF 3) | DRAWING NUMBER: TYP-02 |

PW/ CHECKED BY: D. LEVINE
 DRAFTED BY: J.P. O'LOUGHLIN
 CHECKED BY: D. LEVINE
 DESIGNED BY: J.P. O'LOUGHLIN
 DESIGN SUPERVISOR: D. LEVINE



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| 606.2801 | HPBO (MOD.) CORRUGATED BEAM MEDIAN BARRIER | LF | | | |
| 608.020102 | VEGETATION CONTROL STRIP | TON | | | |

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| DATE | DESCRIPTION | BY | SYM. |
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NEW YORK STATE Thruway Authority

PARSONS

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| TITLE OF PROJECT REHABILITATION OF I-90 BRIDGE OVER NY 55 & ERIE CANAL TRAIL | CONTRACT NUMBER: TAA 25-06B |
| LOCATION OF PROJECT ALBANY DIVISION MP 178.38 | DATE: 12/11/2024 |
| TITLE OF DRAWING TYPICAL SECTION (SHEET 3 OF 3) | DRAWING NUMBER: TYP-03 |

A. GENERAL NOTES

- ALL WORK ZONE TRAFFIC CONTROL SHALL CONFORM TO THE NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, THE NYS SUPPLEMENT AND THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION (NYS DOT) STANDARD SPECIFICATION SECTION 619, AND NYSTA SUPPLEMENT TO THE STANDARD SPECIFICATIONS, EXCEPT WHERE MODIFIED BY THE CONTRACT PLANS AND PROPOSAL.
- IF THE ENGINEER NOTIFIES THE CONTRACTOR OF ANY HAZARDOUS CONDITION OR PRACTICE, ALL OPERATIONS IN THAT AREA SHALL CEASE. IMMEDIATE REMEDIAL ACTION SHALL BE TAKEN TO THE SATISFACTION OF THE ENGINEER BEFORE WORK MAY BE RESUMED.
- MOBILE LANE CLOSURES MAY BE UTILIZED TO INSTALL TRAFFIC CONTROL DEVICES.
- A QUANTITY OF 6 PORTABLE VARIABLE MESSAGE SIGNS IN CONFORMANCE WITH ITEM 619.110513 HAVE BEEN INCLUDED IN THIS CONTRACT 2 ARE FOR USE ON THE MAINLINE, 2 ARE FOR USE ON NYS ROUTE 5S, AND 2 ARE FOR USE ON THE CANALWAY TRAIL. THE LOCATION OF EACH SIGN SHALL BE DETERMINED BY THE NEW YORK STATE THRUWAY AUTHORITY (NYSTA). PORTABLE VARIABLE MESSAGE SIGNS (PVMS) WILL BE OPERATED BY THE THRUWAY STATEWIDE OPERATIONS CENTER (TSOC). THE SOFTWARE CONTROL PACKAGE SHALL BE NTCIP PROTOCOL COMPLIANT COMMUNICATING THROUGH DAKTRONICS VANGUARD SOFTWARE, OR VER-MAC CENTRALO VERSION 3.0.2.1.
- THE CONTRACTOR SHALL MAINTAIN ACCESS FOR EMERGENCY VEHICLES THROUGHOUT THE PROJECT AREA AT ALL TIMES. ALL EXISTING HIGHWAY MEDIAN TURNAROUND AREAS OUTSIDE THE WORK ZONE TRAFFIC CONTROL LIMITS SHALL REMAIN OPEN FOR EMERGENCY VEHICLE USE.
- CLOSURES REQUIRING NIGHTTIME WORK SHALL BE CONDUCTED IN CONJUNCTION WITH THE REQUIREMENTS OF SECTION 619-3.19 OF THE NYS DOT STANDARD SPECIFICATIONS. COST TO BE INCLUDED IN ITEM 619.24.
- ALL EXISTING PAVEMENT MARKINGS ON THE THRUWAY MAINLINE THAT CONFLICT WITH WORK ZONE TRAFFIC CONTROL MARKINGS SHALL BE COVERED UNDER PAY ITEM 619.0803.

B. CHANGES TO WORK ZONE TRAFFIC CONTROL (WZTC) PLANS:

- LANE AND SHOULDER CLOSURES, AND WORK HOUR RESTRICTIONS SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. THE ENGINEER SHALL RETAIN THE RIGHT TO CHANGE LANE AND SHOULDER CLOSURES AND WORK HOUR RESTRICTIONS THROUGHOUT THE DURATION OF THE CONTRACT.
- THE CONTRACTOR MAY SUBMIT TO THE ENGINEER FOR REVIEW AND APPROVAL CHANGES TO THE WORK ZONE TRAFFIC CONTROL SCHEMES AND CONSTRUCTION SEQUENCES PRESENTED IN THESE PLANS.
- THE WORK ZONE TRAFFIC CONTROL SCHEMES INCLUDED IN THESE PLANS DESCRIBE RECOMMENDED METHODS AND NECESSARY CONTROL DEVICES. THE ENGINEER MAY ORDER ADDITIONAL METHODS, ADDITIONAL DEVICES, OR ANY COMBINATION THEREOF, TO BETTER MEET FIELD CONDITIONS.
- ANY PROPOSED TRAFFIC CONTROL SCHEME THAT DEVIATES FROM THE WORK ZONE TRAFFIC CONTROL DRAWINGS, INCLUDING THE RESTRICTIONS STATED THEREIN, WILL ONLY BE CONSIDERED AS PART OF A VALUE ENGINEERING PROPOSAL.

C. WZTC RESTRICTIONS:

- INTERSTATE TRAFFIC SHALL BE MAINTAINED ON A PAVED SURFACE AT ALL TIMES. THE MINIMUM LANE WIDTH SHALL BE 11FT.
- THERE SHALL BE NO WORK OPERATIONS ALLOWED BEFORE DAWN OR AFTER SUNSET WITHOUT AN APPROVED LIGHTING PLAN. THE LIGHTING PLAN SHALL BE APPROVED PRIOR TO START OF WORK BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE A LIGHTING PLAN IN CONFORMANCE WITH SPECIFICATION SECTION 619.5.19 NIGHTTIME OPERATIONS, AND SHALL BEAR ALL COSTS.
- LANE CLOSURES SHALL NOT BE ALLOWED WHEN EITHER VISIBILITY OR PAVEMENT CONDITION ARE JUDGED TO BE INADEQUATE BY THE ENGINEER.
- THE CONTRACTOR SHALL SCHEDULE AND PROGRESS THE CONTRACT WORK IN A MANNER THAT MINIMIZES THE DURATION OF LANE CLOSURES. LANE CLOSURES SHALL BE USED ONLY WHEN WORK IS ACTUALLY IN PROGRESS.
- SEE LANE CLOSURES CHARTS ON NYSTA STANDARD SHEETS TA 619-31 FOR LANE CLOSURE RESTRICTIONS.

D. WZTC COORDINATION:

- THE CONTRACTOR SHALL BE AWARE THAT THERE MAY BE OTHER CONTRACTS, MAINTENANCE OPERATIONS, OR BRIDGE INSPECTIONS IN PROGRESS IN THE WORK AREA. THE ENGINEER AND THE CONTRACTOR SHALL COMMUNICATE WITH, AND COORDINATE OPERATIONS WITH, THE OTHER OPERATIONS SO THAT NO CONFLICT IN WORK SCHEDULING OR LOCATION OCCURS. LANE CLOSURES SHALL BE REPORTED TO THE NYSTA, ONE WEEK IN ADVANCE OF THE CLOSURE.
- BEFORE ANY ROADWAY WIDTH RESTRICTIONS CAN BE MADE, THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH TWENTY-ONE (21) CALENDAR DAYS NOTICE. THE CONTRACTOR SHALL NOTIFY THE ENGINEER IN WRITING IMMEDIATELY UPON THE REMOVAL OF THE LANE WIDTH RESTRICTION. FAILURE OF THE CONTRACTOR TO PROVIDE THE NECESSARY NOTICE MAY RESULT IN DELAYS TO THE CONTRACTOR'S OPERATIONS.

E. WZTC SIGNING AND DEVICES:

- THE SIGN AND APPURTENANCE DISTANCES SHOWN ON THE WZTC PLANS ARE APPROXIMATE. THE DISTANCES MAY BE AMENDED BY THE ENGINEER TO BETTER MEET FIELD CONDITIONS.
- THE CONSTRUCTION AND REGULATORY SIGNS FOR A PARTICULAR WORK ZONE TRAFFIC CONTROL PHASE SHALL BE IN PLACE PRIOR TO THE START OF THAT PHASE. ALL CONSTRUCTION SIGNS SHALL EITHER BE REMOVED OR COVERED COMPLETELY WITH OPAQUE MATERIAL WHEN NOT REQUIRED.
- ALL SIGNS, INCLUDING GUIDE SIGNS, SHALL INDICATE THE ACTUAL CONDITIONS AT ALL TIMES. SIGNS SHALL BE COVERED, REPOSITIONED, OR CHANGED IMMEDIATELY AS DIRECTED BY THE ENGINEER. NO SIGN SHALL BE PLACED AT ANY LOCATION WHERE IT COULD BE OBTURED BY TEMPORARY OR PERMANENT OBJECTS. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01, WORK ZONE TRAFFIC CONTROL.
- THE BOTTOM OF TEMPORARY CONSTRUCTION SIGNS SHALL BE A MINIMUM OF 7 FT ABOVE THE EDGE OF THE ROADWAY GRADE. SIGNS SHALL BE LOCATED OFF THE EDGE OF SHOULDER.
- THE FLAGS SHALL BE INSTALLED ON SIGNS AT LOCATIONS IDENTIFIED ON THE PLANS. THE COST SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 WORK ZONE TRAFFIC CONTROL.
- AT NIGHT, FLASHING ARROW BOARDS SHALL NOT BE OPERATED AT SUCH BRIGHTNESS THAT SIGNS, DRUMS, IMPACT ATTENUATION DEVICES, OR OTHER TRAFFIC CONTROL DEVICES CANNOT EASILY BE SEEN BY APPROACHING MOTORISTS. TO ENSURE THAT FLASHING ARROW BOARDS ARE NOT TOO BRIGHT FOR NIGHT OPERATION, THE CONTRACTOR SHALL MAKE AN ON SITE INSPECTION OF EACH ARROW BOARD AT THE BEGINNING OF ITS FIRST NIGHT OF OPERATION. IF THIS INSPECTION FINDS THAT AN ARROW BOARD IS TOO BRIGHT, THE CONTRACTOR SHALL PROMPTLY REDUCE THE LAMP INTENSITY. IN THIS CONTEXT, "PROMPTLY", SHALL MEAN NO LATER THAN THE BEGINNING OF THE NEXT NIGHT OF THE ARROW BOARD'S OPERATION. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01, WORK ZONE TRAFFIC CONTROL. FAILURE TO COMPLY WITH THIS NOTE SHALL BE CONSIDERED UNSATISFACTORY WORK ZONE TRAFFIC CONTROL. PAYMENT DEDUCTIONS SHALL BE MADE IN CONFORMANCE WITH SECTION 619, WORK ZONE TRAFFIC CONTROL.
- IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 619 OF THE NYS DOT STANDARD SPECIFICATIONS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF ALL TEMPORARY TRAFFIC CONTROL DEVICES. THAT IS, THE CONTRACTOR SHALL ENSURE THAT ALL SUCH ITEMS AS SIGNS, CONES, FLASHERS, DRUMS, ETC. ARE IN PLACE AND IN GOOD CONDITION. THE ENGINEER SHALL BE THE SOLE JUDGE OF THE EFFECTIVENESS OF THE CONTRACTOR'S EFFORTS TOWARD THE MAINTENANCE AND PROTECTION OF TRAFFIC.
- WZTC SIGNS AND APPURTENANCES SHALL BE PLACED/STORED OFF THE EDGE OF SHOULDER. THE CONTRACTOR IS ENCOURAGED TO LEAVE SIGNS AND APPURTENANCES BEHIND APPROVED BARRIER IN THE AREA THAT THEY ARE NEEDED TO REDUCE CLOSURE SETUP TIME AND MAXIMIZE THE AMOUNT OF "WORK TIME" DURING EACH WORK DAY. WZTC SIGNS AND APPURTENANCES SHALL NOT BE STORED OFF THE EDGE OF THE SHOULDER FROM NOVEMBER 15 TO APRIL 1, THEY SHALL BE PICKED UP AT THE END OF EACH WORK DAY.

F. CONSTRUCTION VEHICLES:

- PARKING OF PERSONAL VEHICLES IS PROHIBITED WITHIN THE PROJECT LIMITS. VEHICLES BELONGING TO THE CONTRACTOR OR THE CONTRACTOR'S EMPLOYEES SHALL NOT BE PARKED WITHIN 30 FT OF THE EDGE OF PAVEMENT OF MAINLINE AND ROADS. ALL PARKING PLANS MUST BE APPROVED BY THE ENGINEER.
- DRIVING AGAINST TRAFFIC, REGARDLESS OF WHETHER OR NOT THE AREA HAS BEEN CLOSED TO TRAFFIC, SHALL NOT BE ALLOWED AT ANYTIME EXCEPT FOR TRAFFIC CONE PICKUP AND ON CLOSED RAMPS AND AS SPECIFICALLY PERMITTED BY THE ENGINEER.
- ESCORT VEHICLES EQUIPPED WITH AN APPROVED ROTATING AMBER WARNING LIGHT OR AN OPERATING ARROW BOARD SHALL BE REQUIRED WHEN TRANSPORTING SLOW MOVING CONSTRUCTION EQUIPMENT ALONG ANY PORTION OF THE ROADWAY THAT IS OPEN TO TRAFFIC UNLESS PROTECTED BY AN APPROVED PHYSICAL BARRIER.
- ALL VEHICLES THAT WILL MOVE INTO AND OUT OF TRAFFIC AT WORK AREAS SHALL BE EQUIPPED WITH AN APPROVED ROTATING AMBER WARNING LIGHT THAT SHALL BE MOUNTED SO AS TO BE EASILY SEEN BY APPROACHING TRAFFIC.

G. RESPONSIBILITY FOR EMERGENCY REPAIRS:

- THE CONTRACTOR SHALL SUBMIT, IN WRITING, TO THE ENGINEER IN CHARGE, THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF HIS/HER AND ANY SUB CONTRACTOR'S STAFF WHO ARE AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS.

H. ROADSIDE HAZARDS:

- IF THE SHOULDER TO BE USED FOR WORK ZONE TRAFFIC CONTROL SHOWS SIGNS OF DISTRESS, IT SHALL BE REPLACED PRIOR TO TRAFFIC BEING PLACED ON IT. THE SHOULDER SHALL BE CLOSED USING THE TYPICAL WORK ZONE TRAFFIC CONTROL SCHEMES FOR SHOULDER CLOSURES SHOWN IN THE NYSTA STANDARD SHEETS.
- EXCAVATIONS THAT PRODUCE A DROP OFF ON BOTH SIDES OF THE TRAVEL LANES SHALL NOT BE PERMITTED, UNLESS PROTECTED WITH CONCRETE BARRIER. SEE TABLE 619-3 IN SECTION 619-3.02K FOR PAVEMENT EDGE DROP-OFF PROTECTION.
- WHEN NOT IN USE, THE CONTRACTOR SHALL NOT STORE ANY CONSTRUCTION EQUIPMENT, SIGNS, TRAFFIC CONTROL DEVICES, MATERIALS OR ANY OTHER APPURTENANCES ALONG THE ROADWAY WITHIN THE CLEAR ZONE UNLESS PROTECTED BY AN APPROVED PHYSICAL BARRIER.
- EXISTING GUIDE RAIL SHALL REMAIN IN PLACE UNTIL CONSTRUCTION ACTIVITIES NECESSITATE REMOVAL. NEW GUIDE RAIL SHALL BE INSTALLED WITHIN 14 DAYS IN ACCORDANCE WITH SECTION 619-3.02E. DELINEATION AND DROP OFF PROTECTION SHALL BE AS SPECIFIED IN SECTION 619 WORK ZONE TRAFFIC CONTROL.

I. CHANGING WZTC SCHEMES:

- WHEN IT IS NECESSARY TO CHANGE THE LANES THAT ARE CLOSED TO TRAFFIC, THE CONTRACTOR SHALL BE REQUIRED TO FIRST REMOVE ALL DRUMS, CONES, SIGNS, ARROWS, AND THE LIKE SO THAT ALL LANES ARE OPEN BEFORE THE NEW SCHEME IS SET IN PLACE. THIS METHOD MAY BE MODIFIED BY THE ENGINEER, AS NECESSARY.
- WHEN REOPENING TRAVEL LANES TO TRAFFIC, THE CONTRACTOR SHALL START BY REMOVING THE CONES AT THE FAR END OF THE LANE CLOSURE AND WORK TOWARDS THE SIGNS AT THE BEGINNING OF THE LANE CLOSURE. THE SIGNS ARE NOT TO BE TAKEN DOWN UNTIL ALL TRAFFIC CONTROL DEVICES HAVE BEEN REMOVED.

J. BARRIER VEHICLE WITH MOUNTED ATTENUATORS

- BARRIER VEHICLES (BV) WITH MOUNTED ATTENUATORS ARE REQUIRED WHEN WORKERS ARE PRESENT IN A CLOSED TRAVEL LANE OR CLOSED SHOULDER. A BV SHALL BE USED AT EACH WORK LOCATION WITHIN THE CLOSURE. THE COST SHALL BE INCLUDED IN THE COST BID FOR ITEM 619.01.
- BARRIER VEHICLES WITH MOUNTED ATTENUATORS SHALL BE PLACED TO ACCOMMODATE ANTICIPATED ROLL-AHEAD DISTANCE (DISTANCE BETWEEN THE FRONT OF THE BV TRUCK AND THE FIRST WORKER OR VEHICLE TO BE PROTECTED). FOLLOW TRUCK MANUFACTURER'S INSTRUCTIONS.

K. FLASHING ARROW PANEL

- FLASHING ARROW PANELS SHALL COMPLY WITH SECTION 729-15 OF THE STANDARD SPECIFICATIONS.
- THE COST OF THE FLASHING ARROW PANEL SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01.

L. NIGHTTIME OPERATION

- LANE CLOSURES SHALL BE IN ACCORDANCE WITH THE SCHEDULE AND SUSPENSION OF WORK INCLUDED IN THE CONTRACT PROPOSAL.
- THE CONTRACTOR IS ENCOURAGED TO ESTABLISH PERMANENT LIGHTING SETUPS ALONG THE WORK ZONE TO MAXIMIZE THE AMOUNT OF "WORK TIME" EACH NIGHT. THE CONTRACTOR SHALL SUBMIT HIS LIGHTING PLAN 30 DAYS PRIOR TO THE START OF NIGHTTIME OPERATIONS.
- NIGHTTIME OPERATIONS SHALL COMPLY WITH SECTION 619-3.19 OF THE STANDARD SPECIFICATIONS.

M. SEQUENCE OF OPERATIONS: I-90 OVER NYS ROUTE 5S

STAGE 1
ESTABLISH STAGE 2 WB CROSSOVER AND STAGE 2 EB SHIFT ON MAINLINE.

STAGE 2
INSTALL PEDESTRIAN PROTECTION SYSTEM ON TRAIL.
INSTALL TPB AND SHOULDER CLOSURES ALONG NYS ROUTE 5S.
UTILIZE LANE CLOSURES ON NYS ROUTE 5S AS NEEDED FOR WORK UNDER BRIDGE.
UTILIZE TEMPORARY STOPPAGES ON NYS ROUTE 5S WHILE RAISING AND LOWERING STEEL.
REMOVE WB BRIDGE.
CONSTRUCT WB BRIDGE.

STAGE 3
PLACE WB TRAFFIC BACK ON NEW WB BRIDGE.
REMOVE STAGE 2 CROSSOVER.
WINTER SHUTDOWN IS ANTICIPATED AT THE CONCLUSION OF STAGE 2. UTILIZE INTERIM PHASE OPERATIONS SHOWN ON TC-6A THRU TC-6C DURING THE WINTER SHUTDOWN.
ESTABLISH STAGE 4 WZTC, EB CROSSOVER, WB SHIFT.

STAGE 4
UTILIZE LANE CLOSURES ON NYS ROUTE 5S AS NEEDED FOR WORK UNDER BRIDGE.
UTILIZE TEMPORARY STOPPAGES ON NYS ROUTE 5S WHILE RAISING AND LOWERING STEEL.
REMOVE EB BRIDGE.
CONSTRUCT EB BRIDGE.

STAGE 5
REMOVE PEDESTRIAN PROTECTION SYSTEM ON TRAIL.
REMOVE TPB AND SHOULDER CLOSURES ALONG NYS ROUTE 5S.
PLACE EB TRAFFIC BACK ON NEW EB BRIDGE.
REMOVE STAGE 4 CROSSOVER.

STAGE 6
UTILIZE DAILY LANE CLOSURES TO COMPLETE MILL AND INLAY OPERATIONS ON BOTH EB AND WB.

STAGE 7
COMPLETE ALL FINAL PAVEMENT MARKINGS ON MAINLINE AND NYS ROUTE 5S USING MOBILE OPERATIONS.

CHECKED BY: JMC

DRAFTED BY: RM

CHECKED BY: JMC

DESIGNED BY: GM

DESIGN SUPERVISOR: JMC

ALTERED ON: AFFIXED ON: 01/15/2025

SIGNATURE: JAMES M. CUMMINGS, PE
STAMP:



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HUNT ENGINEERS | ARCHITECTS | SURVEYORS
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TOWANDA, PA 570-265-4888 BINGHAMTON, NY 607-798-8081
ALBANY, NY 607-798-8981 WWW.HUNT-EAS.COM
NY CERTIFICATE NO. 0018220 PA CERTIFICATE NO. TSC220313464-1

TITLE OF PROJECT
REHABILITATION OF I-90 BRIDGE OVER
NY 5S & ERIE CANAL TRAIL

LOCATION OF PROJECT
ALBANY DIVISION
MP 178.38

TITLE OF DRAWING
TRAFFIC CONTROL NOTES

CONTRACT NUMBER:
TAA 25-06B

DATE:
12/6/2024

DRAWING NUMBER:
TCN-1

CHECKED BY: JMC

DRAFTED BY: RM

CHECKED BY: JMC

DESIGNED BY: GM

DESIGN SUPERVISOR: JMC

| TEXT NUMBER | TEXT | M.U.T.C.D. NUMBER | SIZE | COLOR | | REMARKS |
|-------------|------|-------------------|---------|------------|--------|---------|
| | | | | BACKGROUND | LEGEND | |
| 1 | | NYR9-12 | 48 X 72 | WHITE | BLACK | |
| 2 | | W20-1 | 48 X 48 | ORANGE | BLACK | |
| 3 | | R4-9 | 36 X 48 | WHITE | BLACK | |
| 4 | | W1-4bR | 48 X 48 | ORANGE | BLACK | |
| 5 | | W1-4bL | 48 X 48 | ORANGE | BLACK | |
| 6 | | W3-5 | 48 X 48 | ORANGE | BLACK | |
| 7 | | G20-5aP | 36 X 24 | ORANGE | BLACK | |
| 8 | | R2-1 | 36 X 48 | WHITE | BLACK | |

| TEXT NUMBER | TEXT | M.U.T.C.D. NUMBER | SIZE | COLOR | | REMARKS |
|-------------|------|-------------------|---------|------------|--------|---------|
| | | | | BACKGROUND | LEGEND | |
| 9 | | OM3-L | 12 X 36 | YELLOW | BLACK | |
| 10 | | OM3-R | 12 X 36 | YELLOW | BLACK | |
| 11 | | G20-2 | 36 X 18 | ORANGE | BLACK | |
| 12 | | R2-11 | 36 X 48 | WHITE | BLACK | |
| 13 | | R2-12 | 36 X 54 | WHITE | BLACK | |
| 14 | | W1-4bL MOD | 30 X 36 | ORANGE | BLACK | |
| 15 | | W1-4bR MOD | 30 X 36 | ORANGE | BLACK | |
| 16 | | W1-8R | 30 X 36 | YELLOW | BLACK | |
| 17 | | W20-1 | 48 X 48 | ORANGE | BLACK | |

ALTERED ON: _____ AFFIXED ON: 01/15/2025

SIGNATURE: _____ SIGNATURE: JAMES M. CUMMINGS, PE
 STAMP: _____ STAMP: _____

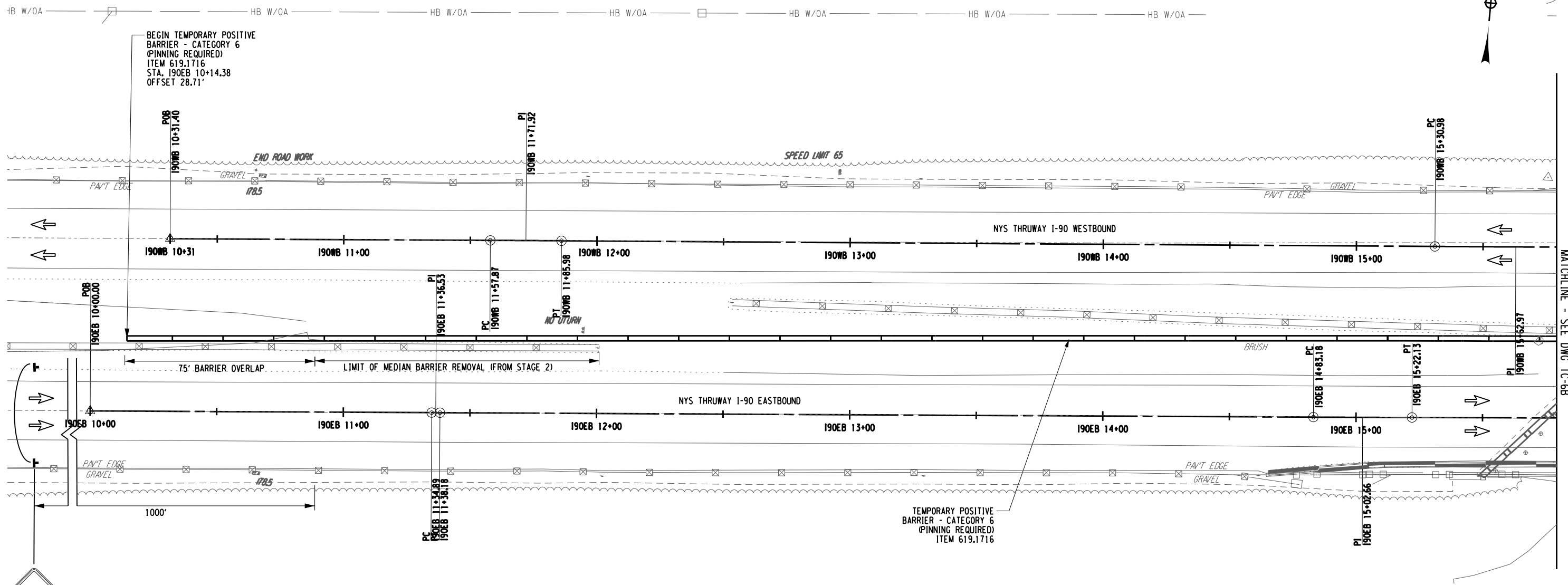


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TITLE OF PROJECT: REHABILITATION OF I-90 BRIDGE OVER NY 5S & ERIE CANAL TRAIL
 LOCATION OF PROJECT: ALBANY DIVISION MP 178.38
 TITLE OF DRAWING: WORK ZONE TRAFFIC CONTROL SIGN TABLE

CONTRACT NUMBER: TAA 25-06B
 DATE: 12/6/2024
 DRAWING NUMBER: WZSD-1



WORK ZONE TRAFFIC CONTROL PLAN - INTERIM PHASE

CHECKED BY: JMC
 DRAFTED BY: RM
 CHECKED BY: JMC
 DESIGNED BY: GM
 DESIGN SUPERVISOR: JMC

MATCHLINE - SEE DWG TC-6B

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| ALTERED ON: | AFFIXED ON: 01/15/2025 |
| SIGNATURE: STAMP: | SIGNATURE: JAMES M. CUMMINGS, PE STAMP: |

LEGEND

- TEMPORARY POSITIVE BARRIER (TPB)
- TEMPORARY SIGN, ITEM 619.01
- DIRECTION OF TRAFFIC

NOTES:
1. RESTORE ALL PAVEMENT MARKINGS COVERED IN STAGE 2.

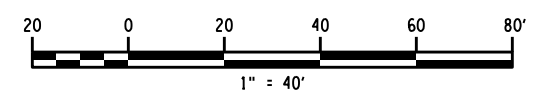
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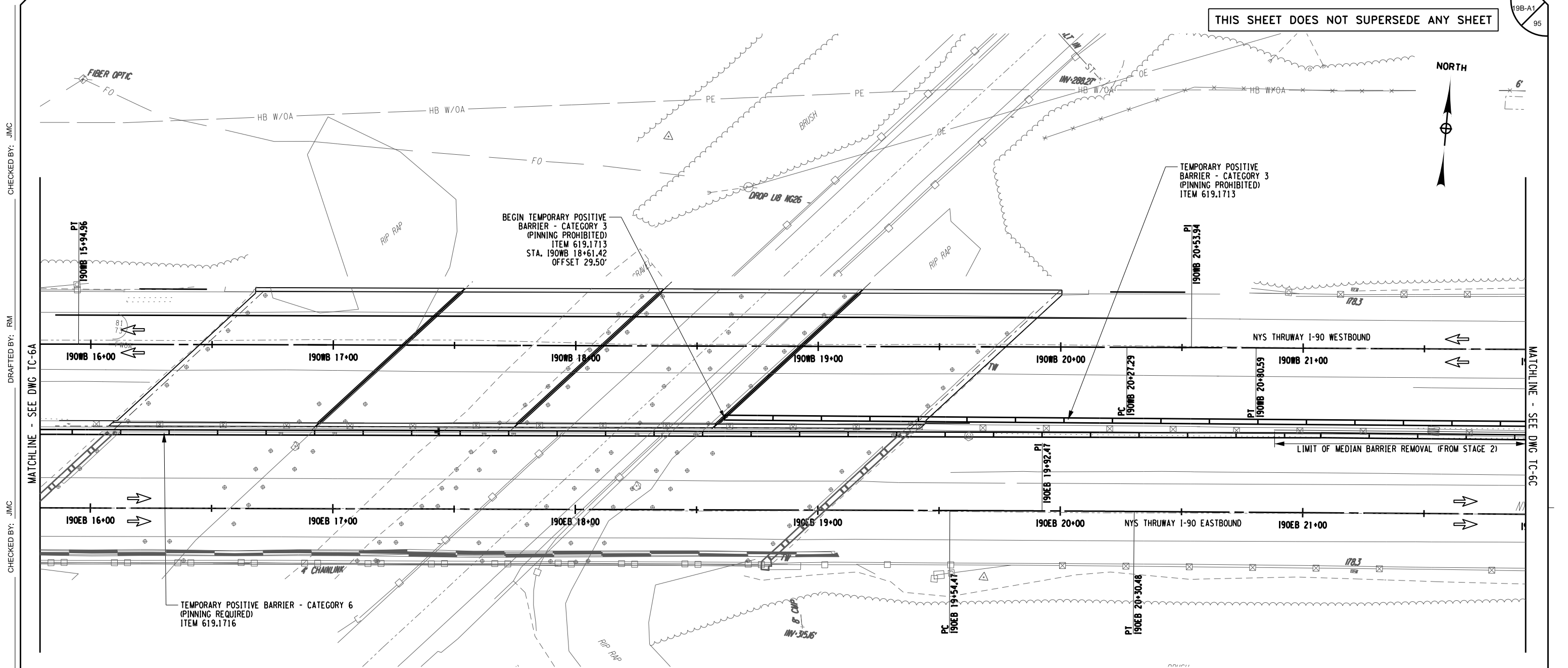
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HUNT ENGINEERS | ARCHITECTS | SURVEYORS
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 NY CERTIFICATE NO. 0018220 PA CERTIFICATE NO. TSC2203131464-1

TITLE OF PROJECT
REHABILITATION OF I-90 BRIDGE OVER
NY 5S & ERIE CANAL TRAIL
 LOCATION OF PROJECT
ALBANY DIVISION
MP 178.38
 TITLE OF DRAWING
WORK ZONE
TRAFFIC CONTROL
INTERIM PHASE


CONTRACT NUMBER:
TAA 25-06B
 DATE:
12/6/2024
 DRAWING NUMBER:
TC-6A





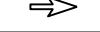


WORK ZONE TRAFFIC CONTROL PLAN - INTERIM PHASE

CHECKED BY: JMC
 DRAFTED BY: RM
 CHECKED BY: JMC
 DESIGNED BY: GM
 DESIGN SUPERVISOR: JMC

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| ALTERED ON: | AFFIXED ON: 01/15/2025 |
| SIGNATURE: STAMP: | SIGNATURE: JAMES M. CUMMINGS, PE STAMP:  |

LEGEND

-  TEMPORARY POSITIVE BARRIER (TPB)
-  TEMPORARY SIGN, ITEM 619.01
-  DIRECTION OF TRAFFIC

NOTES:
1. RESTORE ALL PAVEMENT MARKINGS COVERED IN STAGE 2.

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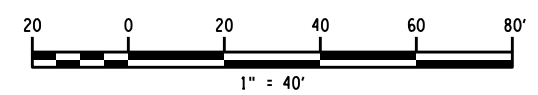
LOCATION OF PROJECT
ALBANY DIVISION
MP 178.38

TITLE OF DRAWING
WORK ZONE TRAFFIC CONTROL INTERIM PHASE

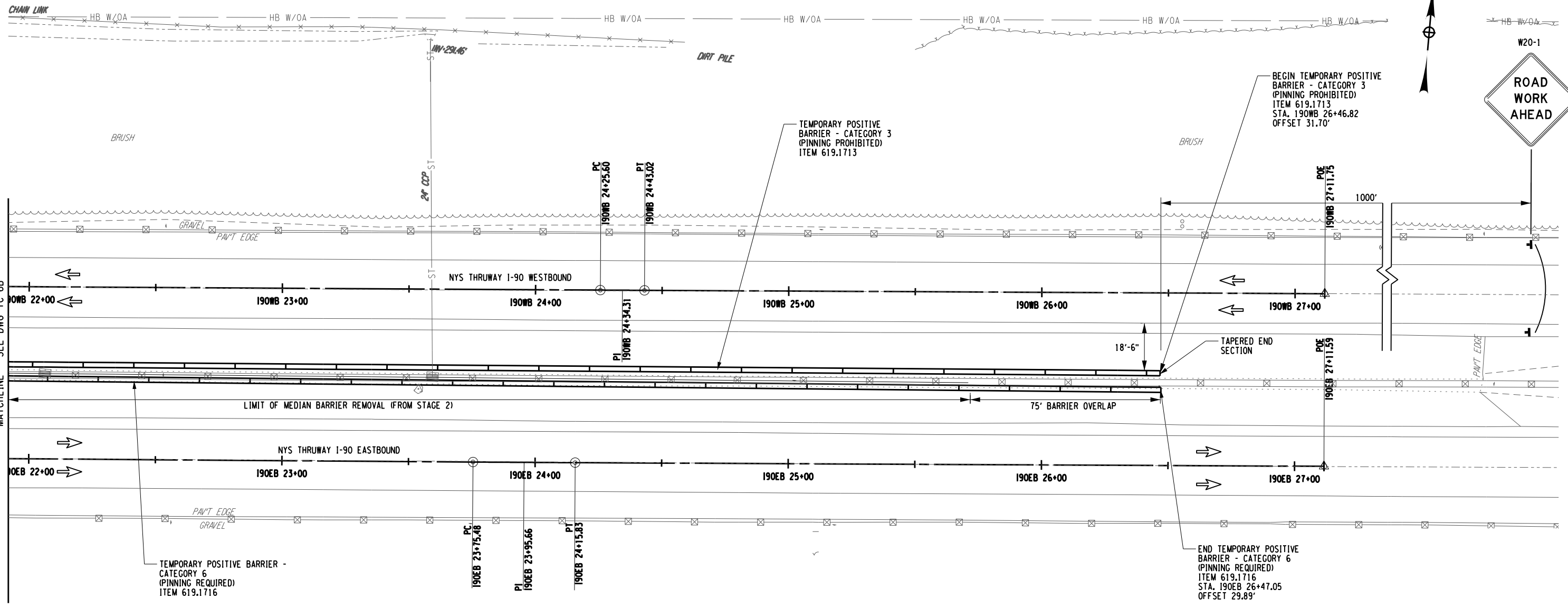
CONTRACT NUMBER:
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DATE:
12/6/2024

DRAWING NUMBER:
TC-6B




NORTH



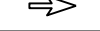


WORK ZONE TRAFFIC CONTROL PLAN - INTERIM PHASE

CHECKED BY: JMC
 DRAFTED BY: RM
 CHECKED BY: JMC
 DESIGNED BY: GM

| | |
|----------------------|---|
| ALTERED ON: | AFFIXED ON: 01/15/2025 |
| SIGNATURE: STAMP: | SIGNATURE: JAMES M. CUMMINGS, PE STAMP:  |

LEGEND

-  TEMPORARY POSITIVE BARRIER (TPB)
-  TEMPORARY SIGN, ITEM 619.01
-  DIRECTION OF TRAFFIC

NOTES:
1. RESTORE ALL PAVEMENT MARKINGS COVERED IN STAGE 2.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

| REVISIONS | | | |
|-----------|-------------|----|--------|
| DATE | DESCRIPTION | BY | SYMBOL |
| | | | |



HUNT ENGINEERS | ARCHITECTS | SURVEYORS
 HORSEHEADS, NY 607-358-1000 ROCHESTER, NY 585-327-7950
 TOWANDA, PA 570-265-4888 BINGHAMTON, NY 607-798-8081
 ALBANY, NY 607-798-3981 WWW.HUNT-EAS.COM
 NY CERTIFICATE NO. 0018220 PA CERTIFICATE NO. TSC2203131464-1

TITLE OF PROJECT
REHABILITATION OF I-90 BRIDGE OVER NY 55 & ERIE CANAL TRAIL

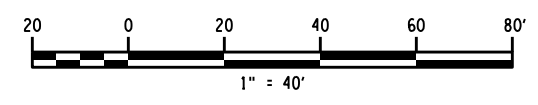
LOCATION OF PROJECT
ALBANY DIVISION MP 178.38

TITLE OF DRAWING
WORK ZONE TRAFFIC CONTROL INTERIM PHASE

CONTRACT NUMBER:
TAA 25-06B

DATE:
12/6/2024

DRAWING NUMBER:
TC-6C





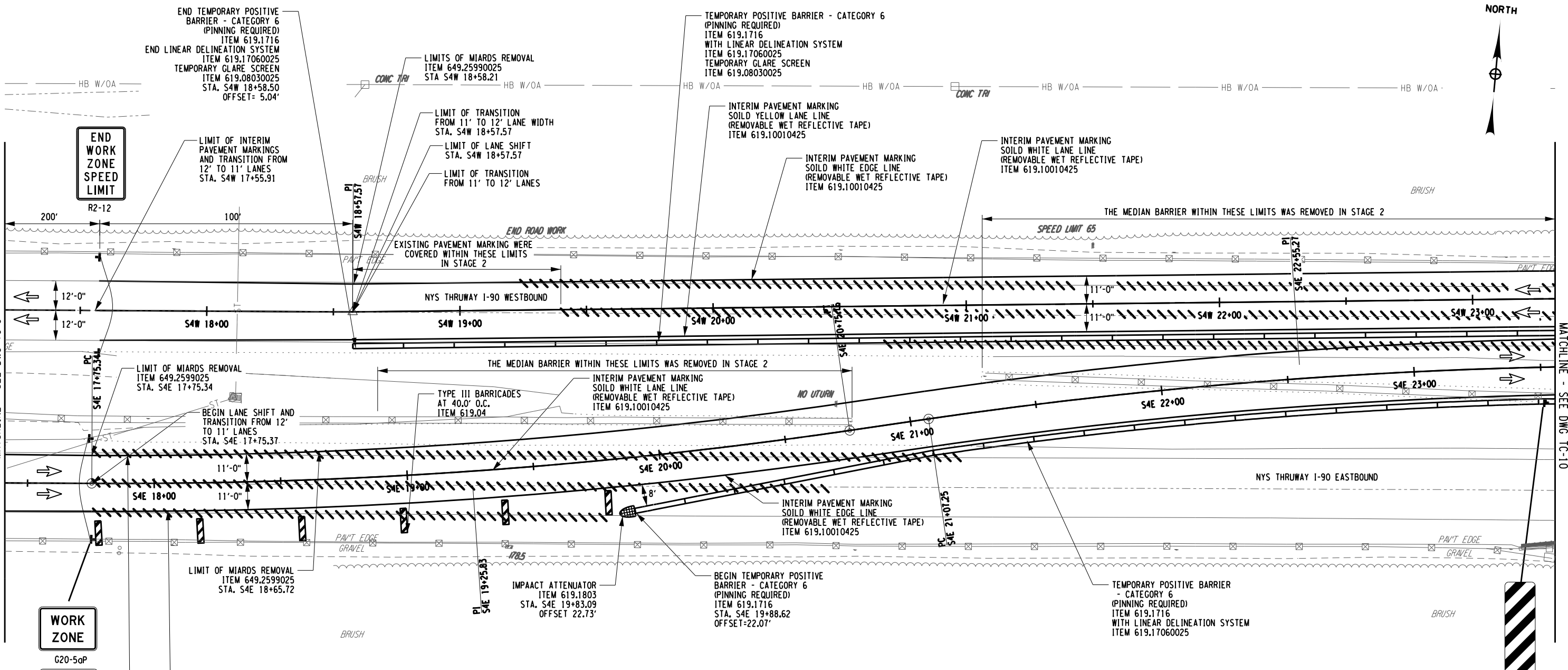
CHECKED BY: JMC

DRAFTED BY: RM

CHECKED BY: JMC

DESIGNED BY: GM

DESIGN SUPERVISOR: JMC



WORK ZONE TRAFFIC CONTROL PLAN - STAGE 4

END WORK ZONE SPEED LIMIT
R2-12

WORK ZONE
G20-50P

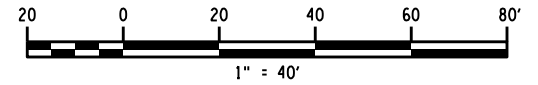
STATE SPEED LIMIT
R2-1
55

LEGEND

| | | | |
|--|----------------------------------|--|---|
| | CONSTRUCTION DRUM, ITEM 619.01 | | INTERIM PAVEMENT MARKING, ITEM 619.10010425 |
| | TEMPORARY POSITIVE BARRIER (TPB) | | STAGE 4 WORK SPACE |
| | DIRECTION OF TRAFFIC | | COVER EXISTING PAVEMENT MARKINGS, ITEM 619.0803 |
| | TEMPORARY SIGN, ITEM 619.01 | | TYPE III BARRICADES, ITEM 619.04 |
| | WARNING FLAGS (MIN. 18" X 18") | | |

NOTES:

1. WORK SPACE (MATERIAL STORAGE, EQUIPMENT, WORKERS, ETC.) SHALL NOT ENCROACH WITHIN THE DEFLECTION DISTANCE OF ADJACENT TPB AND/OR GUIDE RAILING.
2. MILLED-IN AUDIBLE ROADWAY DELINEATORS (MIARDS) REMOVED FOR WORK ZONE TRAFFIC CONTROL ACTIVITIES SHALL BE REESTABLISHED UNDER ITEM 649.01.
3. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL GUIDE RAIL MAINTENANCE AND SHALL RESPOND TO GUIDE RAIL INCIDENTS IMMEDIATELY..



ALTERED ON: _____ AFFIXED ON: 01/15/2025

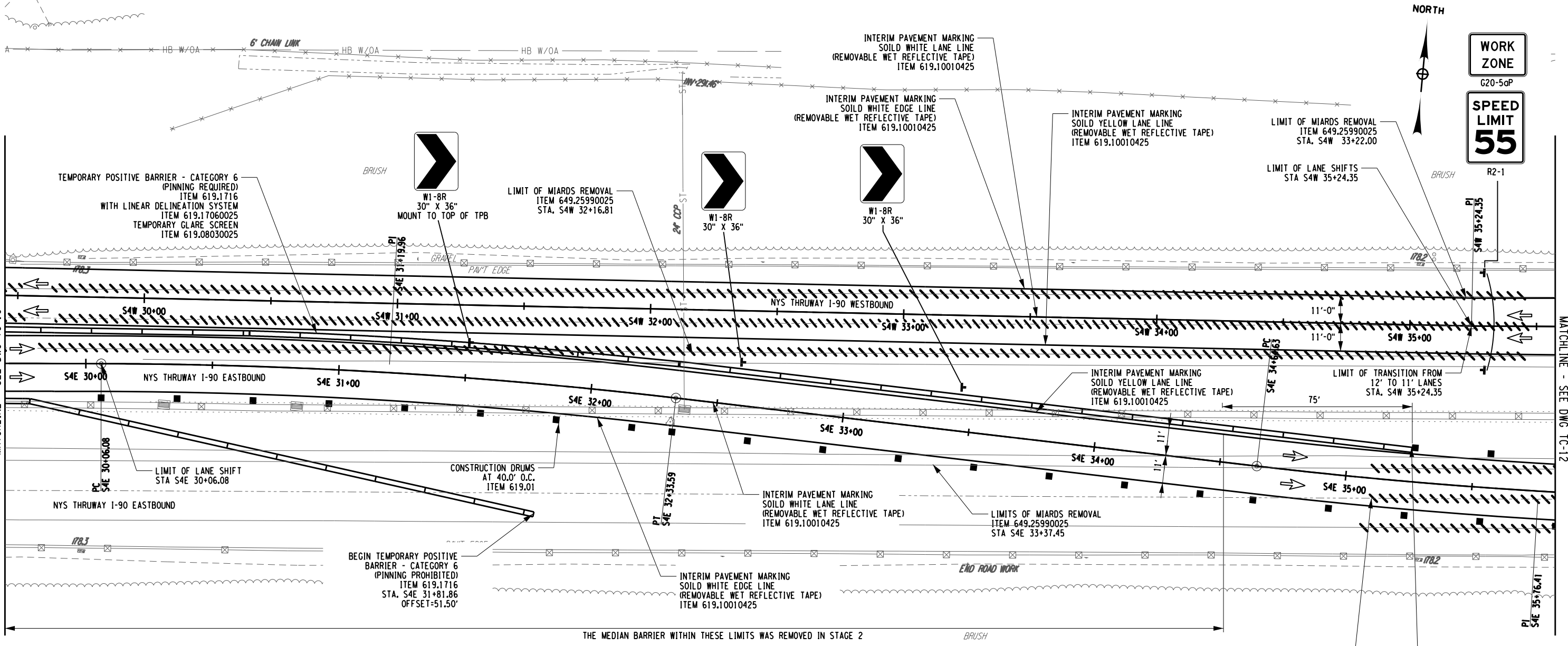
SIGNATURE: _____ STAMP: _____

SIGNATURE: JAMES M. CUMMINGS, PE STAMP: _____

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

| REVISIONS | | | |
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| DATE | DESCRIPTION | BY | SYM. |
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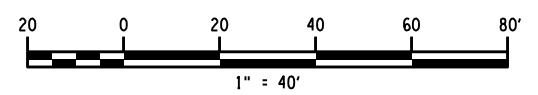
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| TITLE OF PROJECT REHABILITATION OF I-90 BRIDGE OVER NY 5S & ERIE CANAL TRAIL LOCATION OF PROJECT ALBANY DIVISION MP 178.38 TITLE OF DRAWING WORK ZONE TRAFFIC CONTROL STAGE 4 | CONTRACT NUMBER: TAA 25-06B DATE: 12/6/2024 DRAWING NUMBER: TC-9 |
|--|---|



WORK ZONE TRAFFIC CONTROL PLAN - STAGE 4

| LEGEND | | | |
|--------|----------------------------------|--|---|
| | CONSTRUCTION DRUM, ITEM 619.01 | | INTERIM PAVEMENT MARKING, ITEM 619.10010425 |
| | TEMPORARY POSITIVE BARRIER (TPB) | | STAGE 4 WORK SPACE |
| | DIRECTION OF TRAFFIC | | COVER EXISTING PAVEMENT MARKINGS, ITEM 619.0803 |
| | TEMPORARY SIGN, ITEM 619.01 | | TYPE III BARRICADES, ITEM 619.04 |
| | WARNING FLAGS (MIN. 18" X 18") | | |

- NOTES:
- WORK SPACE (MATERIAL STORAGE, EQUIPMENT, WORKERS, ETC.) SHALL NOT ENCRoACH WITHIN THE DEFLECTION DISTANCE OF ADJACENT TPB AND/OR GUIDE RAILING.
 - MILLED-IN AUDIBLE ROADWAY DELINEATORS (MIARDS) REMOVED FOR WORK ZONE TRAFFIC CONTROL ACTIVITIES SHALL BE REESTABLISHED UNDER ITEM 649.01.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR ALL GUIDE RAIL MAINTENANCE AND SHALL RESPOND TO GUIDE RAIL INCIDENTS IMMEDIATELY...



ALTERED ON: _____ AFFIXED ON: 01/15/2025

SIGNATURE: _____ STAMP: _____

SIGNATURE: JAMES M. CUMMINGS, PE STAMP: _____

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

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|-----------|-------------|----|------|
| DATE | DESCRIPTION | BY | SYM. |
| | | | |
| | | | |

HUNT ENGINEERS | ARCHITECTS | SURVEYORS
HORSEHEADS, NY 607-358-1000 ROCHESTER, NY 585-327-7950
TOWANDA, PA 570-265-4888 BINGHAMTON, NY 607-798-8081
ALBANY, NY 607-798-8981 WWW.HUNT-EAS.COM
NY CERTIFICATE NO. 0018220 PA CERTIFICATE NO. TSC2203131464-1

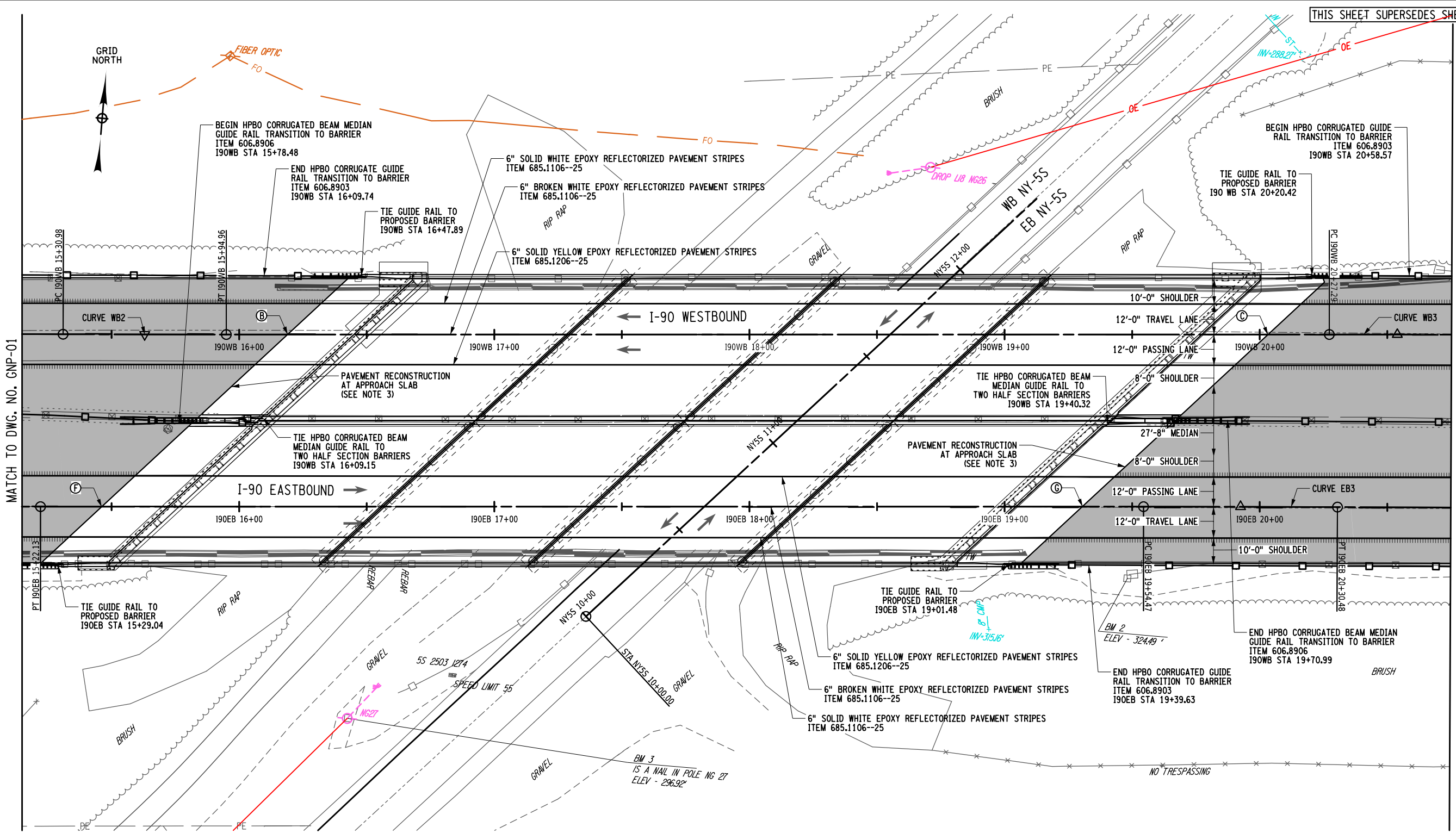
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|---|--------------------------------|
| TITLE OF PROJECT REHABILITATION OF I-90 BRIDGE OVER NY 55 & ERIE CANAL TRAIL | CONTRACT NUMBER: TAA 25-06B |
| LOCATION OF PROJECT ALBANY DIVISION MP 178.38 | DATE: 12/6/2024 |
| TITLE OF DRAWING WORK ZONE TRAFFIC CONTROL STAGE 4 | DRAWING NUMBER: TC-11 |

CHECKED BY: JMC
 DRAFTED BY: RM
 CHECKED BY: JMC
 DESIGNED BY: GM
 DESIGN SUPERVISOR: JMC

MATCHLINE - SEE DWG TC-12

PW/ CHECKED BY: D. LEVINE
 DRAFTED BY: J.P. O'LOUGHLIN
 CHECKED BY: D. LEVINE
 DESIGNED BY: J.P. O'LOUGHLIN
 DESIGN SUPERVISOR: D. LEVINE

THIS SHEET SUPERSEDES SHEET 32 32A1 95



MATCH TO DWG. NO. GNP-01

MATCH TO DWG. NO. GNP-03

ALTERED ON: _____ AFFIXED ON: 01/16/2025

SIGNATURE: _____ STAMP: _____

SIGNATURE: DANIEL A. LEVINE STAMP: _____

- NOTES:**
- SEE GNP-01 FOR CRITICAL STATION TABLE.
 - REMOVE AND REPLACE DELINEATORS IN KIND.
 ITEM 646.50: REMOVE AND DISPOSE DELINEATORS AND MARKERS.
 ITEM 646.0603--25: INSTALL DELINEATOR OR TENTH MILE MARKER ON POST.
 ITEM 646.0604--25: INSTALL DELINEATOR OR TENTH MILE MARKER, BACK-TO-BACK POST.
 - A SMALL AMOUNT OF PAVEMENT RECONSTRUCTION IS INCLUDED AT THE INTERFACE BETWEEN THE APPROACH SLAB AND ASPHALT TO ALLOW FOR INCREASED LIMITS OF EXCAVATION TO CONSTRUCT THE APPROACH SLAB. SEE DWG. NO. ST-42 FOR DETAILS.

LEGEND:

■ AREA OF MILL AND OVERLAY
 OVERLAY TOP COURSE, ITEM 404.0972
 COLD MILLING, ITEM 490.30

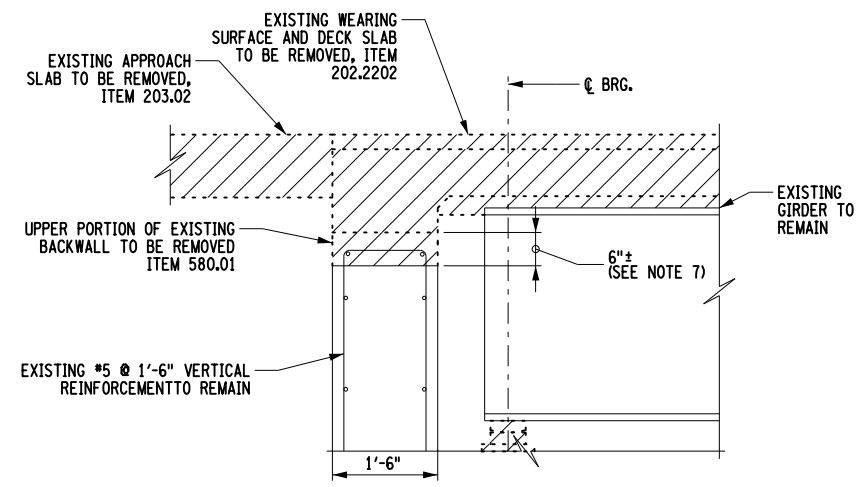


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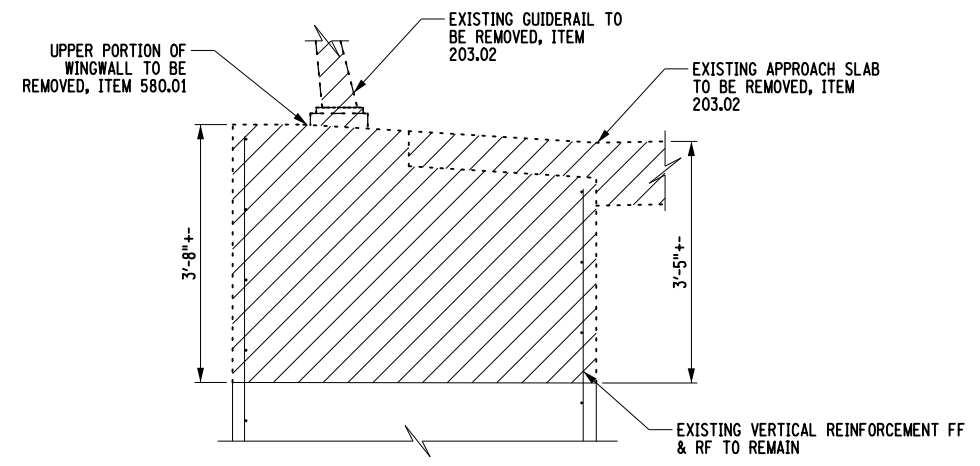
| REVISIONS | | | |
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| DATE | DESCRIPTION | BY | SYM. |
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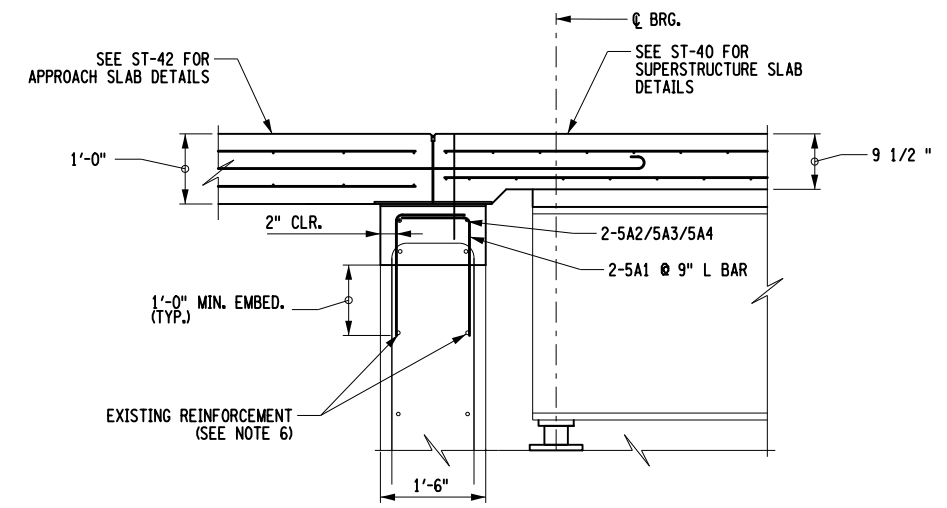
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|---|--------------------------------|
| TITLE OF PROJECT REHABILITATION OF I-90 BRIDGE OVER NY 5S & ERIE CANAL TRAIL | CONTRACT NUMBER: TAA 25-06B |
| LOCATION OF PROJECT ALBANY DIVISION MP 178.38 | DATE: 12/11/2024 |
| TITLE OF DRAWING GENERAL ROADWAY PLAN (SHEET 2 OF 3) | DRAWING NUMBER: GNP-02 |



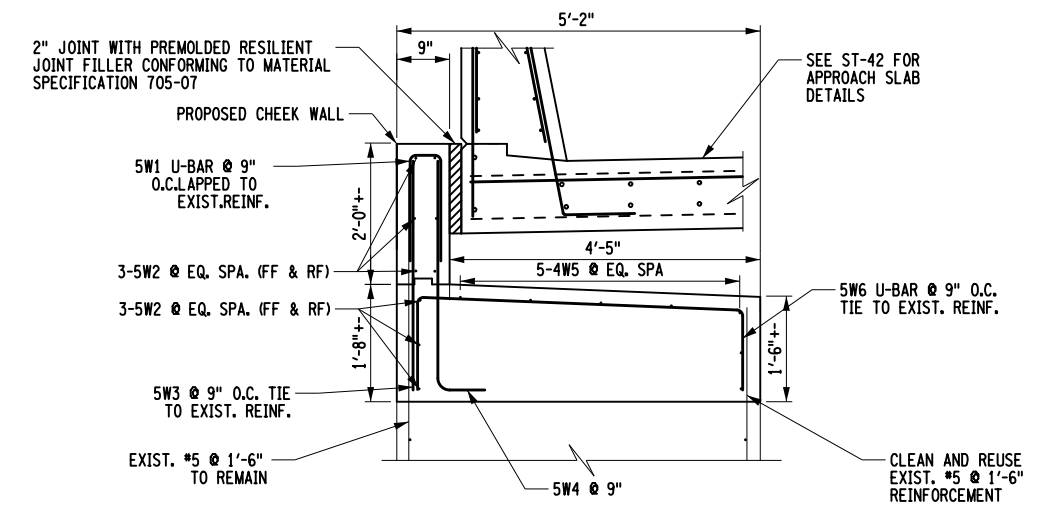
DETAIL "A" EXISTING ABUTMENT
SCALE: 3/8"=1'-0"



DETAIL "B" EXISTING WINGWALL
SCALE: 3/8"=1'-0"



DETAIL "A" PROPOSED ABUTMENT
SCALE: 3/8"=1'-0"




DETAIL "B" PROPOSED WINGWALL
SCALE: 3/8"=1'-0"

NOTES:


- COVER FOR NEW REINFORCEMENT IS 2" UNLESS OTHERWISE NOTED.
- EXISTING REINFORCEMENT EXPOSED BEFORE AND DURING THE CONCRETE REMOVAL SHALL BE CLEANED OF ALL LOOSE RUST, OILS AND FOREIGN MATERIAL IN ACCORDANCE WITH SECTION 584-3.02 OF THE NYS DOT STANDARD SPECIFICATIONS.
- DETERIORATED REINFORCEMENT WITH SECTION LOSS EXCEEDING 25% OF THE BAR'S ORIGINAL DIAMETER SHALL BE SUPPLEMENTED WITH NEW REINFORCEMENT OF THE SAME SIZE AND TYPE AS THE ORIGINAL.
- FOR WELDED SPLICE AND LAP SPLICE CRITERIA SEE DRAWING NO. ST-17. IF SUPPLEMENTAL BARS ARE CONNECTED VIA WELDED SPLICE, WELDERS SHALL BE QUALIFIED IN ACCORDANCE WITH THE "NEW YORK STATE STEEL CONSTRUCTION MANUAL".
- DRILLING AND GROUTING REINFORCING BARS SHALL BE PAID FOR UNDER ITEM 586.0201.
- LOCATE EXISTING LONGITUDINAL BACKWALL REINFORCEMENT USING NON DESTRUCTIVE INVESTIGATIVE METHODS PRIOR TO DRILLING AND GROUTING NEW REINFORCEMENT. NOTIFY THE ENGINEER IF EXISTING REINFORCEMENT WILL CONFLICT WITH NEW BAR.
- REMOVE EXISTING BACKWALL TO A MINIMUM OF 1 1/2" BEYOND THE EXISTING REINFORCEMENT, PAID FOR UNDER ITEM 580.01.

PW/ CHECKED BY: D. LEVINE
 DRAFTED BY: J.P. O'LOUGHLIN
 CHECKED BY: D. LEVINE
 DESIGNED BY: J.P. O'LOUGHLIN
 DESIGN SUPERVISOR: D. LEVINE

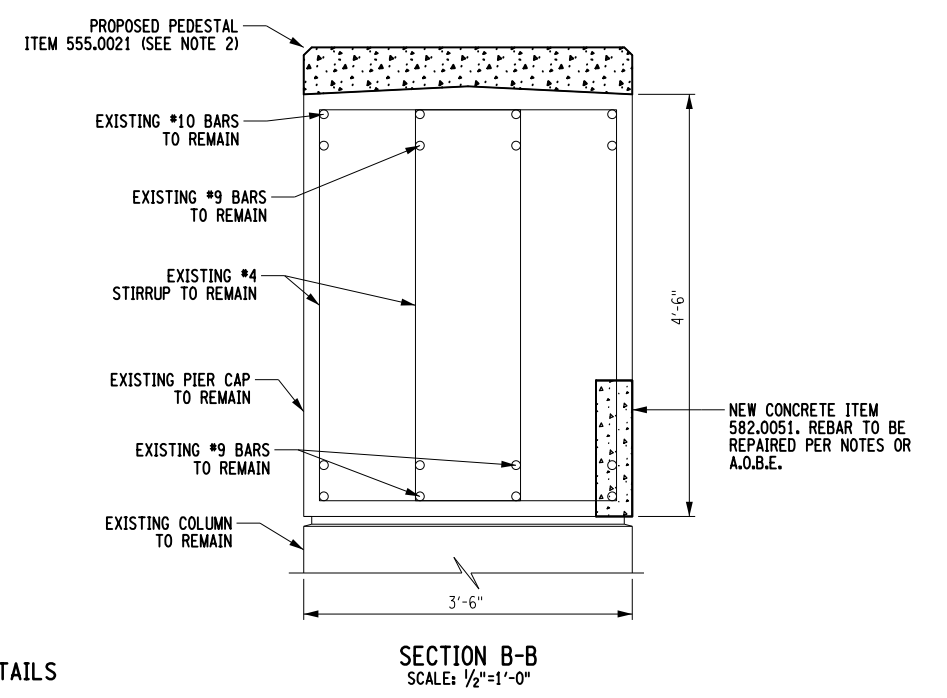
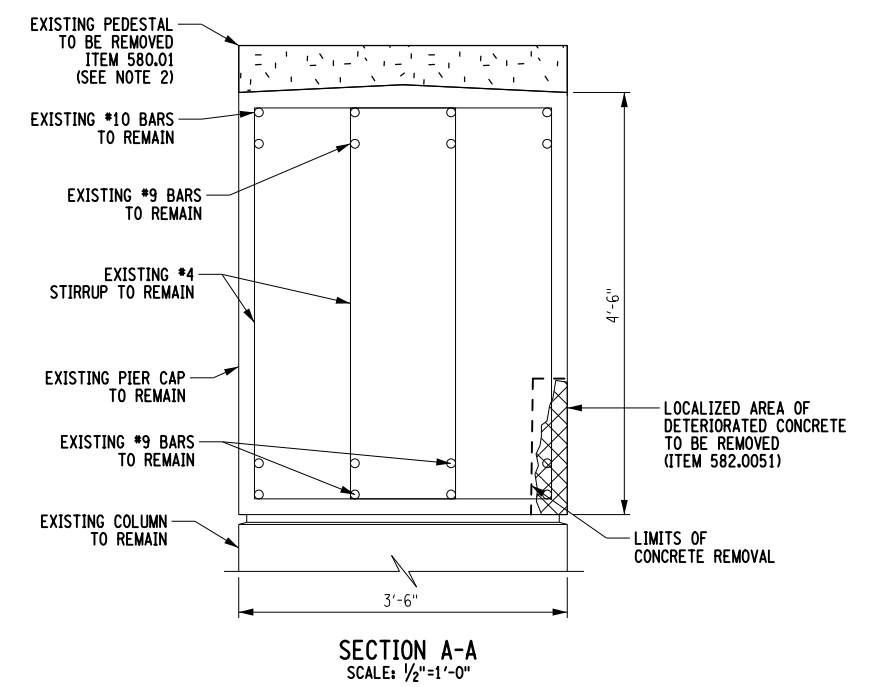
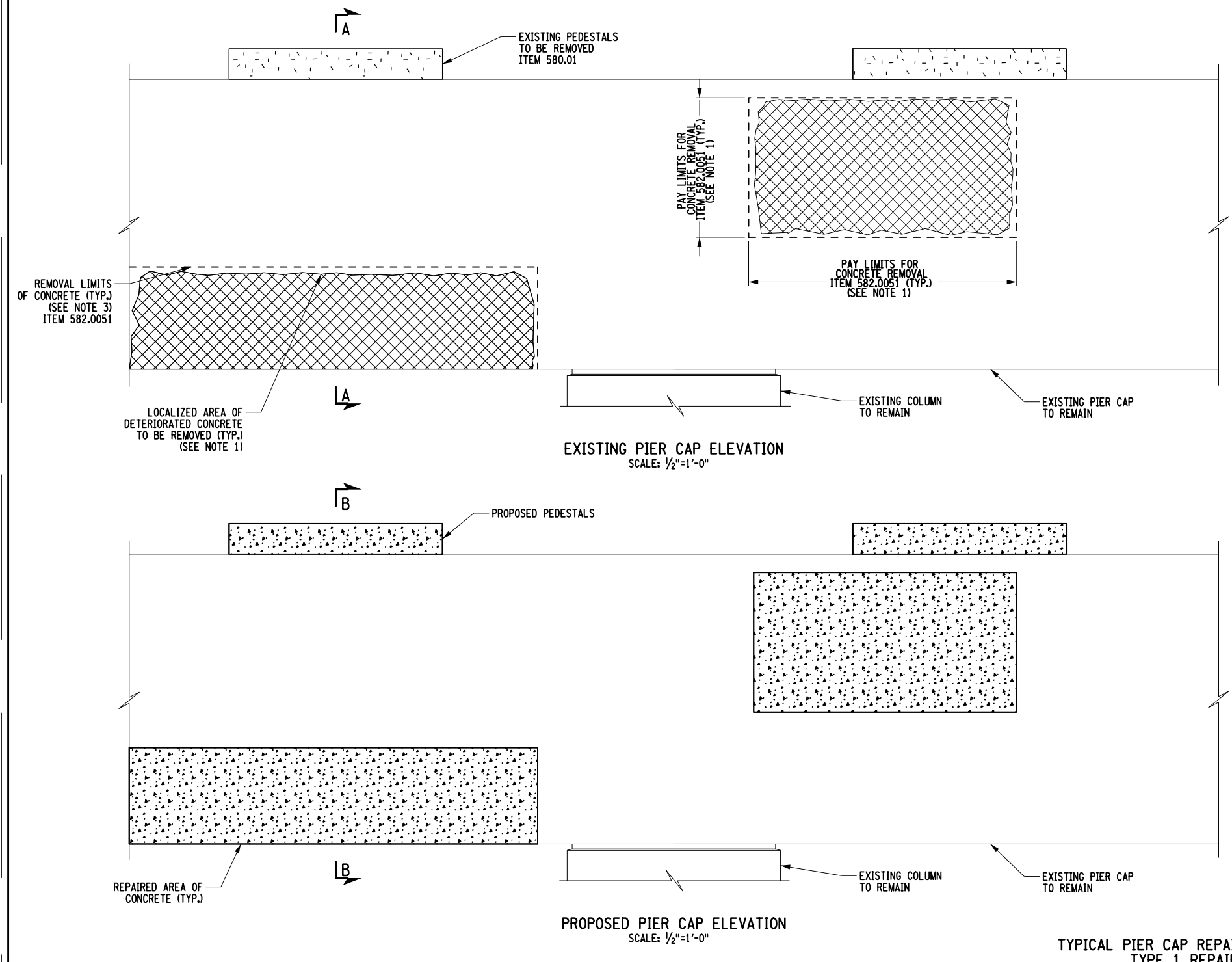
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| ALTERED ON: _____ SIGNATURE: _____ STAMP: _____ | AFFIXED ON: 01/16/2025 SIGNATURE: DANIEL A. LEVINE STAMP: _____  |
|---|--|

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.


| REVISIONS | | | |
|-----------|-------------|----|------|
| DATE | DESCRIPTION | BY | SYM. |
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|---|---|--------------------------------|
|  PARSONS | TITLE OF PROJECT REHABILITATION OF I-90 BRIDGE OVER NY 5S & ERIE CANAL TRAIL | CONTRACT NUMBER: TAA 25-06B |
| | LOCATION OF PROJECT ALBANY DIVISION MP 178.38 | DATE: 12/11/2024 |
| TITLE OF DRAWING PROPOSED ABUTMENT DETAILS (SHEET 1 OF 2) | | DRAWING NUMBER: ST-9 |

PW/ CHECKED BY: D. LEVINE
 DRAFTED BY: J.P. O'LOUGHLIN
 CHECKED BY: D. LEVINE
 DESIGNED BY: J.P. O'LOUGHLIN
 DESIGN SUPERVISOR: D. LEVINE



TYPICAL PIER CAP REPAIR DETAILS
TYPE 1 REPAIR

| | |
|---|--|
| ALTERED ON: SIGNATURE: STAMP: | AFFIKED ON: 01/03/2025 SIGNATURE: DANIEL A. LEVINE STAMP:  |
|---|--|

- LEGEND**
- EXISTING PEDESTALS TO BE REMOVED (ITEM 580.01) AND RECONSTRUCTED (ITEM 555.0201)
 - APPROXIMATE AREA OF DETERIORATED CONCRETE TO BE REMOVED AND REPAIRED (ITEM 582.0051)
 - NEW CONCRETE

- NOTES:**
1. FOR REPAIR LOCATIONS SEE DWG. NOS. ST-11 THRU ST-13. AREAS INDICATED FOR REPAIR INCLUDE CRACKS, DELAMINATED AREAS, AND SPALLS.
 2. FOR PEDESTAL DETAILS AND NOTES, SEE DWG. NOS. ST-18 AND ST-21.
 3. FOR ADDITIONAL NOTES, SEE DWG. NO. ST-14.

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| DATE | DESCRIPTION | BY | SYM. |
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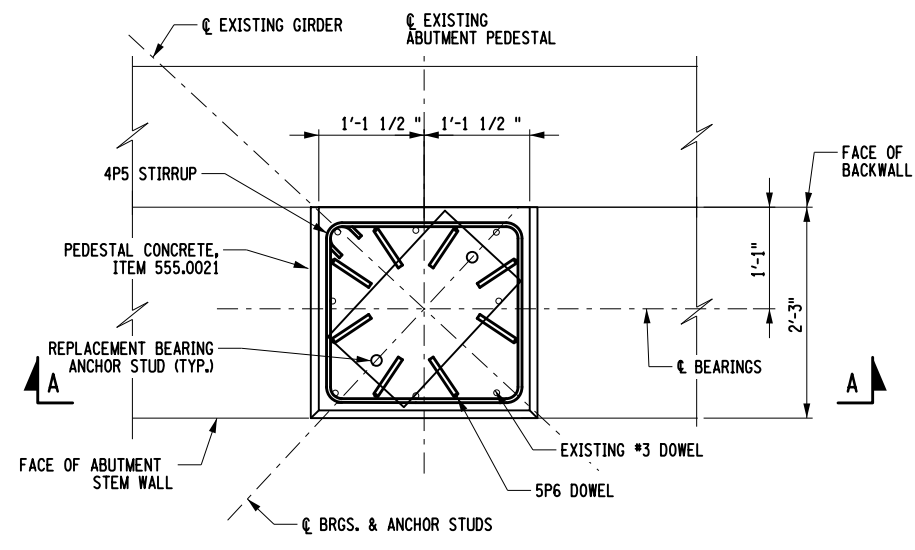


PARSONS

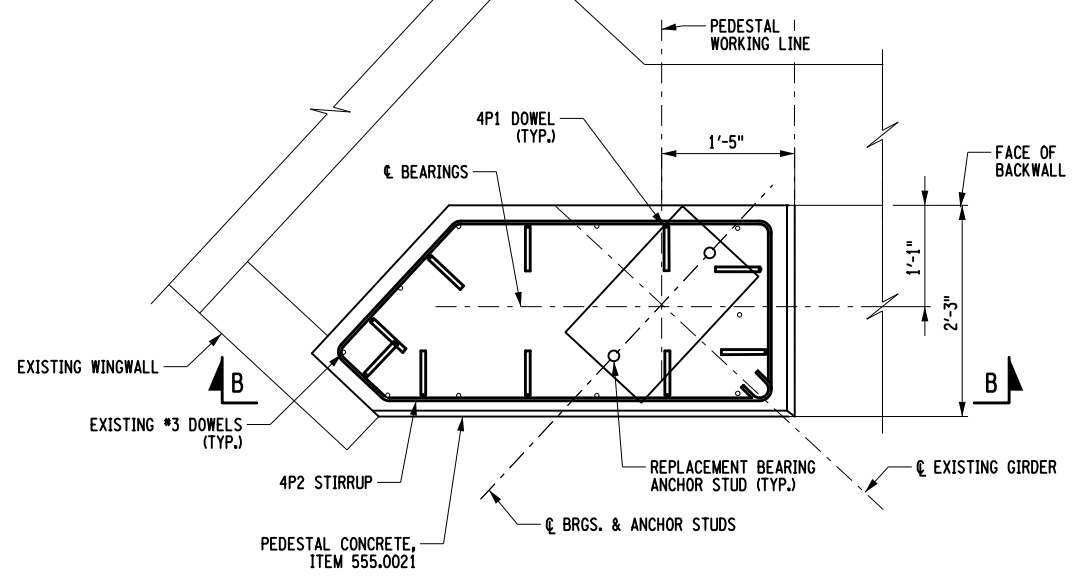
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| TITLE OF PROJECT REHABILITATION OF I-90 BRIDGE OVER NY 55 & ERIE CANAL TRAIL | CONTRACT NUMBER: TAA 25-06B |
| LOCATION OF PROJECT ALBANY DIVISION MP 178.38 | DATE: 12/11/2024 |
| TITLE OF DRAWING PIER REPAIR DETAILS (SHEET 3 OF 4) | DRAWING NUMBER: ST-16 |

PW/ CHECKED BY: D. LEVINE
 DRAFTED BY: J.P. O'LOUGHLIN
 CHECKED BY: D. LEVINE
 DESIGNED BY: J.P. O'LOUGHLIN
 DESIGN SUPERVISOR: D. LEVINE

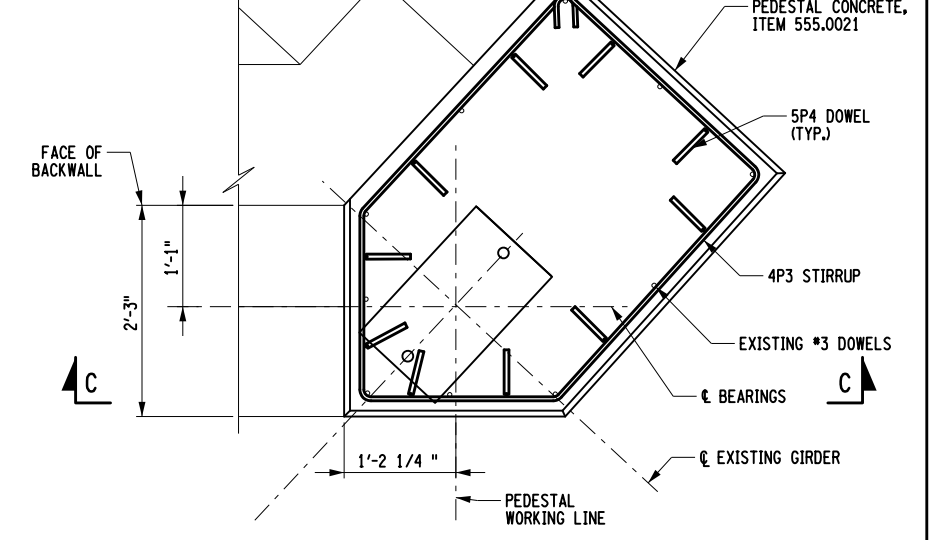
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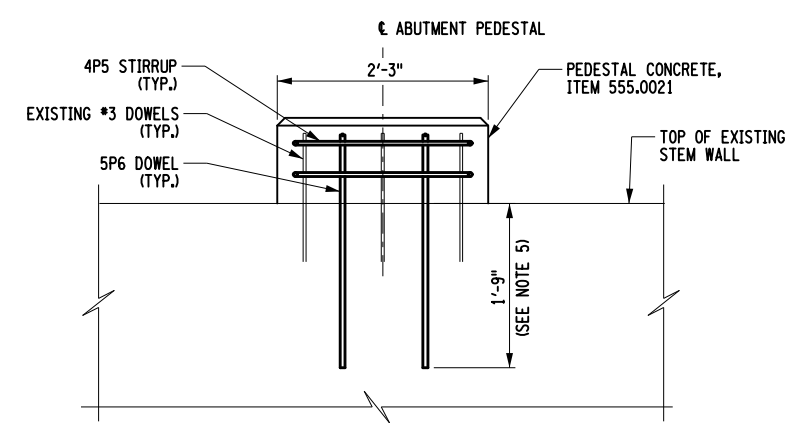
DETAIL 1:
TYPICAL PEDESTAL RECONSTRUCTION PLAN
SCALE: 1/2" = 1'-0"



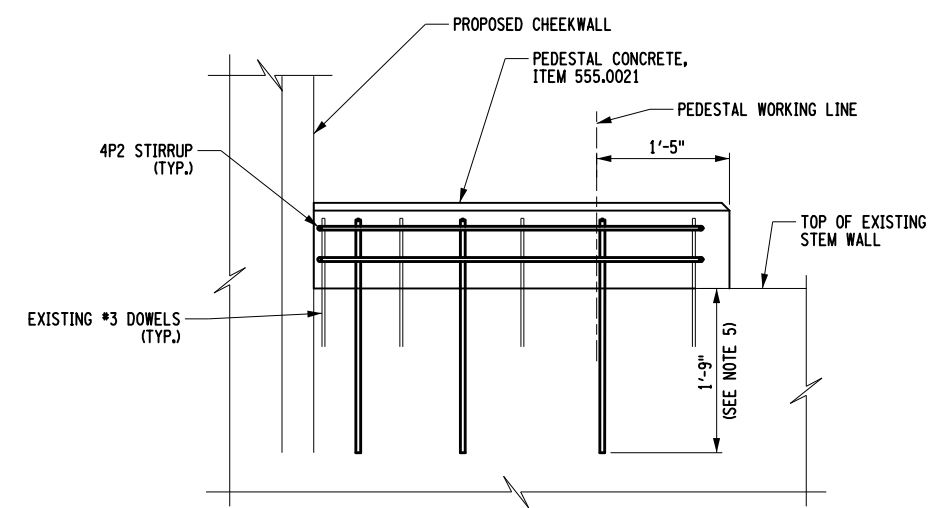
DETAIL 2:
END PEDESTAL RECONSTRUCTION PLAN
SCALE: 1/2" = 1'-0"



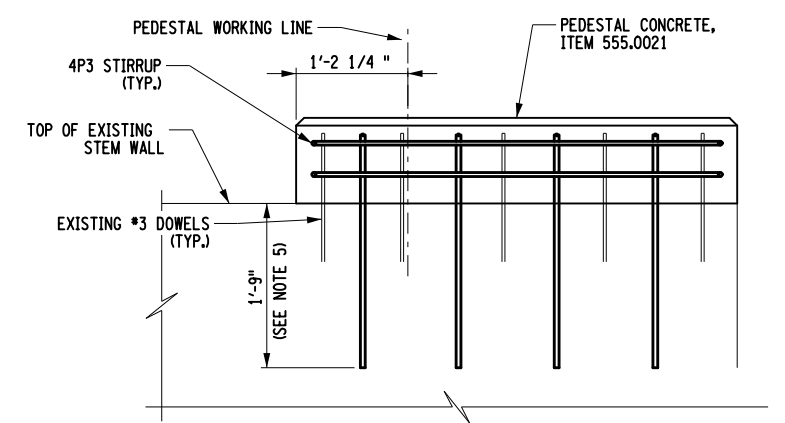
DETAIL 3:
END PEDESTAL RECONSTRUCTION PLAN
SCALE: 1/2" = 1'-0"



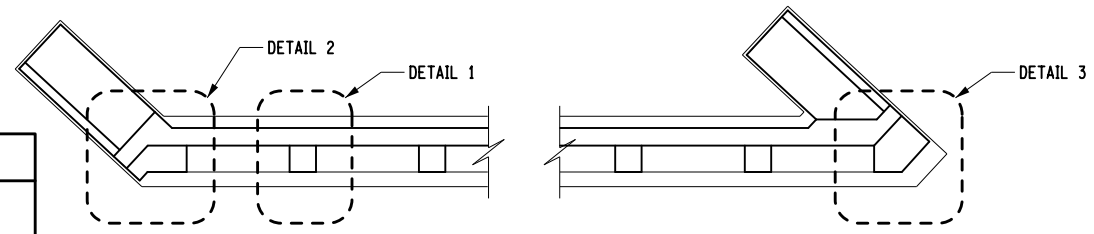
SECTION A-A
SCALE: 1/2" = 1'-0"



SECTION B-B
SCALE: 1/2" = 1'-0"



SECTION C-C
SCALE: 1/2" = 1'-0"



ABUTMENT PEDESTAL RECONSTRUCTION KEY PLAN

NOTES:

1. PEDESTAL RECONSTRUCTION SHALL BE COORDINATED WITH ABUTMENT BACKWALL RECONSTRUCTION AND WINGWALL RECONSTRUCTION.
2. SEE DWG. ST-18 FOR ADDITIONAL NOTES.
3. SEE DWG. ST-21 FOR HOOP REINFORCEMENT TABLE.
4. PEDESTAL REINFORCEMENT SHALL BE UNCOATED REINFORCEMENT PAID FOR UNDER ITEM 556.0201.
5. THE EMBEDMENT DEPTH SHOWN FOR DRILLING AND GROUTING IS FOR ESTIMATING PURPOSES ONLY. DRILLING AND GROUTING PEDESTAL DOWELS SHALL BE PAID FOR UNDER ITEM 586.0201. THE CONTRACTOR'S ENGINEER SHALL DETERMINE THE DEVELOPMENT LENGTH REQUIRED TO DEVELOP THE FULL STRENGTH OF THE REINFORCING BAR. THE CALCULATIONS SHALL BE BASED ON THE SIZE OF THE BAR, ACTUAL EDGE DISTANCE TO THE BAR, THE PROXIMITY TO OTHER BARS, ESTIMATED CONCRETE STRENGTH, AND THE GROUT SUPPLIERS RECOMMENDATION. THE CONTRACTOR SHALL SUBMIT DESIGN CALCULATIONS AND DETAILS SEALED BY A REGISTERED NEW ORK STATE PROFESSIONAL ENGINEER TOT HE ENGINEER FOR APPROVAL.

| | |
|----------------------|---------------------------------------|
| ALTERED ON: | AFFIXED ON: 01/16/2025 |
| SIGNATURE: STAMP: | SIGNATURE: DANIEL A. LEVINE STAMP: |

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| REVISIONS | | | |
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| DATE | DESCRIPTION | BY | SYM. |
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| | | | |

PARSONS

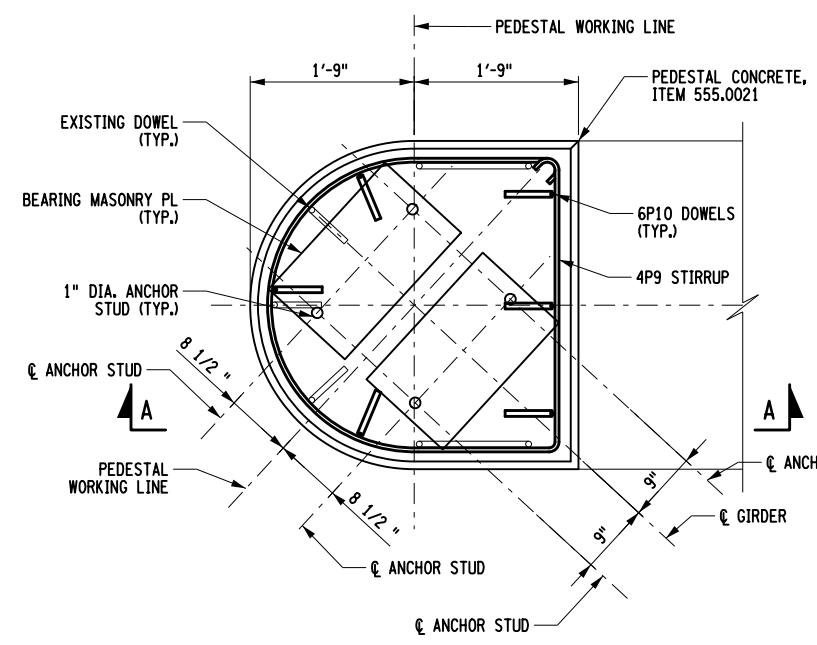
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|---|--------------------------------|
| TITLE OF PROJECT REHABILITATION OF I-90 BRIDGE OVER NY 55 & ERIE CANAL TRAIL | CONTRACT NUMBER: TAA 25-06B |
| LOCATION OF PROJECT ALBANY DIVISION MP 178.38 | DATE: 12/11/2024 |
| TITLE OF DRAWING PEDESTAL DETAILS (SHEET 2 OF 4) | DRAWING NUMBER: ST-19 |

PW/ CHECKED BY: D. LEVINE
 DRAFTED BY: J.P. O'LOUGHLIN
 CHECKED BY: D. LEVINE
 DESIGNED BY: J.P. O'LOUGHLIN
 DESIGN SUPERVISOR: D. LEVINE

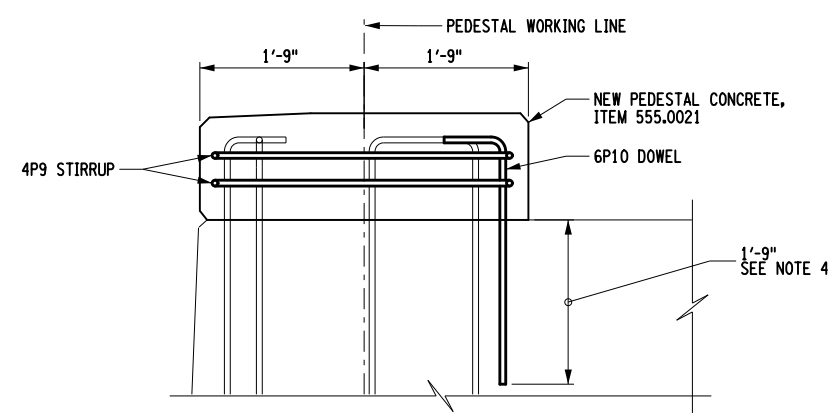
THIS SHEET SUPERSEDES SHEET 61 61A1 95

| | PIER 1 | PIER 2 | PIER 3 |
|-----|--------|--------|--------|
| G1 | 321.11 | 320.81 | 320.55 |
| G2 | 321.28 | 320.99 | 320.73 |
| G3 | 321.45 | 321.17 | 320.90 |
| G4 | 321.56 | 321.28 | 321.02 |
| G5 | 321.43 | 321.16 | 320.89 |
| G6 | 321.29 | 321.03 | 320.77 |
| G7 | 321.15 | 320.91 | 320.65 |
| G8 | 321.01 | 320.79 | 320.52 |
| G9 | 321.07 | 320.77 | 320.48 |
| G10 | 321.23 | 320.94 | 320.66 |
| G11 | 321.39 | 321.12 | 320.84 |
| G12 | 321.55 | 321.30 | 321.02 |
| G13 | 321.70 | 321.48 | 321.19 |
| G14 | 321.61 | 321.41 | 321.13 |
| G15 | 321.45 | 321.29 | 321.01 |
| G16 | 321.30 | 321.17 | 320.89 |

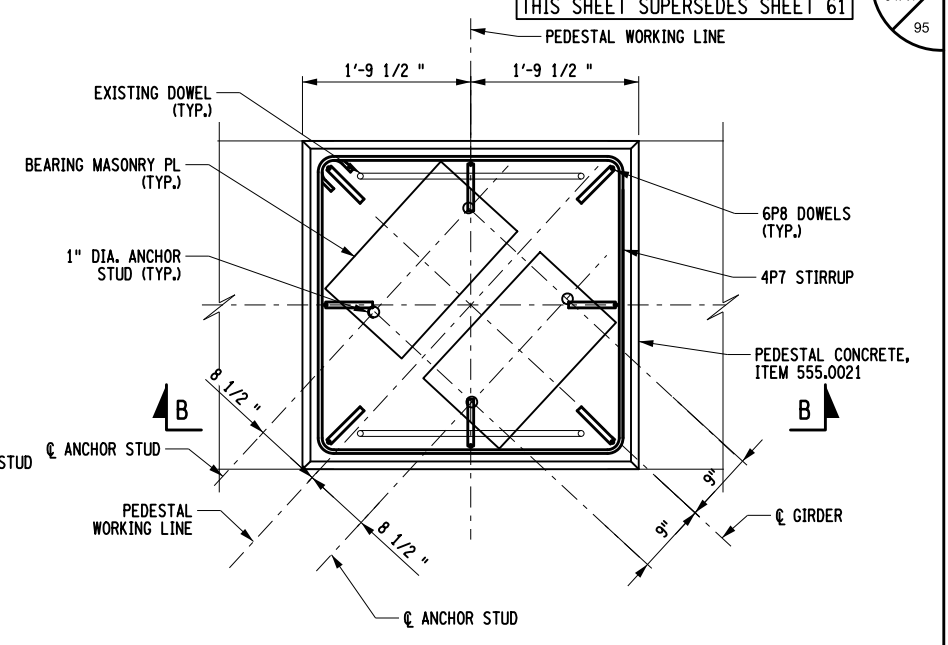
| PEDESTAL HEIGHT | NUMBER OF HOOPS |
|-----------------|-----------------|
| 6" TO 8" | 1 |
| 8" TO 11" | 2 |
| 11" TO 14" | 3 |
| 14" TO 17" | 4 |
| 17" TO 20" | 5 |



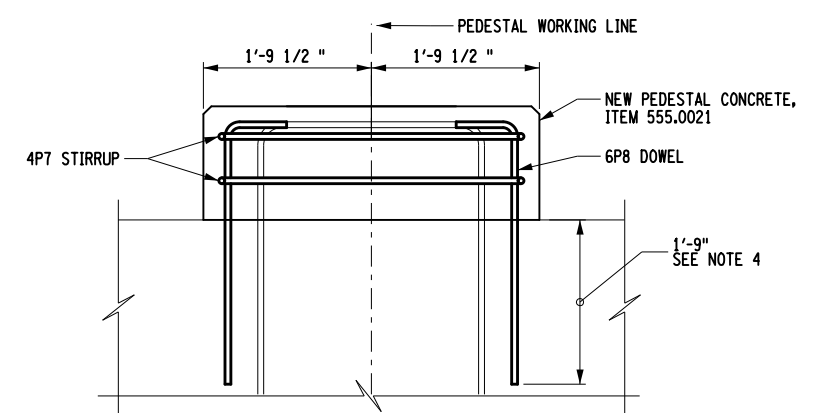
DETAIL 1: END PEDESTAL RECONSTRUCTION PLAN
SCALE: 1/2" = 1'-0"



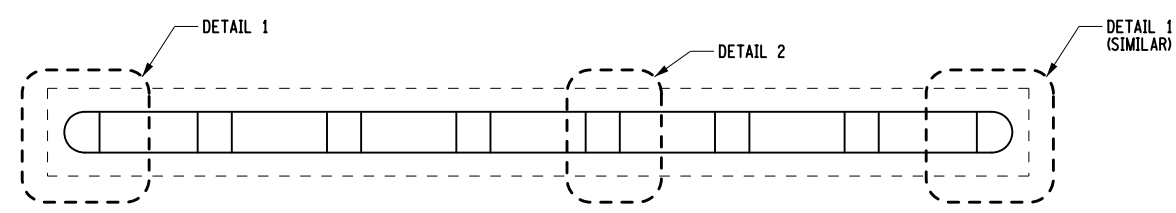
SECTION A-A
SCALE: 1/2" = 1'-0"



DETAIL 2: TYPICAL PEDESTAL RECONSTRUCTION PLAN
SCALE: 1/2" = 1'-0"



SECTION B-B
SCALE: 1/2" = 1'-0"



PIER PEDESTAL RECONSTRUCTION KEY PLAN

NOTES:

1. PEDESTAL REMOVALS SHALL BE COORDINATED WITH ABUTMENT BACKWALL REMOVAL AND PIER REPAIRS.
2. SEE DWG. NO. ST-18 FOR ADDITIONAL NOTES.
3. PEDESTAL REINFORCEMENT SHALL BE UNCOATED REINFORCEMENT PAID FOR UNDER ITEM 556.0201.
4. THE EMBEDMENT DEPTH SHOWN FOR DRILLING AND GROUTING IS FOR ESTIMATING PURPOSES ONLY. DRILLING AND GROUTING PEDESTAL DOWELS SHALL BE PAID FOR UNDER ITEM 586.0201. THE CONTRACTOR'S ENGINEER SHALL DETERMINE THE DEVELOPMENT LENGTH REQUIRED TO DEVELOP THE FULL STRENGTH OF THE REINFORCING BAR. THE CALCULATIONS SHALL BE BASED ON THE SIZE OF THE BAR, ACTUAL EDGE DISTANCE TO THE BAR, THE PROXIMITY TO OTHER BARS, ESTIMATED CONCRETE STRENGTH, AND THE GROUT SUPPLIERS RECOMMENDATION. THE CONTRACTOR SHALL SUBMIT DESIGN CALCULATIONS AND DETAILS SEALED BY A REGISTERED NEW ORK STATE PROFESSIONAL ENGINEER TO THE ENGINEER FOR APPROVAL.

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| ALTERED ON: | AFFIXED ON: 01/16/2025 |
| SIGNATURE: | SIGNATURE: DANIEL A. LEVINE |
| STAMP: | STAMP: |

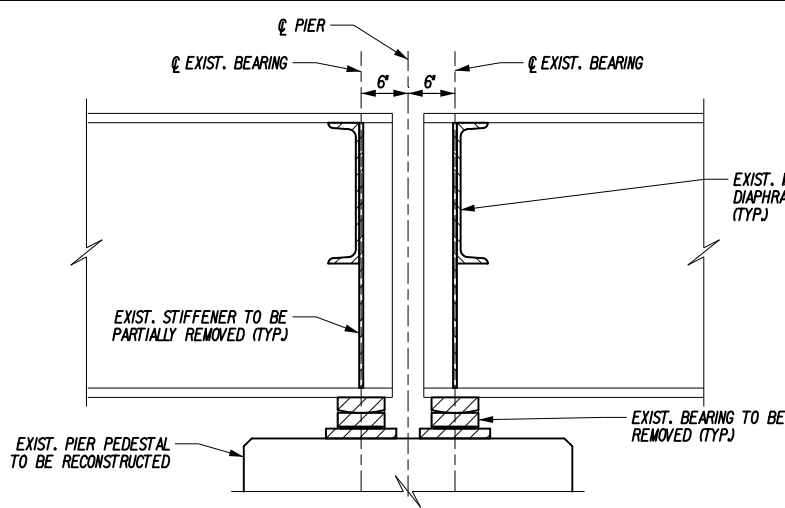
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

| REVISIONS | | | |
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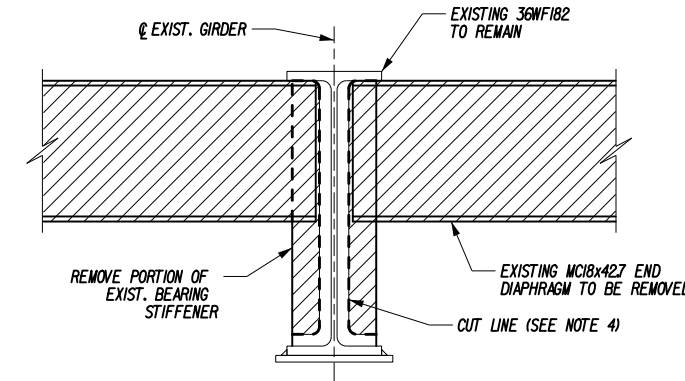
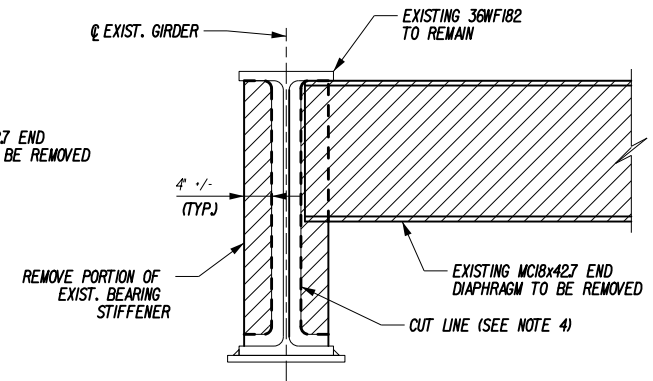
PARSONS

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| TITLE OF PROJECT REHABILITATION OF I-90 BRIDGE OVER NY 55 & ERIE CANAL TRAIL | CONTRACT NUMBER: TAA 25-06B |
| LOCATION OF PROJECT ALBANY DIVISION MP 178.38 | DATE: 12/11/2024 |
| TITLE OF DRAWING PEDESTAL DETAILS (SHEET 4 OF 4) | DRAWING NUMBER: ST-21 |

PW: / CHECKED BY: D. LEVINE / DRAFTED BY: J.P. O'LOUGHLIN / CHECKED BY: D. LEVINE / DESIGNED BY: J.P. O'LOUGHLIN / DESIGN SUPERVISOR: D. LEVINE

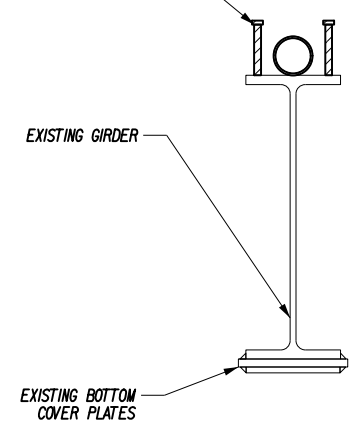


SECTION A-A
SCALE: 1/2"=1'-0"

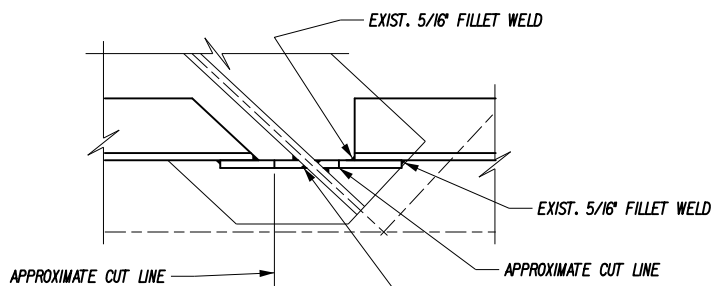


SECTION B-B
SCALE: 1/2"=1'-0"

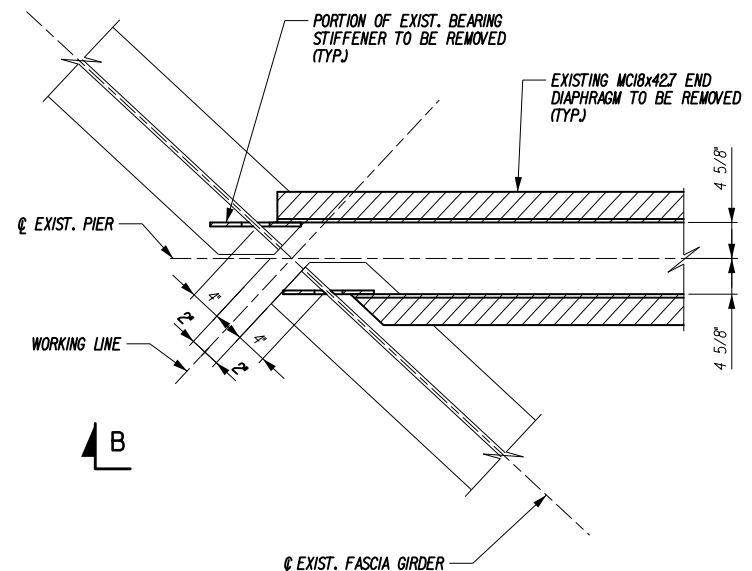
EXISTING SHEAR STUDS AND SHEAR SPIRALS TO BE REMOVED UNDER ITEM 202.2201.



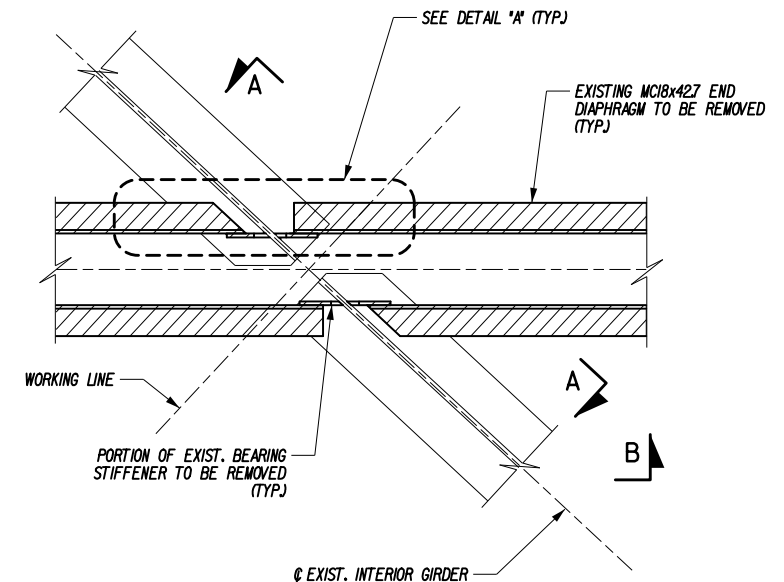
TYPICAL GIRDER SECTION
SCALE: 1"=1'-0"



DETAIL "A"
SCALE: 1"=1'-0"

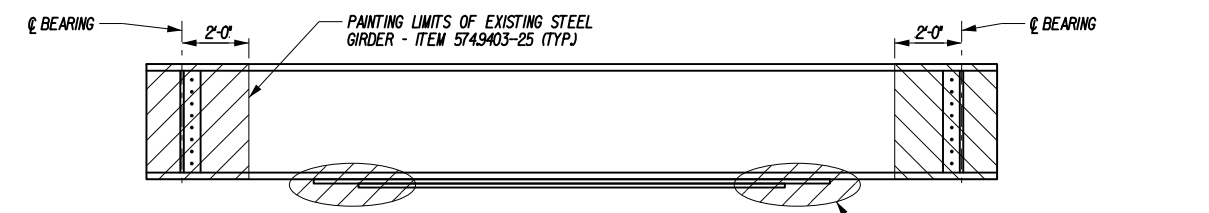


PARTIAL PLANS OF END DIAPHRAGM REMOVAL (PIERS 1, 2 & 3)
SCALE: 1/2"=1'-0"



NOTES:

- COST OF EXISTING STEEL (DIAPHRAGM AND CONNECTION PLATES) REMOVAL TO BE INCLUDED UNDER ITEM 589.010001.
- CONTRACTOR SHALL VERIFY EXISTING GEOMETRY PRIOR TO REMOVALS TO ENSURE FIT-UP OF PROPOSED STEEL.
- REMOVE THE EXISTING WELDS BY AIR CARBON-ARC GAUGING USING AN APPROVED WPS. SURFACES TO REMAIN SHALL BE GROUND SMOOTH TO REMOVE AN IRREGULARITIES.
- THERMALLY CUT PLATES SHALL BE REMOVED TO THE APPROXIMATE CUT LINES SHOWN ON THESE PLANS. PROTECT EXISTING STEEL FROM SPLATTER, SLAG, AND OTHER CUTTING DEBRIS. GRIND FINAL SURFACE SMOOTH TO REMOVE ANY IRREGULARITIES. HARDNESS OF FLAME CUT EDGES SHALL BE LESS THAN A ROCKWELL HARDNESS VALUE OF C 30 PER THE NYS STEEL CONSTRUCTION MANUAL SECTION 602. REMOVE ANY SHARP EDGES USING A GRINDER. CARE SHALL BE TAKEN TO NOT DAMAGE THE MAIN GIRDER.
- MINIMUM RADIUS OF FINISHED SURFACES SHALL BE 1" OR GREATER.
- ALL STRUCTURAL STEEL WORK INCLUDING BUT NOT LIMITED TO REMOVAL OF EXISTING STEEL SHALL BE PERFORMED IN ACCORDANCE WITH THE PROVISIONS OF THE STEEL CONSTRUCTION MANUAL.
- FOR DETAILS OF THE REPLACEMENT DIAPHRAGMS AND CONNECTION PLATES SEE DRAWING ST-30.



TYPICAL GIRDER PAINTING LIMITS
(SINGLE SPAN SHOWN, OTHERS SIMILAR)
N.T.S.

NOTES:
FOR THE PURPOSES OF BIDDING, CLEANING AND PAINTING OF EXISTING STEEL IS LIMITED TO THE AREAS INDICATED. INCLUDE COST OF PAINTING AT ABUTMENT BEARINGS AND PIER BEARINGS.

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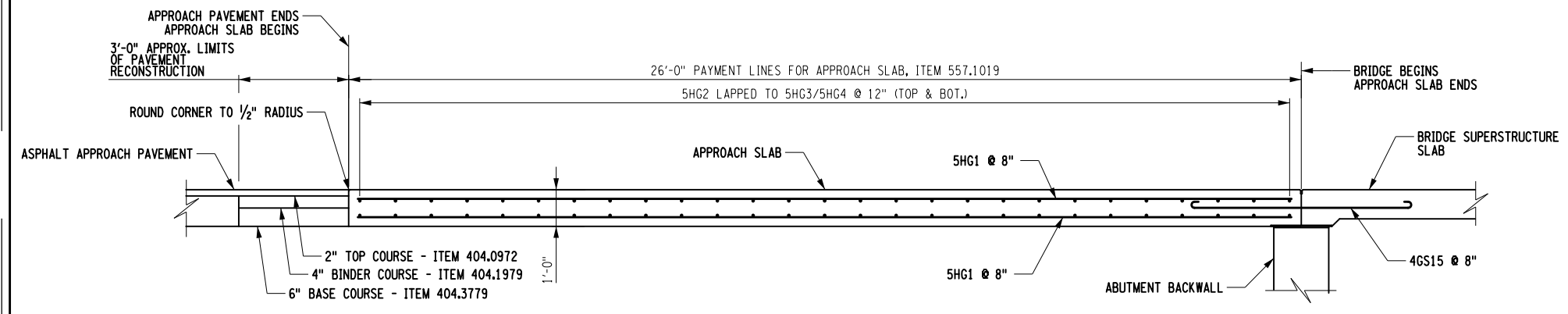
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| ALTERED ON: | AFFIXED ON: 01/16/2025 |
| SIGNATURE: STAMP: | SIGNATURE: DANIEL A. LEVINE STAMP: |
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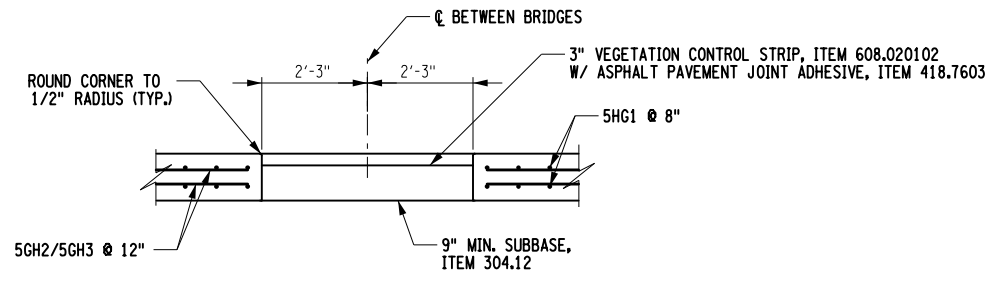
PARSONS

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| TITLE OF PROJECT REHABILITATION OF I-90 BRIDGE OVER NY 55 & ERIE CANAL TRAIL | CONTRACT NUMBER: TAA 25-06B |
| LOCATION OF PROJECT ALBANY DIVISION MP 178.38 | DATE: 12/11/2024 |
| TITLE OF DRAWING TYPICAL STEEL DETAILS (SHEET 1 OF 3) | DRAWING NUMBER: ST-29 |

PW/ CHECKED BY: D. LEVINE DRAFTED BY: J.P. O'LOUGHLIN CHECKED BY: D. LEVINE DESIGNED BY: J.P. O'LOUGHLIN DESIGN SUPERVISOR: D. LEVINE




SECTION A-A
(BEGIN APPROACH SHOWN, END APPROACH SIMILAR)
SCALE: 1/4" = 1'-0"



SECTION B-B
(BEGIN APPROACH SHOWN, END APPROACH SIMILAR)
SCALE: 1/4" = 1'-0"

- NOTES:**
1. ALL REINFORCEMENT SHALL HAVE 3 INCHES OF COVER UNLESS OTHERWISE NOTED.
 2. STRAIGHT TACK COAT, ITEM 407.0103 SHALL BE APPLIED BETWEEN ALL PAVEMENT COURSES AND ON THE MILLED SURFACE

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| ALTERED ON: | AFFIXED ON: 01/16/2025 |
| SIGNATURE: STAMP: | SIGNATURE: DANIEL A. LEVINE STAMP:  |

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| TITLE OF PROJECT REHABILITATION OF I-90 BRIDGE OVER NY 5S & ERIE CANAL TRAIL | CONTRACT NUMBER: TAA 25-06B |
| LOCATION OF PROJECT ALBANY DIVISION MP 178.38 | DATE: 12/11/2024 |
| TITLE OF DRAWING APPROACH SLAB SECTIONS AND DETAILS | DRAWING NUMBER: ST-42 |