

Section 106 Project Submittal Package

Replacement of Syracuse Division Bridges

Milepost 278.93: Exit 35 Ramp, East Syracuse, New York

BIN 5510090

Village of East Syracuse, Onondaga County, New York

NYSTA Project ID:

Prepared for:



New York State Thruway Authority
200 Southern Blvd.
P.O. Box 189
Albany, NY 12201-0189



Stantec
61 Commercial Street, Suite 100
Rochester, NY 14614
www.stantec.com

Prepared by:



**Environmental Design & Research,
Landscape Architecture, Engineering, & Environmental Services, D.P.C.**
217 Montgomery Street, Suite 1000
Syracuse, New York 13202
www.edrdpc.com

February 2017

NEW YORK STATE THRUWAY AUTHORITY (NYSTA) PROJECT SUBMITTAL PACKAGE
Section 106 of the National Historic Preservation Act

A Project Submittal Package is prepared by the NYSTA (Sponsor) or their consultants for federal aid transportation projects to provide sufficient information for NYSTA assessment of Section 106 obligations.

DATE February 10, 2017 NYSTA PROJECT ID _____ BINs 5510090

IDENTIFICATION

Project Name (if any) MP 278.93 Exit 35 Ramp, East Syracuse

Project Area Boundaries See attached mapping for limits of Projects. Section 1.1 contains a full description of Project limits.

(Indicate State or County Route # and/or local street name, and clearly defined endpoints)

County Onondaga

Town/City Syracuse

Village/Hamlet: East Syracuse

Have you consulted the NYSHPO web site at *<http://nysparks.state.ny.us> to determine the preliminary presence or absence of previously identified cultural resources within or adjacent to the project area? If yes: **X** Yes No

- Was the project site wholly or partially included within an identified archaeologically sensitive area? **X** Yes No
- Does the project site involve or is it substantially contiguous to a National Register of Historic Places listed property? Yes **X** No

*<http://nysparks.state.ny.us> then select **HISTORIC PRESERVATION** then **Historic Preservation Field Services Bureau** then **On Line Tools – CRIS**

ALL PROJECTS SUBMITTED FOR REVIEW SHOULD INCLUDE THE FOLLOWING INFORMATION

☒ **Project Description** – Attach a full description of the nature and extent of the work to be undertaken as part of this project. This should include, but not limited to, potential activities that might involve drainage, cutting, excavation, grading, filling, on-site detours, new sidewalks, right-of-way acquisition. Relevant portions of the project applications or environmental statements may be submitted. This could be from sections of the Draft Design Report/ Draft Scoping Document.

☒ **Location Maps** - Provide USGS Quad or DOT Planimetric map showing project area location. The map must clearly show street and road names surrounding the project area as well as all portions of the project.

☒ **Photos** - Provide clear, original color photographs of the entire project area keyed to a site plan. These photos should indicate:

- Buildings/structures more than 50 years old that are located along the property or on adjoining property
- Areas of prior ground disturbance (removal of original topsoil; filling and plowing are not considered disturbance)

LOCAL SPONSOR CONTACT

Name: Albert Mastroianni Title: Project Manager
Firm/Agency: New York State Thruway Authority
Address: 200 Southern Boulevard City: Albany State: NY Zip: 12201
Phone: 518-436-2909 E-Mail: Albert.mastroianni@thruway.ny.gov

Consultant Name: Environmental Design & Research, Landscape Architecture, Engineering & Environmental Services, D.P.C.
Contact Information: 217 Montgomery Street, Suite 1000, Syracuse, NY 13202
Phone: (315) 471-0688

1.0 Project Information

The purpose of this Section 106 Project Submittal Package (PSP) is to document the potential for impact on cultural resources that may result from replacement of the New York State Thruway Exit 35 Ramp bridge over the New York State Thruway, at Milepoint (MP) 278.93 on the New York State Thruway, in the Village of East Syracuse, Onondaga County, New York (hereafter, the Project). This PSP was prepared by Environmental Design & Research, Landscape Architecture, Engineering, & Environmental Services, D.P.C. (EDR) on behalf of the New York State Thruway Authority (NYSTA). This submittal was prepared by EDR cultural resources staff who meet the qualifications specified by the Secretary of the Interior's Standards for Historic Preservation and Archaeology per 36 CFR Part 61.

1.1 Project Location

The proposed Project consists of the replacement of the New York State Thruway Exit 35 Ramp bridge over the New York State Thruway, in the Village of East Syracuse, Onondaga County (see Attachment A). The existing steel multi-girder bridge is oriented north/south and was constructed in 1953.

The following terms are used throughout the PSP to describe the proposed action:

- **NYSTA MP 278.93: Exit 35 Ramp, (BIN 5510090) (the Project):** The proposed Project consists of the replacement an existing steel multi-girder bridge. The existing bridge carries NYS Thruway Exit 35 Ramp over the New York State Thruway (I-90). The existing bridge is approximately 200-feet in length, and was constructed circa 1953 (see Attachment B).
- **Area of Potential Effect (APE):** The APE for this Project is defined as a 1500-foot corridor extending in north and south along the Exit 35 Ramp from the existing bridge, as well as a 500-foot corridor east and west along the New York State Thruway (see Attachment A for limits of the APE).

1.2 Potential Impact on Historic-Architectural Resources

The New York State Office of Parks, Recreation and Historic Preservation (NYSOPRHP) Cultural Resources Information System (CRIS) website was reviewed to determine the location of properties listed on the National Register of Historic Places (NRHP) within and immediately adjacent to the APE defined above. No properties previously listed on, or determined eligible for, the NRHP are located within the APE. Therefore, the proposed Project is not anticipated to affect historic properties previously listed on or eligible for the NRHP.

The proposed Project will include superstructure replacement. This approach will not significantly alter the appearance of the bridge, and therefore, the Project has no potential to adversely impact the setting of any historic resources.

The bridge was initially constructed as a part of the new Interstate 90 (New York State Thruway) circa 1953, as confirmed in the original bridge design plans (see Attachment B). EDR has reviewed the 2002 New York State Department of Transportation (NYSDOT) *Evaluation of National Register Eligibility: Task C3 of the Historic Bridge Inventory and Management Plan*, which does not identify BIN 5510090 as eligible for listing on the NRHP.

1.3 Archaeological Sensitivity

A review of the NYSOPRHP CRIS website determined that the APE is not located in an archaeologically sensitive area and there are no previously reported archaeological sites in the APE. In addition, no previous cultural resources surveys have been conducted within or immediately adjacent to the proposed APE.

A review of historic aerial photographs (see Attachment C) indicates that the land within and adjacent to the APE was primarily agricultural and undeveloped prior to the construction of the New York State Thruway. The east-west length of the APE was initially disturbed by construction of the Thruway in the early 1950s, and the entire APE has been significantly disturbed by additional construction of additional ramps throughout the late twentieth century. The land immediately adjacent to the APE has also been heavily developed for commercial and industrial uses throughout the late twentieth century.

The land within and immediately adjacent to the APE has been heavily disturbed by the construction of the New York State Thruway and associated bridges and ramps. Therefore, the APE for the proposed Project is considered to have low archaeological sensitivity for historic and prehistoric cultural resources.

1.4 Archaeological Impact Assessment

There are no previously reported archaeological sites in the APE. All ground disturbance will be restricted to the areas around existing bridge abutments and piers, which consist of made land built up during the construction of Interstate 90 (the New York State Thruway) circa 1953. Therefore, the proposed Project is not anticipated to impact any archaeological resources.

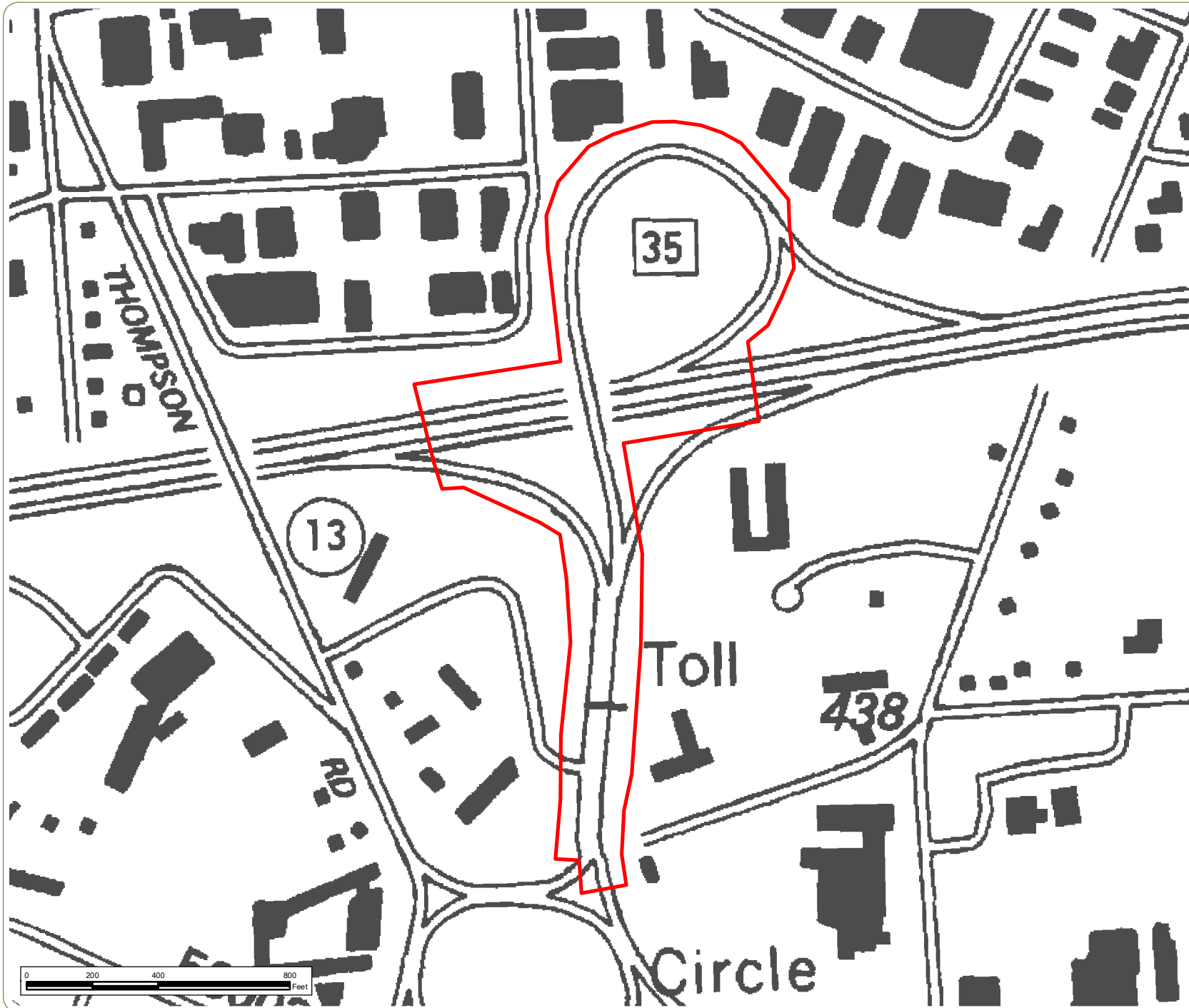
1.5 Photographs

A site visit was conducted by EDR staff on December 1st, 2016, in order to document existing conditions within the project area, including existing land use, visual character, and previous ground disturbance. Photograph locations are noted on a map included as Attachment D and selected photographs from this site visit are included as Attachment E.

LIST OF ATTACHMENTS

- Attachment A. Project Location Map
- Attachment B. 1953 Bridge Design Plans (Excerpt)
- Attachment C. Historic Aerial Photographs
- Attachment D. Photograph Locations
- Attachment E. Photographs

Attachment A:
Project Location Map




Replacement of Syracuse Division Bridges

**MP 278.93: Exit 35 Ramp
(BIN 5510090)**

Village of East Syracuse,
Onondaga County, New York

Attachment A: Project Location

February 2017

 Area of Potential Effect

Notes:
1. Basemap: NYSDOT Syracuse East, NY
1:24000 planimetric quadrangles.
2. This is a color graphic. Reproduction
in grayscale may misrepresent the data.



www.edrdpc.com

Attachment B:
1953 Bridge Design Plans (Excerpt)



NEW YORK STATE THRUWAY AUTHORITY
PLANS FOR CONSTRUCTING ACCELERATION AND DECELERATION LANES ON A PORTION OF THE
NEW YORK STATE THRUWAY, ONTARIO SECTION: Subdivision 8A
 From Station 2244+52 to Station 2287+80, a length of 0.82 mile in the Town of Salina
AND FOR CONSTRUCTING THE
ELECTRONICS PARKWAY (HOPKINS ROAD) INTERCHANGE
 At Station 2264+95, a length of 1.51 miles in the Town of Salina
A TOTAL LENGTH OF 2.33 MILES CONTRACT No. O.T. 53-8
AND FOR CONSTRUCTING ACCELERATION AND DECELERATION LANES ON A PORTION OF THE
NEW YORK STATE THRUWAY, MOHAWK SECTION: Subdivision 8B
 From Station 2502+47 to Station 2542+11, a length of 0.75 mile in the Town of DeWitt
AND FOR CONSTRUCTING THE
THOMPSON ROAD INTERCHANGE
 At Station 2519+90, a length of 1.30 miles in the Town of DeWitt
A TOTAL LENGTH OF 2.05 MILES CONTRACT No. M.T. 53-8
AND FOR CONSTRUCTING PORTIONS OF
ELECTRONICS PARKWAY (HOPKINS ROAD)
 From Station 29+20 to Station 42+65, a length of 0.25 mile in the Town of Salina
EXTENSION OF SEVENTH NORTH STREET
 From Station 3+53 to Station 20+92, a length of 0.33 mile in the Town of Salina
THOMPSON ROAD
 From Station B 9+10 to Station B 42+36, a length of 0.78 mile in the Town of DeWitt
COLLEGE PLACE
 From Station C 15+00 to Station C 26+89, a length of 0.22 mile in the Town of DeWitt
A TOTAL LENGTH OF 1.58 MILES CONTRACT No. S.T. 53-20
AND FOR RECONSTRUCTING A PORTION OF THE
SYRACUSE - BRIDGEPORT, PART 1 (KINNE ST.) S.H. No. 672
 Between Station 22+32 and Station 46+33, a length of 0.62 mile in the Town of DeWitt
A TOTAL COMBINED LENGTH OF 6.58 MILES CONTRACT No. R.C. 53-36
ONONDAGA COUNTY

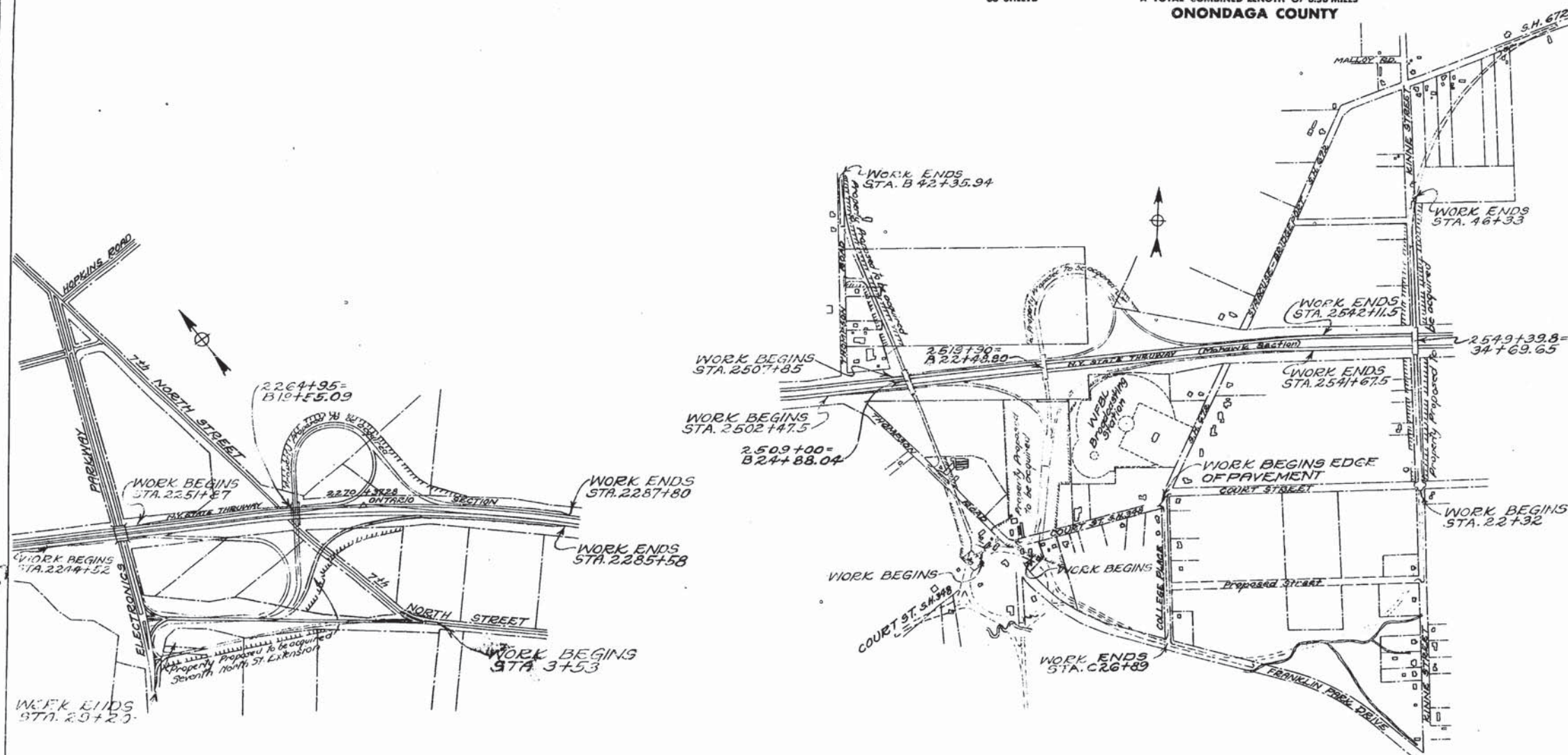
66 SHEETS

COUNTY	SHEET No.	TOTAL SHEETS
ONONDAGA	1	66
N.Y. STATE THRUWAY, ONTARIO SECTION, SUBDIV. 8A		
INTERCHANGE AT ELECTRONICS PARKWAY (HOPKINS ROAD)		
N.Y. STATE THRUWAY, MOHAWK SECTION, SUBDIV. 8B		
INTERCHANGE AT THOMPSON ROAD		

TYPE OF CONSTRUCTION	
Reinforced Cement Concrete Pavement	4.16 Miles
Asphalt Concrete, Type 1A, Opt.	0.37 Mile
Foundation Course-Gravel	0.20 Mile
Miscellaneous Work	1.85 Miles
Including	
H.G.S. Electronics P'kway Interchange Station 2264+95	
Composite Beam, 4 Span, 2 @ 37' 9", 1 @ 64' 9", 1 @ 57' 9"	
H.G.S., Thompson Road, Station 2509+00 Comp. Beam,	
4 Spans, 2 @ 39' 0", 1 @ 59' 10", 1 @ 67' 1"	
H.G.S., Thompson Road Interchange, Station 2519+90	
Composite Beam, 4 Span, 1 @ 37' 0", 1 @ 64' 9", 1 @ 57' 9", 1 @ 37' 3"	
H.G.S., Kinne St. (Relocating of S.H. 672) Sta. 2549+40	
Composite Beam, 4 Spans, 2 @ 37' 6", 2 @ 72' 9"	

STANDARD STRUCTURE SHEETS	
39-9, 46-4, 47-37, 49-65, 49-7, 49-42, 50-1R, 50-34,	
51-3, 51-11R, 51-11W, 51-20, 51-21, 51-40, 52-17A,	
52-17B, 52-17C, 52-17D, 52-43, 53-41, 53-106	

All work contemplated under this contract to be covered by and in conformity with the specifications adopted January 2, 1951, except as modified on these plans and in the Itemized Proposal.

**INTERCHANGE AT ELECTRONICS PARKWAY****INTERCHANGE AT THOMPSON ROAD**

Made By D. F. Muckert Traced By D. F. Muckert Checked By W. C. Hoffmann

O.T. 53-8, M.T. 53-8, S.T. 53-20, R.C. 53-36

NEW YORK STATE DEPARTMENT OF PUBLIC WORKS
 DIVISION OF CONSTRUCTION

Approved April 16, 1953
J. B. McMorran Chief Engineer

Approved April 16, 1953
E. T. Gawkins Deputy Chief Engineer

Approved April 16, 1953
E. W. Wendell Deputy Chief Engineer

Approved April 16, 1953
B. D. Tallamy Chairman
C. H. Lang Deputy Chief Engineer

Prepared pursuant to the
 Highway Law and recommended by
4/1/53 Engineer District No. 3

O.T. 53-8, M.T. 53-8, S.T. 53-20, R.C. 53-36



NEW YORK STATE THRUWAY AUTHORITY
 PLANS FOR CONSTRUCTING ACCELERATION AND DECELERATION LANES ON A PORTION OF THE
NEW YORK STATE THRUWAY, ONTARIO SECTION: Subdivision 8A
 From Station 2244+52 to Station 2287+80, a length of 0.75 mile in the Town of Salina
 AND FOR CONSTRUCTING THE
ELECTRONICS PARKWAY (HOPKINS ROAD) INTERCHANGE
 At Station 2264+95, a length of 1.74 miles in the Town of Salina
 A TOTAL LENGTH OF 2.49 MILES CONTRACT No. O.T. 53-8

AND FOR CONSTRUCTING ACCELERATION AND DECELERATION LANES ON A PORTION OF THE
NEW YORK STATE THRUWAY, MOHAWK SECTION: Subdivision 8B
 From Station 2502+47 to Station 2542+11, a length of 0.93 mile in the Town of DeWitt
 AND FOR CONSTRUCTING THE
THOMPSON ROAD INTERCHANGE
 At Station 2519+90, a length of 1.40 miles in the Town of DeWitt
 A TOTAL LENGTH OF 2.33 MILES CONTRACT No. M.T. 53-8

AND FOR CONSTRUCTING PORTIONS OF
ELECTRONICS PARKWAY (HOPKINS ROAD)
 From Station 29+20 to Station 42+65, a length of 0.32 mile in the Town of Salina
EXTENSION OF SEVENTH NORTH STREET
 From Station 3+53 to Station 20+92, a length of 0.44 mile in the Town of Salina
THOMPSON ROAD
 From Station 8+10 to Station 42+36, a length of 0.76 mile in the Town of DeWitt
COLLEGE PLACE
 From Station C 15+00 to Station C 26+89, a length of 0.23 mile in the Town of DeWitt
 A TOTAL LENGTH OF 1.65 MILES CONTRACT No. S.T. 53-20

AND FOR RECONSTRUCTING A PORTION OF THE
SYRACUSE - BRIDGEPORT, PART 1 (KINNE ST.) S.H. No. 672
 Between Station 22+32 and Station 46+33, a length of 0.55 mile in the Town of DeWitt
 66 SHEETS A TOTAL COMBINED LENGTH OF 6.85 MILES CONTRACT No. R.C. 53-36
ONONDAGA COUNTY

COUNTY	SHEET No.	TOTAL SHEETS
ONONDAGA	1	66
N.Y. STATE THRUWAY, ONTARIO SECTION, SUB DIV. 8A		
INTERCHANGE AT ELECTRONICS PARKWAY (HOPKINS ROAD)		
N.Y. STATE THRUWAY, MOHAWK SECTION, SUB DIV. 8B		
INTERCHANGE AT THOMPSON ROAD		

TYPE OF CONSTRUCTION
 Reinforced Cement Concrete Pavement 6.49 Miles
 Asphalt Concrete, Type 1A, Opt. 0.38 Mile
 Foundation Course-Gravel 0.13 Mile
 Miscellaneous Work 0.02 Miles

Including
 H.G.S. Electronics Parkway Interchange Station 2264+95
 Composite Beam, 4 Spans, 2 @ 37' 9", 1 @ 64' 9",
 1 @ 57' 9"
 H.G.S., Thompson Road, Station 2509+00 Comp. Beam,
 4 Spans, 2 @ 39' 0", 1 @ 59' 10", 1 @ 67' 1"
 H.G.S., Thompson Road Interchange, Station 2519+90
 Composite Beam, 4 Spans, 1 @ 37' 0", 1 @ 64' 9",
 1 @ 57' 9", 1 @ 37' 3"
 H.G.S., Kinne St. (Relocating of S.H. 672) Sta: 2549+40
 Composite Beam, 4 Spans, 2 @ 37' 6", 2 @ 72' 9"

STANDARD STRUCTURE SHEETS
 39-9, 46-4, 47-37, 49-65, 49-7, 49-42, 50-1R, 50-34,
 51-3, 51-11R, 51-11W, 51-20, 51-21, 51-40, 52-17A,
 52-17B, 52-17C, 52-17D, 52-43, 53-41, 53-106

All work contemplated under this contract to be covered
 by and in conformity with the specifications adopted
 January 2, 1951, except as modified on these plans and
 in the Itemized Proposal.

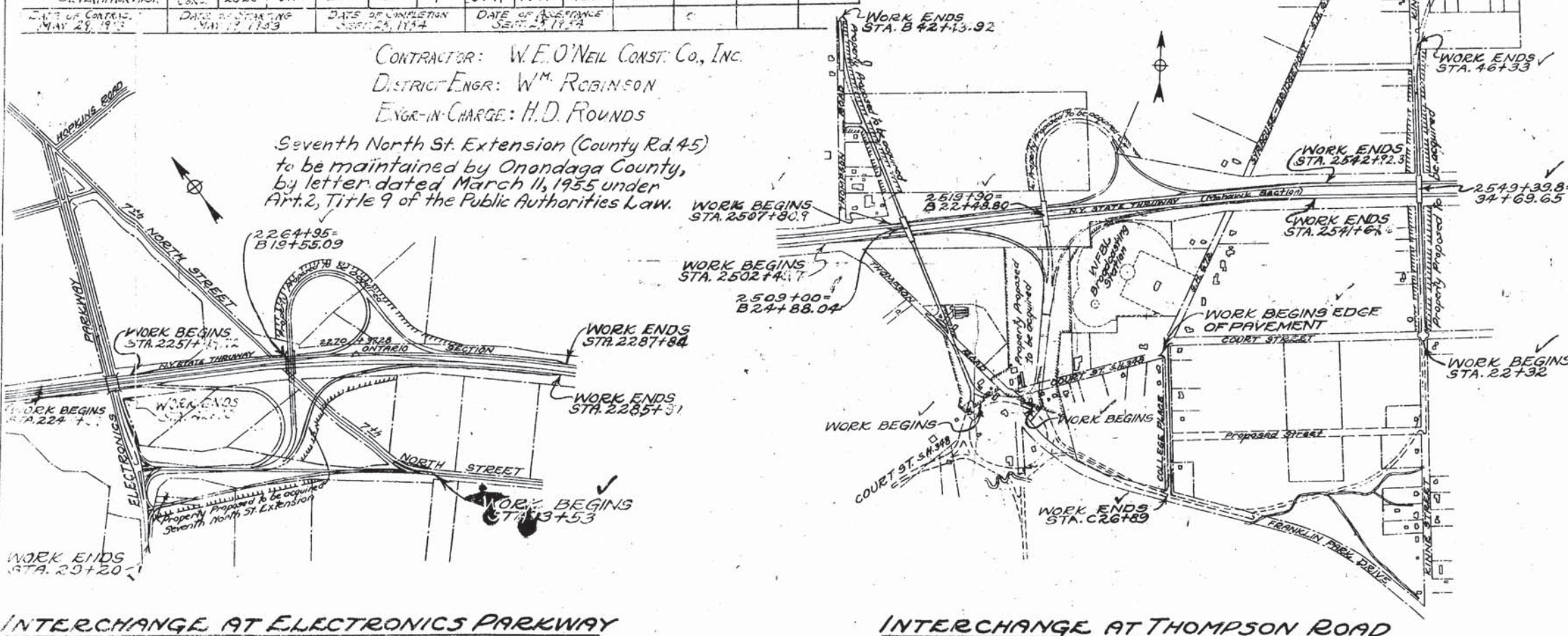
Thompson Road (County Road 13) to be
 maintained by Onondaga County, by
 letter dated March 16, 1955 under Art. 2,
 Title 9 of the Public Authorities Law.

Syracuse-Bridgeport, Pt. 1, S.H. 672 to
 be maintained by New York State Dept.
 of Public Works, by letter dated
 March 16, 1955 under Art. 2, Title 9
 of the Public Authorities Law.

CONTRACT NO.	TYPE	LENGTH FEET	WIDTH FEET	THICK TOP	S.Y. PAV.	C.Y. CONC.	TONS OF TOP	MATERIALS SPRINKLING AND GEN. CONSTRUCTION	CEMENT ALPHA	ASPH. CONC. BARRIT	GRAVEL ACCESS RD.
MT 53-8	REINF. CONC.	11,922	2.33	Various	Various	9"	24,680	5817			
OT 53-8	REINF. CONC.	13,170	2.49	Various	Various	9"	26,473	6422			
RC 53-36	REINF. CONC.	2,112	0.55	24'	44'	9"	5,794	1448			710
ST 53-20 Composite Pkwy	ASPH. CONC.	1,194	0.23	22'	34'	2 1/2"	3,326		FEDERAL	BARRIT	
Thompson Road	REINF. CONC.	4,232	0.76	24'	44'	9"	8,970	2243		No. AMER.	
Hopkins Road	REINF. CONC.	1,189	0.22	12'	12'	9"	1,157	289		CENTURY	
Seventh North St.	REINF. CONC.	2,320	0.44	24'	44'	9"	6,797	1,699			
DATE OF CONTRACT	DATE OF STARTING	DATE OF COMPLETION	DATE OF ACCEPTANCE								
MAY 28, 1953	MAY 12, 1953	SEP. 24, 1954	SEP. 14, 54								

CONTRACTOR: W.E.O'NEIL CONST. CO., INC.
 DISTRICT ENGR: W.M. ROBINSON
 ENGR-IN-CHARGE: H.D. ROUNDS

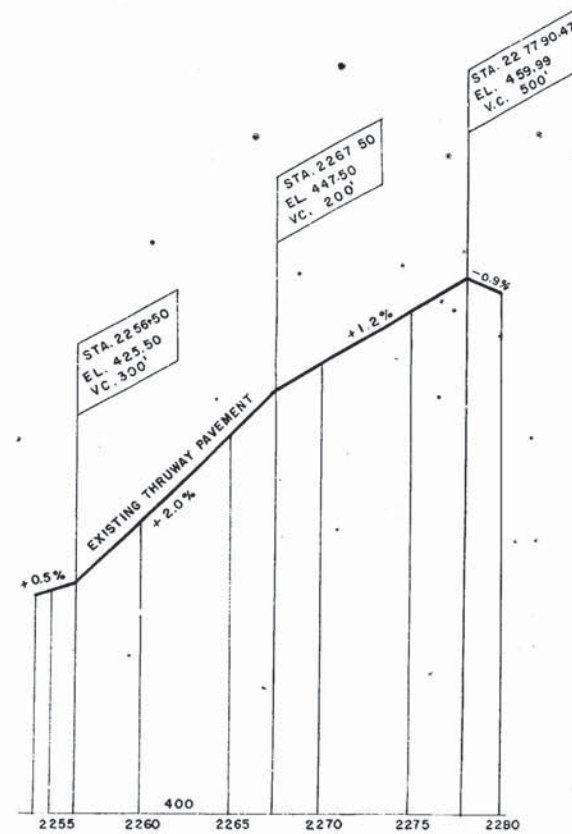
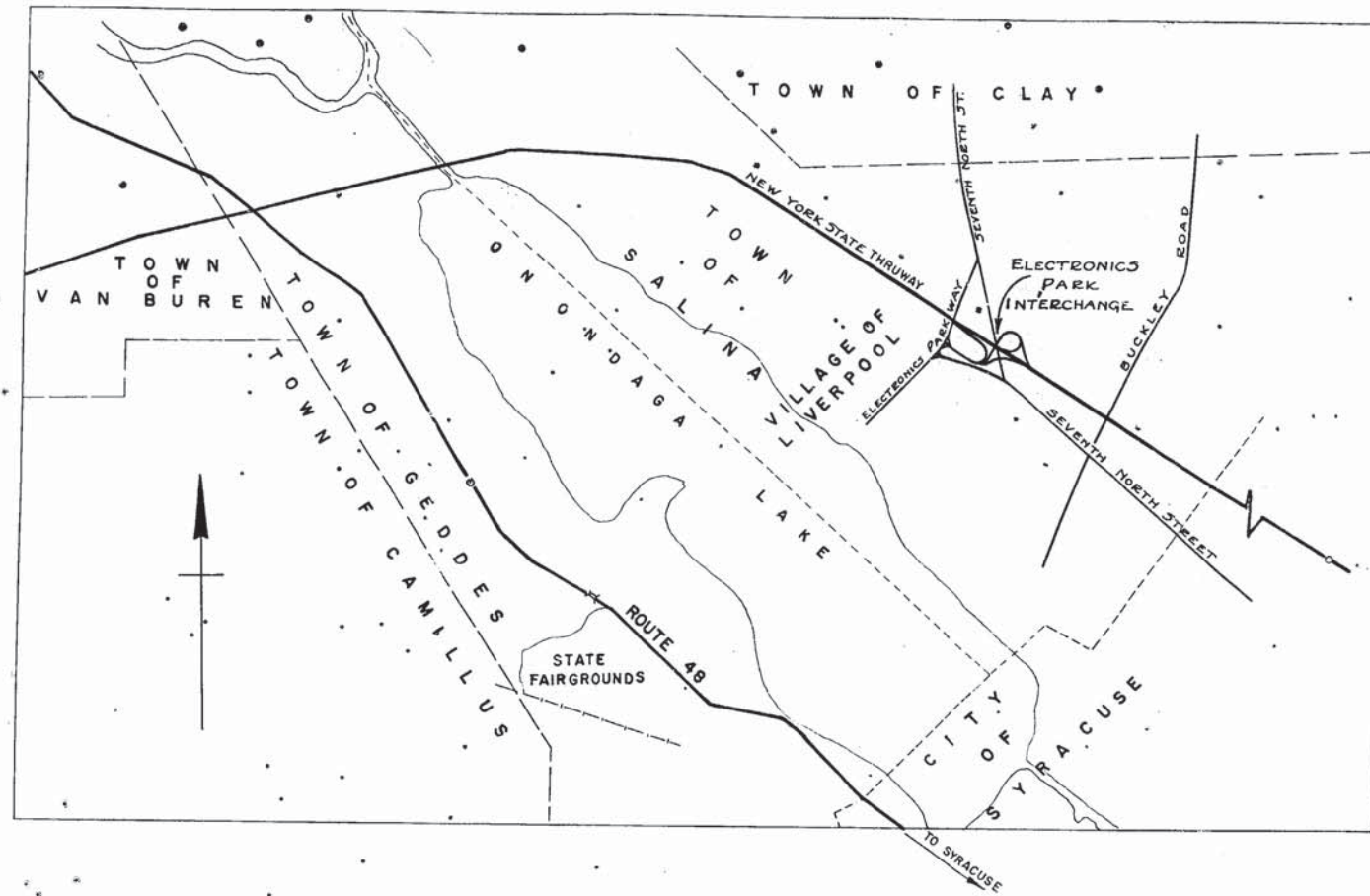
Seventh North St. Extension (County Rd. 45)
 to be maintained by Onondaga County,
 by letter dated March 11, 1955 under
 Art. 2, Title 9 of the Public Authorities Law.



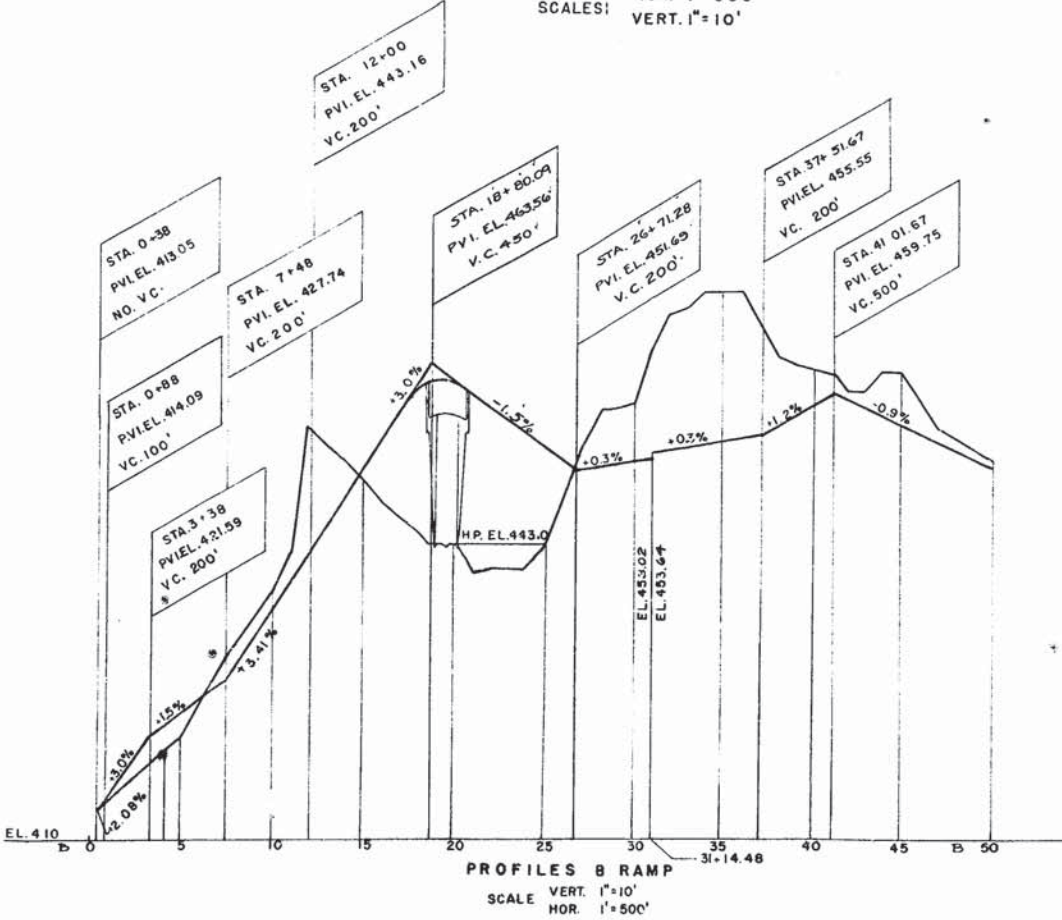
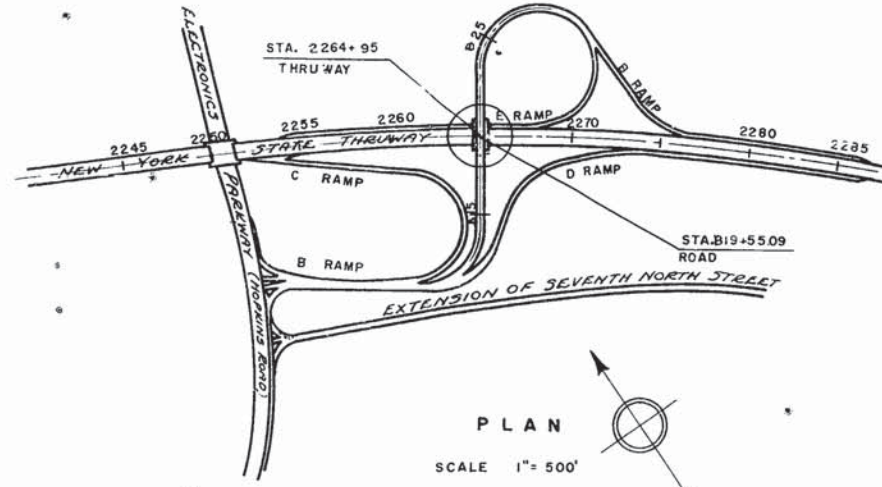
NEW YORK STATE DEPARTMENT OF PUBLIC WORKS DIVISION OF CONSTRUCTION	
Approved	1953
J. McMorran	Chief Engineer
Approved	1953
E. T. GAWKINS	Deputy Chief Engineer
Approved	APRIL 16, 1953
E. W. WENDT	Deputy Chief Engineer
NEW YORK STATE THRUWAY AUTHORITY	
B. D. TALLAMY, Chairman	
By: C. H. LANG	Deputy Chief Engineer

Prepared pursuant to the
 Highway Law and recommended by
 the Engineer District No. 3

COUNTY	SHEET NO.	TOTAL SHEETS
ONONDAGA	5	66
N.Y. STATE THRUWAY, ONTARIO SECTION		
N.Y. STATE THRUWAY, ONTARIO SECTION, SUB DIV. 8A		
INTERCHANGE AT ELECTRONICS PARKWAY (HOPKINS ROAD)		



PROFILE ON THRUWAY
THEORETICAL GRADE
HOR. 1" = 500'
VERT. 1" = 10'



PROFILES B RAMP
VERT. 1" = 10'
HOR. 1" = 500'

DEPARTMENT OF PUBLIC WORKS

RECOMMENDED Joseph C. Frederick Feb. 26, 1953
JOSEPH C. FEDERICK
ASST. DISTRICT ENGINEER
DATE

APPROVED E.T. GAWKINS 3/4/53
E.T. GAWKINS
DEPUTY CHIEF ENGINEER
DATE

E.W. WENDELL MAR 1 1953
E.W. WENDELL
DEPUTY CHIEF ENGINEER
DATE

J.B. MC MORRAN 3/2/53
J.B. MC MORRAN
CHIEF ENGINEER
DATE

APPROVED March 2, 1953
NEW YORK STATE THRUWAY AUTHORITY

B.D. TALLAM, CHAIRMAN
BY C.H. LANG
DEPUTY CHIEF ENGINEER

PRELIMINARY LAYOUT
NEW YORK STATE THRUWAY AND HOPKINS ROAD
TRAFFIC INTERCHANGE

PREPARED AND RECOMMENDED

URQUHART & DOYLE CONSULTING ENGINEERS
NEW YORK STATE PROFESSIONAL ENGINEERS LICENSE NO. 5667
DATE Feb 16, 1953

COUNTY ONONDAGA **SHEET NO.** 8 **TOTAL SHEETS** 66
NEW YORK STATE THRUWAY ONTARIO SECTION SUB DIV. 8A
INTERCHANGE AT ELECTRONICS PARKWAY (HOPKINS ROAD)

8R

INTERCHANGE LAYOUT
NEW YORK STATE THRUWAY AND HOPKINS ROAD
TRAFFIC INTERCHANGE

THRUWAY CURVE DATA
 New P.I. Sta. 2270+37.28
 Δ = 12°-19'
 D = 0°-30'
 E = 11459.16'
 T = 1236.14'
 L.C. = 2463.34'

SCALE: 1" = 100'

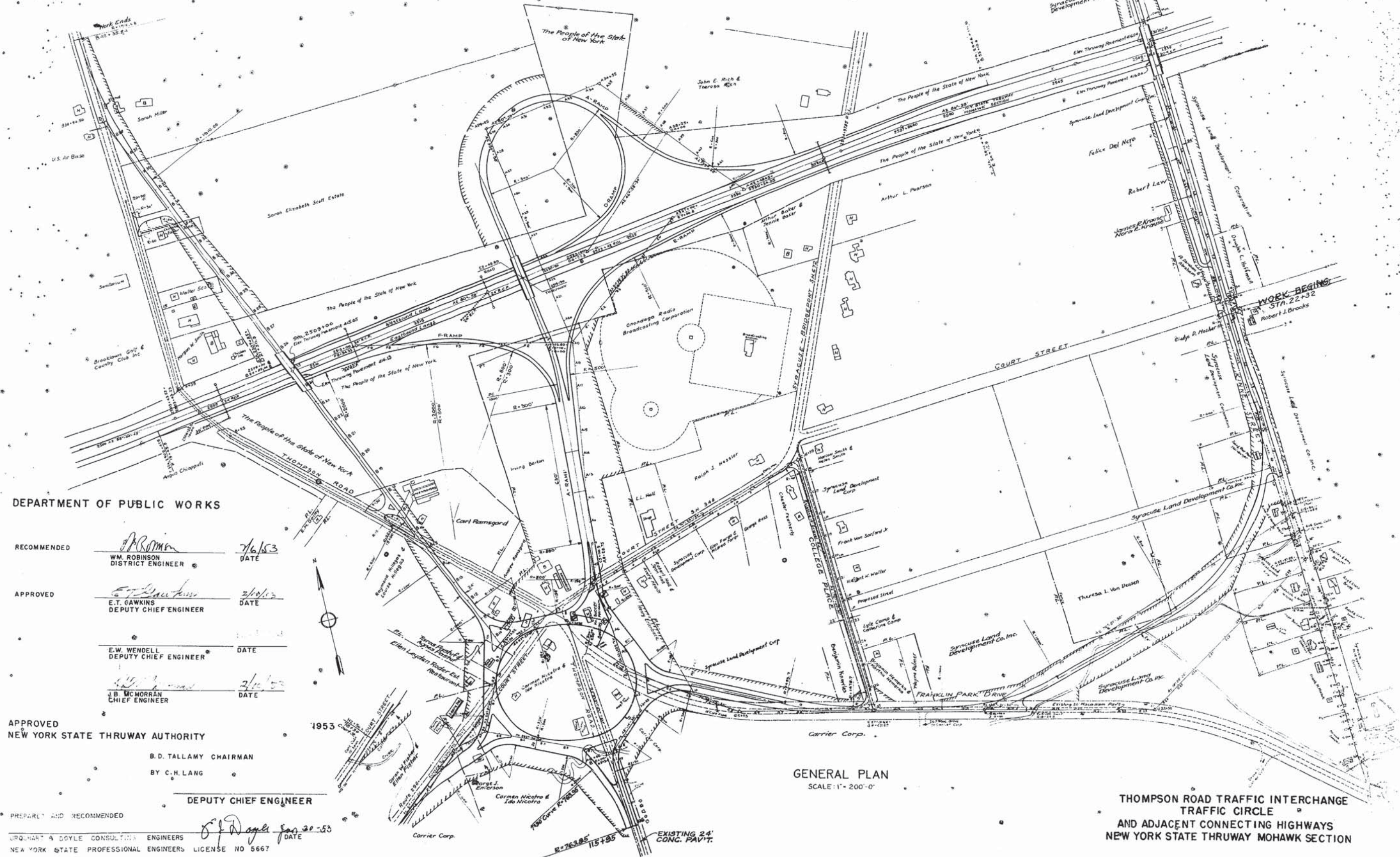
PREPARED AND RECOMMENDED *W. H. H. & D. O.* **DATE** Feb 16-53
 W. H. H. & D. O. CONSULTING ENGINEERS
 NEW YORK STATE PROFESSIONAL ENGINEERS LICENSE NO. 5667

INTERCHANGE LAYOUT
NEW YORK STATE THRUWAY AND HOPKINS ROAD
TRAFFIC INTERCHANGE

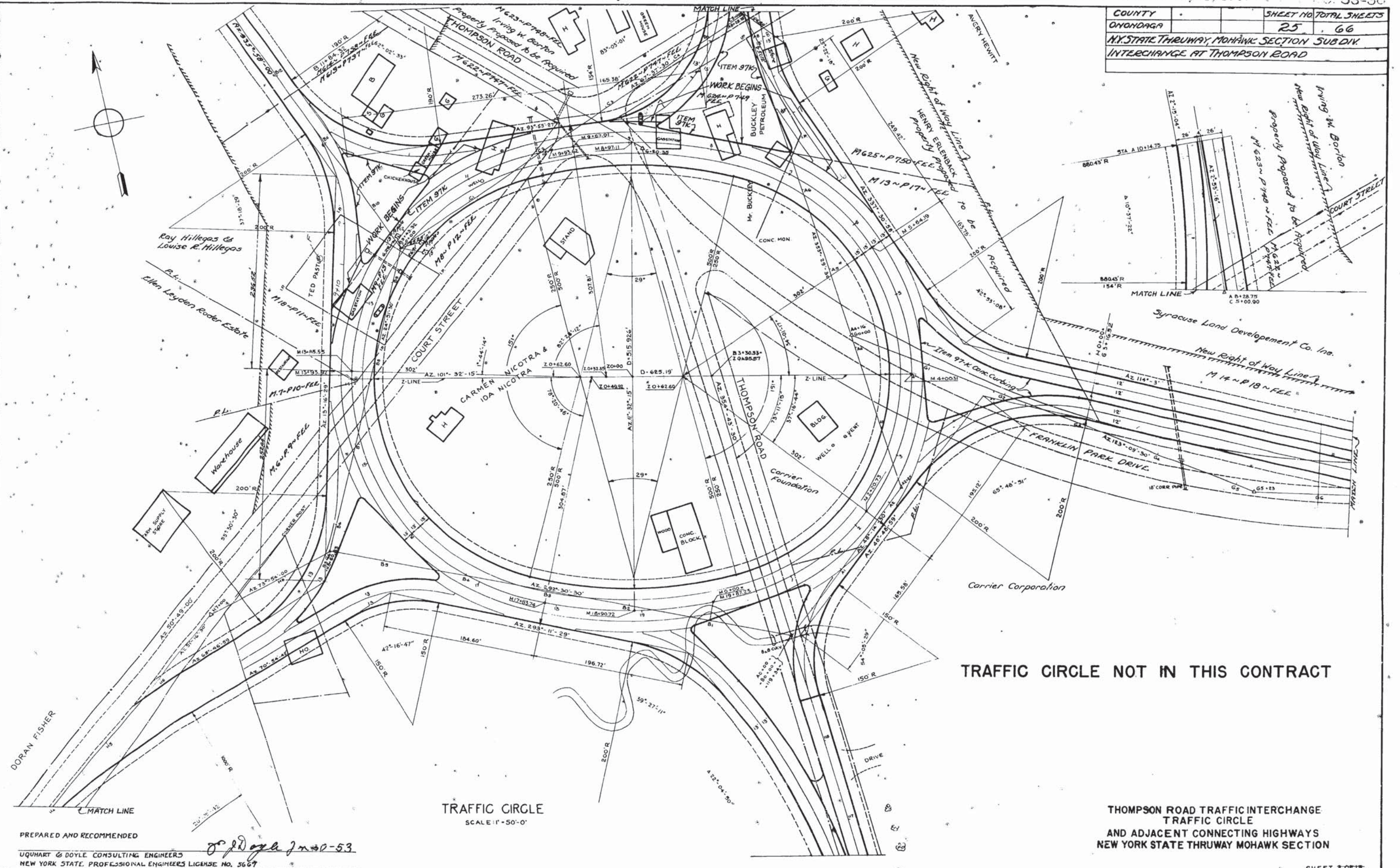
PREPARED AND RECOMMENDED *JRD* *Feb 16 - 53*
 _____ DATE
 JIRQUHART & DOYLE CONSULTING ENGINEERS
 NEW YORK STATE PROFESSIONAL ENGINEERS LICENSE NO. 5667

SHEET ~~3087~~

COUNTY	SHEET NO.	TOTAL SHEETS
ONONDAGA	22	66
N.Y. STATE THRUWAY, MOHAWK SECTION SUBDIV. B.B.		
INTERCHANGE AT THOMPSON ROAD		



COUNTY	SHEET NO	TOTAL SHEETS
ONONDAGA	25	66
NY STATE THRUWAY, MOHAWK SECTION SUB DIV.		
INTERCHANGE AT THOMPSON ROAD		



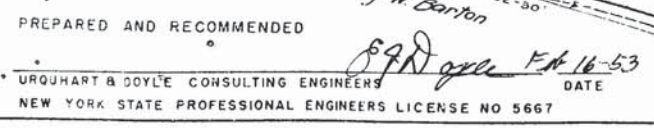
TRAFFIC CIRCLE
SCALE: 1" = 50'-0"

TRAFFIC CIRCLE NOT IN THIS CONTRACT

THOMPSON ROAD TRAFFIC INTERCHANGE
TRAFFIC CIRCLE
AND ADJACENT CONNECTING HIGHWAYS
NEW YORK STATE THRUWAY MOHAWK SECTION

PREPARED AND RECOMMENDED

UQUHART & DOYLE CONSULTING ENGINEERS
NEW YORK STATE PROFESSIONAL ENGINEERS LICENSE NO. 5667



THOMPSON ROAD TRAFFIC INTERCHANGE
TRAFFIC CIRCLE
AND ADJACENT CONNECTING HIGHWAYS
NEW YORK STATE THRUWAY MOHAWK SECTION

WILLIAM A. DOYLE CONSULTING ENGINEERS
NEW YORK STATE PROFESSIONAL ENGINEERS LICENSE NO. 5667

SHEET

Attachment C:
Historic Aerial Photographs



Exit 35 Ramp

I-90 Exit 35

East Syracuse, NY 13057

Inquiry Number: 4843392.5

February 02, 2017

The EDR Aerial Photo Decade Package



6 Armstrong Road, 4th floor
Shelton, CT 06484
Toll Free: 800.352.0050
www.edrnet.com

EDR Aerial Photo Decade Package

02/02/17

Site Name:

Exit 35 Ramp
I-90 Exit 35
East Syracuse, NY 13057
EDR Inquiry # 4843392.5

Client Name:

Environmental Design & Research, d.p.c
217 Montgomery Street
Syracuse, NY 13202
Contact: Caitlin Graff



Environmental Data Resources, Inc. (EDR) Aerial Photo Decade Package is a screening tool designed to assist environmental professionals in evaluating potential liability on a target property resulting from past activities. EDR's professional researchers provide digitally reproduced historical aerial photographs, and when available, provide one photo per decade.

Search Results:

<u>Year</u>	<u>Scale</u>	<u>Details</u>	<u>Source</u>
2011	1"=500'	Flight Year: 2011	USDA/NAIP
2009	1"=500'	Flight Year: 2009	USDA/NAIP
2008	1"=500'	Flight Year: 2008	USDA/NAIP
2006	1"=500'	Flight Year: 2006	USDA/NAIP
1995	1"=500'	Acquisition Date: March 27, 1995	USGS/DOQQ
1988	1"=500'	Flight Date: October 31, 1988	NYDOT
1986	1"=500'	Flight Date: April 14, 1986	USGS
1978	1"=500'	Flight Date: September 13, 1978	USDA
1966	1"=500'	Flight Date: June 22, 1966	USDA
1959	1"=500'	Flight Date: May 25, 1959	USDA
1951	1"=500'	Flight Date: October 15, 1951	USDA
1938	1"=500'	Flight Date: July 05, 1938	USDA

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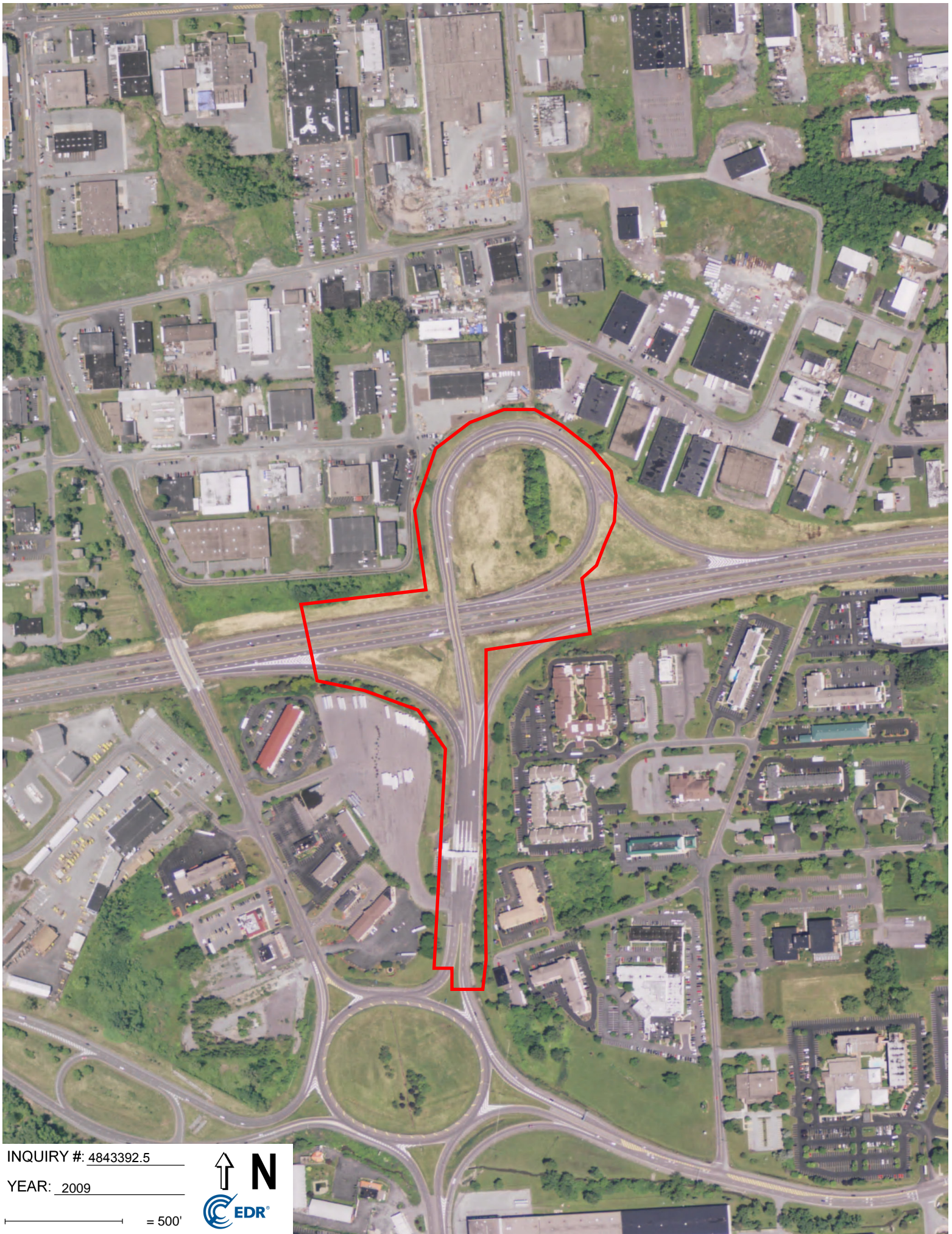


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YEAR: 2011

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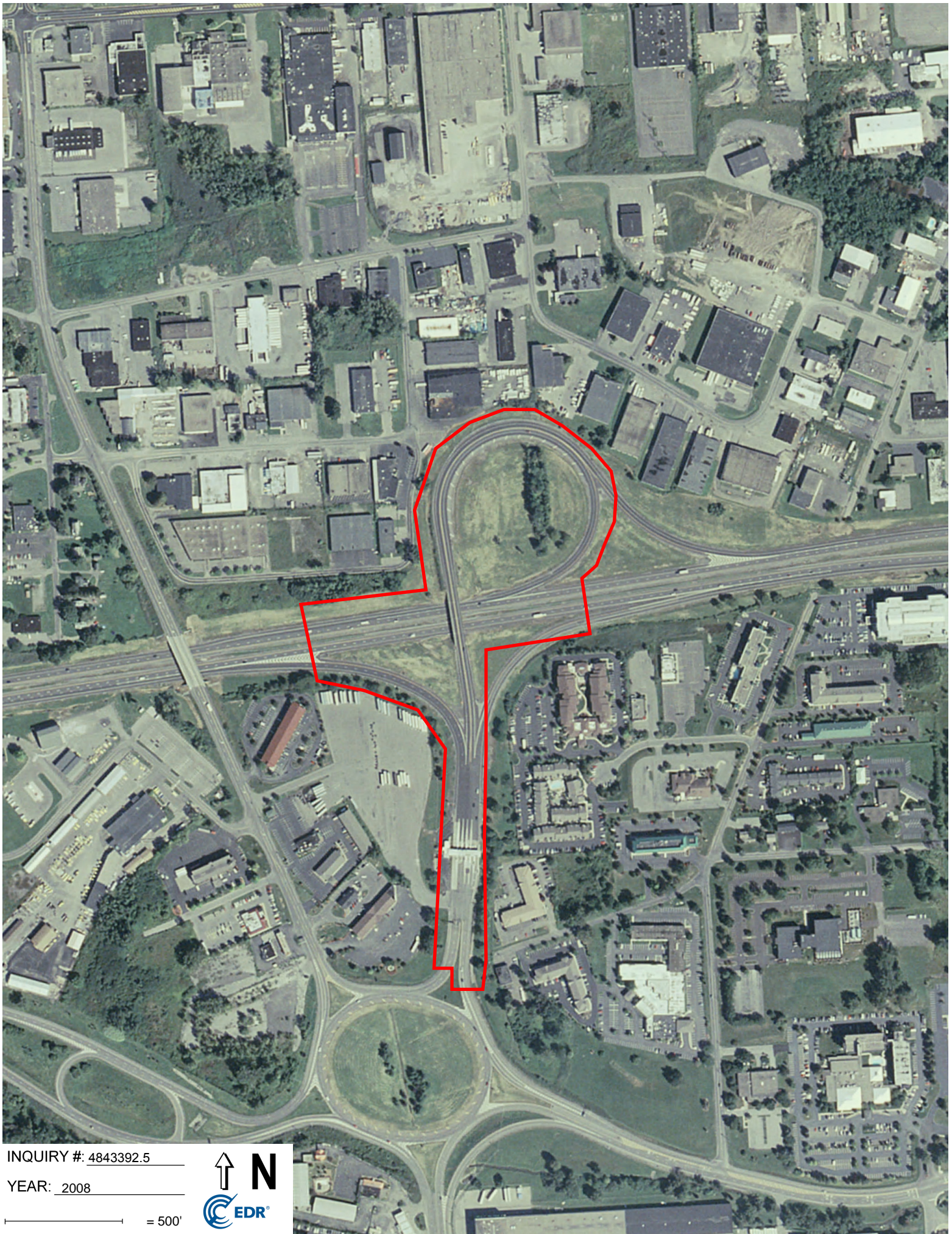


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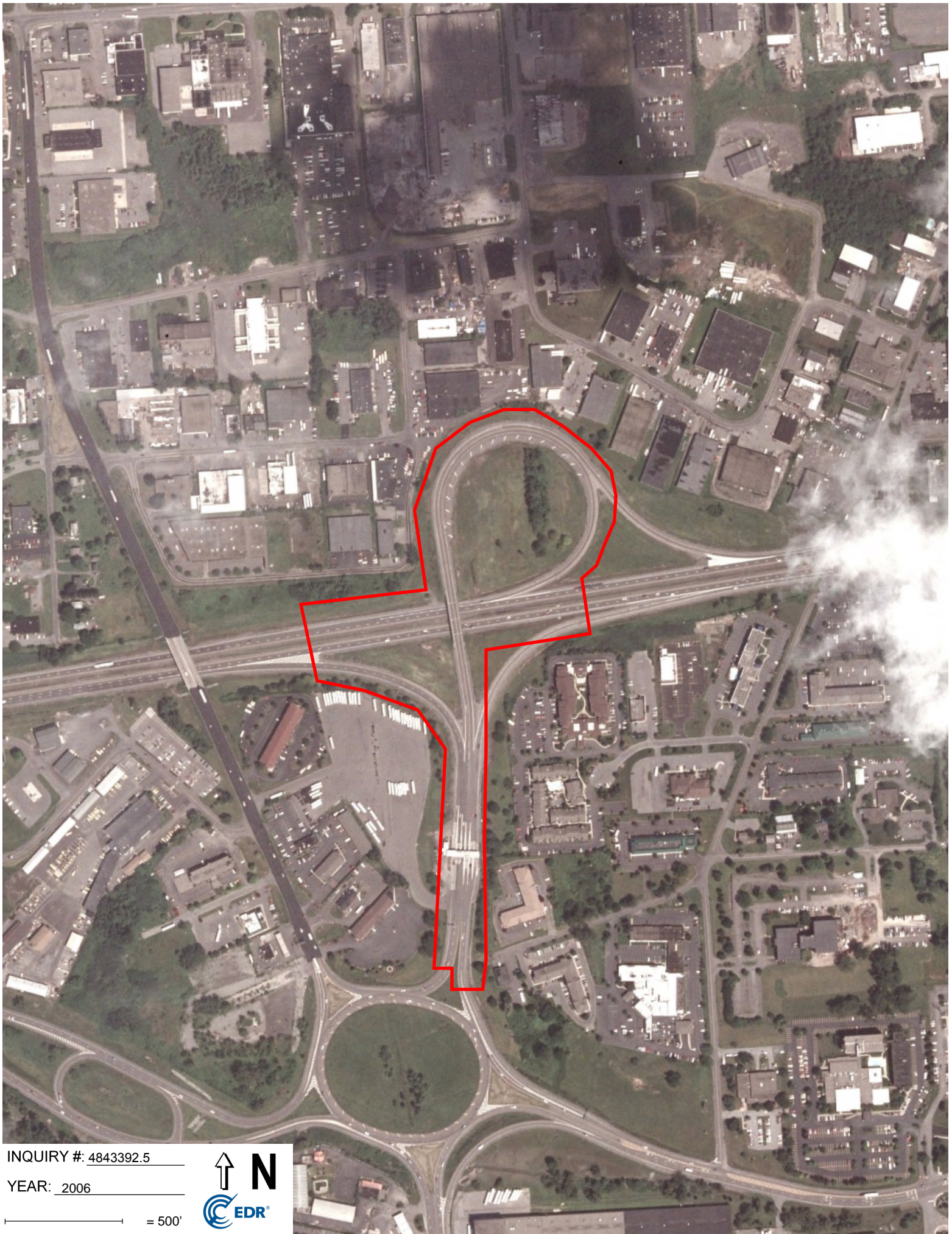


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YEAR: 2008

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YEAR: 2006

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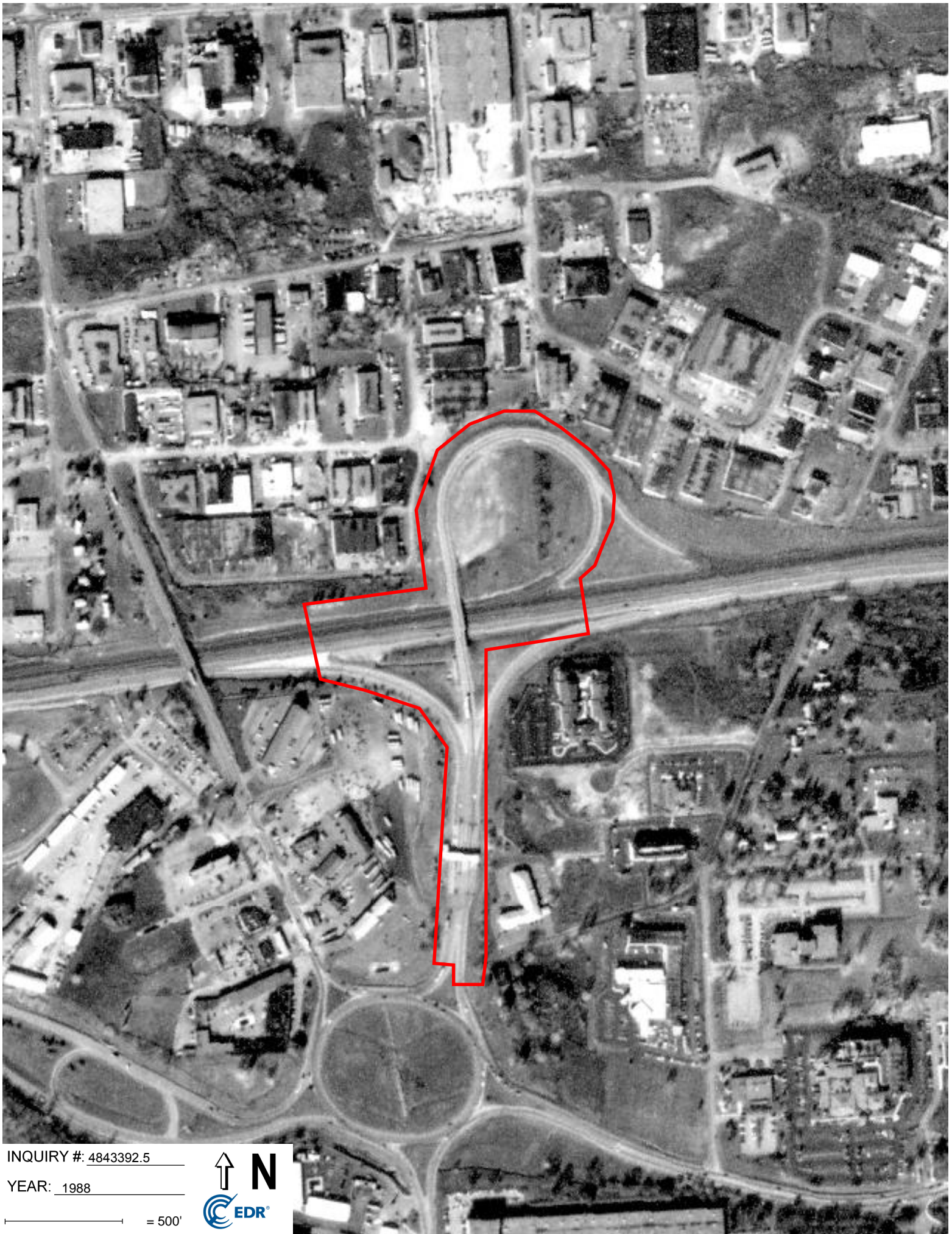


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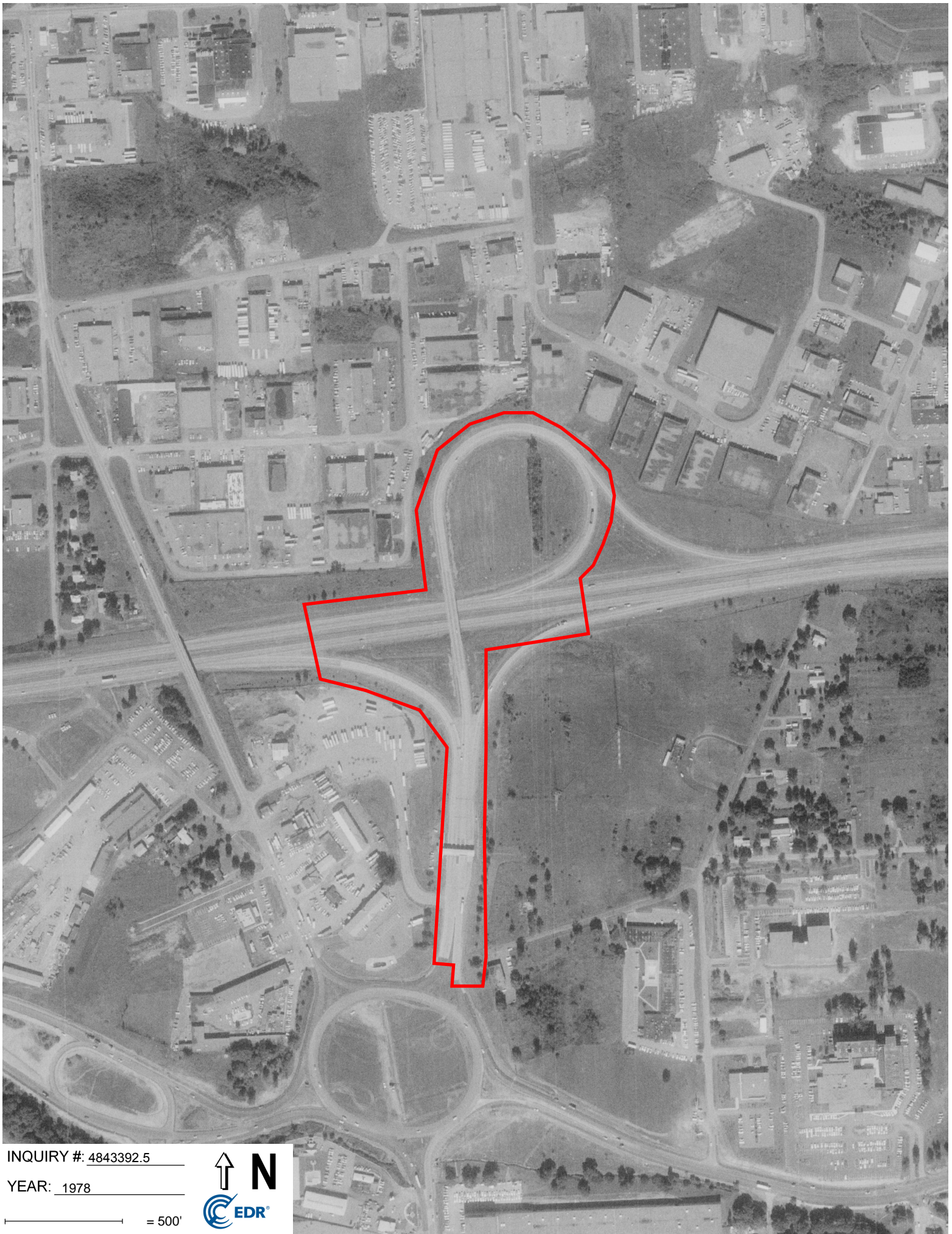


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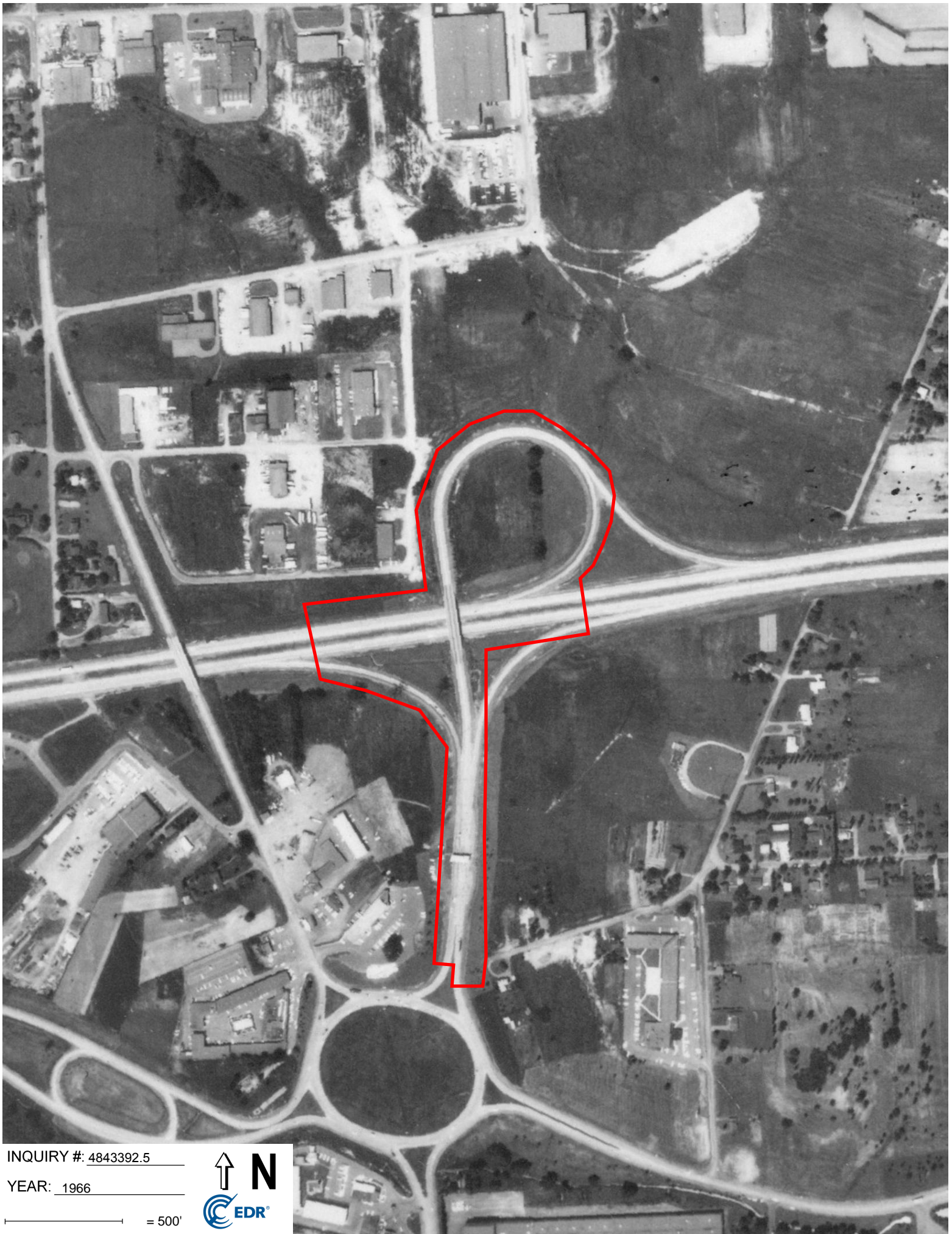


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YEAR: 1978

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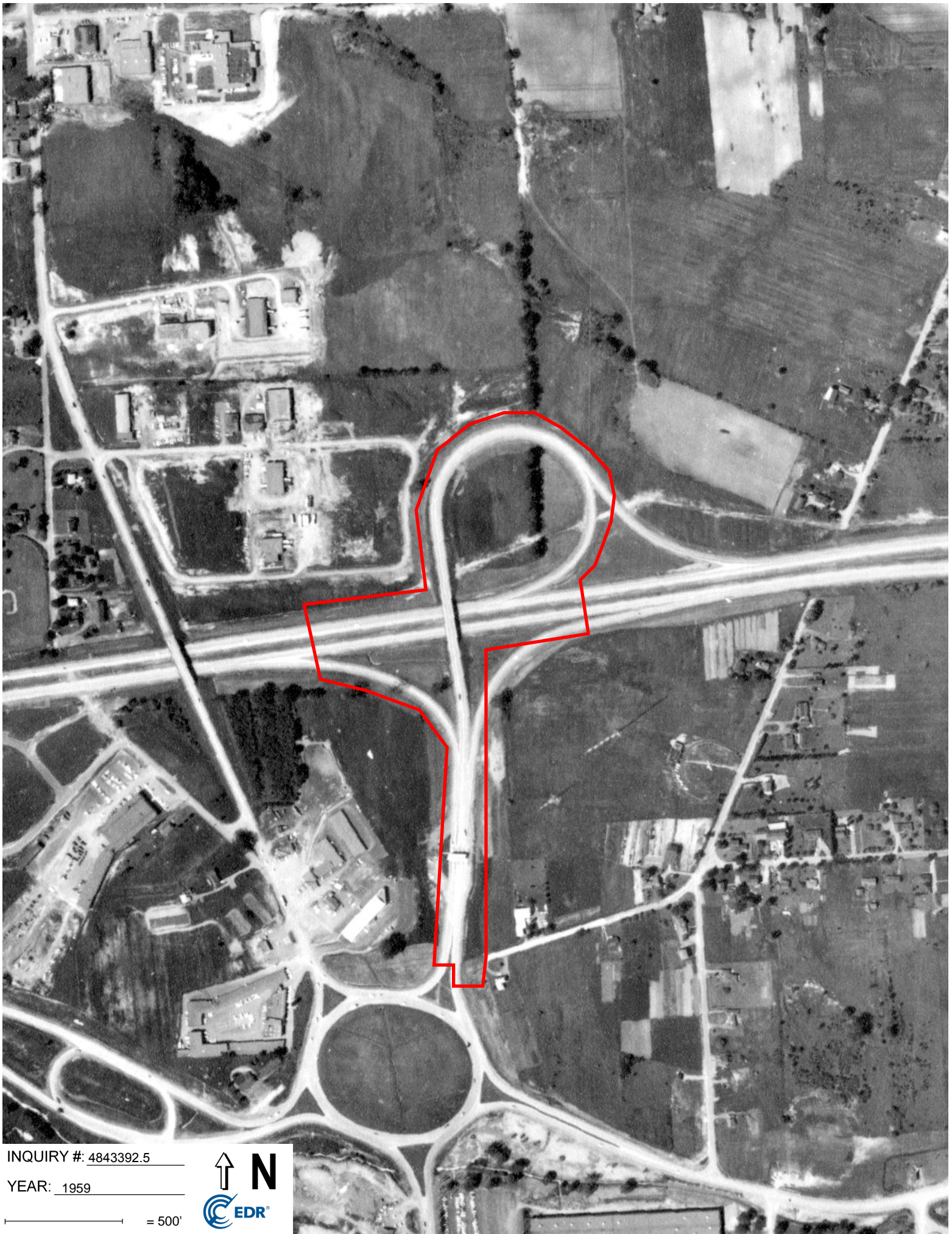


INQUIRY #: 4843392.5

YEAR: 1966

— = 500'



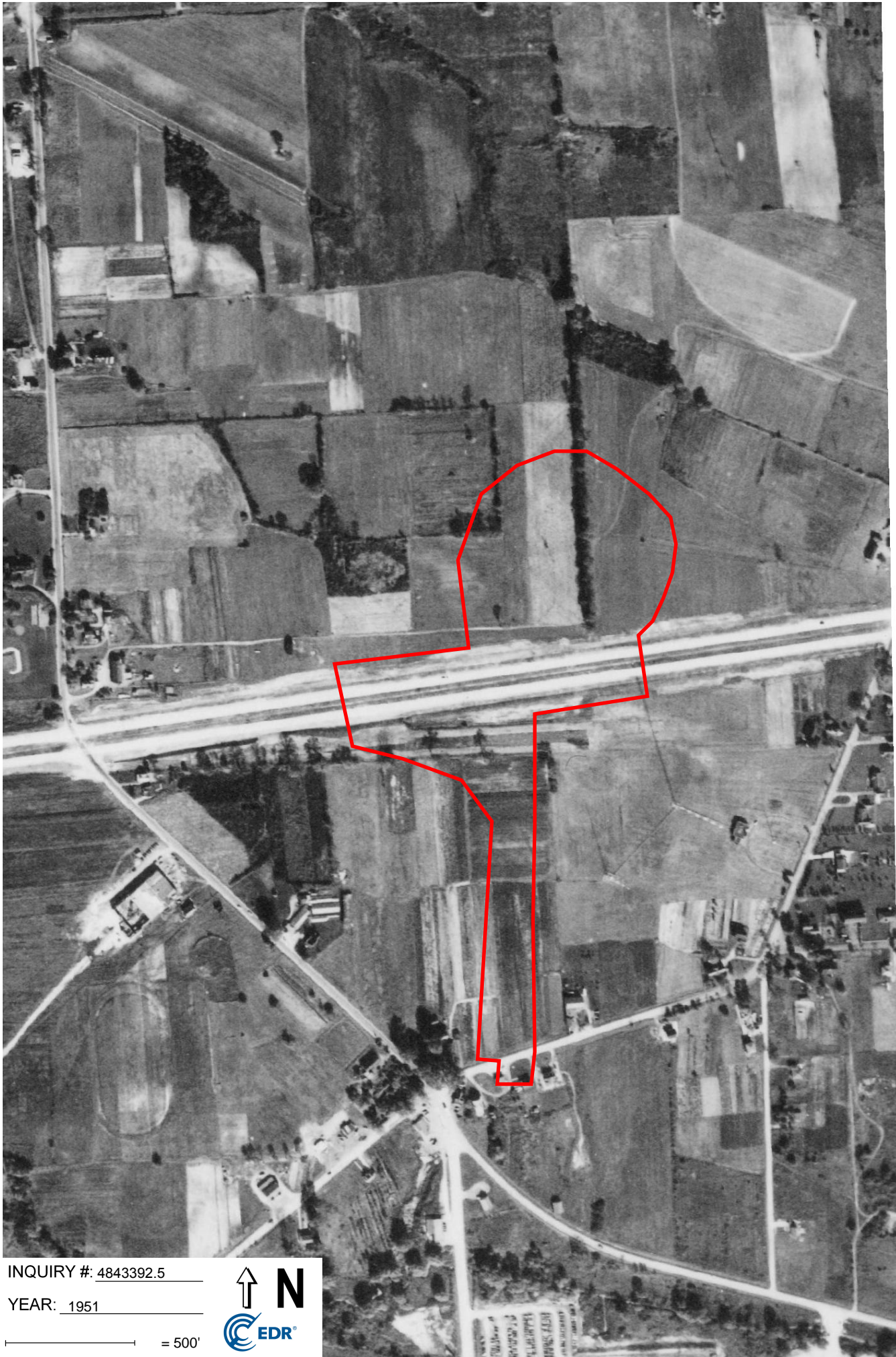


INQUIRY #: 4843392.5

YEAR: 1959

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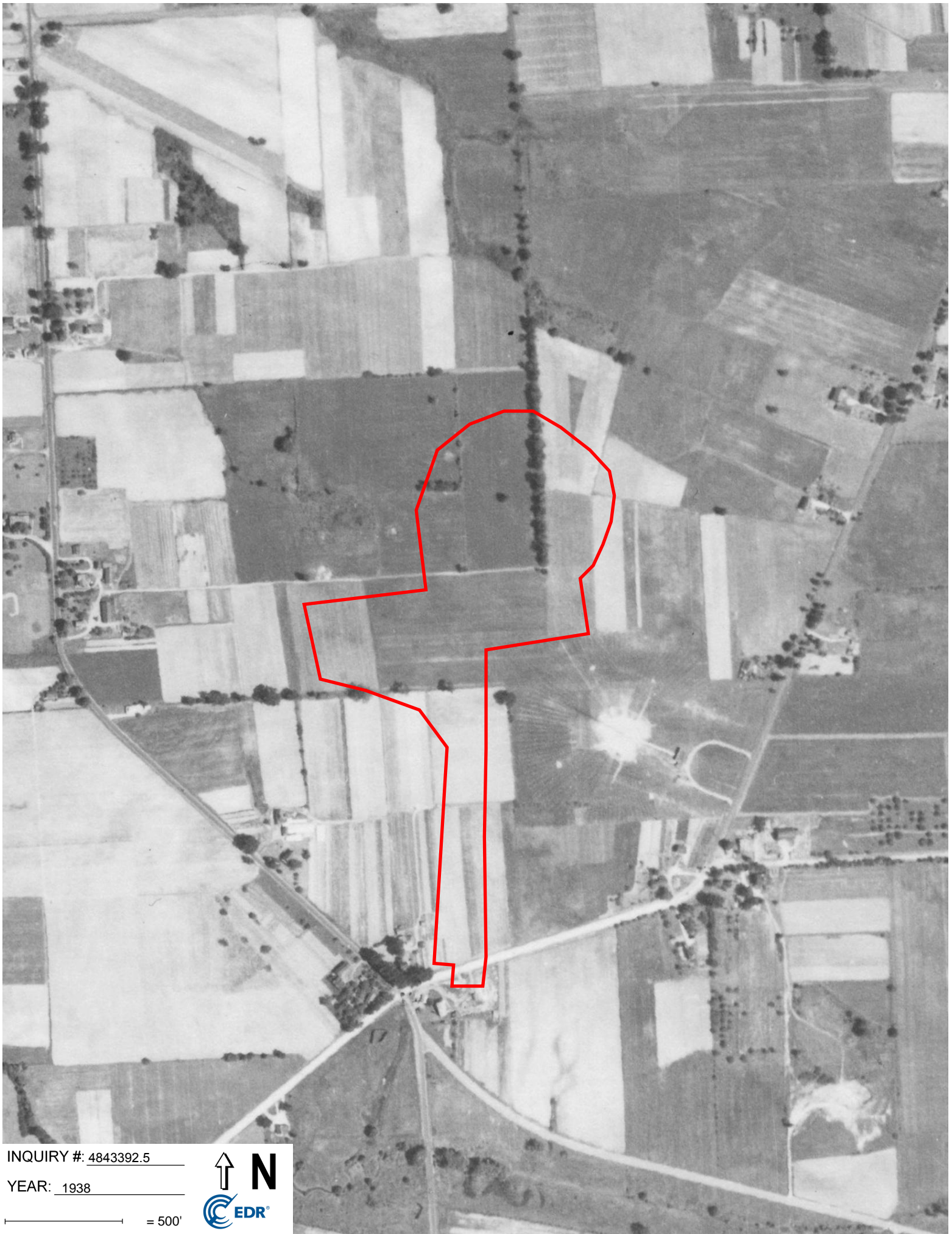


INQUIRY #: 4843392.5

YEAR: 1951

— = 500'





INQUIRY #: 4843392.5

YEAR: 1938

— = 500'



Attachment D:
Photograph Locations





Replacement of Syracuse Division Bridges

**MP 278.93:
Exit 35 Ramp
(BIN 5510090)**

Village of East Syracuse
Onondaga County
New York

Attachment D: Photograph Locations

February 2017

-  Photograph Location
 Area of Potential Effect

Notes:
1. Basemap: ESRI ArcGIS "World Imagery" online map database.
2. This is a color graphic. Reproduction in grayscale may misrepresent the data.

Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AEX, Getmapping, Aerogrid, IGN, IGP, swisstopo, and the GIS User Community

Attachment E:
Photographs



Photo 1

View of the Exit 35 interchange showing cut-and-fill disturbance. From I-90 west bound off-ramp shoulder, view to the southwest.



Photo 2

View of the Exit 35 interchange showing cut-and-fill disturbance and grading. From I-90 west bound off-ramp shoulder, view to the south.

Replacement of Syracuse Division Bridges

MP 278.93: Exit 35 Ramp (BIN 5510090)

Village of East Syracuse, Onondaga County, New York

Attachment E: Photographs

Sheet 1 of 2



Photo 3

View of the Exit 35 interchange showing cut-and-fill disturbance. From toll booth intersection lanes, view to the north.

Replacement of Syracuse Division Bridges

MP 278.93: Exit 35 Ramp (BIN 5510090)

Village of East Syracuse, Onondaga County, New York

Attachment E: Photographs

Sheet 2 of 2