



TYPE OF CONSTRUCTION:
STEEL REPAIRS AND MISCELLANEOUS BRIDGE PAINTING.

STANDARD SHEETS:

THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY NYSOT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEETS LISTED ON DWG. SS-1 UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

THE LATEST REVISIONS OF THE NYSTA STANDARD SHEETS MAINTAINED BY THE AUTHORITY, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEETS UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS. SEE DWG. SS-1 FOR LISTING.

INDEX OF DRAWINGS		
DWG. NO.	TITLE OF DWG.	SHT. NO.
TITLE	TITLE SHEET	1
GN-1	GENERAL NOTES (SHEET 1 OF 2)	2
GN-2	GENERAL NOTES (SHEET 2 OF 2)	3
SS-1	STANDARD SHEETS	4
WZTC-1	MAINLINE WORK ZONE TRAFFIC CONTROL PLAN (SHOULDERS) (SHEET 1 OF 2)	5
WZTC-2	MAINLINE WORK ZONE TRAFFIC CONTROL PLAN (SHOULDERS) (SHEET 2 OF 2)	6
LP-1	REPAIR LOCATION PLAN	7
RD-1	REPAIR DETAILS (SHEET 1 OF 2)	8

ALBANY DIVISION
PLANS FOR
STEEL REPAIRS & MISCELLANEOUS PAINTING
OF THE
ROUTE 295 BRIDGE
AT
MILEPOST B16.26
IN
COLUMBIA COUNTY

TABS 20-9B D214785
8 SHEETS BIN 1045110

BRIDGE MAINTENANCE GUIDELINES
UPON COMPLETION OF THIS PROJECT, THE BRIDGE STRUCTURES REPAIRED, REHABILITATED OR RECONSTRUCTED HEREUNDER SHALL BE MAINTAINED IN ACCORDANCE WITH THE CURRENT AASHTO MANUAL FOR BRIDGE MAINTENANCE, AND THE NEW YORK STATE THRUWAY AUTHORITY MAINTENANCE DIRECTIVES:
BRIDGE MANAGEMENT PROGRAM MD 05-16, BRIDGE MANAGEMENT ACTIVITIES MD 05-3, AND BRIDGE INSPECTION PROGRAM MD 95-5.
SPECIAL MAINTENANCE REQUIREMENTS: NONE

NOTES:

ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (US CUSTOMARY) REFERENCED IN THE CONTRACT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR CHANGES SET FORTH IN THE CONTRACT "PROPOSAL".

MAINTENANCE JURISDICTION

100% MUNICIPALITY

APPROACH GUIDE RAILING, APPROACH PAVEMENT, SHOULDERS, SIDEWALKS, APPROACH SLAB AND BRIDGE DECK WEARING COURSE, AND BRIDGE RAIL/BARRIER/PARAPET.

100% AUTHORITY

ALL OTHER BRIDGE ELEMENTS NOT LISTED ABOVE.

	PROJECT LIMITS		CONTRACT LIMITS	
	FROM STA.	TO STA.	FROM STA.	TO STA.
ROUTE 295 (M.P. B16.26)	17+28.69	21+17.03	17+18.69	21+27.03
BERKSHIRE CONNECTOR (I-90)	998+90	1001+10	WZTC LIMITS	

DIG SAFELY NEW YORK
UNDERGROUND FACILITIES PROTECTION ORGANIZATION
CALL 811

NOTE:

MINOR STEEL REPAIRS. NO CHANGE TO CURRENT LOAD RATING.



REVIEWED BY: David T. Vorburg

PROJECT MANAGER: Brian Powell

BUREAU DIRECTOR: David T. Vorburg

CONTRACTOR'S NAME: _____
 AWARD DATE: _____
 COMPLETION DATE: _____
 FINAL ACCEPTANCE DATE: _____
 INSPECTION FIRM'S NAME: _____
 RESIDENT ENG./E/C: _____
 FINAL COST TOTAL: _____

FISCAL SHARE	COST(S)

INSPECTION FIRM CONSULTANT STAMP: _____

RECOMMENDED BY: *[Signature]* 8/31/20
 DIRECTOR, OFFICE OF DESIGN DATE

RECOMMENDED BY: *[Signature]* 9/2/2020
 DIVISION DIRECTOR DATE

RECOMMENDED BY: *[Signature]* 9/2/20
 TRAFFIC ENGINEER DATE

RECOMMENDED BY: *[Signature]* 9/2/20
 DIRECTOR, OFFICE OF CONSTRUCTION MANAGEMENT DATE

RECOMMENDED BY: *[Signature]* 9/3/20
 DIRECTOR OF MAINTENANCE AND OPERATIONS DATE

APPROVED BY: *[Signature]* 9/4/2020
 CHIEF ENGINEER DATE

GENERAL NOTES

- DESIGN SPECIFICATIONS; AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES (17TH EDITION), AND AS AMENDED BY NEW YORK STATE DEPARTMENT OF TRANSPORTATION BLUE PAGES AND THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION BRIDGE MANUAL, 2019 U.S. CUSTOMARY EDITION.
- CONSTRUCTION AND MATERIAL SPECIFICATIONS; ALL WORK CONTEMPLATED UNDER THIS CONTRACT AS SHOWN ON THESE PLANS AND/OR SPECIFICATIONS AND THE ACCOMPANYING PROPOSAL IS TO BE IN CONFORMITY WITH "STANDARD SPECIFICATIONS" (U.S. CUSTOMARY UNITS), NEW YORK STATE DEPARTMENT OF TRANSPORTATION OFFICE OF ENGINEERING, CURRENT VERSION, WITH ALL CURRENT ADDENDA, & ALL REVISIONS ISSUED BY NYS DOT ENGINEERING BULLETINS, DIRECTIVES OR INSTRUCTIONS BEFORE THE FIRST PUBLISH DATE OF THE ADVERTISEMENT FOR THIS CONTRACT, EXCEPT AS AMENDED ON THESE PLANS AND/OR IN THE SPECIFICATIONS IN THE PROJECT PROPOSAL.
- THE CONTRACTOR SHALL EXAMINE AND VERIFY IN THE FIELD, ALL EXISTING CONDITIONS & DIMENSIONS WITH THOSE SHOWN ON THE PLANS, IF FIELD CONDITIONS & DIMENSIONS DIFFER FROM THOSE SHOWN ON THE PLANS, THE CONTRACTOR SHALL USE THE FIELD CONDITIONS & DIMENSIONS AND MAKE THE APPROPRIATE CHANGES TO THOSE SHOWN ON THE PLANS AS APPROVED BY THE ENGINEER.
- THE CONTRACTOR SHOULD NOTE THAT ADDITIONAL WORK MAY BE REQUIRED AS THE CONTRACT PROGRESSES WHICH IS NOT SHOWN OR NOTED ON THE PLANS. THIS WORK SHALL BE PERFORMED BY THE CONTRACTOR AS ORDERED BY THE ENGINEER AND PAYMENT SHALL BE MADE AT THE UNIT BID PRICE FOR THE APPROPRIATE ITEMS.
- THE CONTRACTOR SHALL EXERCISE CAUTION AND PERFORM ALL WORK WITH CARE SO THAT ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR BE RE-USED, OR WHICH ARE TO REMAIN THE PROPERTY OF THE THRUWAY AUTHORITY WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN IN PLACE, OR WHICH ARE TO REMAIN THE PROPERTY OF THE THRUWAY AUTHORITY, THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR, WITH NO ADDITIONAL COST TO THE AUTHORITY.
- THE CONTRACTOR WILL BE HELD RESPONSIBLE FOR ALL DAMAGES CAUSED BY HIS OPERATIONS TO THE EXISTING ROADWAYS WHICH ARE NOT INCLUDED AS PART OF THE INTENDED WORK. ALL DAMAGE TO EXISTING ROADWAYS WHICH ARE NOT PART OF THE INTENDED WORK SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER, WITHOUT COST TO THE THRUWAY AUTHORITY.
- ALL WORK SHALL BE LIMITED TO WITHIN THE RIGHT OF WAY, EASEMENTS, OR LIMITS OF RELEASE.
- WHENEVER ITEMS IN THE CONTRACT REQUIRE MATERIALS TO BE REMOVED AND DISPOSED OF, THE COST OF SUPPLYING A DISPOSAL AREA AND TRANSPORTATION TO THAT AREA SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THOSE ITEMS.
- THIS BRIDGE SHALL BE MAINTAINED IN ACCORDANCE WITH THE GUIDELINES CONTAINED IN THE CURRENT EDITION OF THE AASHTO MAINTENANCE MANUAL FOR ROADWAYS AND BRIDGES.
- RECORD PLANS FOR THE EXISTING BRIDGE WILL BE AVAILABLE ON THE THRUWAY AUTHORITY WEBSITE DURING BIDDING PHASE.
- NO DIRECT PAYMENT SHALL BE MADE FOR WORK INDICATED ON THE DRAWINGS OR WITHIN THE SPECIFICATIONS FOR WHICH NO ITEM NUMBER IS INDICATED, THE COST FOR SUCH WORK SHALL BE INCLUDED IN THE VARIOUS ITEMS OF THE CONTRACT.
- THE CONTRACTOR IS ADVISED THAT ADDITIONAL "NOTES" WILL BE FOUND ON SUBSEQUENT SHEETS OF THE CONTRACT PLANS AND SUCH "NOTES", WHILE PERTAINING TO THE SPECIFIC DRAWINGS THEY ARE PLACED ON, ALSO SUPPLEMENT THE GENERAL NOTES LISTED HEREIN.
- THE CONTRACTOR IS REQUIRED TO PROTECT HIS WORKERS IN ACCORDANCE WITH SECTION 107-05 OF THE NYS DOT STANDARD SPECIFICATIONS AND SHALL SUBMIT HEALTH AND SAFETY PLAN TO THE ENGINEER PRIOR TO COMMENCING WORK.

STEEL & FABRICATION NOTES

- ALL STEEL FABRICATION IS GOVERNED BY, AND SHALL MEET THE REQUIREMENTS OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL, 4TH EDITION (JANUARY 2018) AND SECTION 564 OF THE STANDARD SPECIFICATIONS EXCEPT AS DIRECTED WITHIN THESE PLANS OR AS ORDERED BY THE ENGINEER.
- SHOP DRAWINGS SHALL BE SUBMITTED IN ENGLISH UNITS OF MEASURE AND IN ACCORDANCE WITH NYS SCM. THE WEIGHTS OF ALL COMPONENTS SHALL BE INDICATED ON THE BILL OF MATERIALS WITHIN THE SHOP DRAWINGS.
- NEW STRUCTURAL STEEL: STRUCTURAL STEEL PLATES OR SHAPES SHALL BE ASTM A709 GRADE 50 KSI.
- NO CHARPY V-NOTCH CERTIFICATION IS REQUIRED FOR ANY OF THE NEW STEEL COMPONENTS.
- TEMPORARY STEEL IF REQUIRED, MAY BE 36 KSI OR 50 KSI STEEL.
- THE COST OF ALL STRUCTURAL STEEL REMOVAL & DISPOSAL, TORCH CUTTING, ANY LOCALIZED PAINT STRIPPING, WELD REMOVAL AND ALL ACCESS REQUIRED SHALL BE INCLUDED IN THE STEEL REPAIR.
- WHERE HOLES ARE INDICATED, CONNECTIONS SHALL BE MADE WITH 7/8"Ø HIGH-STRENGTH BOLTS UNLESS OTHERWISE SPECIFIED. ALL BOLTS SHALL BE ASTM A325 TYPE 1 (HDG), WITH A563 DH HEAVY HEX NUTS AND (2) - F436 HARDENED WASHERS. ALL HOLES ARE 15/16"Ø U.N.O.
- ALL NUTS, BOLTS, AND WASHERS SHALL BE HOT DIP GALVANIZED IN ACCORDANCE WITH THE SPECIFICATIONS. ALL BOLTS SHALL BE FULLY TENSIONED IN ACCORDANCE WITH NYSSCM.
- THE COST FOR FIELD DRILLING & INSTALLATION OF ALL BOLTS, NUTS, WASHERS, WELDS AND MISC. SHIM/FILLPLATES SHALL BE INCLUDED IN THE STEEL REPAIR ITEM.
- NEW HOLES IN EXISTING MATERIAL SHALL BE DRILLED USING THE NEW PIECE AS THE TEMPLATE.
- ANY INCIDENTAL FIELD MODIFICATIONS, FIELD TRIMMING, RETROFITS, SHIMMING OR ADJUSTMENTS TO THE EXISTING OR NEW STEEL (EG; COPEs, CUTS, CLIPS, REAMING) NECESSARY TO ENSURE PROPER FIT-UP OF ALL NEW STEEL COMPONENTS SHALL BE INCLUDED IN THE STEEL REPAIR ITEM.
- ADDITIONAL NOTES ARE INCLUDED IN THE APPLICABLE STEEL INSTALLATION OR STEEL REPAIR DRAWINGS.
- ALL NEW STEEL COMPONENTS SHALL BE OF DOMESTIC ORIGIN. THIS SHALL INCLUDE STRUCTURAL STEEL SHAPES, PLATES, FASTENERS, ETC.

PAINTING NOTES

- 4 FEET OF ALL BEAM ENDS SHALL BE BLAST CLEANED AND PAINTED.
- THE COST FOR ALL CLEANING AND SURFACE PREPARATION OF THE NEW AND EXISTING STEEL AND THE COST OF TOUCH-UP PAINTING OF BOTH THE NEW & EXISTING STEEL SHALL BE INCLUDED IN THE UNIT PRICE FOR THE APPLICABLE STEEL REPAIR ITEM.
- PAINT SYSTEM MUST BE FROM NYS DOT APPROVED LIST, AND APPROVED BY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH THE MANUFACTURER'S CERTIFICATION STATING THAT THE PAINT MEETS THE ABOVE REQUIREMENTS.
- ALL COATINGS SHALL BE APPLIED IN A NEAT, WORKMANLIKE MANNER. COATINGS SHALL BE APPLIED UNIFORMLY AND SHALL BE FREE OF RUNS, SAGS, DRIPS, OR OTHER DEFECTS. THE INTERMEDIATE EPOXY COAT AND THE URETHANE FINISH COAT MAY BE SHOP-APPLIED BY BRUSH, ROLLER OR SPRAY. THE INORGANIC ZINC PRIMER SHALL BE APPLIED BY SPRAY ONLY.
- GALVANIZED BOLTS USED FOR THE STRUCTURAL STEEL REPAIRS WILL BE REQUIRED TO BE PAINTED IN FIELD.
- ALONG ANY CUT LINE, AND AT ALL FIELD WELDED REPAIRS, THE EXISTING PAINTED SURFACES SHALL BE CLEANED & PREPARED IN ACCORDANCE WITH SSPC SP 11 "POWER TOOL CLEANING TO BARE METAL" WITHIN 3 INCHES ON EACH SIDE OF THE CUT LINE, AND THEN PRIMED AFTER WITH AN APPROVED ORGANIC ZINC-RICH PRIMER.
- WHERE NEW MATERIAL IS TO BE BOLTED TO EXISTING MATERIAL, THE EXISTING CONTACT SURFACE SHALL BE CLEANED PER THE REQUIREMENTS OF SSPC SP 11 "POWER TOOL CLEANING TO BARE METAL." THE CONTACT SURFACES SHALL BE COATED WITH THE ORGANIC ZINC-RICH PRIMER PRIOR TO INSTALLATION OF NEW MATERIAL. WHERE NECESSARY, TARPS AND SCREENS WILL BE REQUIRED DURING PAINT REMOVAL OPERATION AND LARGE RUST SCALE WILL NOT BE PERMITTED TO BE DROPPED ON THE ROADWAY OR GROUND BELOW.
- FINISH COLOR : (SAGE GREEN), MUNSELL *7.5 GY 5/4.
- TEMPORARY COMPONENTS, IF REQUIRED, HAVE NO CLEANING OR PAINTING REQUIREMENTS.

LEAD PAINT NOTES

- THE CONTRACTOR IS STILL REQUIRED TO USE VACUUM SHROUDED TOOLS TO REDUCE EMISSIONS TO A MINIMUM. THE CONTRACTOR SHALL USE TARPS OR OTHER MEANS TO CONTAIN, AND MINIMIZE EMISSION OF PAINT DUST TO BE PAID UNDER ITEM 570.150001.
- LIMITS AND METHODS FOR REMOVAL OF PAINT AT LOCATIONS OF FASTENER REMOVAL OR FLAME CUTTING SHALL BE AS DESCRIBED IN SUBSECTIONS 202-3.05 AND 574 OF THE STANDARD SPECIFICATIONS. THE COST OF ANY PAINT REMOVAL OR PAINT STRIPPING REQUIRED SHALL BE INCLUDED IN UNIT PRICE OF THE APPLICABLE STRUCTURAL STEEL REPAIR ITEM. PAINT WASTE SHALL BE COLLECTED FOLLOWING THE REQUIREMENTS OF THE ENVIRONMENTAL GROUND PROTECTION, AND PAINT WASTE SHALL BE DISPOSED OF FOLLOWING THE REQUIREMENTS OF SECTION 571 - DISPOSAL OF PAINT REMOVAL WASTE.
- DISPOSAL OF PAINT WASTE PAID FOR UNDER ITEM 571.03.

MAINTENANCE & PROTECTION OF TRAFFIC

- WORK ZONE TRAFFIC CONTROL IS TO BE PROVIDED BY THE CONTRACTOR, UNLESS OTHERWISE DIRECTED BY THE ENGINEER IN CHARGE.
- SEE DWG'S WZTC-1 AND WZTC-2 FOR APPROPRIATE SITE SPECIFIC WORK ZONE TRAFFIC CONTROL SIGNAGE.
- LANE CLOSURES SHALL BE REQUESTED & SCHEDULED WITH ALBANY DIVISION TRAFFIC AT LEAST ONE WEEK IN ADVANCE OF ANY PROPOSED LANE CLOSURES.

CONSTRUCTION DETAILS

- ALL REPAIR WORK SHALL BE PERFORMED TO THE STRUCTURAL STEEL AS LOCATED AND/OR SHOWN IN THE CONTRACT DOCUMENTS AND ON THE APPROVED REPAIR DRAWINGS.
- CERTAIN SPECIALTY WORK MAY REQUIRE SPECIFIC PERSONNEL QUALIFICATION AS IDENTIFIED IN THE CONTRACT DOCUMENTS.
- PRIOR TO THE START OF WORK, ANY LOOSE OR DAMAGED CONCRETE WHICH MAY POSE A HAZARD SHALL BE REMOVED TO THE SATISFACTION OF THE ENGINEER.
- REPAIR OPERATIONS SHALL NOT BEGIN UNTIL ALL BLAST-CLEANED AREAS HAVE BEEN VISUALLY INSPECTED AND TESTED USING MAGNETIC PARTICLE INSPECTION TO IDENTIFY ANY CRACKS. CRACKS FOUND SHALL BE REPORTED TO THE METALS ENGINEERING UNIT FOR DISPOSITION.
- AT THE END OF DAILY WORK OPERATIONS, ALL EQUIPMENT OVER HEAD AND ADJACENT TO TRAVEL LANES SHALL BE REMOVED FROM THE STRUCTURE TO THE SATISFACTION OF THE ENGINEER. THIS SHALL INCLUDE, BUT ARE NOT LIMITED TO, THE REMOVAL OF JACKING DEVICES, TEMPORARY BRACING, COME-ALONGS, STEEL SHIMS, WOOD BLOCKING, BEAM CLAMPS, DRILLING EQUIPMENT, LOOSE MISCELLANEOUS IRON, FASTENERS, ETC.

BOLTED CONNECTIONS

- ALL BOLTED CONNECTIONS SHALL BE FRICTION TYPE. PRIOR TO ASSEMBLY, INNER PLIES SHALL BE FREE OF OIL, GREASE, LOOSE SCALE, BURRS, DIRT, PAINT AND OTHER FOREIGN MATERIAL THAT WILL PREVENT THE SOLID SEATING OF THE PARTS. THE INNER LAYER SHALL BE FILLED WITH CARBOLINE CARBOGUARD A-788, INSTALLED PER THE MANUFACTURERS RECOMMENDATIONS.
- BOLT HOLES SHALL BE 1/16" LARGER IN DIAMETER THAN THE BOLT DIAMETER.
- WHENEVER EXISTING HOLES IN STEEL MEMBERS ARE USED AS A TEMPLATE FOR THE DRILLING OF NEW HOLES, THE NEW BOLT HOLES SHALL BE MADE USING HOUGEN- OR JANCY-TYPE DRILLS. TWIST DRILLS WILL NOT BE ALLOWED.
- BOLTING AND BOLT TENSION VERIFICATION SHALL BE IN ACCORDANCE WITH SECTION 10 OF THE NYSSCM.

WELDING

- ALL WELDING SHALL BE PERFORMED BY NEW-YORK-STATE-CERTIFIED WELDERS/WELDING OPERATORS QUALIFIED TO PROCESS AND POSITION IN ACCORDANCE WITH SECTION 8 OF THE NYSSCM. ALL COSTS ASSOCIATED WITH WELDER/WELDING OPERATOR QUALIFICATION SHALL BE BORNE BY THE CONTRACTOR.
- TEMPERATURE-INDICATING CRAYONS SHALL BE IN THE POSSESSION OF ALL WELDERS WHILE PERFORMING WORK.
- THE INSTALLATION OF TEMPORARY ATTACHMENTS, TEMPORARY WELDS AND/OR TACK WELDS NOT TO BE INCORPORATED INTO PERMANENT WELDS WILL NOT BE ALLOWED UNLESS SPECIFICALLY APPROVED. ALL UNAUTHORIZED WELDING WILL BE CAUSE FOR THE REJECTION OF THE AFFECTED MATERIAL. THE COST TO REPAIR OR REPLACE THE REJECTED MATERIAL SHALL BE BORNE BY THE CONTRACTOR.
- AFTER ALL REQUIRED REPAIRS ARE COMPLETE, A MAGNETIC PARTICLE INSPECTION SHALL BE PERFORMED ON AND WITHIN 12" OF ANY AREAS THAT WERE HEATED, STRAIGHTENED, GROUND TO REMOVE SCRAPES AND GOUGES, OR WELDED.

CHECKED BY: B. MUSCANELL

DRAFTED BY: D. HOLLINER

CHECKED BY: D. VOSBURGH

DESIGNED BY: B. MUSCANELL

DESIGN SUPERVISOR: J. ADAMCZAK

ALTERED ON:	AFFIXED ON: 9/2/20
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 Thruway Authority	TITLE OF PROJECT STEEL REPAIRS OF ROUTE 295 BRIDGE OVER THRUWAY	CONTRACT NUMBER: TABS 20-9B
	LOCATION OF PROJECT ALBANY DIVISION MP B16.26	DATE: 08/28/2020
TITLE OF DRAWING GENERAL NOTES (SHEET 1 OF 2)	DRAWING NUMBER: GN-1	

EVALUATING, REMOVING AND INSTALLING REPLACEMENT DIAPHRAGMS AND CONNECTION PLATES

- WHEN AN EXISTING DIAPHRAGM AND/OR CONNECTION PLATE IS TO BE EVALUATED FOR REUSE, THEY SHALL BE EVALUATED AS FOLLOWS:

REMOVE PAINT AND DEBRIS FROM WITHIN 6" OF THE DIAPHRAGM CONNECTION BOLTS AND/OR CONNECTION PLATE AND 12" ON EITHER SIDE OF THE CONNECTION PLATE FILLET WELDS.

CAREFULLY REMOVE THE EXISTING FASTENERS WITHOUT CAUSING ANY DAMAGE TO THE DIAPHRAGM OR CONNECTION PLATE.

A VISUAL INSPECTION AND MAGNETIC PARTICLE TEST OF THE CLEANED AREAS SHALL BE PERFORMED TO IDENTIFY DAMAGE. AREAS OF INTEREST SHALL INCLUDE COMPONENT SURFACES, BOLT HOLES AND CONNECTION PLATE FILLET WELDS.

ALL REJECTABLE CONDITIONS AND INDICATIONS SHALL BE REPORTED TO THE NYS THRUWAY AUTHORITY METALS ENGINEERING UNIT FOR DISPOSITION.

- WHEN AN EXISTING DIAPHRAGM AND/OR CONNECTION PLATE IS TO BE REMOVED AND REPLACED, PAINT AND DEBRIS SHALL BE REMOVED FROM THE CONNECTION PLATE AND 12" ON EITHER SIDE OF THE CONNECTION PLATE WELDS AND ON BOTH SIDES OF THE GIRDER.

REMOVAL MAY BE BY AIR CARBON ARC GOUGING OR FLAME CUTTING.

CONNECTION PLATES SHALL BE REMOVED BY CUTTING TO WITHIN 1/8" MINIMUM OF ADJACENT BASE METAL. THE REMAINING WELD AND CONNECTION PLATE BASE METAL SHALL BE REMOVED BY GRINDING FLUSH WITH ADJACENT BASE METAL.

A VISUAL INSPECTION AND MAGNETIC PARTICLE TEST OF THE WELD REMOVAL AREA AND ADJACENT BASE METAL SHALL BE PERFORMED.

ALL REJECTABLE INDICATIONS SHALL BE REPORTED TO THE NYS THRUWAY AUTHORITY METALS ENGINEERING UNIT FOR DISPOSITION.

- UPON COMPLETION OF ALL REPAIR PROCEDURES AND ALL ASSOCIATED NONDESTRUCTIVE TESTING, INSTALL REPLACEMENT DIAPHRAGMS AND CONNECTION PLATES IN ACCORDANCE WITH DETAILS PROVIDED IN THE CONTRACT DOCUMENTS.

REPAIR OF NICKS, SCRAPES AND GOUGES

- NICKS, SCRAPES AND GOUGES IN COMPONENTS OF STEEL MEMBERS MAY BE REPAIRED BY GRINDING TO A SLOPE OF 1 ON 10, AND BLENDING IN SMOOTHLY TO ADJACENT BASE METAL, PROVIDING:

- THE THICKNESS OF THE MATERIAL IS NOT REDUCED BY MORE THAN 20% OF THE ORIGINAL NOMINAL THICKNESS.
- THE WIDTH OF THE FLANGE OR COVER PLATE IS NOT REDUCED BY MORE THAN 5% OF THE ORIGINAL NOMINAL WIDTH.
- THE GOUGE DOES NOT REDUCE THE NOMINAL CROSS-SECTIONAL AREA OF THE COMPONENT BY MORE THAN 5% OF THE ORIGINAL NOMINAL CROSS SECTION. A COMPONENT IS DEFINED AS A WEB OR FLANGE OF A PLATE GIRDER, A WEB OR FLANGE OF A ROLLED BEAM GIRDER, A COVER PLATE EXCLUSIVE OF WELDS, A LEG OF AN ANGLE, ETC.

- NICKS, SCRAPES AND GOUGES THAT EXCEED THE ABOVE LIMITS MUST BE REPAIRED USING AN APPROVED PROCEDURE SIMILAR TO THE FOLLOWING:

- PREPARE THE SURFACE BY GRINDING.
- WELD WITH APPROVED PROCEDURES TO COMPLETELY FILL THE GOUGE.
- GRIND FLUSH AND SMOOTH WITH THE ADJACENT BASE METAL.
- ULTRASONICALLY TEST THE REPAIR WELD.

- ALL FINAL GRINDING MUST BE PARALLEL TO THE DIRECTION OF APPLIED STRESS IN THE MEMBER UNLESS THE SURFACE ROUGHNESS MEETS OR EXCEEDS AN ANSI 125 FINISH.

INSPECTION

QUALIFICATIONS:

- THE CONTRACTOR SHALL EMPLOY THE SERVICES OF QUALIFIED PERSONNEL TO PERFORM INSPECTION AND TESTING OF THE WORK. THE TYPE OF INSPECTION AND THE LOCATION SHALL BE AS SHOWN IN THE CONTRACT DOCUMENTS. THE FOLLOWING ARE CONSIDERED ACCEPTABLE QUALIFICATIONS:
- PERSONNEL PERFORMING VISUAL INSPECTION SHALL POSSESS A CURRENT CERTIFICATION AS AN AMERICAN WELDING SOCIETY (AWS)-CERTIFIED WELDING INSPECTOR (CWI) IN ACCORDANCE WITH THE PROVISIONS OF AWS QC1, "STANDARD AND GUIDE FOR QUALIFICATION AND CERTIFICATION OF WELDING INSPECTORS."
- PERSONNEL PERFORMING DYE PENETRANT, MAGNETIC PARTICLE INSPECTION OR RADIOGRAPHIC INSPECTION SHALL BE QUALIFIED IN ACCORDANCE WITH THE CURRENT EDITION OF THE AMERICAN SOCIETY OF NONDESTRUCTIVE TESTING'S RECOMMENDED PRACTICE NO. SNT-TC-1A. ONLY INDIVIDUALS QUALIFIED FOR NDT LEVEL I WORKING UNDER THE SUPERVISION OF AN INDIVIDUAL QUALIFIED TO NDT LEVEL II, OR AN INDIVIDUAL QUALIFIED FOR NDT LEVEL II, MAY PERFORM NONDESTRUCTIVE TESTING.
- PERSONNEL PERFORMING ULTRASONIC TESTING MUST BE CERTIFIED BY TESTS ADMINISTERED BY NYS DOT. A LIST OF NYS DOT-CERTIFIED ULTRASONIC TECHNICIANS IS AVAILABLE UPON REQUEST.

NONDESTRUCTIVE TESTING

- RADIOGRAPHIC INSPECTION (RT), WHEN REQUIRED, SHALL BE PERFORMED IN ACCORDANCE WITH THE PROVISIONS OF SECTION 16 OF THE NYSSCM.
- ULTRASONIC TESTING (UT), WHEN REQUIRED, SHALL BE PERFORMED IN ACCORDANCE WITH THE PROVISIONS OF SECTION 17 OF THE NYSSCM. WELD FLAWS SHALL BE EVALUATED FOR ACCEPTANCE OR REJECTION IN ACCORDANCE WITH TABLE 1700B, "HIGHWAY & RAILWAY BRIDGES."
- MAGNETIC PARTICLE INSPECTION (MT), WHEN REQUIRED, MUST BE PERFORMED IN ACCORDANCE WITH THE PROVISIONS OF SECTION 18 OF THE NYSSCM USING THE YOKE TECHNIQUE, EXCEPT THAT SEPARATE TESTS MUST BE PERFORMED USING BOTH AC OUTPUT CURRENT TO INSPECT FOR SURFACE DISCONTINUITIES AND HALF-WAVE RECTIFIED DC OUTPUT CURRENT TO INSPECT FOR NEAR-SURFACE DISCONTINUITIES. THE YOKE MUST BE CAPABLE OF PRODUCING A FIELD STRENGTH OF 10 POUNDS (4.5 KG) MINIMUM FOR ALTERNATING CURRENT AND 40 POUNDS (18 KG) MINIMUM FOR DIRECT CURRENT AT THE MAXIMUM POLE SPACING TO BE USED IN THE WORK. THE PROD METHOD SHALL NOT BE USED.
- DYE PENETRANT INSPECTION (PT), WHEN REQUIRED, MUST BE PERFORMED IN ACCORDANCE WITH SECTION 19 OF THE NYSSCM. DYE PENETRANT TESTING MAY BE SUBSTITUTED FOR MAGNETIC PARTICLE TESTING WHEN APPROVED BY THE ENGINEER.

REPORTS

DISCONTINUITIES FOUND AS A RESULT OF VISUAL INSPECTION OR NONDESTRUCTIVE TESTING SHOULD BE LOCATED, EVALUATED AND REPORTED IN ACCORDANCE WITH THE APPROPRIATE SECTIONS OF THE NYSSCM UPON COMPLETION OF THE INSPECTION. INDICATIONS OF CRACKS MUST ALSO BE REPORTED TO THE ENGINEER IMMEDIATELY.

FINAL PAINTING

4 FEET OF ALL BEAM ENDS SHALL BE BLAST CLEANED AND PAINTED. (60 LOCATIONS)

FINAL AS-BUILT DRAWINGS

UPON COMPLETION OF ALL WORK, THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH AS-BUILT DRAWINGS OF ALL REPAIR WORK PERFORMED. THESE AS-BUILT DRAWINGS SHALL BE INCLUDED IN THE FINAL SUBMITTAL OF THE REPAIR WORK DOCUMENTS FOR INCLUSION IN THE B.I.N. FOLDER FOR FUTURE BRIDGE INSPECTIONS.

WORK TO BE DONE

THE FOLLOWING IS A GENERAL DESCRIPTION OF THE WORK TO BE DONE UNDER THIS CONTRACT. THIS LIST IS INTENDED TO GIVE THE CONTRACTOR A GENERAL DESCRIPTION OF THE WORK INVOLVED IN THIS CONTRACT AND IS NOT A COMPLETE LISTING OF ALL WORK TO BE DONE. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS EVEN THOUGH NOT SPECIFICALLY MENTIONED IN THIS LIST.

- IMPLEMENT PROVISIONS OF THE WORK ZONE TRAFFIC CONTROL PLAN AS REQUIRED FOR THE WORK.
- PERFORM REPAIRS ON MP B16.26, ROUTE 295 BRIDGE OVER BERKSHIRE CONNECTOR (I-90).
- REINFORCE DETERIORATED GIRDER ENDS WITH BENT STEEL PLATES.
- AREA, ZONE, CRITICAL AREA AND FULL OVER COAT PAINTING OF STRUCTURAL STEEL WITH REQUIRED ENVIRONMENTAL AND WORKER PROTECTION AS DESCRIBED IN THE CONTRACT DOCUMENTS.
- RESTORE DISTURBED AREAS AND CLEAN UP AS DIRECTED BY THE ENGINEER.

SEQUENCE OF WORK

- REMOVE PAINT FROM REPAIR AREA
- CLEAN REPAIR AREA
- COMPLETE STEEL REPAIR
- BLAST CLEAN 4 FEET OF BEAM END
- PAINT
- CAULK REPAIR

REPAIR SEQUENCE FOR CONNECTION PLATE

- REMOVE AND STORE DIAPHRAGM FROM ONE SIDE OF THE GIRDER.
- REMOVE EXISTING CONNECTION PLATE BY AIR CARBON ARC GOUGING AND/OR BY FLAME CUTTING THE CONNECTION PLATE TO WITHIN 1/8" OF THE WEB. THE REMAINING CONNECTION PLATE AND WELD METAL SHALL BE REMOVED BY GRINDING FLUSH AND SMOOTH WITH THE WEB SURFACE.
- PERFORM MAGNETIC PARTICLE TESTS ON THE CLEANED/REMOVAL AREA PRIOR TO INSTALLING NEW BENT PLATE. MAGNETIC PARTICLE INSPECTION SHALL BE PERFORMED AS DESCRIBED IN INSPECTION AND NONDESTRUCTIVE TESTING NOTES ON THIS DRAWING.
- INSTALL NEW CONNECTION PLATE.
- DRILL CONNECTION PLATE HOLES USING EXISTING DIAPHRAGM AS A TEMPLATE.
- REINSTALL DIAPHRAGM REMOVED IN STEP 1 USING NEW 7/8" BOLTS.
- REPEAT STEPS 1-6 ON THE OPPOSITE SIDE OF GIRDER.
- AFTER ALL FIELD WELDING IS COMPLETED PERFORM MAGNETIC PARTICLE TESTS. MAGNETIC PARTICLE INSPECTION SHALL BE PERFORMED AS DESCRIBED IN INSPECTION AND NONDESTRUCTIVE TESTING ON THIS DRAWING.
- BLAST CLEAN AND PAINT AS DESCRIBED IN THE CONTRACT DOCUMENT.

REPAIR SEQUENCE FOR BENT PLATE INSTALLATION

- REMOVE PAINT FROM END OF GIRDER PER THE CONTRACT PLANS.
- REMOVE AND STORE DIAPHRAGMS FROM EACH SIDE OF THE GIRDER.
- REMOVE CONNECTION PLATE FROM 1 SIDE OF GIRDER ONLY. REMOVE THE CONNECTION PLATE BY AIR CARBON ARC GOUGING AND/OR BY FLAME CUTTING THE CONNECTION PLATE TO WITHIN 1/8" OF THE WEB. THE REMAINING CONNECTION PLATE AND WELD METAL SHALL BE REMOVED BY GRINDING FLUSH AND SMOOTH WITH THE WEB SURFACE.
- PERFORM MAGNETIC PARTICLE TESTS ON THE CLEANED/REMOVAL AREA PRIOR TO INSTALLING NEW BENT PLATE. MAGNETIC PARTICLE INSPECTION SHALL BE PERFORMED AS DESCRIBED IN INSPECTION AND NONDESTRUCTIVE TESTING NOTES ON THIS DRAWING.
- INSTALL BENT PLATE ON THE GIRDER THAT THE CONNECTION PLATE WAS REMOVED. DO NOT WELD THE BOTTOM OF THE BENT PLATE UNTIL BOTH BENT PLATES AND ALL OF THE BOLTS ARE INSTALLED, DRILL ALL HOLES THROUGH EXISTING GIRDER WEB AND INSTALL A MINIMUM OF 50% OF THE BOLTS REQUIRED FOR THE ASSEMBLY.
- INSTALL CONNECTION PLATE ON THE GIRDER/BENT PLATE.
- REMOVE THE CONNECTION PLATE ON THE OPPOSITE SIDE OF THE GIRDER BY AIR CARBON ARC GOUGING AND/OR BY FLAME CUTTING THE CONNECTION PLATE TO WITHIN 1/8" OF THE WEB. THE REMAINING CONNECTION PLATE AND WELD METAL SHALL BE REMOVED BY GRINDING FLUSH AND SMOOTH WITH THE WEB SURFACE.
- PERFORM MAGNETIC PARTICLE TESTS ON THE CLEANED/REMOVAL AREA PRIOR TO INSTALLING NEW BENT PLATES. MAGNETIC PARTICLE INSPECTION SHALL BE PERFORMED AS DESCRIBED IN INSPECTION AND NONDESTRUCTIVE TESTING NOTES ON THIS DRAWING.
- INSTALL THE NEW BENT PLATE AND ALL BOLTS.
- INSTALL THE CONNECTION PLATE AND WELD THE BOTTOM OF THE BENT PLATES TO THE BOTTOM FLANGE OF THE GIRDER.
- AFTER ALL FIELD WELDING IS COMPLETED PERFORM MAGNETIC PARTICLE TESTS. MAGNETIC PARTICLE INSPECTION SHALL BE PERFORMED AS DESCRIBED IN INSPECTION AND NONDESTRUCTIVE TESTING NOTES ON THIS DRAWING.
- DRILL THE CONNECTION PLATES USING THE EXISTING DIAPHRAGMS AS A TEMPLATE.
- INSTALL THE DIAPHRAGMS REMOVED IN STEP 2 IF THE REPAIRS TO THE ADJACENT GIRDERS ARE COMPLETE. THE COPELS ON THE DIAPHRAGMS MAY NEED TO BE MODIFIED TO FIT THE NEW CONNECTION PLATES.
- BLAST CLEAN AND PAINT AS DESCRIBED IN THE CONTRACT DOCUMENTS.

CHECKED BY: B. MUSCANELL

DRAFTED BY: D. HOLLNER

CHECKED BY: D. VOSBURGH

DESIGNED BY: B. MUSCANELL

DESIGN SUPERVISOR: J. ADAMCZAK

ALTERED ON:	AFFIXED ON: 9/2/20
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 Thruway Authority	TITLE OF PROJECT STEEL REPAIRS OF ROUTE 295 BRIDGE OVER THRUWAY	CONTRACT NUMBER: TABS 20-9B
	LOCATION OF PROJECT ALBANY DIVISION MP B16.26	DATE: 08/28/2020
TITLE OF DRAWING GENERAL NOTES (SHEET 2 OF 2)		DRAWING NUMBER: GN-2

New York State Thruway Authority Standard Sheets

The following NYS Thruway Authority standard sheets, marked with an "X" in first column, apply to this project.

X	SHEET NO.	SUBJECT
	TA 201-01	Clearing and Grubbing (Dwg. CG)
	TA 203-01	Shoulder Backup 1R Projects (Dwg. SB)
	TA 203-02	Slope Flattening Details
	TA 402-01	Highway Pavement Repair Details (Dwg. PRD)
	TA 402-02	Bridge Deck Wearing Course Resurfacing (Dwg. BDR)
	TA 402-03	Overhead Bridge Underclearance Improvement (Dwg. BU)
	TA 605-01	Underdrain Details
	TA 606-01	NYSTA Thrie Beam Guiderail (Sheets 1-2) (Dwgs. GR-1 and GR-2)
	TA 606-02	Modified G2 Corrugated Rail and HPBO Rail Transitions into Existing Corrugated Guiderail Systems (Maintenance Support) (Dwg. GR-3)
	TA 606-03	Corrugated Median Barrier to MG2 Corrugated Beam Guide Railing Transition Detail D (Dwg. GR-4)
	TA 606-04	Box Beam to 42" Single Slope Half Section Concrete Barrier Pier Protection (Dwg. GR-5)
	TA 606-05	HPBO (Mod.) Corrugated Beam to 42" Single Slope Half Section Concrete Barrier Pier Protection (Dwg. GR-6)
	TA 606-06	Typical U-Turn median Rail Layout and Roadway Transverse Section
	TA 611-01	Living Snow Fences
X	TA 619-01	Work Zone Traffic Control Tables & Legend
X	TA 619-02	General Work Zone Traffic Control Notes & Channelizing Devices
X	TA 619-03	Shoulder Closure Short-Term or Intermediate-Term Stationary
X	TA 619-04	Shoulder Closure Short-Duration Stationary and Mobile
X	TA 619-05	Signing & Delineation for Shoulder Work Spaces with Temporary Concrete Barrier
X	TA 619-06	Work Beyond Shoulder
X	TA 619-07	Single Lane Closure Short- or Intermediate-Term Stationary: 65 MPH Zone
	TA 619-08	Double Lane Closure Short- or Intermediate-Term Stationary: 65 MPH Zone
	TA 619-09	Center Lane Closure Short- or Intermediate-Term Stationary: 65 MPH Zone
	TA 619-10	Lane Shift: 65 MPH Zone
	TA 619-11	Single Lane Closure Short- or Intermediate-Term Stationary: 55 MPH Zone
	TA 619-12	Double Lane Closure Short- or Intermediate-Term Stationary: 55 MPH Zone
	TA 619-13	Center Lane Closure Short- or Intermediate-Term Stationary: 55 MPH Zone
	TA 619-14	Lane Shift: 55 MPH Zone
	TA 619-15	Work Zone Traffic Control at Interchanges, Service Areas and Parking Areas
	TA 619-16	Work Zone Traffic Control for Miscellaneous Operations
X	TA 619-17	Mobile Lane Closure
	TA 619-18	Mobile Lane Closure: Narrow Shoulder Area
X	TA 619-19	Short-Duration Lane Closure
	TA 619-20	Short-Duration Double Lane Closure
	TA 619-21	Work Zone Traffic Control Guide for Pavement Striping Operations
	TA 619-22	Mobile Lane Closure for Pavement Striping Operations
	TA 619-23	Mobile Lane Closure for Pavement Striping Operations: Narrow Shoulder Area
	TA 619-24	Work Zone Traffic Control for Pavement Striping Operations at Interchanges, Service Areas and Parking Areas
	TA 619-30	New York Division Traffic Management Tables (Sheets 1-24)
X	TA 619-31	Albany Division 1,150 Veh/Hr/Lane Traffic Management Tables (Sheets 1-18)
	TA 619-32	Syracuse Division 1,150 Veh/Hr/Lane Traffic Management Tables (Sheets 1-18)
	TA 619-33	Buffalo Division 1,150 Veh/Hr/Lane Traffic Management Tables (Sheets 1-37)
	TA 619-34	Vacant
X	TA 619-35	Albany Division 1,300 Veh/Hr/Lane Traffic Management Tables (Sheets 1-18)
	TA 619-36	Syracuse Division 1,300 Veh/Hr/Lane Traffic Management Tables (Sheets 1-18)
	TA 619-37	Buffalo Division 1,300 Veh/Hr/Lane Traffic Management Tables (Sheets 1-37)
	TA 625-01	ROW and Survey Markers
	TA 645-01	Wrong Way Deterrence Sign
	TA 646-01	Reference Marker Details (Sheets 1-2)
	TA 670-01	Fiber Optic & Backbone Handhole Relocation Details
	TA 680-01	Inductance Loop Installation
	TA 680-02	Highway Advisory Radio (Sheets 1-9)
	TA 685-01	Pavement Striping Details: Asphalt and Concrete Pavement (Sheets 1-2)
	TA 685-02	Interchange Gore Area Pavement Striping Details over Existing: Tapered
	TA 685-03	Toll Island Gore and Crosswalk Striping Details
	TA 685-04	Temporary Pavement Marking Details
	TA 690-01	Loop and Treadle Plan (Sheets 1-2)
	TA 690-02	Toll Lane Slab Reinforcement Plan
	TA 690-03	10 ft Treadle Frame (Sheets 1-2)

The officially adopted New York State Thruway Authority Standard Sheets book is available on the Thruway Authority's website at:

<http://www.thruway.ny.gov/business/contractors/standard-sheets/index.shtml>

New York State Department of Transportation Standard Sheets

The following NYSDOT standard sheets marked with an "X" apply to this project.

X	SHEET NO.														
	203-01		502-05		606-02		606-33	X	619-01		619-63		646-15		670-03
	203-02		502-06		606-04		606-35	X	619-02		619-64		646-16		680-01
	203-03		502-07		606-05		606-36		619-04		619-65		649-02		680-02
	203-04		502-08		606-06		606-37		619-10		619-66		649-03		680-03
	203-05		502-09		606-07		606-38		619-11		624-01		649-04		680-04
	204-01		502-10		606-08		606-40		619-12		625-01		655-01		680-05
	209-01		502-11		606-09		606-41		619-20		630-01		655-02		680-06
	209-02		502-12		606-10		606-42		619-21		632-01		655-03		680-07
	209-03		502-13		606-11		606-43		619-22		645-01		655-04		680-08
	209-04		502-14		606-13		606-44		619-23		645-02		655-05		680-10
	209-05		502-15		606-14		607-01		619-24		645-03		655-06		680-11
	209-06		554-01		606-15		607-04		619-30		645-05		655-07		680-12
	209-07		554-02		606-19		607-05		619-31		645-06		655-08		680-13
	212-01		601-01		606-20		607-06		619-32		645-07		663-01		680-14
	212-02		603-01		606-21		608-01		619-33		645-09		663-02		680-15
	212-03		603-02		606-22		608-02		619-34		645-10		663-03		680-16
	212-04		603-03		606-24		608-03		619-40		645-11		663-04		680-17
	212-05		603-04		606-25		608-07		619-41		645-12		663-05		685-01
	402-01		603-05		606-27		609-01		619-50		645-14		663-06		
	502-01		604-01		606-28		609-02		619-51		646-11		663-07		
	502-02		604-02		606-29		609-03		619-60		646-12		664-01		
	502-03		605-01		606-31		609-04		619-61		646-13		670-01		
	502-04		606-01		606-32		611-01		619-62		646-14		670-02		

The officially adopted New York State Department of Transportation Standard Sheets book is available on the NYSDOT website at:

<https://www.dot.ny.gov/main/business-center/engineering/specifications/busi-e-standards-usc>

Structure Work Type

The marked types apply to the indicated milepost(s) below.

MILEPOST:		B16.26			
PROJECT TYPE	X	X	X	X	
Bridge Washing					
Scour Protection					
Channel Cleaning					
Railing System					
Protective Screening					
Painting					
Steel Repair		X			
Wearing Surface Treatment					
Deck Repairs					
Joint Rehabilitation					
Joint Replacement					
Bearing Rehabilitation					
Bearing Replacement					
Hanger Pin Replacement					
Security					
Seismic Retrofit					
Substructure Rehabilitation					
Electrical					
Cathodic Protection System					
Fender or Pier Protection System					
Deck Replacement					
Superstructure Replacement					
Bridge Replacement					
Added Bridge (New Location)					
Abandoned Bridge					
Other: Miscellaneous Painting (Beam Ends)	X				

CHECKED BY: B. MUSCANELL

DRAFTED BY: D. HOLLNER

CHECKED BY: D. VOSBURGH

DESIGNED BY: B. MUSCANELL

DESIGN SUPERVISOR: J. ADAMCZAK

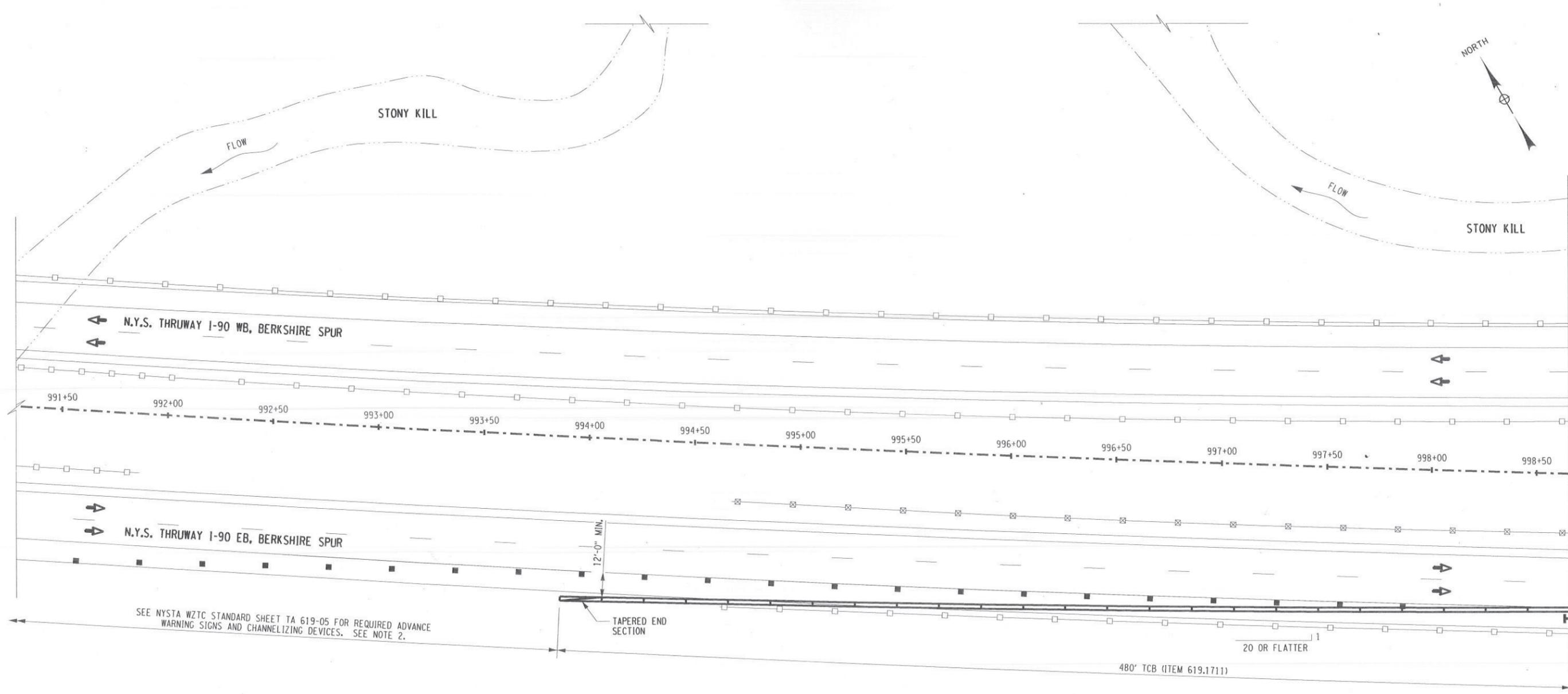
REVISIONS			
DATE	DESCRIPTION	BY	SYM.



Thruway Authority

TITLE OF PROJECT STEEL REPAIRS OF ROUTE 295 BRIDGE OVER THRUWAY	CONTRACT NUMBER: TABS 20-9B
LOCATION OF PROJECT ALBANY DIVISION MP B16.26	DATE: 08/28/2020
TITLE OF DRAWING STANDARD SHEETS	DRAWING NUMBER: SS-1

CHECKED BY: R. HOSLEY
DRAFTED BY: D. HOLLNER
CHECKED BY: R. HOSLEY
DESIGNED BY: J. SHINSKI
DESIGN SUPERVISOR: J. ADAMCZAK



MAINLINE WORK ZONE TRAFFIC CONTROL PLAN
SCALE: 1" = 50'-0"

MATCH LINE A-A (SEE WZTC-2)



LEGEND

- TEMPORARY CONCRETE BARRIER, ITEM 619.1711
- CHANNELIZING DEVICES @ 40' SPACING, ITEM 619.01
- TEMPORARY SIGN, ITEM 619.01

NOTES:

1. WORK SPACE (MATERIAL STORAGE, EQUIPMENT, WORKERS, ETC.) SHALL NOT ENCR OACH WITHIN THE DEFLECTION DISTANCE OF ADJACENT TCB AND/OR GUIDE RAILING.
2. SIGNS NYR9-11 AND W20-1 SHALL BE INSTALLED ON BOTH SIDES OF THE THRUWAY.

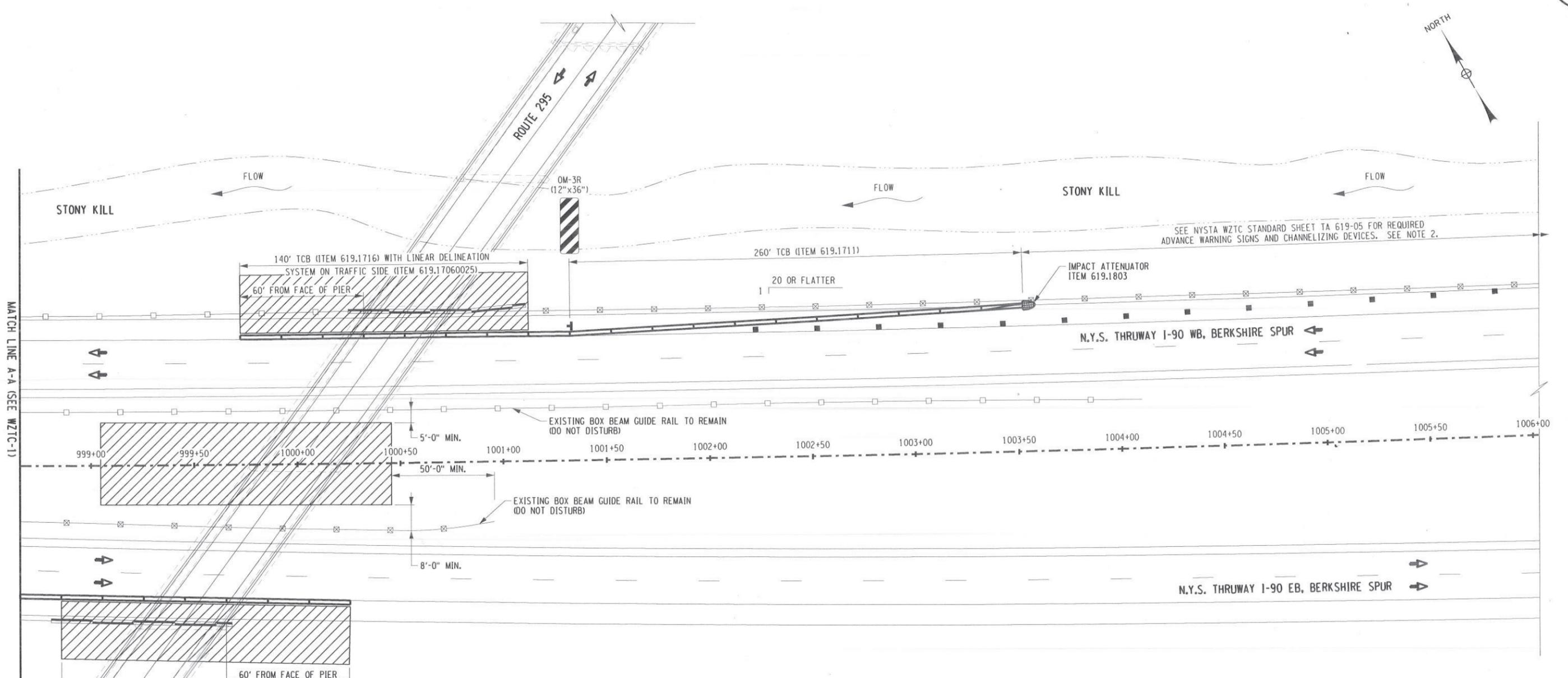
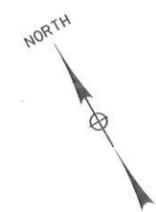
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

Thruway Authority

TITLE OF PROJECT STEEL REPAIRS OF ROUTE 295 BRIDGE OVER THRUWAY	CONTRACT NUMBER: TABS 20-9B
LOCATION OF PROJECT ALBANY DIVISION MP B16.26	DATE: 08/28/2020
TITLE OF DRAWING MAINLINE WORK ZONE TRAFFIC CONTROL PLAN (SHOULDERS) (SHEET 1 OF 2)	DRAWING NUMBER: WZTC-1



SEE NYSTA WZTC STANDARD SHEET TA 619-05 FOR REQUIRED ADVANCE WARNING SIGNS AND CHANNELIZING DEVICES. SEE NOTE 2.

MAINLINE WORK ZONE TRAFFIC CONTROL PLAN

SCALE: 1" = 50'-0"

LEGEND

- TEMPORARY CONCRETE BARRIER, ITEM 619.1711, ITEM 619.1716
- CHANNELIZING DEVICES @ 40' SPACING, ITEM 619.01
- WORK SPACE
- TEMPORARY SIGN, ITEM 619.01
- IMPACT ATTENUATOR, ITEM 619.1803

NOTES:

1. WORK SPACE (MATERIAL STORAGE, EQUIPMENT, WORKERS, ETC.) SHALL NOT ENCRoACH WITHIN THE DEFLECTION DISTANCE OF ADJACENT TCB AND/OR GUIDE RAILING.
2. SIGNS NYR9-11 AND W20-1 SHALL BE INSTALLED ON BOTH SIDES OF THE THRUWAY.

CHECKED BY: R. HOSLEY

DRAFTED BY: D. HOLLNER

CHECKED BY: R. HOSLEY

DESIGNED BY: J. SHINSKI

DESIGN SUPERVISOR: J. ADAMCZAK

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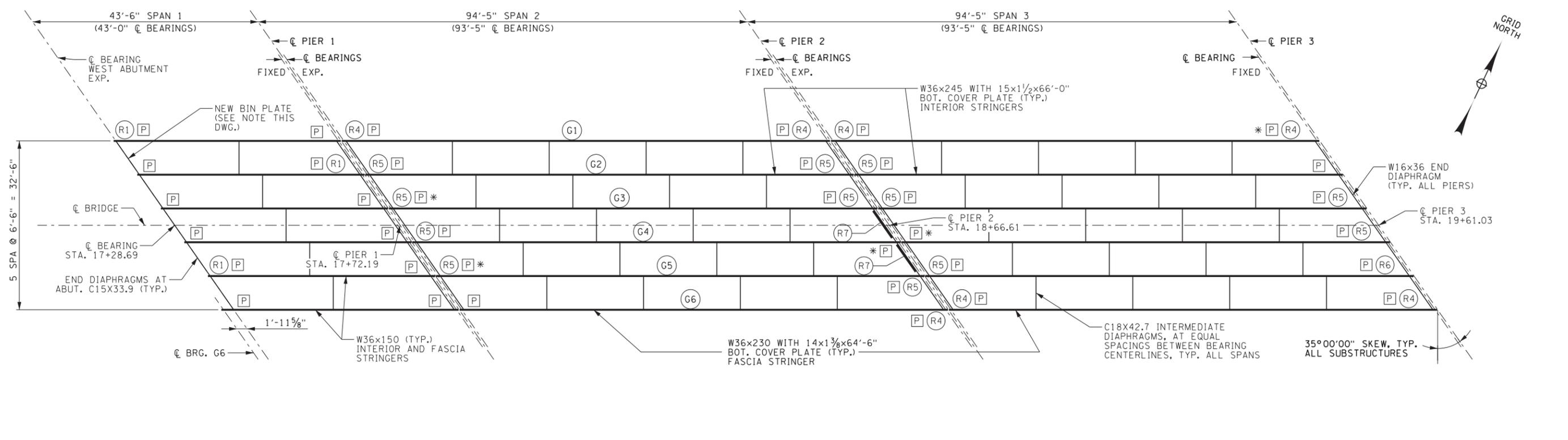
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

Thruway Authority

TITLE OF PROJECT STEEL REPAIRS OF ROUTE 295 BRIDGE OVER THRUWAY	CONTRACT NUMBER: TABS 20-9B
LOCATION OF PROJECT ALBANY DIVISION MP B16.26	DATE: 08/28/2020
TITLE OF DRAWING MAINLINE WORK ZONE TRAFFIC CONTROL PLAN (SHOULDERS) (SHEET 2 OF 2)	DRAWING NUMBER: WZTC-2

CHECKED BY: B. MUSCANELL
 DRAFTED BY: J. MCCOOL
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 DESIGNED BY: B. MUSCANELL
 DESIGN SUPERVISOR: J. ADAMCZAK



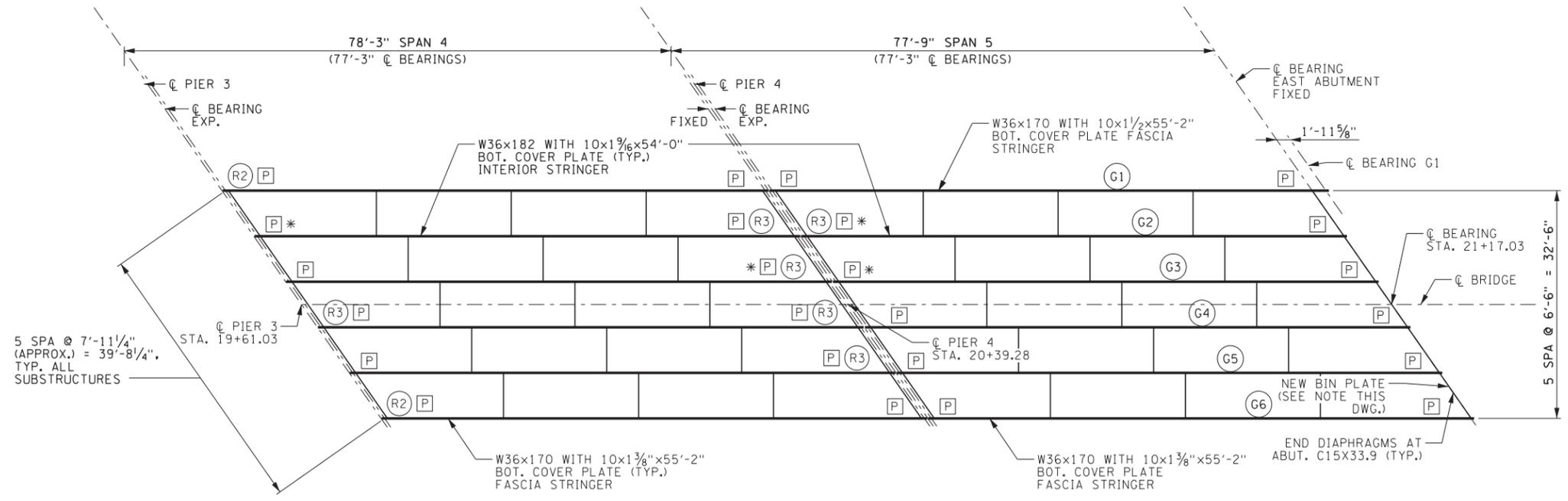
LEGEND

- (Rx) - REPAIR DETAIL, SEE DWG. RD-1
- (P) - 4 FEET OF BEAM END TO BE PAINTED IN ITS ENTIRETY
- (*) - INSPECTION REPORTS AND FIELD OBSERVATION SHOW BEAM ENDS REPAIRED WITH 1" BEARING STIFFENERS, FULL HEIGHT.

x = INDICATED REPAIR DETAIL NUMBER

NOTE

TWO NEW BRIDGE IDENTIFICATION NUMBER (BIN) PLATES SHALL BE PROVIDED AND INSTALLED. SEE BRIDGE (BIN) PLATE SPECIAL NOTES IN THE PROPOSAL.



REPAIR LOCATION PLAN
SCALE: 1" = 20'-0"

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

Thruway Authority	TITLE OF PROJECT STEEL REPAIRS OF ROUTE 295 BRIDGE OVER THRUWAY	CONTRACT NUMBER: TABS 20-9B
	LOCATION OF PROJECT ALBANY DIVISION MP B16.26	DATE: 08/28/2020
TITLE OF DRAWING REPAIR LOCATION PLAN	DRAWING NUMBER: LP-1	

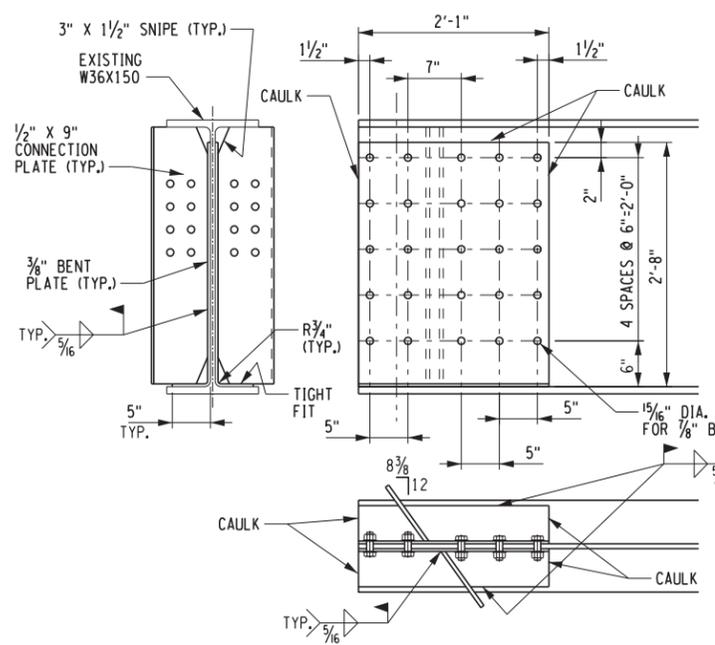
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DRAFTED BY: J. DISHON

CHECKED BY: D. VOSBURGH

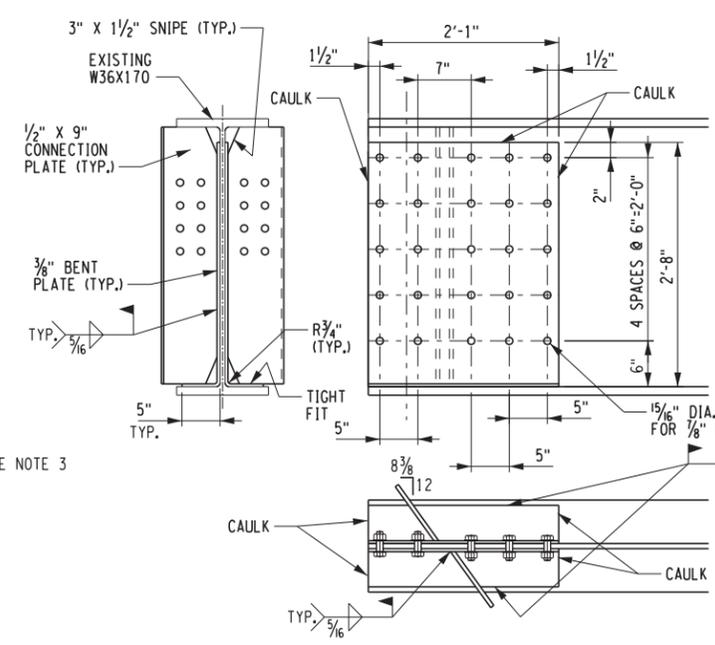
DESIGNED BY: B. MUSCANELL

DESIGN SUPERVISOR: J. ADAMCZAK



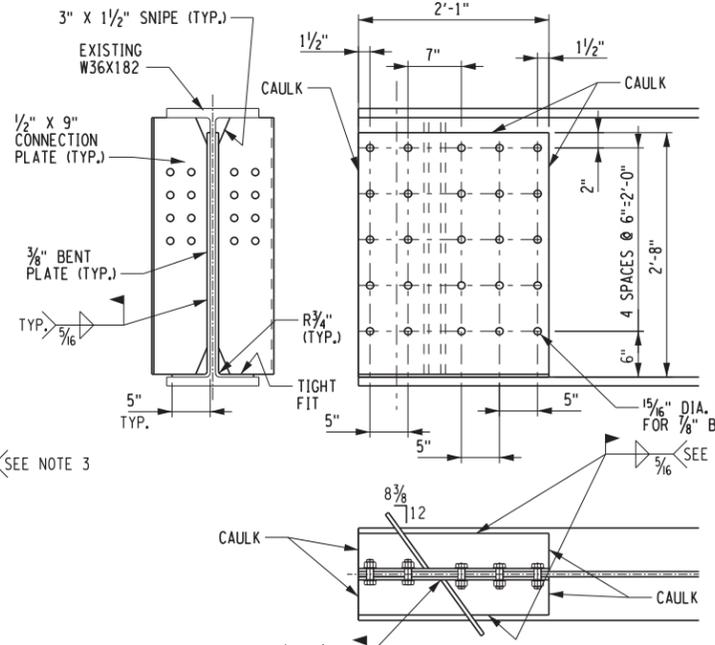
REPAIR DETAIL 1 (R1)

(3) REQUIRED
SCALE: 1" = 2'-0"



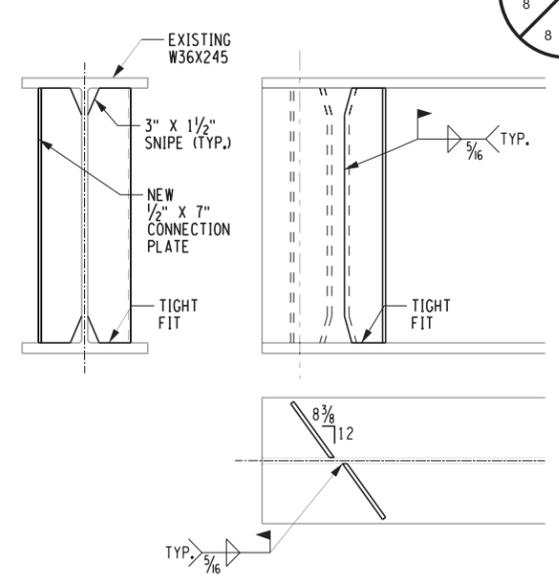
REPAIR DETAIL 2 (R2)

(2) REQUIRED
SCALE: 1" = 2'-0"



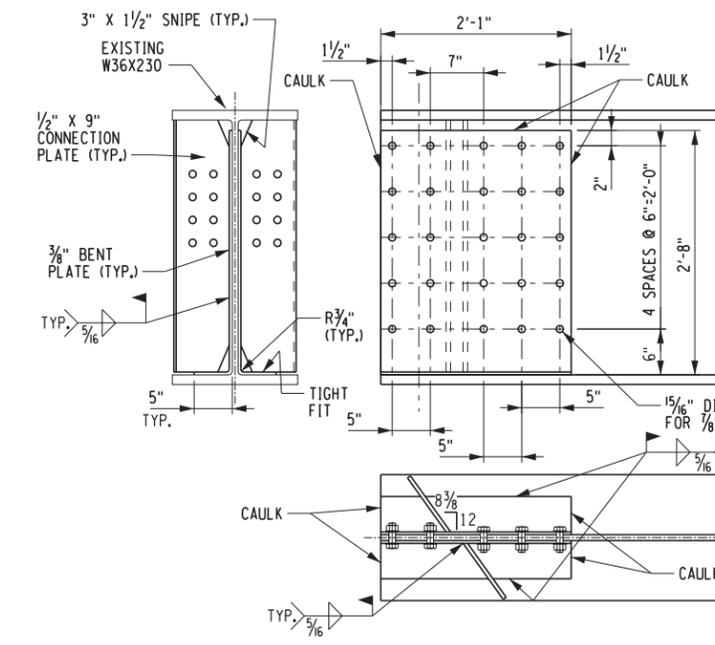
REPAIR DETAIL 3 (R3)

(6) REQUIRED
SCALE: 1" = 2'-0"



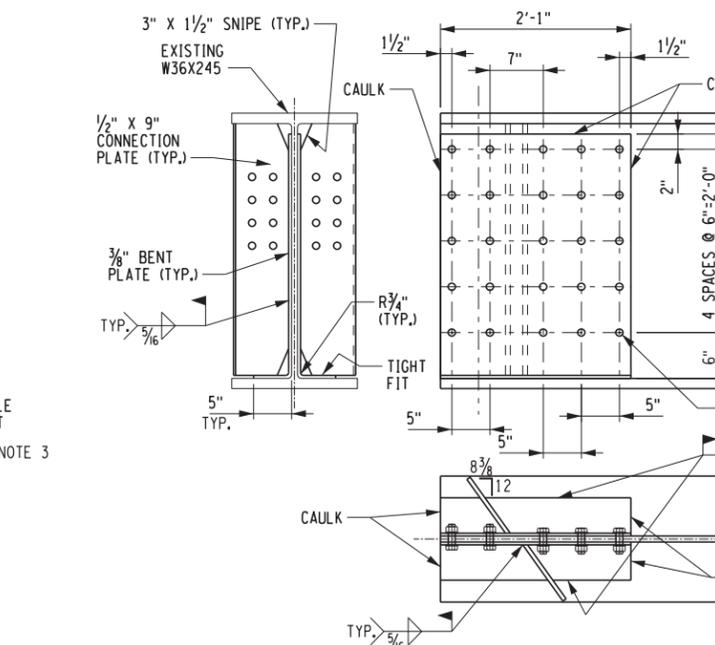
CONNECTION PLATE REPAIR DETAIL (R6)

(1) REQUIRED
SCALE: 1" = 2'-0"



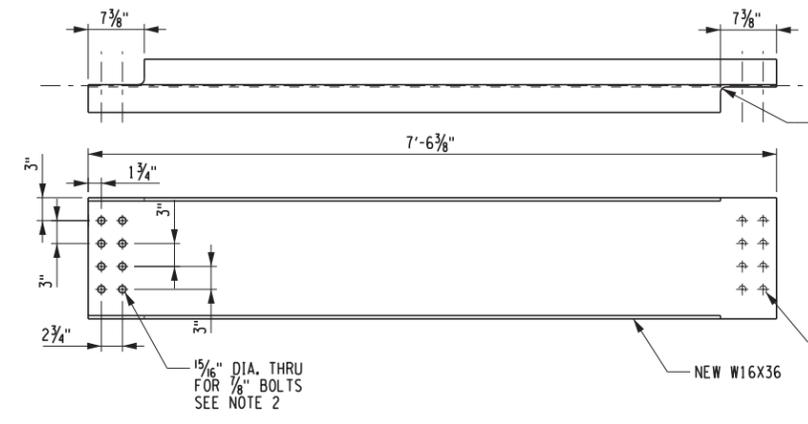
REPAIR DETAIL 4 (R4)

(7) REQUIRED
SCALE: 1" = 2'-0"



REPAIR DETAIL 5 (R5)

(12) REQUIRED
SCALE: 1" = 2'-0"



DIAPHRAGM DETAIL (R7)

(2) REQUIRED
SCALE: 1" = 2'-0"

NOTES:

- BENT PLATES SHALL BE MATCH DRILLED AND SUPPLIED IN SETS.
- DRILL CONNECTION PLATE USING DIAPHRAGM AS A TEMPLATE.
- WELD AFTER ALL BOLTS ARE INSTALLED.
- CAULK ALL EDGES OF PLATE NOT WELDED, EXCEPT LOW POINT.
- OMIT CONNECTION PLATE ON THE OUTSIDE FACE OF THE FASCIA GIRDERS.
- FOR SUGGESTED REPAIR SEQUENCES SEE DWG. GN-2.

ALTERED ON:

SIGNATURE:
STAMP:

AFFIXED ON: 9/2/20

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

Thruway Authority

TITLE OF PROJECT STEEL REPAIRS OF ROUTE 295 BRIDGE OVER THRUWAY	CONTRACT NUMBER: TABS 20-9B
LOCATION OF PROJECT ALBANY DIVISION MP B16.26	DATE: 08/28/2020
TITLE OF DRAWING REPAIR DETAILS	DRAWING NUMBER: RD-1