



BUFFALO DIVISION  
PLANS FOR  
REHABILITATION  
OF THE  
NYS THRUWAY BRIDGE OVER PITTSFORD MENDON CENTER ROAD  
AT  
MILEPOST 356.69 & 356.70  
IN  
MONROE COUNTY

TAB 16-33B D214504

86 SHEETS BIN(S) 5510582 & 5510581

TYPE OF CONSTRUCTION:  
REHABILITATION

STANDARD SHEETS:

THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY NYS DOT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) LISTED ON DWG. GN-1

BRIDGE MAINTENANCE GUIDELINES

UPON COMPLETION OF THIS PROJECT, THE BRIDGE STRUCTURES REPAIRED, REHABILITATED OR RECONSTRUCTED HEREUNDER SHALL BE MAINTAINED IN ACCORDANCE WITH THE CURRENT AASHTO MANUAL FOR BRIDGE MAINTENANCE, AND THE NEW YORK STATE THRUWAY AUTHORITY MAINTENANCE DIRECTIVES:

BRIDGE MANAGEMENT PROGRAM MD 05-16, BRIDGE MANAGEMENT ACTIVITIES MD 05-3, AND BRIDGE INSPECTION PROGRAM MD 95-5.

SPECIAL MAINTENANCE REQUIREMENTS: NONE

NOTES:

ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (US CUSTOMARY) REFERENCED IN THE CONTRACT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR CHANGES SET FORTH IN THE CONTRACT "PROPOSAL"

MAINTENANCE JURISDICTION

100% MUNICIPALITY

APPROACH GUIDE RAILING, APPROACH PAVEMENT, SHOULDERS, SIDEWALKS, APPROACH SLAB AND BRIDGE DECK WEARING COURSE, AND BRIDGE RAIL/BARRIER/PARAPET.

100% AUTHORITY

ALL OTHER BRIDGE ELEMENTS NOT LISTED ABOVE.

MP 356.69 LOAD RATING TABLE

CONTROLLING MEMBER	INVENTORY LOAD RATING	OPERATING LOAD RATING
SPAN 2 EXTERIOR STRINGER	HS 24.6 (44.28 TONS US)	HS 41.2 (72.36 TONS US)
SPAN 2 EXTERIOR STRINGER (LRFR)	HL-93 R.F.= 1.36	HL-93 R.F.= 1.76

HL-93 LIVE LOADING, LRFR AND HS 20 LIVE LOADING, LFD.  
DOES NOT INCLUDE FUTURE WEARING COURSE.

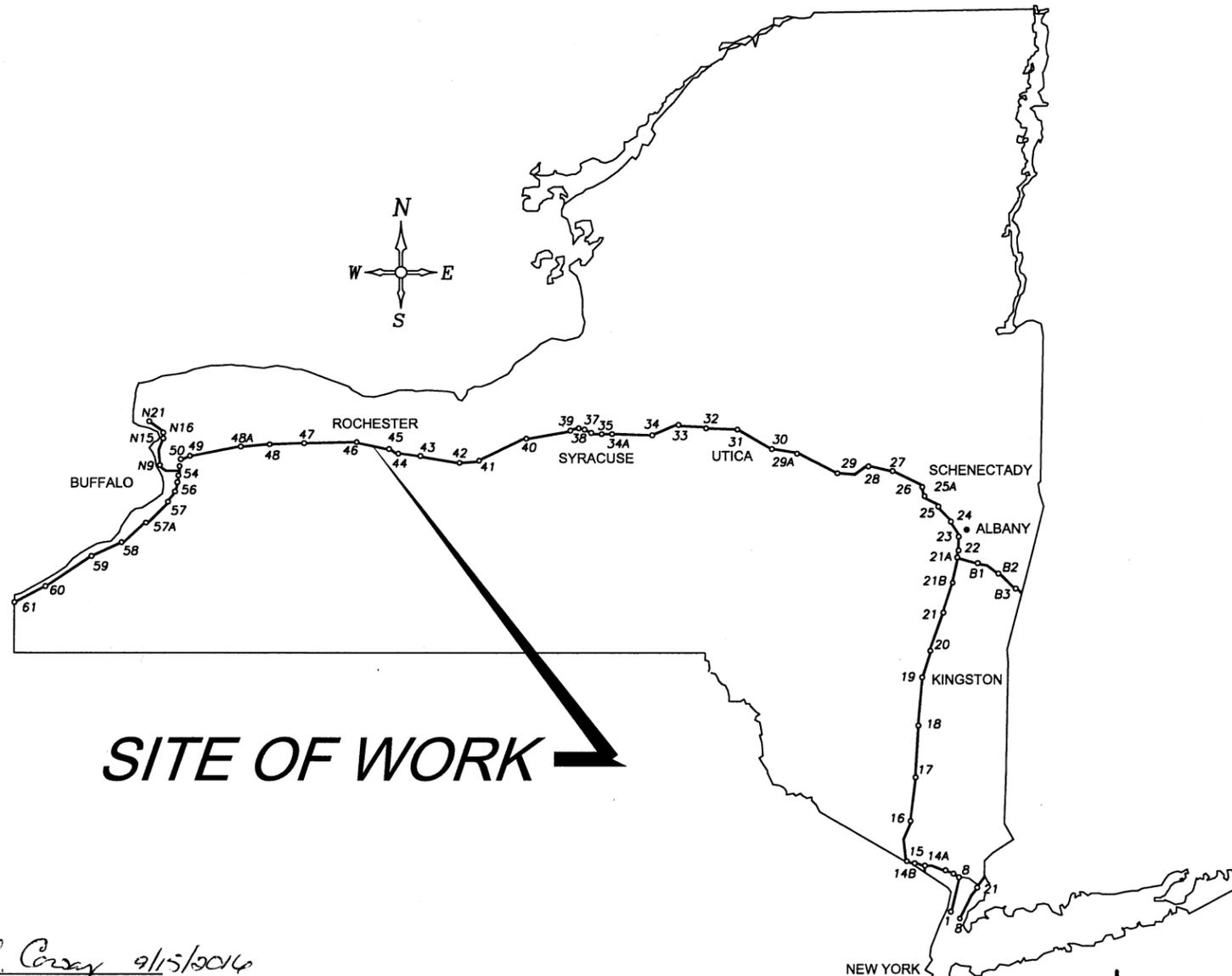
MP 356.70 LOAD RATING TABLE

CONTROLLING MEMBER	INVENTORY LOAD RATING	OPERATING LOAD RATING
*SPAN 3 INTERIOR STRINGER	HS 20.4 (36.72 TONS US)	HS 33.8 (60.84 TONS US)
*SPAN 3 INTERIOR STRINGER (LRFR)	HL-93 R.F.= 1.02	HL-93 R.F.= 1.32

HL-93 LIVE LOADING, LRFR AND HS 20 LIVE LOADING, LFD.  
DOES NOT INCLUDE FUTURE WEARING COURSE.  
\* MP 356.70, SPAN 3 IS A NON-COMPOSITE SECTION

	PROJECT LIMITS		CONTRACT LIMITS	
			FROM STA.	TO STA.
TWY MAINLINE	2481+00	2518+00	2478+75	2523+00
PITTSFORD MENDON CENTER RD	35+00	39+30	34+00	40+30

DIG SAFELY NEW YORK  
UNDERGROUND FACILITIES PROTECTION ORGANIZATION  
CALL 811



SITE OF WORK

CONTRACTOR'S NAME: .....

AWARD DATE: .....

COMPLETION DATE: .....

FINAL ACCEPTANCE DATE: .....

INSPECTION FIRM'S NAME: .....

RESIDENT ENG./EIC: .....

FINAL COST TOTAL: .....

FISCAL SHARE	COST(S)

INSPECTION FIRM CONSULTANT STAMP:

RECOMMENDED BY: *Timothy R. Casey* 9/15/2016  
DIRECTOR, OFFICE OF DESIGN DATE

RECOMMENDED BY: *Douglas J. Sobanyczyk*  
DIVISION DIRECTOR DATE 9-2-16

RECOMMENDED BY: *Paul J. ...* 8/30/16  
TRAFFIC ENGINEER DATE

RECOMMENDED BY: *...* 8/30/16  
DIRECTOR, OFFICE OF CONSTRUCTION MANAGEMENT DATE

RECOMMENDED BY: *...* 9/6/16  
DIRECTOR OF MAINTENANCE AND OPERATIONS DATE

APPROVED BY: *Timothy R. Casey* 9/15/2016  
CHIEF ENGINEER DATE

REVIEWED BY: *David T. ...*

PROJECT MANAGER: *Michael ...*

BUREAU DIRECTOR

File  
 CHECKED BY: TA  
 DRAFTED BY: TA  
 CHECKED BY: TA  
 DESIGNED BY: TA  
 DESIGN SUPERVISOR: TA

ESTIMATE OF QUANTITIES			
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
203.02	UNCLASSIFIED EXCAVATION AND DISPOSAL	CY	6500.0
203.20	SELECT GRANULAR SUBGRADE	CY	3775.0
203.21	SELECT STRUCTURE FILL	CY	24.0
206.05	TEST PIT EXCAVATION	EA	2.0
304.12	SUBBASE COURSE, TYPE 2	CY	213.0
402.000013	PLANT PRODUCTION QUALITY ADJUSTMENT TO HMA ITEMS	QU	90.0
402.128903	12.5 F9 TOP COURSE HMA, SHOULDER COURSE, 80 SERIES COMPACTION	TON	125.0
402.197903	19 F9 BINDER COURSE HMA, 70 SERIES COMPACTION	TON	1325.0
402.198903	19 F9 BINDER COURSE HMA, 80 SERIES COMPACTION	TON	250.0
402.377903	37.5 F9 BASE COURSE HMA, 70 SERIES COMPACTION	TON	85.0
407.0102	DILUTED TACK COAT	GAL	122.0
490.30	MISCELLANEOUS COLD MILLING OF BITUMINOUS CONCRETE	SY	545.0
520.05000010	SAW CUTTING PCC AND COMPOSITE PAVEMENT	LF	11850.0
556.0201	UNCOATED BAR REINFORCEMENT FOR CONCRETE STRUCTURES	LB	52.0
556.03	STUD SHEAR CONNECTORS FOR BRIDGES	EA	10.0
558.02	LONGITUDINAL SAWCUT GROOVING OF STRUCTURAL SLAB SURFACE	SY	2910.0
559.1596 25	SOLID COLOR PROTECTIVE SEALER FOR CONCRETE	SF	11050.0
559.18960118	PROTECTIVE SEALING OF STRUCTURAL CONCRETE ON NEW BRIDGE DECKS AND BRIDGE DECK OVERLAYS	SF	13075.0
564.0501	STRUCTURAL STEEL, TYPE 1	LS	1.0
564.85010125	REPAIR OF STRUCTURAL STEEL	LS	1.0
565.4303 25	BRIDGE BEARING CLEANING AND LUBING	EA	84.0
567.6100 25	REMOVAL OF EXISTING BRIDGE JOINT	LF	260.0
567.8204 25	SILICONE EXPANSION JOINT SYSTEM	LF	260.0
568.70	TRANSITION BRIDGE RAILING	LF	144.0
570.01	LEAD EXPOSURE CONTROL PLAN	LS	1.0
570.02	MEDICAL TESTING	DC	7000.0
570.03	PERSONAL EXPOSURE MONITORING SAMPLE ANALYSIS	DC	7000.0
570.04	DECONTAMINATION FACILITIES	CW	10.0
570.150001	CLASS A CONTAINMENT FOR PAINT REMOVAL	LS	1.0

ESTIMATE OF QUANTITIES			
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
570.150002	CLASS A CONTAINMENT FOR PAINT REMOVAL	LS	1.0
571.05000125	MANAGEMENT AND DISPOSAL OF PAINT REMOVAL WASTE WITH DISPOSAL AS HAZARDOUS OR NON-HAZARDOUS WASTE AS DETERMINED BY TEST	LB	474.0
571.05000225	MANAGEMENT AND DISPOSAL OF PAINT REMOVAL WASTE WITH DISPOSAL AS HAZARDOUS OR NON-HAZARDOUS WASTE AS DETERMINED BY TEST	LB	4550.0
573.93010125	CLEANING AND COATING SYSTEM FOR EXISTING STRUCTURAL STEEL (FIELD APPLIED)	LS	1.0
573.93010225	CLEANING AND COATING SYSTEM FOR EXISTING STRUCTURAL STEEL (FIELD APPLIED)	LS	1.0
579.02	REINFORCING BAR EXPOSURE	SF	13080.0
580.51030525	FULL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH CLASS D CONCRETE	SF	275.0
581.01	REMOVAL OF BITUMINOUS CONCRETE OVERLAY (BRIDGE)	SF	1480.0
582.06	REMOVAL OF STRUCTURAL CONCRETE - REPLACEMENT WITH CLASS D CONCRETE	SF	140.0
584.310201	OVERLAY CONCRETE - CLASS DP - TYPE 2 FRICTION	SY	1460.0
584.55 25	PARTIAL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH RAPID HARDING CONCRETE	SF	78.0
584.56 25	FULL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH RAPID HARDING CONCRETE	SF	68.0
584.6014 25	POLYESTER CONCRETE OVERLAY - SURFACE PREPARATION AND PLACEMENT (ROADWAY)	SY	1450.0
584.6016 25	POLYESTER CONCRETE OVERLAY- FURNISH MATERIAL	SY	1450.0
603.541016	CORRUGATED ALUMINUM END SECTIONS, PIPE 12 INCH DIAMETER (2'-2/3" X 1/2" CORRUGATION) 16 GA	EA	4.0
605.1502	PERFORATED CORRUGATED POLYETHYLENE UNDERDRAIN TUBING, 6 INCH DIAMETER	LF	5720.0
605.1605	PERFORATED POLYVINYL CHLORIDE UNDERDRAIN PIPE, 12 INCH DIAMETER	LF	500.0
606.10	BOX BEAM GUIDE RAILING	LF	325.0
606.100002	BOX BEAM GUIDE RAILING (SHOP BENT OR SHOP MITERED)	LF	60.0
606.120201	BOX BEAM GUIDE RAILING END ASSEMBLY, TYPE IIA	EA	4.0
606.120202	BOX BEAM GUIDE RAILING END ASSEMBLY, TYPE I	EA	1.0
606.3042	SINGLE-SLOPE CONCRETE MEDIAN BARRIER (PRECAST)	LF	320.0
606.52	RESETTING CORRUGATED BEAM MEDIAN BARRIER	LF	1552.0
606.53	RESETTING BOX BEAM GUIDE RAILING	LF	1586.0
606.5601	RESETTING HEAVY POST BLOCKED-OUT CORRUGATED BEAM MEDIAN BARRIER (NEW 12 IN BLOCKOUTS)	LF	75.0
606.5910	RESETTING ANCHORAGE UNITS FOR CORRUGATED BEAM GUIDE RAILING OR MEDIAN BARRIER	EA	3.0
606.5920	RESETTING BOX BEAM GUIDE RAILING END ASSEMBLY	EA	4.0
606.72	REMOVING AND DISPOSING CORRUGATED BEAM MEDIAN BARRIER	LF	75.0
606.73	REMOVING AND DISPOSING BOX BEAM GUIDE RAILING	LF	275.0

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT: NYS THRUWAY BRIDGE REHABILITATION  
 LOCATION OF PROJECT: TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY  
 TITLE OF DRAWING: ESTIMATE OF QUANTITIES SHEET 1 OF 2

CONTRACT NUMBER: TAB 16-33B  
 DATE: 9/16/2016  
 DRAWING NUMBER: EQ-1

File  
 CHECKED BY: TA  
 DRAFTED BY: TA  
 CHECKED BY: TA  
 DESIGNED BY: TA  
 DESIGN SUPERVISOR: TA

ESTIMATE OF QUANTITIES			
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
606.79200125	REMOVE AND DISPOSE END TERMINAL FOR BOX BEAM GUIDE RAIL AND MEDIAN BARRIER	EA	1.0
606.8803	TRANSITION BETWEEN BOX BEAM GUIDE RAIL AND SINGLE SLOPE HALF SECTION CONCRETE BARRIER ONE OR TWO WAY OPERATION	EA	4.0
606.8905	TRANSITION - HPBO (MOD) CORRUGATED BEAM MEDIAN BARRIER TO WEAKE POST CORRUGATED MEDIAN BARRIER	EA	1.0
606.9003	TRANSITION - HALF SECTION AND FULL SECTION SINGLE SLOPE CONCRETE BARRIER (LEFT POCKET)	EA	4.0
610.1402	TOPSOIL - ROADSIDE	CY	470.0
610.1601	TURF ESTABLISHMENT - ROADSIDE	SY	4200.0
610.19	WATERING VEGETATION	MGAL	235.0
610.21	MOWING	SY	4200.0
619.01	BASIC WORK ZONE TRAFFIC CONTROL	LS	1.0
619.0101	BASIC WORK ZONE TRAFFIC CONTROL (DAILY OPERATIONS)	LS	1.0
619.04	TYPE III CONSTRUCTION BARRICADE	EA	2.0
619.08010225	REMOVE PAVEMENT MARKING STRIPES 6 INCH - EPOXY PAINT	LF	19650.0
619.08010425	REMOVE PAVEMENT MARKING STRIPES (6 INCHES) - REMOVABLE WET REFLECTIVE TAPE	LF	43596.0
619.10010225	INTERIM PAVEMENT MARKINGS STRIPES (EPOXY PAINT) 6"	LF	11300.0
619.10010425	INTERIM PAVEMENT MARKINGS (REMOVABLE WET REFLECTIVE TAPE) 6"	LF	15595.0
619.1701	TEMPORARY CONCRETE BARRIER, (UNPINNED)	LF	950.0
619.1703	TEMPORARY CONCRETE BARRIER, (PINNED)	LF	6060.0
619.1706 25	LINEAR DELINEATION SYSTEM	LF	1480.0
619.1803	TEMPORARY IMPACT ATTENUATOR - REDIRECTIVE (TEST LEVEL 3)	EA	5.0
619.24	NIGHTTIME OPERATIONS	LS	1.0
620.08	BEDDING MATERIAL	CY	250.0
620.12 25	STAMPED CONCRETE SLOPE PROTECTION	SF	11050.0
625.01	SURVEY OPERATIONS	LS	1.0
627.5014 08	CUTTING PAVEMENT	LF	5800.0
637.11 25	ENGINEER'S FIELD OFFICE - TYPE 1	MNTH	12.0
645.5102	GROUND MOUNTED SIGN PANELS, LESS THAN OR EQUAL TO 30 SQUARE FEET, WITH Z-BARS	SF	2.0
646.0603 25	INSTALL DELINEATOR OR TENTH MILE MARKER ON POST	EA	8.0
646.0610 25	INSTALL DELINEATOR OR TENTH MILE MARKER BAND OR BRACKET MOUNTED	EA	2.0
646.0802 25	INSTALL SNOWPLOW MARKER, DOUBLE UNIT	EA	53.0

ESTIMATE OF QUANTITIES			
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
646.50 25	REMOVE AND DISPOSE OF DELINEATORS AND MARKERS	EA	62.0
649.2504 25	SHOULDER TREATMENT FOR ACCIDENT REDUCTION (STAR)	LF	5370.0
685.11	WHITE EPOXY REFLECTORIZED PAVEMENT STRIPES - 0.51MM	LF	500.0
685.12	YELLOW EPOXY REFLECTORIZED PAVEMENT STRIPES - 0.51MM	LF	560.0
685.1707 25	WHITE HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH X 20 MILS	LF	6022.5
685.1708 25	YELLOW HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH X 20 MILS	LF	4818.0
697.0203 25	FIELD CHANGE ORDER (THRUWAY)	DC	210000.0
698.06	STEEL/IRON PRICE ADJUSTMENT	DC	1000.0
699.04 25	MOBILIZATION	LS	1.0

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

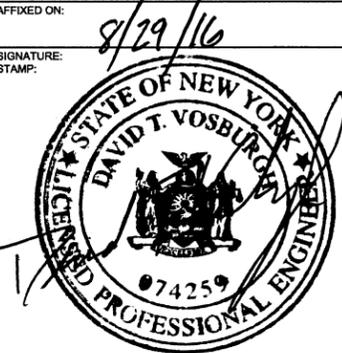


TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 9/16/16
TITLE OF DRAWING ESTIMATE OF QUANTITIES SHEET 2 OF 2	DRAWING NUMBER: EQ-2

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH

SHEET NUMBER	TITLE OF DRAWING	DRAWING NUMBER
1-	TITLE SHEET	COVER
2-	ESTIMATE OF QUANTITIES	EQ-1
3-	ESTIMATE OF QUANTITIES	EQ-2
4-	INDEX OF DRAWINGS	IN-1
5-	GENERAL NOTES	GN-1
6-	PLAN AND ELEVATION	EPE-1
7-	TRANSVERSE SECTION	TS-1
8-	TRANSVERSE SECTION M.P. 356.70	TS-2
9-	EAST ABUTMENT (E.B. TWY)	AB-1
10-	EAST ABUTMENT (W.B. TWY)	AB-2
11-	WEST ABUTMENT (E.B. TWY)	AB-3
12-	WEST ABUTMENT (W.B. TWY)	AB-4
13-	PIER 1 NORTH REPAIRS (WB. TWY STA 2496+83.25)	P1-NR
14-	PIER 1 SOUTH REPAIRS (E.B. TWY STA 2497+20.17)	P1-SR
15-	PIER 2 NORTH REPAIRS (WB. TWY STA 2497+45.01)	P2-NR
16-	PIER 2 SOUTH REPAIRS (E.B. TWY STA 2497+81.83)	P2-SR
17-	M.P. 356.69 EXISTING AND PROPOSED JOINTS	JD-1
18-	M.P. 356.70 EXISTING AND PROPOSED JOINTS	JD-2
19-	PROPOSED DRAINAGE PLAN	PDP-1
20-	SLOPE PROTECTION ELEVATION	SPE-1
21-	PROPOSED PIER PROTECTION	PP-1
22-	BRIDGE DECK REPAIR PLAN	BDRP-1
23-	CONCRETE DECK REPAIR DETAILS	CUR-1
24-	CONCRETE REPAIR DETAILS	ICRD-1
25-	STEEL REPAIRS	SR-1
26-	SHOULDER RECONSTRUCTION DETAILS W.B. THRUWAY	SRD-1
27-	TYPICAL TRAFFIC CONTROL M.P. 356.70	TC-1
28-	TRAFFIC CONTROL M.P. 356.70 (SHT 2 OF 2)	TC-2
29-	M.P. 356.69 APPROACH SLAB	AS-1
30-	PAVEMENT TRANSITION	PT-1
31-	ROADWAY PLATE DETAILS	RPGN-1
32-	ROADWAY PLATES GENERAL NOTES	RPD-2
33-	STAR GROOVE DETAILS	SGD-1
34-	GUIDE RAIL PLAN (1 OF 6)	GR 1-6
35-	GUIDE RAIL PLAN (2 OF 6)	GR 2-6
36-	GUIDE RAIL PLAN (3 OF 6)	GR 3-6
37-	GUIDE RAIL PLAN (4 OF 6)	GR 4-6
38-	GUIDE RAIL PLAN (5 OF 6)	GR 5-6
39-	GUIDE RAIL PLAN (6 OF 6)	GR 6-6
40-	STAGE 1 WZTC (6 OF 6)	WZTC 1-6
41-	STAGE 1 WZTC (2 OF 6)	WZTC 2-6
42-	STAGE 1 WZTC (3 OF 6)	WZTC 3-6
43-	STAGE 1 WZTC (4 OF 6)	WZTC 4-6
44-	STAGE 1 WZTC (5 OF 6)	WZTC 5-6
45-	STAGE 1 WZTC (6 OF 6)	WZTC 6-6
46-	STAGE 2 WZTC (1 OF 6)	WZTC2 1-6
47-	STAGE 2 WZTC (2 OF 6)	WZTC2 2-6
48-	STAGE 2 WZTC (3 OF 6)	WZTC2 3-6
49-	STAGE 2 WZTC (4 OF 6)	WZTC2 4-6
50-	STAGE 2 WZTC (5 OF 6)	WZTC2 5-6
51-	STAGE 2 WZTC (6 OF 6)	WZTC2 6-6
52-	STAGE 2A WZTC (1 OF 4)	WZTC 2A 1-4
53-	STAGE 2A WZTC (2 OF 4)	WZTC 2A 2-4
54-	STAGE 2A WZTC (3 OF 4)	WZTC 2A 3-4
55-	STAGE 2A WZTC (4 OF 4)	WZTC 2A 4-4
56-	STAGE 3 WZTC (1 OF 4)	WZTC3 1-4
57-	STAGE 3 WZTC (2 OF 4)	WZTC3 2-4
58-	STAGE 3 WZTC (3 OF 4)	WZTC3 3-4
59-	STAGE 3 WZTC (4 OF 4)	WZTC3 4-4
60-	FINAL STRIPING PLAN (1 OF 6)	FS 1-6
61-	FINAL STRIPING PLAN (2 OF 6)	FS 2-6
62-	FINAL STRIPING PLAN (3 OF 6)	FS 3-6
63-	FINAL STRIPING PLAN (4 OF 6)	FS 4-6
64-	FINAL STRIPING PLAN (5 OF 6)	FS 5-6
65-	FINAL STRIPING PLAN (6 OF 6)	FS 6-6
66-	WORK ZONE TRAFFIC CONTROL GUIDE FOR PAVEMENT STRIPING OPERATIONS	PM-G
67-	TEMPORARY PAVEMENT MARKING DETAILS	PM-T
68-	MILE MARKER DETAILS	MMO-1
69-	MILE MARKER LAYOUT	MLL 1-2
70-	WZTC GENERAL NOTES AND CHANNELIZING DEVICE	GWZN
71-	MOBILE LANE CLOSURE	MLC

SHEET NUMBER	TITLE OF DRAWING	DRAWING NUMBER
72-	WORK ZONE TRAFFIC CONTROL FOR MISCELLANEOUS OPERATIONS	MO
73-	LANE SHIFT	LS-55
74-	MOBILE LANE CLOSURE (NARROW SHOULDER AREA)	MLC-NS
75-	SINGLE LANE CLOSURE (SHORT-OR INTERMEDIATE-TERM STATIONARY 55 MPH)	SLC-55
76-	SINGLE LANE CLOSURE (SHORT-OR INTERMEDIATE-TERM STATIONARY 65 MPH)	SLC-65
77-	WZTC PLAN	WZTCP 1
78-	SHORT TERM LANE CLOSURE	STLC-1
79-	SHOULDER CLOSURE (SHORT-DURATION STATIONARY AND MOBILE)	SCM
80-	WORK BEYOND SHOULDER	WBS
81-	WORK ZONE TRAFFIC CONTROL TABLES AND LEGEND	TL
82-	PAVEMENT STRIPING DETAILS (ASPHALT PAVEMENT)	PMA-1
83-	SIGNING AND DELINEATION FOR SHOULDER WORK SPACES WITH TEMPORARY CONCRETE BARRIER	SCSD
84-	SHORT DURATION LANE CLOSURE	SDLC
85-	MILE MARKER, TENTH MILE MARKER DELINEATOR AND SNOWPLOW MARKER INSTALLATION DETAILS	DMM-1
86-	COVER PLATE DETAILS AND PLAN	CP-1

ALTERED ON:	AFFIXED ON:
SIGNATURE: STAMP:	8/29/16 

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY MP. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 07/2016
	TITLE OF DRAWING INDEX OF DRAWINGS	DRAWING NUMBER: IN-1

GENERAL NOTES

- MATERIAL AND CONSTRUCTION SPECIFICATIONS NEW YORK STATE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS (U.S. CUSTOMARY) DATED AS SHOWN ON THE FRONT COVER OF THE PROPOSAL, EXCEPT AS MODIFIED IN THESE PLANS AND THE PROPOSAL. DESIGN SPECIFICATIONS: AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES 17th EDITION - 2002, AND THE CURRENT NEW YORK STATE STEEL CONSTRUCTION MANUAL, INCLUDING CURRENT ADDITIONS AND MODIFICATIONS, EXCEPT AS MODIFIED BY THE NEW YORK STATE THRUWAY AUTHORITY.
- LIVE LOAD: HL-93 AND HS-20. THE LOAD RATINGS ARE IN ACCORDANCE WITH THE AASHTO "MANUAL FOR MAINTENANCE INSPECTION OF BRIDGES".
- CONCRETE DATA: THE MINIMUM CONCRETE COMPRESSIVE STRENGTH SHALL BE 3,000 PSI AT 28 DAYS.
- REINFORCING DATA: ALL NEW AND REPLACEMENT BAR REINFORCEMENT SHALL BE ASTM A615 GRADE 60. NO SUBSTITUTIONS WILL BE ALLOWED. BARS SHALL BE FABRICATED IN ACCORDANCE, AND MEET THE REQUIREMENTS OF NYSDOT MATERIAL SPECIFICATION 709-01. ALL NEW AND REPLACEMENT WIRE FABRIC REINFORCEMENT SHALL BE ASTM A1064. NO SUBSTITUTIONS WILL BE ALLOWED. WIRE FABRIC SHALL BE MEET THE REQUIREMENTS OF NYSDOT MATERIAL SPECIFICATION 709-02. REINFORCEMENT FOR REINFORCED CONCRETE PIPE AND RELATED DRAINAGE STRUCTURES SHALL MEET THE REQUIREMENTS OF NYSDOT MATERIAL SPECIFICATION "SECTION 706 -CONCRETE, CLAY AND PLASTIC PIPE".
- RECORD PLANS: RECORD PLANS COVERING PREVIOUS WORK ARE AVAILABLE TO ALL PROSPECTIVE BIDDERS UPON REQUEST. THEY WILL BE PROVIDED ELECTRONICALLY ON CD. REFER TO CONTRACT(S) OT 51-2
- NO SPOIL LOCATIONS AVAILABLE ON THE THRUWAY AUTHORITY RIGHT OF WAY (ROW) THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT NO SPOIL AREA FOR SURPLUS MATERIAL IS AVAILABLE FOR THIS CONTRACT WITHIN THE AUTHORITY'S ROW. THEREFORE, ALL MATERIAL TO BE REMOVED FROM THE JOB SITE SHALL BE DISPOSED OF AND/OR MANAGED BY THE CONTRACTOR OFF THE AUTHORITY'S PROPERTY, IN ACCORDANCE WITH THE OFF- SITE OPTIONS OF THE SPOIL REQUIREMENTS OF SECTION 107-10 MANAGING SURPLUS MATERIAL AND WASTE. THE LOCATION(S) FOR OFF- SITE DISPOSAL SHALL BE SUBMITTED TO THE THRUWAY PROJECT ENGINEER FOR ACCEPTANCE PRIOR TO REMOVAL FROM THE SITE. THE CONTRACTOR SHALL PROVIDE THE AUTHORITY A COPY OF ALL EASEMENTS AND/OR AGREEMENT LETTERS RECEIVED FROM LANDOWNER(S) OF OFF- SITE DISPOSAL AREA(S) PRIOR TO DISPOSAL OF ANY MATERIAL. THE EASEMENTS AND/OR LETTERS MUST INCLUDE A STATEMENT BY THE CONTRACTOR AND THE LANDOWNER THAT THE DISPOSAL OF THE MATERIAL IS IN COMPLIANCE WITH ALL APPLICABLE FEDERAL, STATE AND LOCAL LAWS AND REGULATIONS. ALL COSTS ASSOCIATED WITH THE SPOIL AREA AND REMOVAL OF SPOIL MATERIAL SHALL BE INCLUDED IN THE VARIOUS ITEMS OF THE CONTRACT.
- THE CONTRACTOR SHALL HAVE AN ENGINEER LICENSED IN THE STATE OF NEW YORK PREPARE AND STAMP A SET OF PLANS AND ALL CALCULATIONS FOR THE FOLLOWING WORK:  
STRUCTURAL LIFTING ALL CALCULATIONS FOR THE ABOVE WORK SHALL BE INDEPENDENTLY CHECKED.
- WHEN JOINING FRESH CONCRETE TO NEW CONCRETE WHICH HAS ALREADY SET, OR TO EXISTING CONCRETE, THE CONCRETE IN PLACE SHALL HAVE ITS SURFACE SCOURED OR ABRADED WITH A SUITABLE TOOL TO REMOVE ALL LOOSE AND FOREIGN MATERIALS. THIS SURFACE SHALL BE SCRUBBED WITH WIRE BROOMS. AFTER THE SURFACE PREPARATION HAS BEEN ACCEPTED, THIS SURFACE SHALL BE THOROUGHLY WET AND KEPT SO FOR A PERIOD OF 12 HOURS IMMEDIATELY PRIOR TO PLACING THE NEW CONCRETE. THIS MAY BE ACCOMPLISHED BY CONTINUOUS WETTING WITH SOAKER HOSES OR THE USE OF BURLAP/BURLENE/ETC. SO THAT MOISTURE CAN BE MAINTAINED. IF, IN THE OPINION OF THE ENGINEER, CONDITIONS OR THE SITUATION PROHIBITS THIS, THEN THE SURFACES SHALL BE WETTED FOR AS LONG AS POSSIBLE AT THE DISCRETION OF THE ENGINEER. THE CONTRACTOR SHALL REMOVE ANY PUDDLES OR FREE STANDING WATER WITH OIL- FREE COMPRESSED AIR, AND PROTECT THE SURFACES FROM DRYING, SO THAT THE EXISTING CONCRETE REMAINS IN A CLEAN, SATURATED SURFACE DRY CONDITION UNTIL PLACEMENT OF THE NEW CONCRETE. IMMEDIATELY BEFORE PLACING THE NEW CONCRETE, THE FORMS SHALL BE DRAWN TIGHT AGAINST THE CONCRETE ALREADY IN PLACE.
- ALL METAL REINFORCING BAR CHAIRS AND SUPPORTS SHALL HAVE PLASTIC SHOES.
- WARNING: UNDERGROUND FIBER OPTIC CONDUIT**  
THE THRUWAY'S FIBER OPTIC BACKBONE IS LOCATED WITHIN THE ENTIRE WORK LIMITS OF THIS PROJECT. THE APPROXIMATE LOCATION IS SHOWN ON THE DRAWINGS. THE CONTRACTOR IS ADVISED TO CONTACT DIG SAFELY NY AT 1-800-962-7962 PRIOR TO ANY EXCAVATION. FURTHERMORE, PURSUANT TO N.Y.S. CODE RULE 753, THE CONTRACTOR MUST BE PREPARED TO VERIFY THE LOCATION OF THE FIBER OPTIC LINE THROUGH HAND DUG TEST HOLES ITEM 206.05 AT ONE OR MORE LOCATIONS WITHIN THE WORK AREA PRIOR TO ANY EXCAVATION. HAND DUG TEST HOLES SHALL BE PAID FOR UNDER ITEM 206.05 TEST PIT EXCAVATION.
- WHEN "PULL-OUT-TESTING" IS REQUIRED FOR ANCHOR BOLTS OR REINFORCING STEEL BARS DRILLED AND GROUTED INTO NEW OR EXISTING CONCRETE, THE BOLTS/BARS SHALL BE REQUIRED TO WITHSTAND A TENSILE FORCE OF NOT LESS THAN 150% OF THE DESIGN TENSILE LOAD WITHOUT DAMAGE TO THE CONCRETE.

REMOVAL, EXCAVATION AND BACKFILL NOTES

- ALL EXCAVATIONS TO BE PLATED SHALL UTILIZE A MINIMUM 1 INCH THICK PLATE. ALSO, THE EDGE OF THE PLATE FACING ONCOMING TRAFFIC SHALL BE BEVELED. THE PLATE SHALL BE SECURELY FASTENED DOWN TO THE SATISFACTION OF THE ENGINEER AND SHALL BE STRUCTURALLY CAPABLE OF CARRYING ALL IMPOSED LOADS. ALL ROAD PLATES SHALL BE DESIGNED BY THE CONTRACTOR TO CARRY A MINIMUM HS20 LIVE LOAD.
- DURING REMOVAL OPERATIONS, THE CONTRACTOR SHALL NOT DROP WASTE CONCRETE, DEBRIS OR OTHER MATERIAL TO RAILROAD RIGHT-OF-WAY, RIVERS AND ROADWAYS BELOW THE BRIDGE EXCEPT WHERE THE PLANS OR SPECIFICATIONS SPECIFICALLY PERMIT THE DROPPING OF MATERIAL. PLATFORMS, NETS, SCREENS OR OTHER PROTECTIVE DEVICES SHALL BE USED TO CATCH THE MATERIAL. IF THE ENGINEER DETERMINES THAT ADEQUATE PROTECTIVE DEVICES ARE NOT BEING EMPLOYED, THE WORK SHALL BE SUSPENDED UNTIL ADEQUATE PROTECTION IS PROVIDED. IF MATERIAL FALLS ON THE AREA BELOW AND ADJACENT TO THE BRIDGE, IT SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR ON A DAILY BASIS.
- THE COST OF FURNISHING, INSTALLING, MAINTAINING, REMOVING AND DISPOSING OF ALL PLATFORMS, NETS, SCREENS AND OTHER PROTECTIVE DEVICES SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE APPROPRIATE ITEMS IN THE CONTRACT.
- EXCAVATION BELOW THE PROPOSED BOTTOM OF THE NEW FOOTING ELEVATION WILL NOT BE ALLOWED WITHOUT WRITTEN PERMISSION FROM THE ENGINEER. BACKFILL OF UNAUTHORIZED EXCAVATIONS BELOW OR BEYOND PAYMENT LINES WILL BE AT THE CONTRACTOR'S EXPENSE. BACKFILL MATERIAL WILL BE CONCRETE FOR STRUCTURES CLASS A, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
- CARE SHALL BE EXERCISED IN EXCAVATING THE LAST 1 FOOT OF MATERIAL TO AVOID DISTURBING OR SOFTENING THE MATERIAL BELOW THE BOTTOM OF THE FOOTING ELEVATION. THE EXCAVATION SHALL BE PROGRESSED CONTINUOUSLY TO COMPLETION, THE FOOTING PLACED, FORMS REMOVED AND BACKFILL MATERIAL PLACED AND COMPACTED WITHOUT DELAY.
- CARE SHALL BE TAKEN TO RETAIN NATURAL GROWTH AND PREVENT DAMAGE TO TREES WITHIN AND OUTSIDE THE LIMITS OF CONSTRUCTION, AND NOT SCHEDULED FOR REMOVAL. ANY DAMAGE CAUSED TO THIS NATURAL GROWTH SHALL BE RESTORED AT THE EXPENSE OF THE CONTRACTOR AS DIRECTED BY THE ENGINEER.

SUBSTRUCTURE NOTES

- ALL DETERIORATED STRUCTURAL CONCRETE SHALL BE REMOVED TO A DEPTH WHERE SOUND CONCRETE IS EXPOSED. BEFORE STARTING THIS WORK, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER FOR APPROVAL A PLAN SHOWING THE PROPOSED METHOD, EQUIPMENT AND SEQUENCE FOR THE REMOVAL WORK. BLASTING OR DYNAMITING WILL NOT BE PERMITTED.
- ALL EXPOSED EDGES OF CONCRETE ARE TO BE CHAMFERED 1 INCH UNLESS OTHERWISE NOTED.
- ALL PLACEMENTS OF SELECT STRUCTURE FILL, ITEM 203.02, SHALL BE COMPACTED TO 95 PERCENT OF STANDARD PROCTOR MAXIMUM DENSITY.
- HIGHWAY EMBANKMENT MATERIAL (HIGHWAY ESTIMATE) AND SELECT STRUCTURE FILL, ITEM 203.02, SHALL BE PLACED SIMULTANEOUSLY, IN CONTACT, ON BOTH SIDES OF THE VERTICAL PAYMENT LINE.
- PROTECTIVE SEALER FOR STRUCTURAL CONCRETE, ITEM(S) 559.18960118, (A SOLID COLOR PROTECTIVE SEALER) SHALL BE APPLIED TO ALL EXPOSED REPAIRED CONCRETE SURFACES OF THE SUBSTRUCTURES, EXCEPT THE BOTTOM SURFACE OF PIERCAPS.
- CONCRETE ANCHOR STUDS FOR PIER NOSING:  
ALL CONCRETE ANCHOR STUDS THAT ARE ATTACHED TO THE VARIOUS STEEL DETAILS SHALL MEET THE REQUIREMENTS LISTED IN MATERIALS SUBSECTION 709-05, STUD SHEAR CONNECTORS. PAYMENT FOR FURNISHING AND PLACING THE CONCRETE ANCHORS WILL BE DONE UNDER THE STRUCTURAL STEEL ITEM OF THE PIER NOSING REINFORCEMENT.
- CONSTRUCTION EQUIPMENT (INCLUDING ERECTION CRANES) PLACED BEHIND ALL NEW ABUTMENTS SHALL BE PLACED OUTSIDE THE LOCATION OF THE SOIL SLOPE FAILURE PLAIN AS SHOWN ON THE PLANS UNTIL THE DECK CONCRETE FOR AN INDIVIDUAL STAGE HAS BEEN POURED AND CURED FOR AT LEAST 7 DAYS.

STANDARD SHEETS:

NYSDOT STANDARD SHEETS 619-10 THROUGH 619-66, IF REFERENCED IN THE CONTRACT DOCUMENTS, ARE NOT TO BE USED ON THE NYSTA MAINLINE, NEW ENGLAND SECTION, GARDEN STATE PARKWAY CONNECTOR, BERKSHIRE SPUR AND NIAGARA SECTION.

3203-01	203.05	402-01	603-02	606-04	606-05	606-07
619-01	619-02	619-04	685-10	619-12	619-20	619-60

NYSDOT STANDARD SHEETS 619-10 THROUGH 619-66, IF REFERENCED IN THE CONTRACT DOCUMENTS, ARE NOT TO BE USED ON THE NYSTA MAINLINE, NEW ENGLAND SECTION, GARDEN STATE PARKWAY CONNECTOR, BERKSHIRE SPUR AND NIAGARA SECTION.

EXISTING MILE MARKER /TENTH -MILE MARKERS

NOTE: APPROXIMATE LOCATIONS OF EXISTING MILE MARKERS AND TENTH MARKERS ARE SHOWN ON THE PLANS. THE MARKER LOCATIONS SHOWN ON THE PLANS ARE NOT TO BE USED FOR LAYING OUT LENGTH-OF-NEED FOR PROPOSED GUIDERAIL OR MEDIAN BARRIER. ALL PROPOSED GUIDE RAIL SHOULD BE STAKED OUT FROM EXISTING FIXED FEATURES AS SHOWN IN THE PLANS.

PROPOSED DELINEATORS AND SNOWPLOW MARKERS

NOTE: EXISTING DELINEATORS AND MARKERS SHALL BE REMOVED UNDER ITEM 646.50--25 REPLACEMENT OF NEW DELINEATORS AND MARKERS SHALL BE PAID FOR UNDER ITEMS 646.0603--25, 646.0610--25 AND 646.0802--25

INVASIVE SPECIES INFESTED TOPSOIL EXCAVATION AND DISPOSAL

WHEN EXCAVATING, MOVING, STOCKPILING, OR SPREADING TOPSOIL, THE CONTRACTOR SHALL TAKE ALL REASONABLE PRECAUTIONS TO COMPLY WITH CURRENT FEDERAL, STATE, AND LOCAL GUIDELINES AND REQUIREMENTS TO PREVENT THE SPREAD OF INVASIVE PLANT SPECIES, SPECIFICALLY PHRAGMITES AUSTRALIS (COMMON REED), LYTHRUM SALICARIA (PURPLE LOOSESTRIFE), POLYGONUM CUSPIDATUM (JAPANESE KNOTWEED), AND ANY OTHER NOXIOUS OR INVASIVE PLANTS IDENTIFIED. REFER TO NYSDOT STANDARD SPECIFICATIONS SECTIONS 107-01 AND 713-01 FOR ADDITIONAL INVASIVE SPECIES PRECAUTIONS. IF NOTED ON THE PLANS, REFER TO NYSDOT STANDARD SPECIFICATIONS SECTION 617-01 FOR SITE SPECIFIC INFORMATION.

2. SHOULDER AREAS DISTURBED BY THE CONTRACTOR, AS PART OF THE WORK TO BE PERFORMED UNDER THIS CONTRACT, SHALL BE RESTORED AS SPECIFIED AND TO THE SATISFACTION OF THE ENGINEER. ALL DISTURBED GRASS AREAS OUTSIDE OF THE SPECIFIED WORK LIMITS SHALL BE GRADED IN A MANNER APPROVED BY THE ENGINEER AND SEED AS SPECIFIED IN THE STANDARD SEEDING ITEM. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS ITEMS IN THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE. THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

- PROTECTION OF WETLANDS: THE CONTRACTOR SHALL AVOID ENTRY INTO AND CONDUCT OPERATIONS TO PREVENT ANY DAMAGE OR ADVERSE IMPACTS TO STATE AND FEDERAL PROTECTED WETLAND AREAS INCLUDING THE 100 FT. ADJACENT AREA (BUFFER) WITHIN OR CONTIGUOUS TO THE PROJECT. EXCEPTIONS ARE ONLY AS ORDERED BY THE ENGINEER AND APPROVED BY REGULATORY AGENCIES IN ACCORDANCE WITH PROJECT REQUIREMENTS. ALL WETLAND AREAS SHOWN ON THE PLANS ADJACENT TO CLEARING AND GRUBBING AND SLOPE FLATTENING ZONES SHALL BE PROTECTED AND LEFT UNDISTURBED. ACTIVITIES, WHICH ARE NOT TO ENCROACH ON WETLANDS INCLUDE, BUT ARE NOT LIMITED TO, MOVEMENT OF VEHICLES, CONSTRUCTION STAGING, IMPLEMENTATION OF EROSION CONTROL MEASURES; AND SPREADING OF SPOILED MATERIAL.

SUPERSTRUCTURE NOTES

- STRUCTURAL STEEL: STRUCTURAL STEEL SHALL BE ASTM A709 GRADE 50W, UNLESS OTHERWISE NOTED.
- SHOP DRAWINGS, SHALL BE PREPARED AND SUBMITTED IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL.
- WELDING: ALL WELDING SHALL CONFORM TO THE LATEST VERSION OF THE NEW YORK STATE STEEL CONSTRUCTION MANUAL UNLESS OTHERWISE NOTED.
- ALL DECK FORMS USED SHALL BE REMOVABLE FORMS.
- ALL LONGITUDINAL AND TRANSVERSE TOP MAT DECK REINFORCING STEEL SHALL BE STAGGERED 1/2SPACE ABOVE BOTTOM MAT DECK REINFORCING STEEL. THIS REQUIREMENT APPLIES TO ISOTROPIC REINFORCED DECKS AS WELL AS CONVENTIONALLY REINFORCED DECKS.
- THE USE OF WHIPHAMMERS WILL NOT BE ALLOWED IN THE REMOVAL OF STRUCTURAL CONCRETE DECKS WHERE THE EXISTING STEEL IS TO BE REUSED.
- THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT NO SEALER WAS APPLIED TO THE STRUCTURAL SLAB WHEN ORIGINALLY CONSTRUCTED OR REHABILITATED. THEREFORE DIFFICULTY MAY BE ENCOUNTERED WHEN REMOVING THE EXISTING WEARING COURSE AND SIDEWALKS FROM THE STRUCTURAL DECK.
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE AND ADVISE THE AUTHORITY OF THE TYPE, SIZE AND WEIGHT OF ALL VEHICLES HE INTENDS TO USE ON THE STRUCTURE(S) DURING CONSTRUCTION BASED ON THE CONDITION OF THE EXISTING STRUCTURE(S). THIS DETERMINATION SHALL BE MADE BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF NEW YORK EMPLOYED AND PAID BY THE CONTRACTOR.

THE DETERMINATION BY THIS PROFESSIONAL ENGINEER IS TO BE SUBMITTED TO THE AUTHORITY 14 DAYS PRIOR TO THE USE OF ANY VEHICLES ON THE STRUCTURE(S) WITH ALL RESTRICTIONS ENUMERATED BY HIM BEING STRICTLY ADHERED TO.

IN THE EVENT THAT THE CONTRACTOR/SUBCONTRACTOR FAILS TO COMPLY WITH THE INSTRUCTIONS OF THE PROFESSIONAL ENGINEER FOR THE USE OF ANY VEHICLE, THE WORK WILL BE IMMEDIATELY SUSPENDED UNTIL CORRECTIVE PROCEDURES SATISFACTORY TO THE PROFESSIONAL ENGINEER AND THE AUTHORITY ARE EMPLOYED.

COSTS OF ALL DAMAGE, DIRECT OR INDIRECT, SHALL BE BORNE AND SUSTAINED BY THE CONTRACTOR.

- ALL WEARING SURFACES SHALL BE TINED LONGITUDINALLY.

WORK TO BE DONE:

THE FOLLOWING IS A GENERAL DESCRIPTION OF THE WORK TO BE DONE UNDER THIS CONTRACT. THIS LIST IS INTENDED TO GIVE THE CONTRACTOR A GENERAL DESCRIPTION OF THE WORK INVOLVED IN THE CONTRACT AND IS NOT A COMPLETE LISTING OF ALL WORK TO BE DONE. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS EVEN THOUGH NOT SPECIFICALLY MENTIONED IN THIS LIST.

PRIOR TO STAGE CONSTRUCTION, THE CONTRACTOR SHALL REMOVE STARS AND RECONSTRUCT THE ROADWAY SHOULDER.

MP 356.70 - WESTBOUND THRUWAY OVER PITTSFORD MENDON CENTER ROAD

- ESTABLISH STAGE 1 WORK ZONE TRAFFIC CONTROL IN ACCORDANCE WITH THE PLANS.
- RECONSTRUCT LEFT SHOULDER DETOUR PAVEMENT
- REMOVE EXISTING ASPHALT WEARING COURSE AND STRUCTURAL SLAB
- PERFORM FULL- DEPTH DECK REPAIRS AS ORDERED BY THE ENGINEER, INSTALL MONO-DECK, INSTALL NEW JOINT SYSTEM, MAKE REPAIRS TO CONCRETE SUBSTRUCTURES AND STEEL SUPERSTRUCTURE AND PAINT.
- SWITCH TRAFFIC TO STAGE 2 WORK ZONE TRAFFIC CONTROL PLAN.
- REPEAT STEPS 2 THROUGH 4 ON THE OTHER HALF OF THE BRIDGE.
- REMOVE TEMPORARY TRAFFIC CONTROL DEVICES, INSTALL PERMANENT STRIPING AND RESTORE SHE.

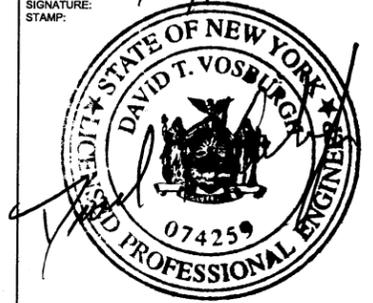
MP 356.69 - EASTBOUND THRUWAY OVER PITTSFORD MENDON CENTER ROAD

- ESTABLISH STAGE 1 WORK ZONE TRAFFIC CONTROL IN ACCORDANCE WITH THE PLANS.
- PERFORM FULL- AND PARTIAL-DEPTH DECK REPAIRS, INSTALL NEW POLYMER CONCRETE OVERLAY, INSTALL NEW JOINT SYSTEM, MAKE REPAIRS TO CONCRETE SUBSTRUCTURES AND STEEL SUPERSTRUCTURE AND PAINT.
- SWITCH TRAFFIC TO STAGE 2 WORK ZONE TRAFFIC CONTROL PLAN.
- REPEAT STEPS 1 & 2.
- REMOVE TEMPORARY TRAFFIC CONTROL DEVICES, INSTALL PERMANENT STRIPING AND RESTORE THE SITE

FILE  
CHECKED BY: TA  
DRAFTED BY: M. COX  
CHECKED BY: TA  
DESIGNED BY: M. COX  
DESIGN SUPERVISOR: D. VOSBURGH

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 9/15/16

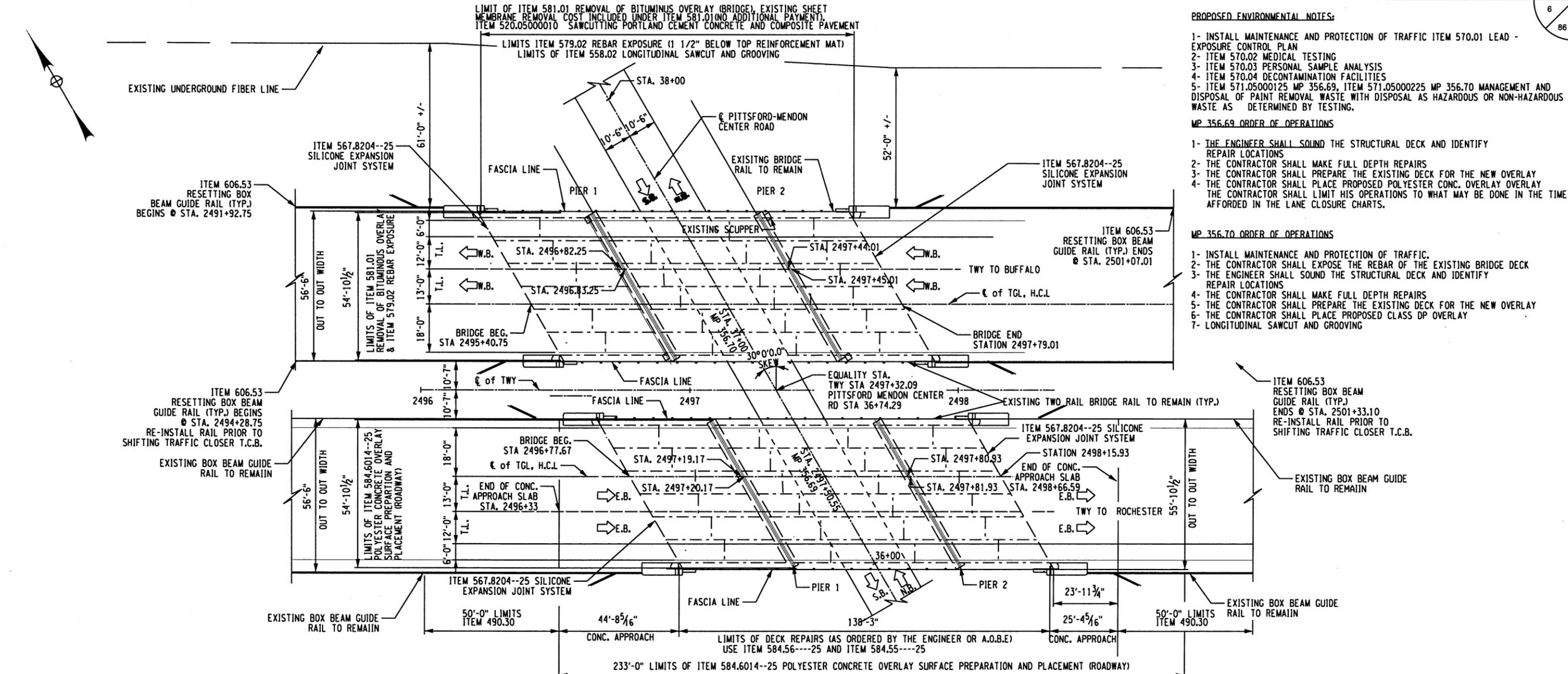
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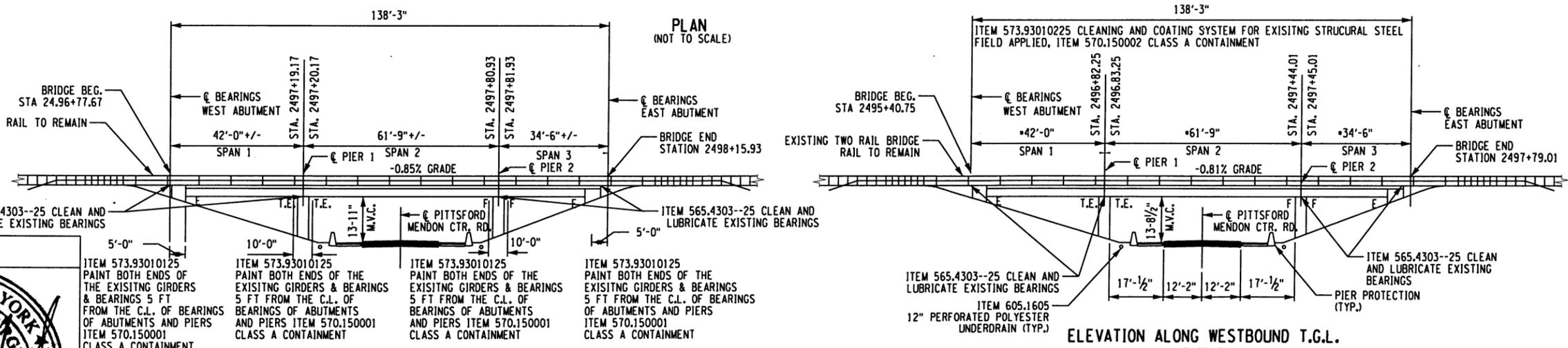
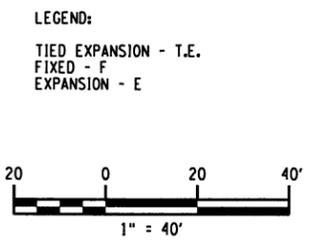

**Thruway Authority | Canal Corporation**

TITLE OF PROJECT PITTSFORD MENDON CENTER RD. MONROE COUNTY	CONTRACT NUMBER: TAB16-33B
LOCATION OF PROJECT TWY M.P. 36.69 & 356.70 BUFFALO DIVISION	DATE: 06/28/2016
TITLE OF DRAWING GENERAL NOTES	DRAWING NUMBER: GN -1

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M.COX  
 CHECKED BY: TA  
 DESIGNED BY: M.COX  
 DESIGN SUPERVISOR: D. VOSBURGH



- PROPOSED ENVIRONMENTAL NOTES:**
- 1- INSTALL MAINTENANCE AND PROTECTION OF TRAFFIC ITEM 570.01 LEAD - EXPOSURE CONTROL PLAN
  - 2- ITEM 570.02 MEDICAL TESTING
  - 3- ITEM 570.03 PERSONAL SAMPLE ANALYSIS
  - 4- ITEM 570.04 DECONTAMINATION FACILITIES
  - 5- ITEM 571.05000125 MP 356.69, ITEM 571.05000225 MP 356.70 MANAGEMENT AND DISPOSAL OF PAINT REMOVAL WASTE WITH DISPOSAL AS HAZARDOUS OR NON-HAZARDOUS WASTE AS DETERMINED BY TESTING.
- MP 356.69 ORDER OF OPERATIONS**
- 1- THE ENGINEER SHALL SOUND THE STRUCTURAL DECK AND IDENTIFY REPAIR LOCATIONS
  - 2- THE CONTRACTOR SHALL MAKE FULL DEPTH REPAIRS
  - 3- THE CONTRACTOR SHALL PREPARE THE EXISTING DECK FOR THE NEW OVERLAY
  - 4- THE CONTRACTOR SHALL PLACE PROPOSED POLYESTER CONC. OVERLAY OVERLAY
  - 5- THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO WHAT MAY BE DONE IN THE TIME AFFORDED IN THE LANE CLOSURE CHARTS.
- MP 356.70 ORDER OF OPERATIONS**
- 1- INSTALL MAINTENANCE AND PROTECTION OF TRAFFIC.
  - 2- THE CONTRACTOR SHALL EXPOSE THE REBAR OF THE EXISTING BRIDGE DECK
  - 3- THE ENGINEER SHALL SOUND THE STRUCTURAL DECK AND IDENTIFY REPAIR LOCATIONS
  - 4- THE CONTRACTOR SHALL MAKE FULL DEPTH REPAIRS
  - 5- THE CONTRACTOR SHALL PREPARE THE EXISTING DECK FOR THE NEW OVERLAY
  - 6- THE CONTRACTOR SHALL PLACE PROPOSED CLASS DP OVERLAY
  - 7- LONGITUDINAL SAWCUT AND GROOVING



ALTERED ON: \_\_\_\_\_

AFFIXED ON: **9/15/16**

SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_

**ELEVATION ALONG EASTBOUND T.G.L.**  
 M.P. 356.691

**ELEVATION ALONG WESTBOUND T.G.L.**  
 M.P. 356.701

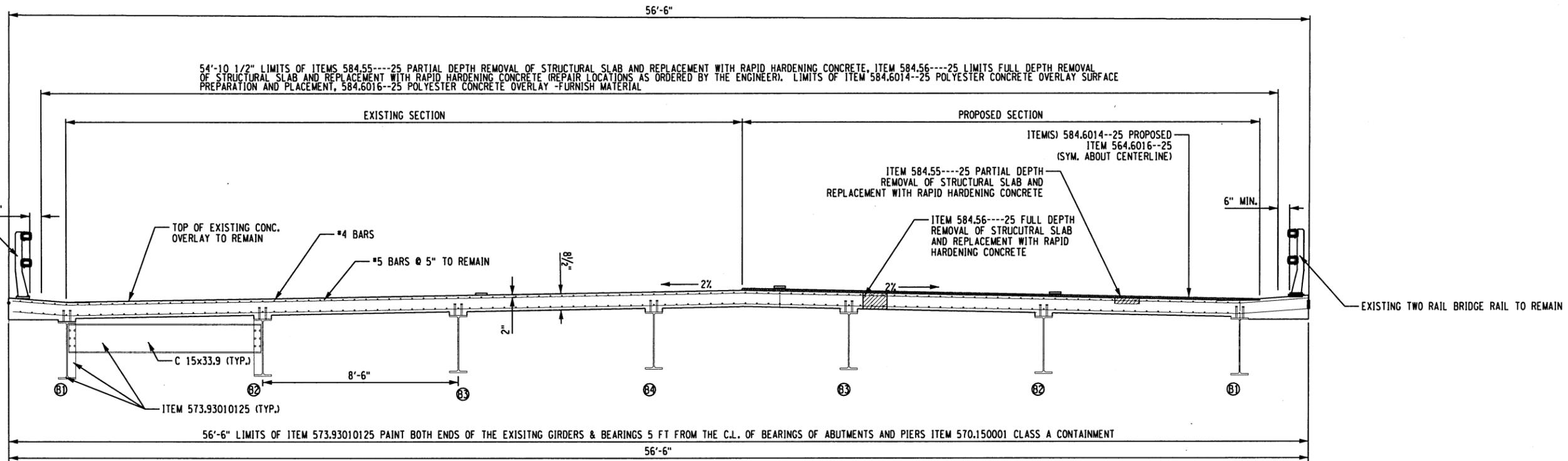
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT T.W. M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 07 / 2016
TITLE OF DRAWING PLAN & ELEVATION	DRAWING NUMBER: EPE -1

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSSBURGH



54'-10 1/2" LIMITS OF ITEMS 584.55----25 PARTIAL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH RAPID HARDENING CONCRETE, ITEM 584.56----25 LIMITS FULL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH RAPID HARDENING CONCRETE (REPAIR LOCATIONS AS ORDERED BY THE ENGINEER). LIMITS OF ITEM 584.6014--25 POLYESTER CONCRETE OVERLAY SURFACE PREPARATION AND PLACEMENT, 584.6016--25 POLYESTER CONCRETE OVERLAY -FURNISH MATERIAL

ITEM 570.150001 CLASS A CONTAINMENT  
 MP 356.69  
 NTS

- ENVIRONMENTAL NOTES:**
- 1- ITEM 570.01 LEAD EXPOSURE
  - 2- ITEM 570.02 MEDICAL TESTING
  - 3- ITEM 570.03 PERSONAL- EXPOSURE MONITORING SAMPLE ANALYSIS
  - 4- ITEM 570.04 DECONTAMINATION FACILITIES
  - 6- ITEM 570.150001 CLASS A CONTAINMENT
  - 7- ITEM 571.05000125 MANAGEMENT AND DISPOSAL OF PAINT REMOVAL WASTE WITH DISPOSAL AS HAZARDOUS WASTE DETERMINED BY TESTING

- SUGGESTED SEQUENCE OF DECK REPAIRS & OVERLAY INSTALLATION:**
- 1- THE CONTRACTOR SHALL UTILIZE STANDARD LANE CLOSURE CHARTS AND MAINTENANCE AND PROTECTION OF TRAFFIC DRAWINGS TO PERFORM REQUIRED WORK.
  - 2- THE ENGINEER SHALL SOUND THE EXISTING DECK AND IDENTIFY REPAIR LOCATIONS
  - 3- THE CONTRACTOR SHALL MAKE NECESSARY REPAIRS UNDER ITEM 584.55----25 PARTIAL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH RAPID HARDENING CONCRETE AND ITEM 584.56----25 FULL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH RAPID HARDENING CONCRETE.
  - 4- THE CONTRACTOR SHALL PLACE ITEMS) 584.6014--25 POLYESTER CONCRETE OVERLAY SURFACE PREPARATION AND PLACEMENT (ROADWAY), ITEM 584.6016--25 POLYESTER CONCRETE OVERLAY FURNISH MATERIAL OVER REPAIRED CONCRETE OVERLAY.

**BEAM TABLE**

SPAN 1 41.5 FT SPAN  
 BEAM 1 30 WF X 108 COVERPLATE : N.A.  
 BEAMS 2,3,4 30 WF X 108 COVERPLATE: 8" X 5/8" X 26'-0"

SPAN 2 60.75 FT SPAN  
 BEAM 1 30 WF X 172 COVERPLATE : 12" X 3/8" X 60'-0"  
 BEAMS 2,3,4 30 WF X 172 COVERPLATE: 14" X 1" X 42'-0"

SPAN 3  
 BEAM 1 30 WF X 108 COVERPLATE : N.A.  
 BEAMS 2,3,4 30 WF X 108 COVERPLATE: N.A.

**SPECIAL NOTE:**  
 CONTRACTOR AND ENGINEER TO VERIFY CONCRETE DECK REPAIR LOCATIONS PRIOR TO STATRING WORK

ALTERED ON:	AFFIXED ON: 9/15/16
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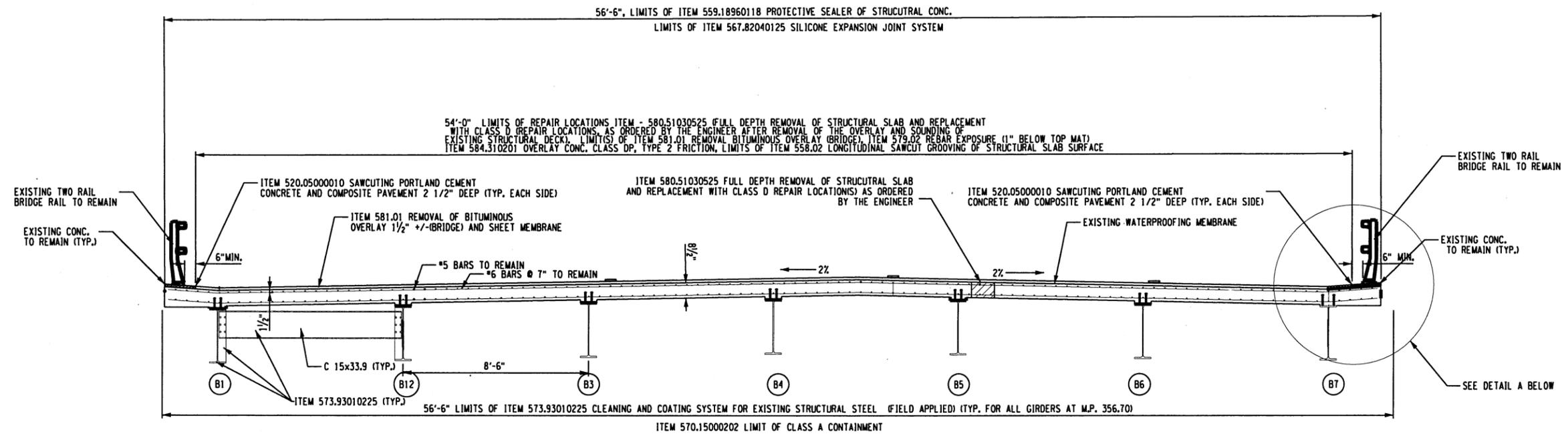
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DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16 -33B
LOCATION OF PROJECT TWY MP. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 07/2016
TITLE OF DRAWING TRANSVERSE SECTION MP. 356.69	DRAWING NUMBER: TS -1

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSSBURGH

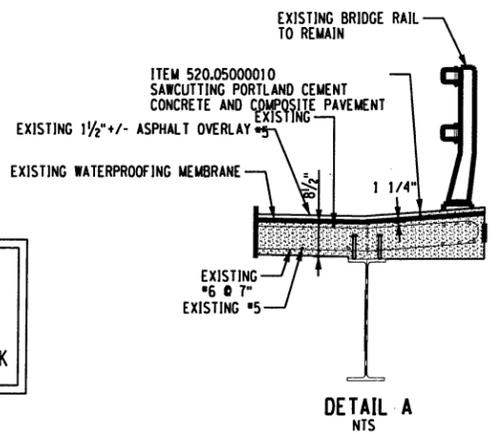


M.P. 356.70  
NTS

**BEAM TABLE**

SPAN 1 41.5 FT SPAN
BEAM 1 30 WF X 108 COVERPLATE : N.A.
BEAMS 2,3,4 30 WF X 108 COVERPLATE: 8" X 3/8" X 26'-0"
SPAN 2 60.75 FT SPAN
BEAM 1 30 WF X 172 COVERPLATE : 12" X 3/8" X 60'-0"
BEAMS 2,3,4 30 WF X 172 COVERPLATE: 14" X 1" X 42'-0"
SPAN 3
BEAM 1 30 WF X 108 COVERPLATE : N.A.
BEAMS 2,3,4 30 WF X 108 COVERPLATE: N.A.

**SPECIAL NOTE:**  
CONTRACTOR AND ENGINEER TO VERIFY CONCRETE DECK REPAIR LOCATIONS PRIOR TO STATRING WORK



**PROPOSED ENVIRONMENTAL NOTES:**

- ITEM 570.01 LEAD EXPOSURE
- ITEM 570.02 MEDICAL TESTING
- ITEM 570.03 PERSONAL- EXPOSURE MONITORING SAMPLE ANALYSIS
- ITEM 570.04 DECONTAMINATION FACILITIES
- ITEM 570.15000202 CLASS A CONTAINMENT
- ITEM 571.05000225 MANAGEMENT OF PAINT REMOVAL WASTE WITH DISPOSAL AS HAZARDOUS OR NON -HAZARDOUS WASTE AS DETERMINED BY TESTING

**DESCRIPTION OF WORK:**

- THE CONTRACTOR SHALL UTILIZE STANDARD LANE CLOSURE CHARTS AND MAINTENANCE AND PROTECTION OF TRAFFIC DRAWINGS TO PERFORM REQUIRED WORK.
- THE CONTRACTOR SHALL REMOVE EXISTING ASPHALT WEARING SURFACE AND SHEET MEMBRANE UNDER ITEM 581.01 REMOVAL OF BITUMINOUS OVERLAY (BRIDGE) AND STRUCTURAL SLAB UNDER ITEM 579.02 REBAR EXPOSURE (1" BELOW TOP MAT)
- THE CONTRACTOR SHALL MAKE NECESSARY FULL DEPTH REPAIRS UNDER AND REPLACEMENT WITH CLASS D CONCRETE AND ITEM 580.051030525 REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH CLASS D CONCRETE.
- THE CONTRACTOR SHALL PLACE ITEM 584.310201 OVERLAY CONCRETE CLASS DP, TYPE 2 FRICTION REPAIRED CONCRETE OVERLAY. THE OVERLAY SHALL BE PLACED TO THE SAME GRADE AND ELEVATION AS THE EXISTING TOP WEARING SURFACE
- THE CONTRACTOR SHALL LONGITUDINALLY SAWCUT AND GROOVE PROPOSED OVERLAY COST INCLUDED ITEM 558.02
- THE CONTRACTOR SHALL SEAL PROPOSED OVERLAY ITEM 559.18960118

ALTERED ON: \_\_\_\_\_

AFFIXED ON: 9/15/16

SIGNATURE: \_\_\_\_\_

STAMP:

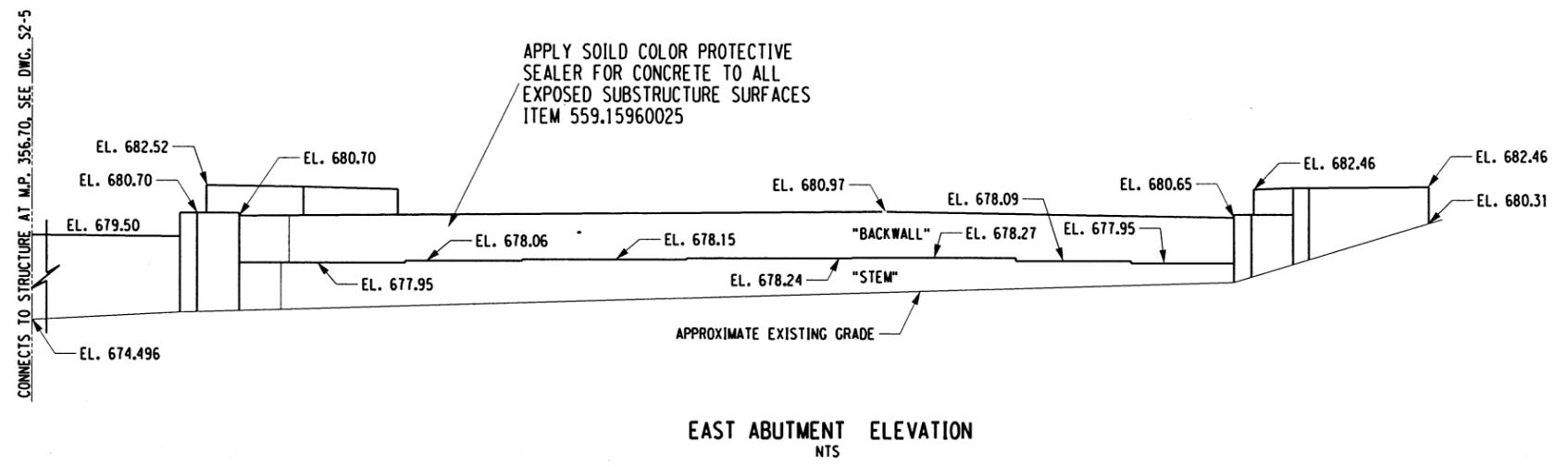
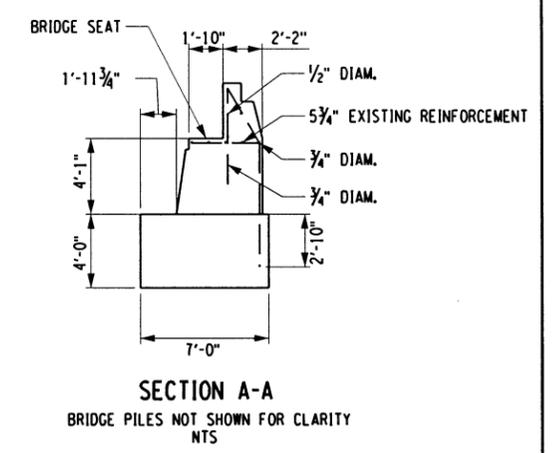
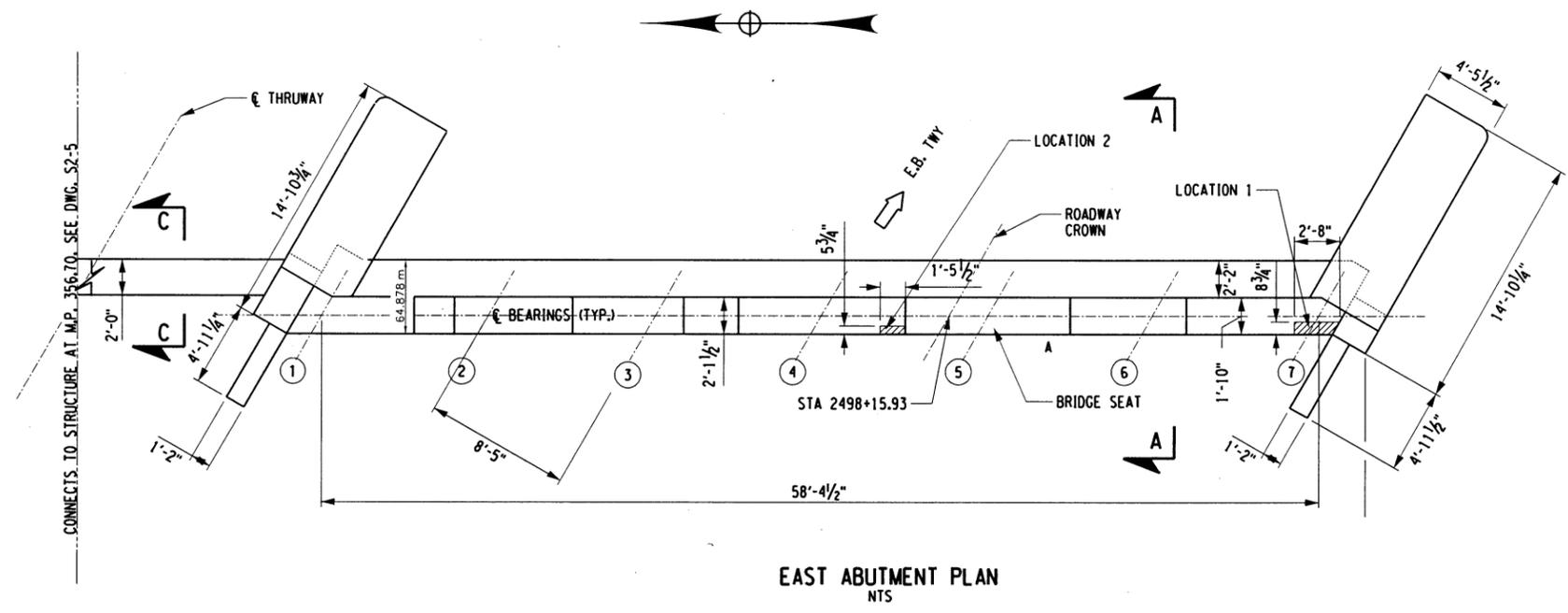
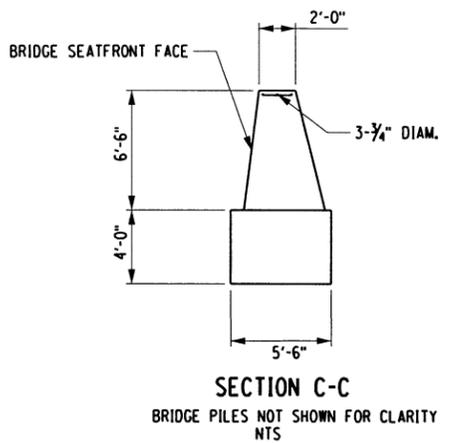
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



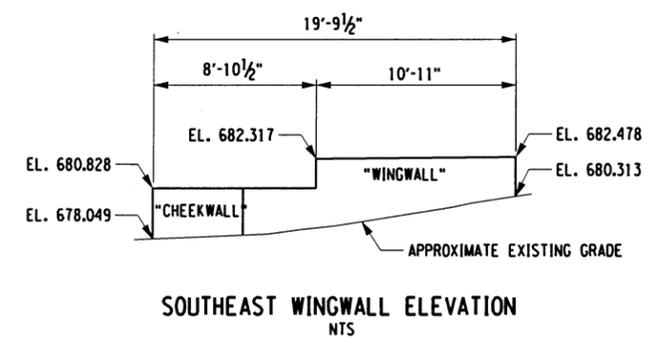
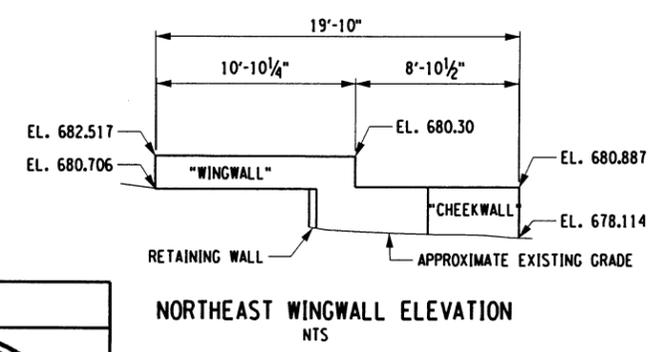
TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TYW MP. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 07/2016
TITLE OF DRAWING TRANSVERSE SECTION MP. 356.70	DRAWING NUMBER: TS-2

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



**ITEM 559.15960025**

EAST ABUTMENT (EB TWY)	
LOCATION	AREA (SF)
STEM	240
BACKWALL & SEAT	360
CHEEKWALLS	160
WINGWALL	160
MEDIAN RETAINING WALL	150



**ITEM 582.06**

EAST ABUTMENT (EB TWY)			
LOCATION	L (FT)	H (FT)	AREA (SF)
1	2.6	.756	2
2	1.5	.5	.75

**LEGEND:**  
 AREA TO BE REPAIRED UNDER ITEM NO. 582.06  
 REMOVAL OF STRUCTURAL CONCRETE - REPLACE WITH CLASS D

**NOTE:**  
 1- SEE AB-1 FOR SECTIONS  
 2- REINFORCEMENT NOT SHOWN FOR CLARITY  
 3- SEE TYPICAL CONCRETE REPAIR DETAILS FOR ADDITIONAL INFO

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 8/29/16

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

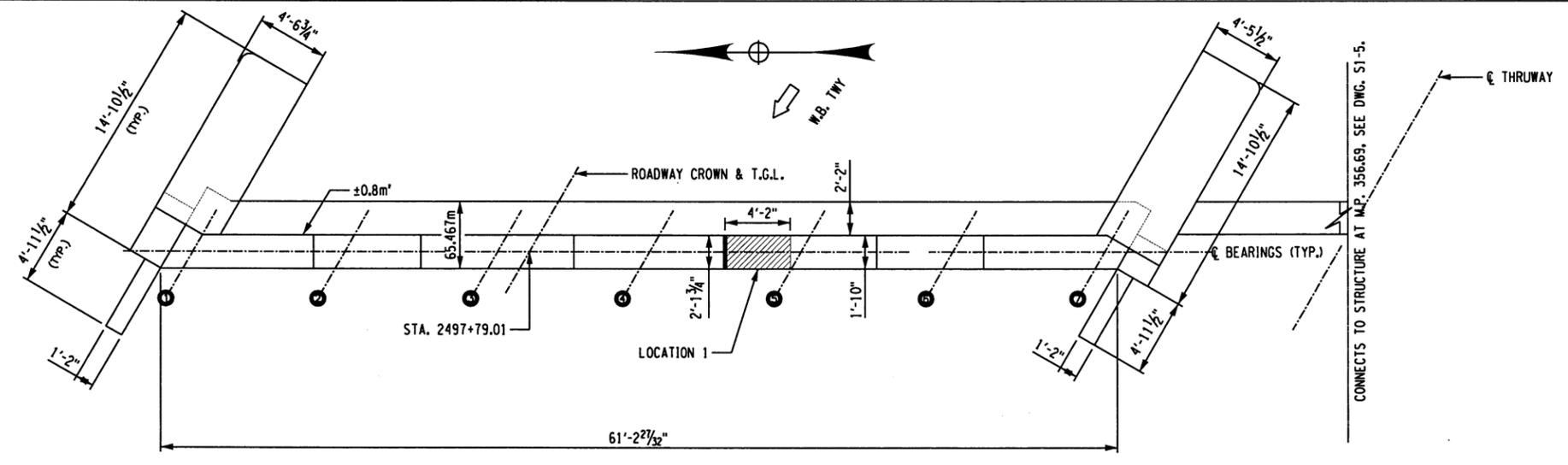
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REVISIONS			
DATE	DESCRIPTION	BY	SYM

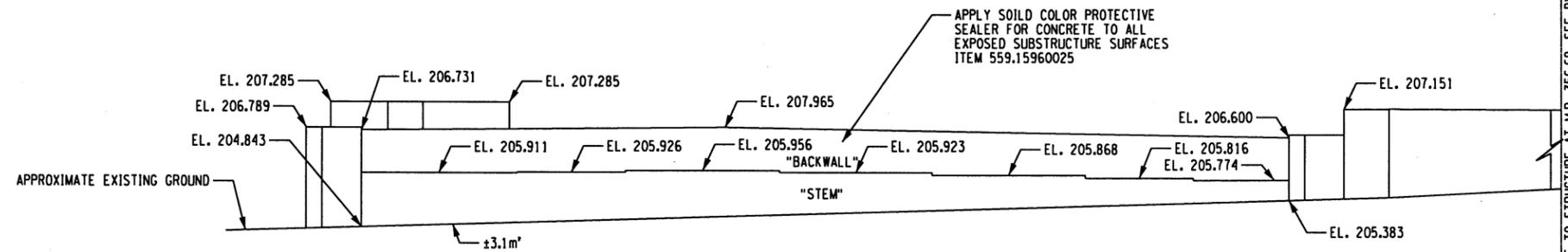
**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 07/2016
TITLE OF DRAWING EAST ABUTMENT (E.B. TWY)	DRAWING NUMBER: AB-1

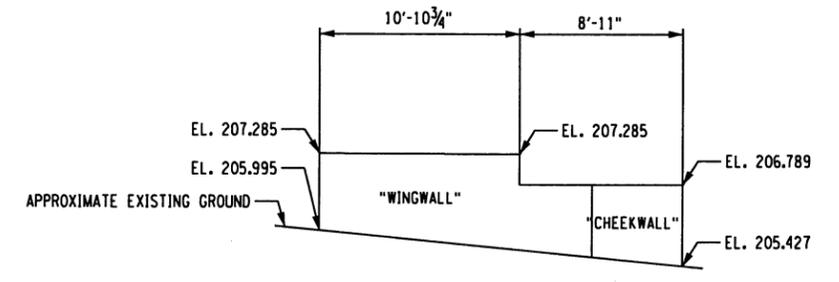
FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



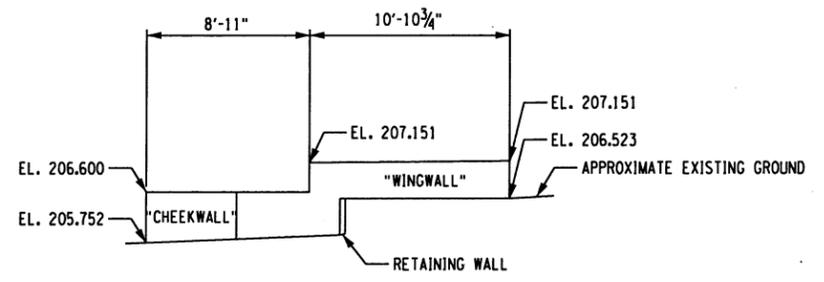
**EAST ABUTMENT PLAN**  
NTS



**EAST ABUTMENT ELEVATION**  
NTS



**SOUTHWEST WINGWALL ELEVATION**  
NTS



**NORTHWEST WINGWALL ELEVATION**  
NTS

**ITEM 559.15960025**

EAST ABUTMENT (EB TWY)	
LOCATION	AREA (SF)
STEM	240
BACKWALL & SEAT	360
CHEEKWALLS	160
WINGWALL	160

**ITEM 582.06**

EAST ABUTMENT (EB TWY)			
LOCATION	L (FT)	H (FT)	AREA (SF)
1	4.16	2.16	8.85

**LEGEND:**

AREA TO BE REPAIRED UNDER ITEM NO. 582.06  
REMOVAL OF STRUCTURAL CONCRETE - REPLACE WITH CLASS D

**NOTE:**  
 1- SEE AB-1 FOR SECTIONS  
 2- REINFORCEMENT NOT SHOWN FOR CLARITY  
 3- SEE TYPICAL CONCRETE REPAIR DETAILS FOR ADDITIONAL INFO

ALTERED ON:	AFFIXED ON: 8/29/16
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

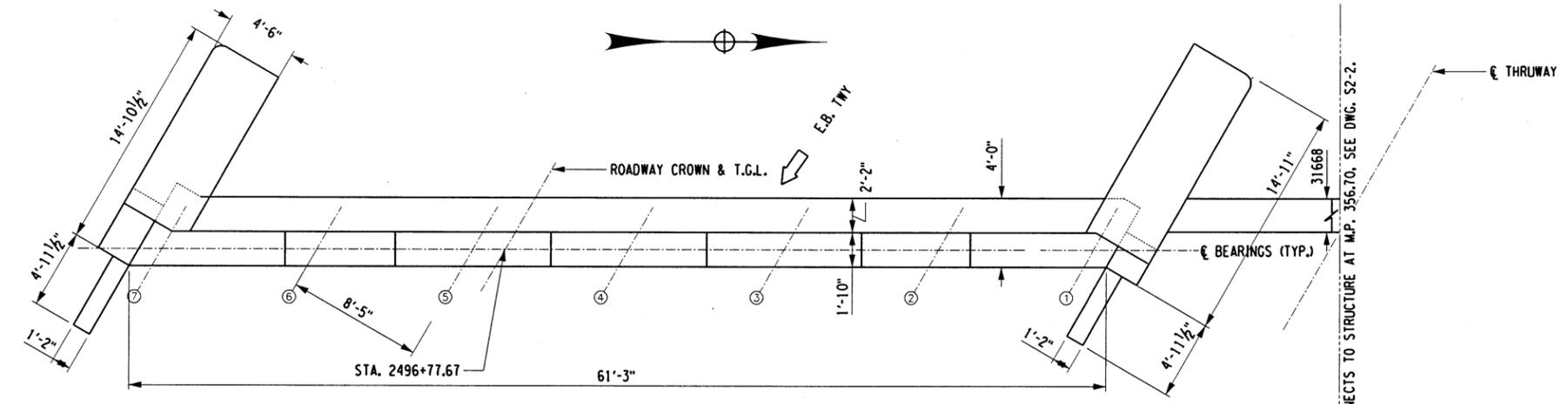
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

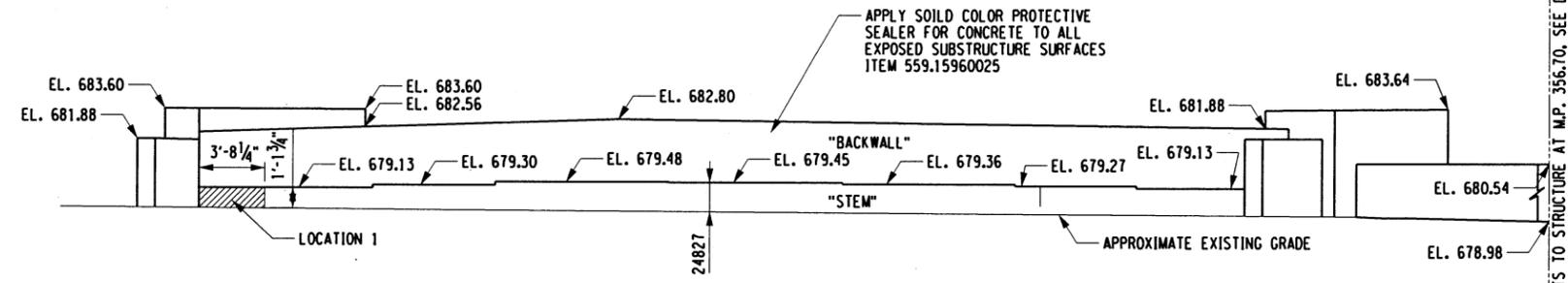
**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGEW REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 07/2016
TITLE OF DRAWING EAST ABUTMENT (W.B. TWY)	DRAWING NUMBER: AB-2

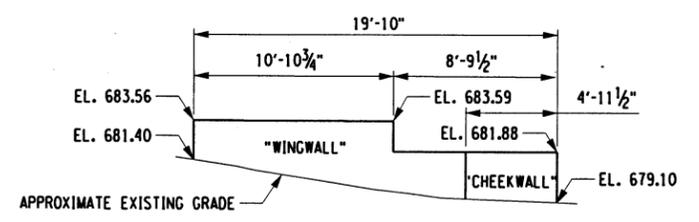
FILED: TA  
 CHECKED BY: M. COX  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



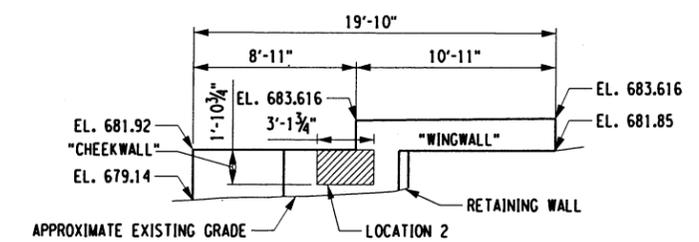
WEST ABUTMENT PLAN  
NTS



WEST ABUTMENT ELEVATION  
NTS



SOUTHWEST WINGWALL ELEVATION  
NTS



NORTHWEST WINGWALL ELEVATION  
NTS

ITEM 559.15960025

EAST ABUTMENT (EB TWY)	
LOCATION	AREA (SF)
STEM	240
BACKWALL & SEAT	360
CHEEKWALLS	160
WINGWALL	160
MEDIAN RETAINING WALL	150

ITEM 582.06

EAST ABUTMENT (EB TWY)			
LOCATION	L (FT)	H (FT)	AREA (SF)
1	3.67	1.167	4.3
2	3.167	1.87	5.92

**LEGEND:**

AREA TO BE REPAIRED UNDER ITEM 582.06  
REMOVAL OF STRUCTURAL CONCRETE - REPLACE WITH CLASS D

**NOTE:**  
 1- SEE AB-1 FOR SECTIONS  
 2- REINFORCEMENT NOT SHOWN FOR CLARITY  
 3- SEE TYPICAL CONCRETE REPAIR DETAILS FOR ADDITIONAL INFO

ALTERED ON:	AFFIXED ON: 8/29/16
SIGNATURE: STAMP:	SIGNATURE: STAMP:

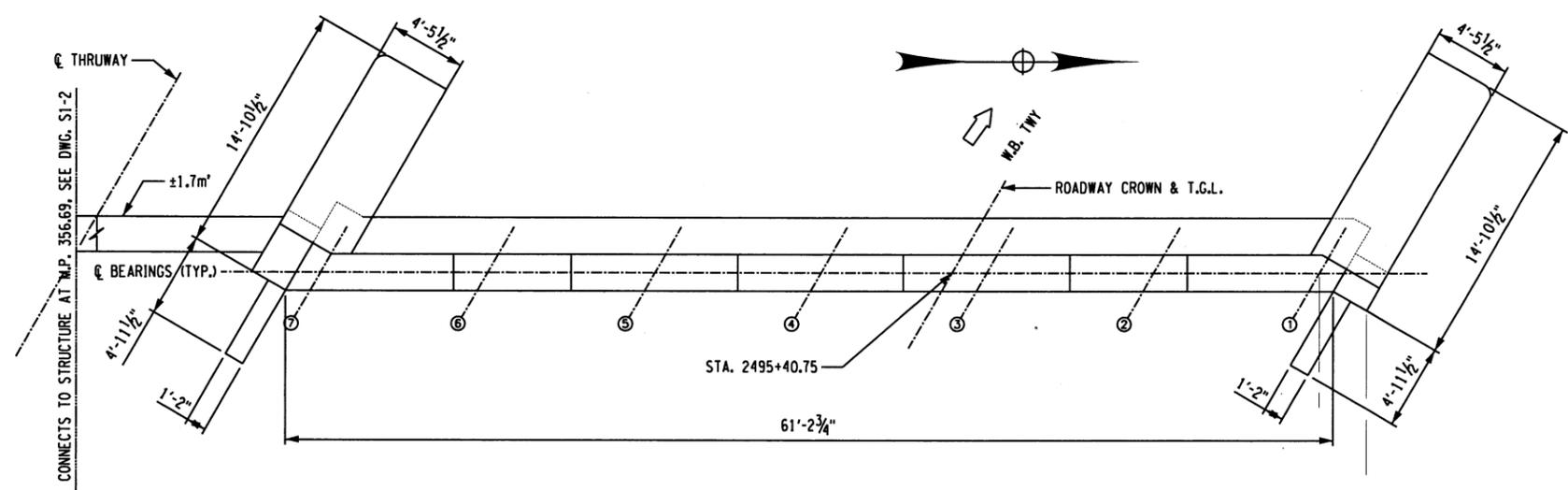
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

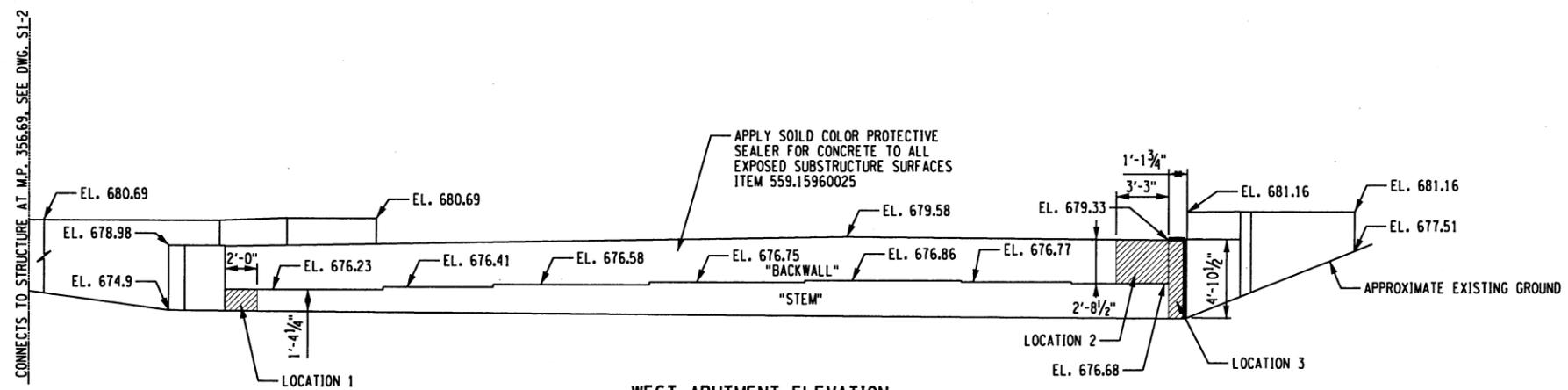
**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69& 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 07/2016
TITLE OF DRAWING WEST ABUTMENT (E.B. TWY)	DRAWING NUMBER: AB-3

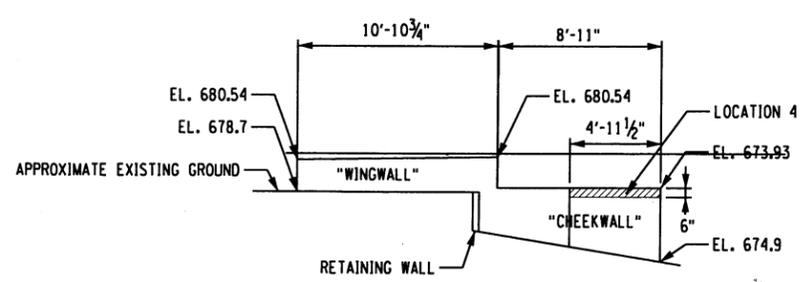
FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



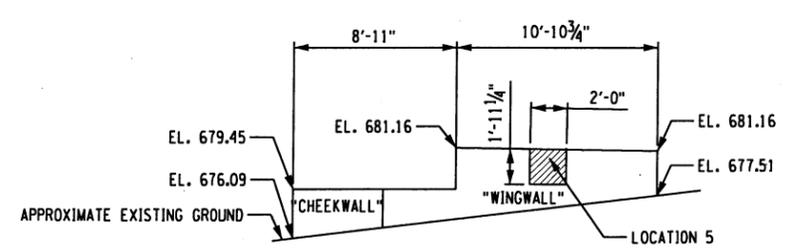
WEST ABUTMENT PLAN  
NTS



WEST ABUTMENT ELEVATION  
NTS



SOUTHWEST WINGWALL ELEVATION  
NTS



NORTHWEST WINGWALL ELEVATION  
NTS

ITEM 559.15960025

EAST ABUTMENT (EB TWY)	
LOCATION	AREA (SF)
STEM	240
BACKWALL & SEAT	360
CHEEKWALLS	160
WINGWALL	160

ITEM 582.06

EAST ABUTMENT (EB TWY)			
LOCATION	L (FT)	H (FT)	AREA (SF)
1	2.0	1.35	2.708
2	3.25	2.675	8.69
3	1.208	4.83	5.3
4	4.95	.5	2.479
5	2.0	2.0	4.0

**LEGEND:**

AREA TO BE REPAIRED UNDER ITEM 582.06  
REMOVAL OF STRUCTURAL CONCRETE - REPLACE WITH CLASS D

**NOTE:**  
 1- SEE AB-1 FOR SECTIONS  
 2- REINFORCEMENT NOT SHOWN FOR CLARITY  
 3- SEE TYPICAL CONCRETE REPAIR DETAILS FOR ADDITIONAL INFO

ALTERED ON: \_\_\_\_\_

AFFIXED ON: 9/15/16

SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_

SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_

**STATE OF NEW YORK**  
**DAVID T. VOSBURGH**  
 LICENSED PROFESSIONAL ENGINEER  
 074259

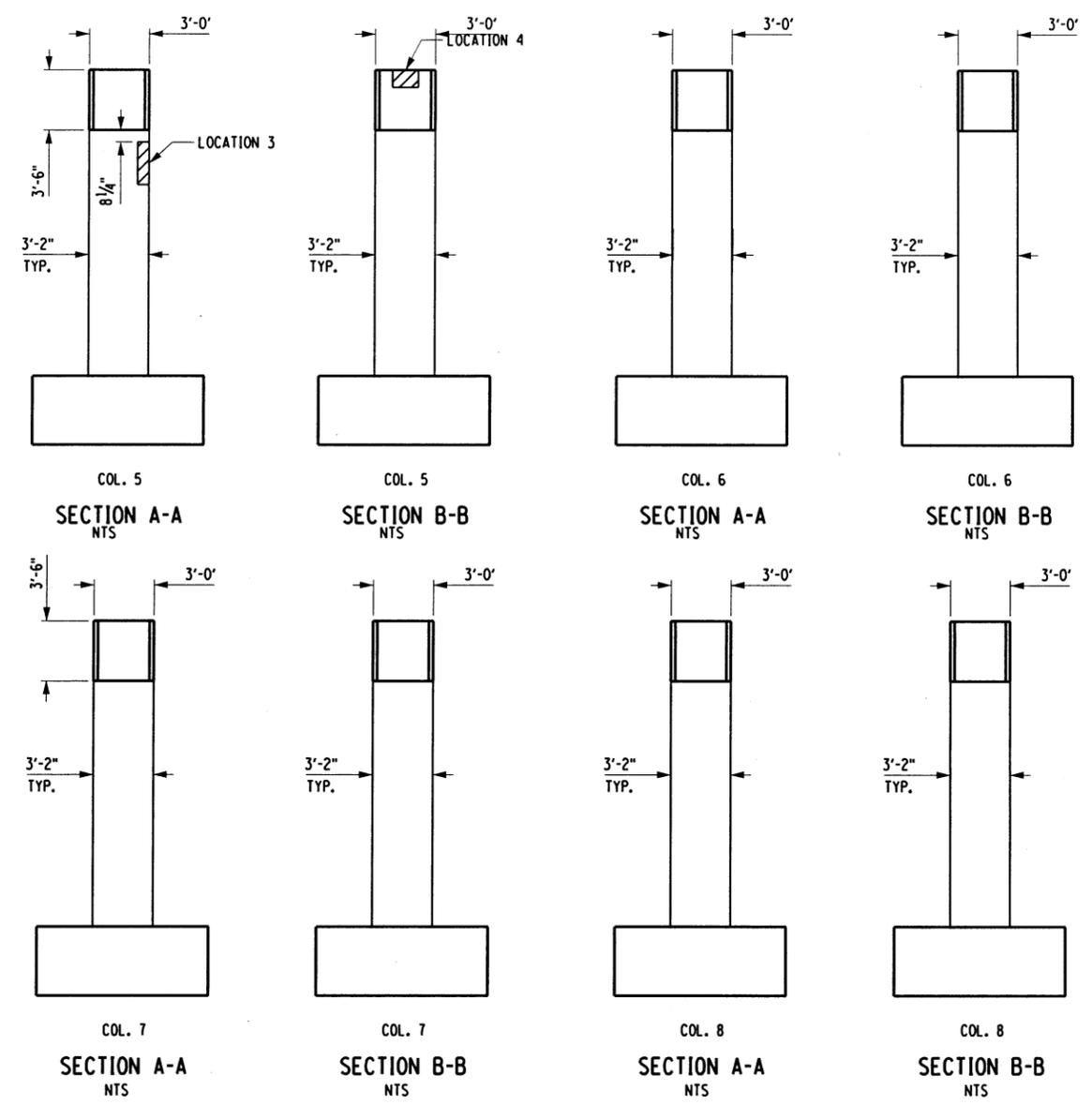
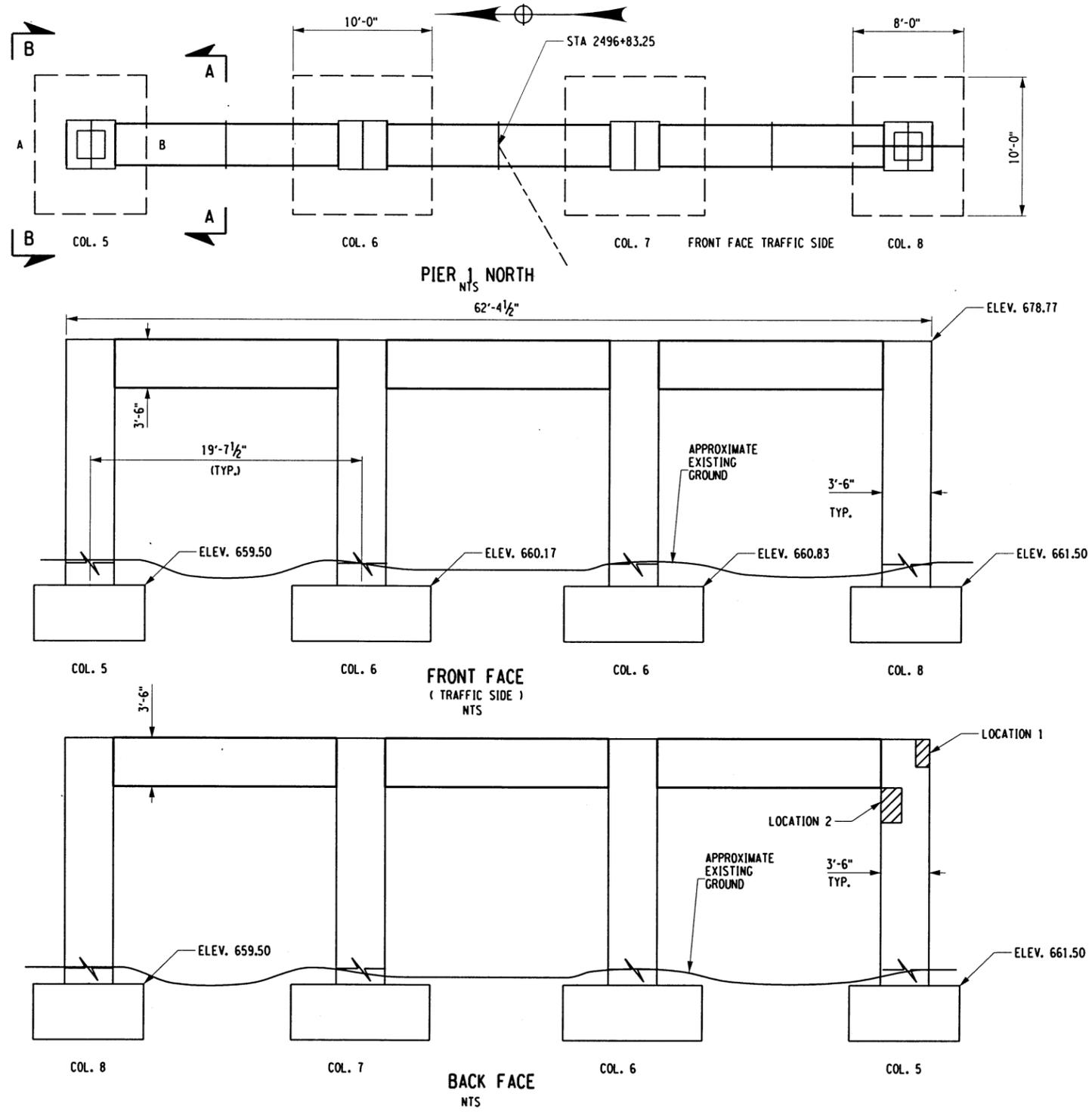
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY** | **Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT: NYS THRUWAY BRIDGE REHABILITATION  
 LOCATION OF PROJECT: TWY MP. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE CNTY  
 TITLE OF DRAWING: WEST ABUTMENT (W.B. TWY)

CONTRACT NUMBER: TAB 16-33B  
 DATE: 07/2016  
 DRAWING NUMBER: AB-4



- PIER REPAIR NOTES:
- 1- ALL COSTS FOR SUBSTRUCTURE REPAIR SHALL BE INCLUDED IN ITEM 582.06 REMOVAL OF STRUCTURAL CONCRETE AND REPLACEMENT WITH CLASS D.
  - 2- TRAFFIC ON LOCAL ROADS SHALL BE MAINTAINED. THE CONTRACTOR SHALL USE STANDARD MAINTENANCE AND PROTECTION TRAFFIC DRAWINGS FOR LOCAL ROADS.
  - 3- EXISTING REINFORCEMENT IS NOT SHOWN FOR CLARITY.
  - 4- EXISTING REINFORCEMENT IS TO REMAIN.
  - 5- SEE DWG. CRD-1 FOR TYPICAL REPAIR DETAILS.
  - 6- APPLY SOLID COLOR PROTECTIVE SEALER TO ALL EXPOSED SUBSTRUCTURE SURFACES, EXCEPT THE UNDERSIDE OF CAP BEAM SURFACES, ITEM 559.1596--25 (TYP.)



ITEM 559.1596--25

PIER 1 NORTH	
LOCATION:	AREA (SF)
COLUMNS	728
PIER CAP	654.94

ITEM 582.06

PER 1 NORTH			
LOCATION:	L	H	AREA (SF)
1	1.0	2.0	2
2	1.5	2.5	3.75
3	0.67	2.5	1.67
4	1.5	1.0	1.5
5			

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 8/29/16

SIGNATURE: \_\_\_\_\_ SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_ STAMP: \_\_\_\_\_

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

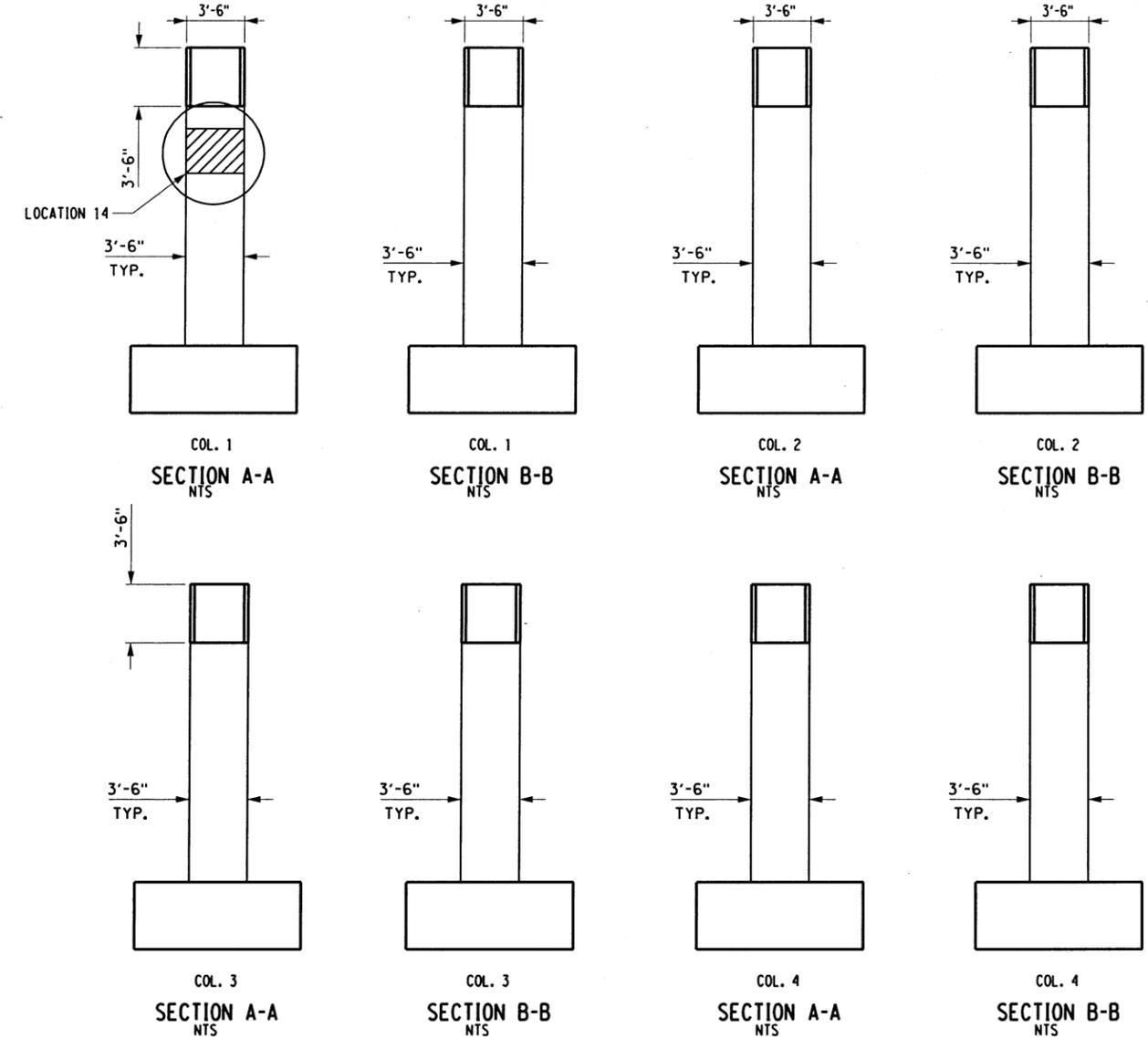
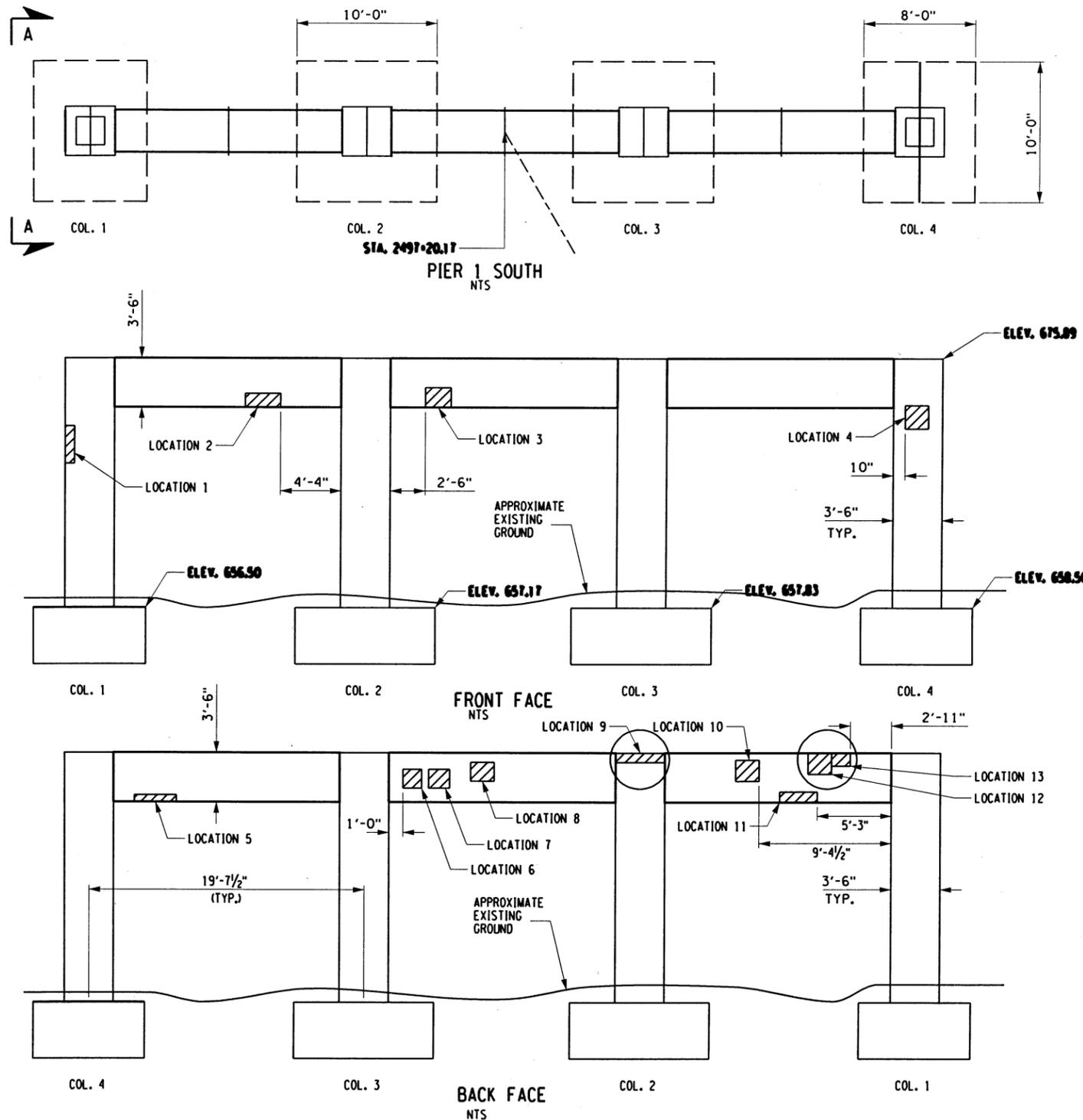
REVISIONS			
DATE	DESCRIPTION	BY	SYM.

NEW YORK STATE OF OPPORTUNITY

**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT: NYS THRUWAY BRIDGE REHABILITATION  
 LOCATION OF PROJECT: TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY  
 TITLE OF DRAWING: PIER 1 NORTH REPAIRS (WB TWY STA 2496+83.25)

CONTRACT NUMBER: TAB 16-33B  
 DATE: 06/29/2016  
 DRAWING NUMBER: P1-NR



- PIER REPAIR NOTES:**
- ALL COSTS FOR SUBSTRUCTURE REPAIR SHALL BE INCLUDED IN ITEM 582.06 REMOVAL OF STRUCTURAL CONCRETE AND REPLACEMENT WITH CLASS D.
  - TRAFFIC ON LOCAL ROADS SHALL BE MAINTAINED. THE CONTRACTOR SHALL USE STANDARD MAINTENANCE AND PROTECTION TRAFFIC DRAWINGS FOR LOCAL ROADS.
  - EXISTING REINFORCEMENT IS NOT SHOWN FOR CLARITY.
  - EXISTING REINFORCEMENT IS TO REMAIN.
  - SEE DWG. CRD-1 FOR TYPICAL REPAIR DETAILS.
  - APPLY SOLID COLOR PROTECTIVE SEALER TO ALL EXPOSED SUBSTRUCTURE SURFACES, EXCEPT THE UNDERSIDE OF CAP BEAM SURFACES, ITEM 559.1596--25 (TYP.)

- ITEM 582.06

PIER 1 SOUTH			
LOCATION	L	H	AREA (SF)
1	0.67	2.67	1.79
2	2.5	1.0	2.5
3	1.83	1.42	2.6
4	1.66	1.66	2.76
5	3.0	0.5	1.5
6	1.33	1.33	1.77
7	1.5	1.33	2.0
8	1.33	1.66	2.21
9	1.0	0.66	0.66
10	1.66	1.5	2.49
11	2.66	0.75	2.0
12	1.66	1.5	2.49
13	1.33	0.92	1.22

ITEM 559.1596--25

PIER 1 SOUTH	
LOCATION:	AREA (SF)
COLUMNS	728
PIER CAP	654.94

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 8/29/16

SIGNATURE: \_\_\_\_\_ SIGNATURE:

STAMP: \_\_\_\_\_

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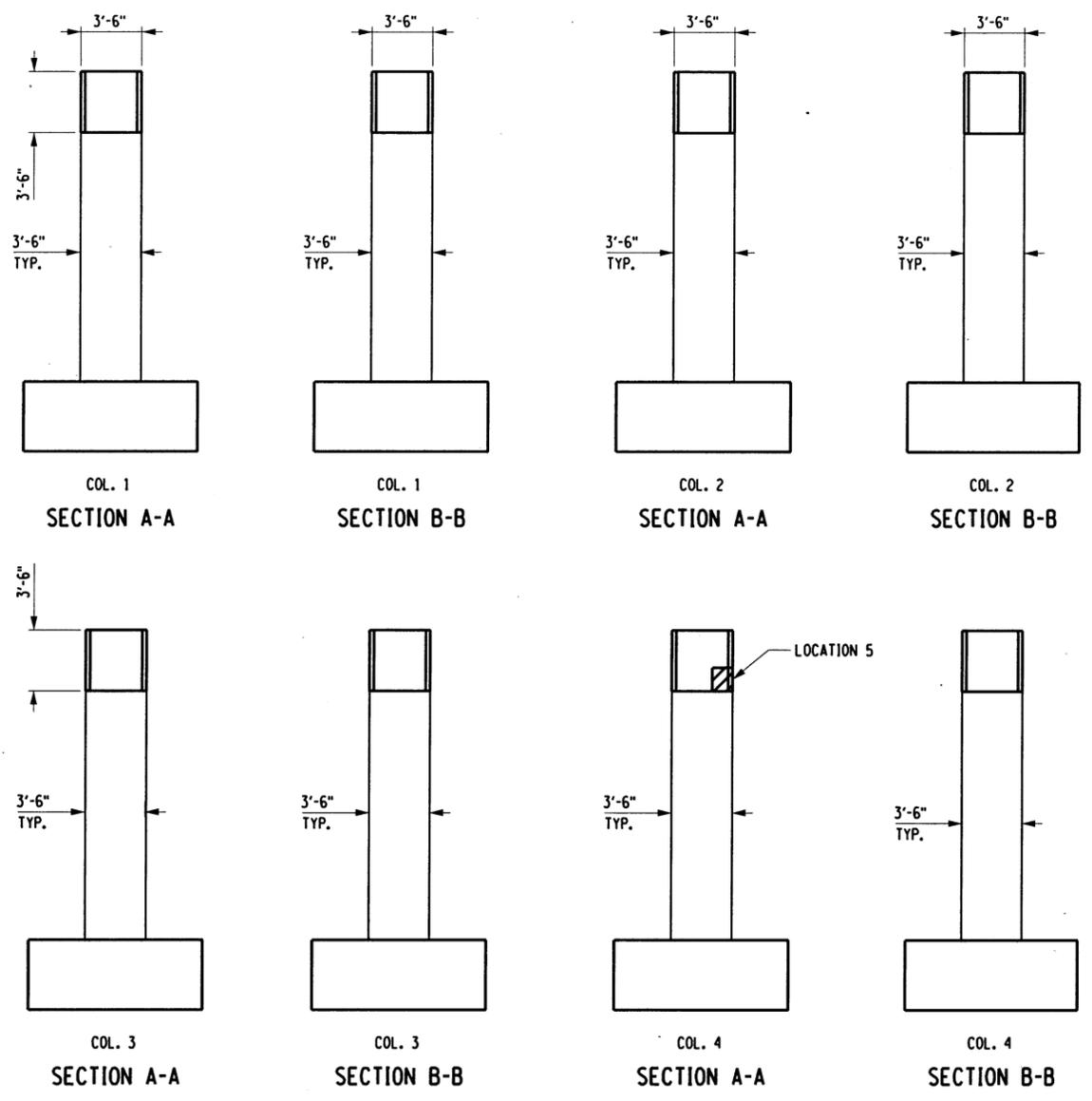
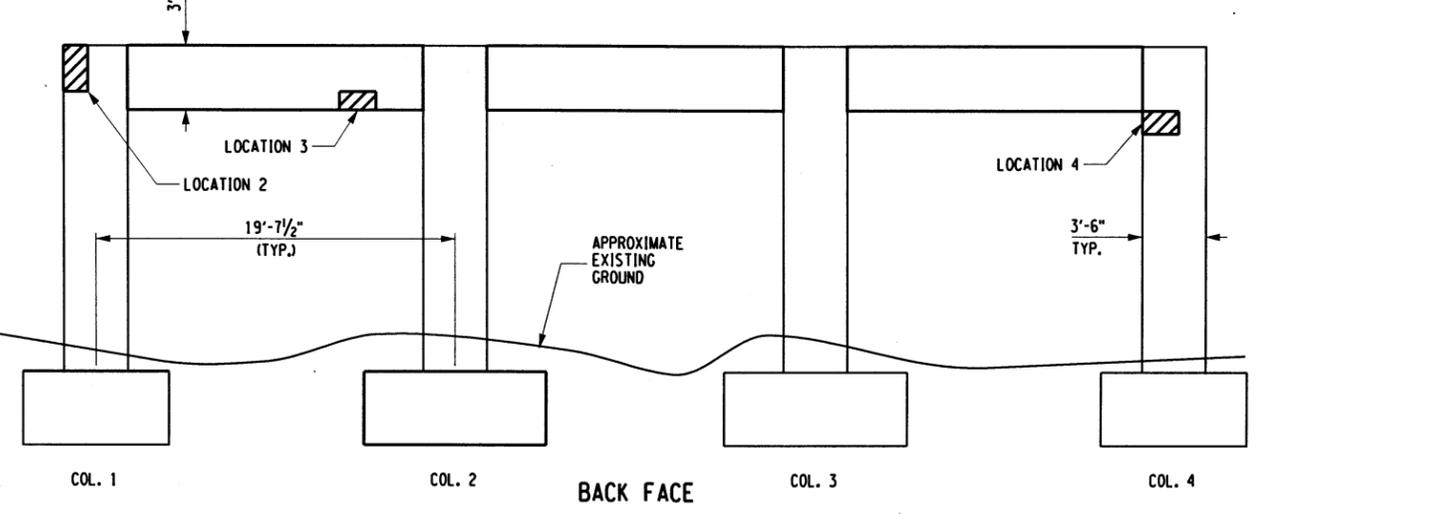
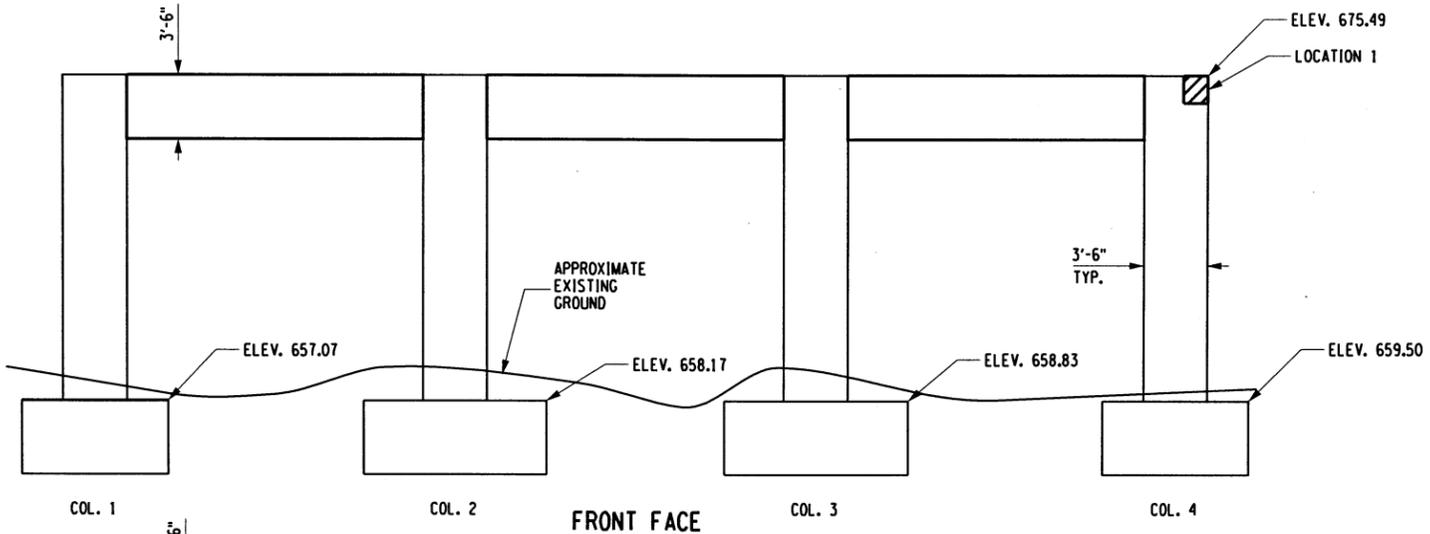
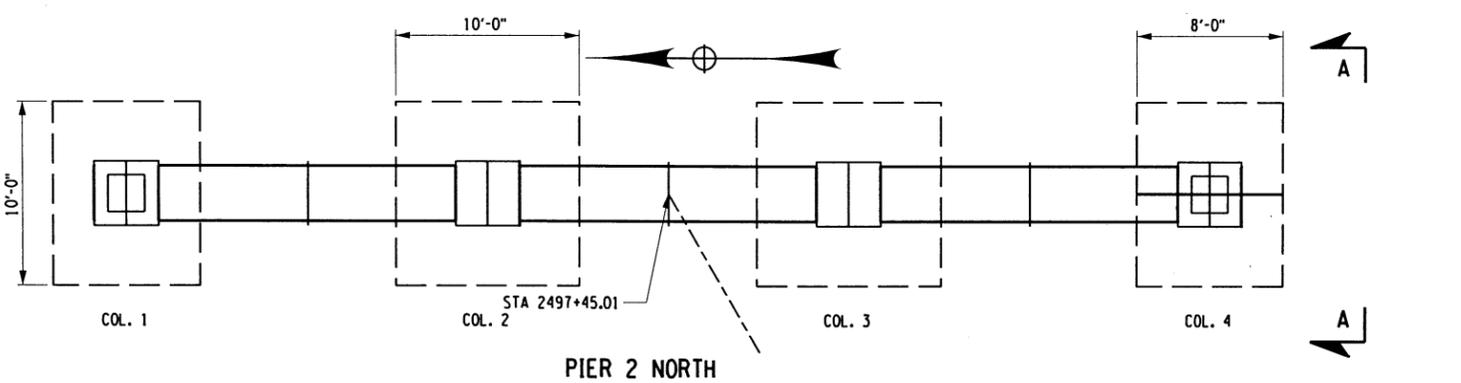
REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT: NYS THRUWAY BRIDGE REHABILITATION  
 LOCATION OF PROJECT: TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY  
 TITLE OF DRAWING: PIER 1 SOUTH REPAIRS (STA. 2497+20.17)

CONTRACT NUMBER: TAB 16-33B  
 DATE: 06/29/2016  
 DRAWING NUMBER: P1-SR

FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



- PIER REPAIR NOTES:**
- 1- ALL COSTS FOR SUBSTRUCTURE REPAIR SHALL BE INCLUDED IN ITEM 582.06 REMOVAL OF STRUCTURAL CONCRETE AND REPLACEMENT WITH CLASS D.
  - 2- TRAFFIC ON LOCAL ROADS SHALL BE MAINTAINED. THE CONTRACTOR SHALL USE STANDARD MAINTENANCE AND PROTECTION TRAFFIC DRAWINGS FOR LOCAL ROADS.
  - 3- EXISTING REINFORCEMENT IS NOT SHOWN FOR CLARITY.
  - 4- EXISTING REINFORCEMENT IS TO REMAIN.
  - 5- SEE DWG. CRD-1 FOR TYPICAL REPAIR DETAILS.
  - 6- APPLY SOLID COLOR PROTECTIVE SEALER TO ALL EXPOSED SUBSTRUCTURE SURFACES, EXCEPT THE UNDERSIDE OF CAP BEAM SURFACES, ITEM 559.1596--25 (TYP.)

- ITEM 582.06

**ITEM 559.1596--25**

PIER 2 NORTH	
LOCATION:	AREA (SF)
COLUMNS	728
PIER CAP	654.94

**ITEM 582.06**

PIER 2 NORTH			
LOCATION	L	H	AREA (SF)
1	1.33	1.5	2.0
2	1.33	2.5	3.33
3	2.0	1.0	2.0
4	2.0	1.25	2.5
5	1.16	1.33	1.54

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 8/29/16

SIGNATURE: \_\_\_\_\_ SIGNATURE: \_\_\_\_\_

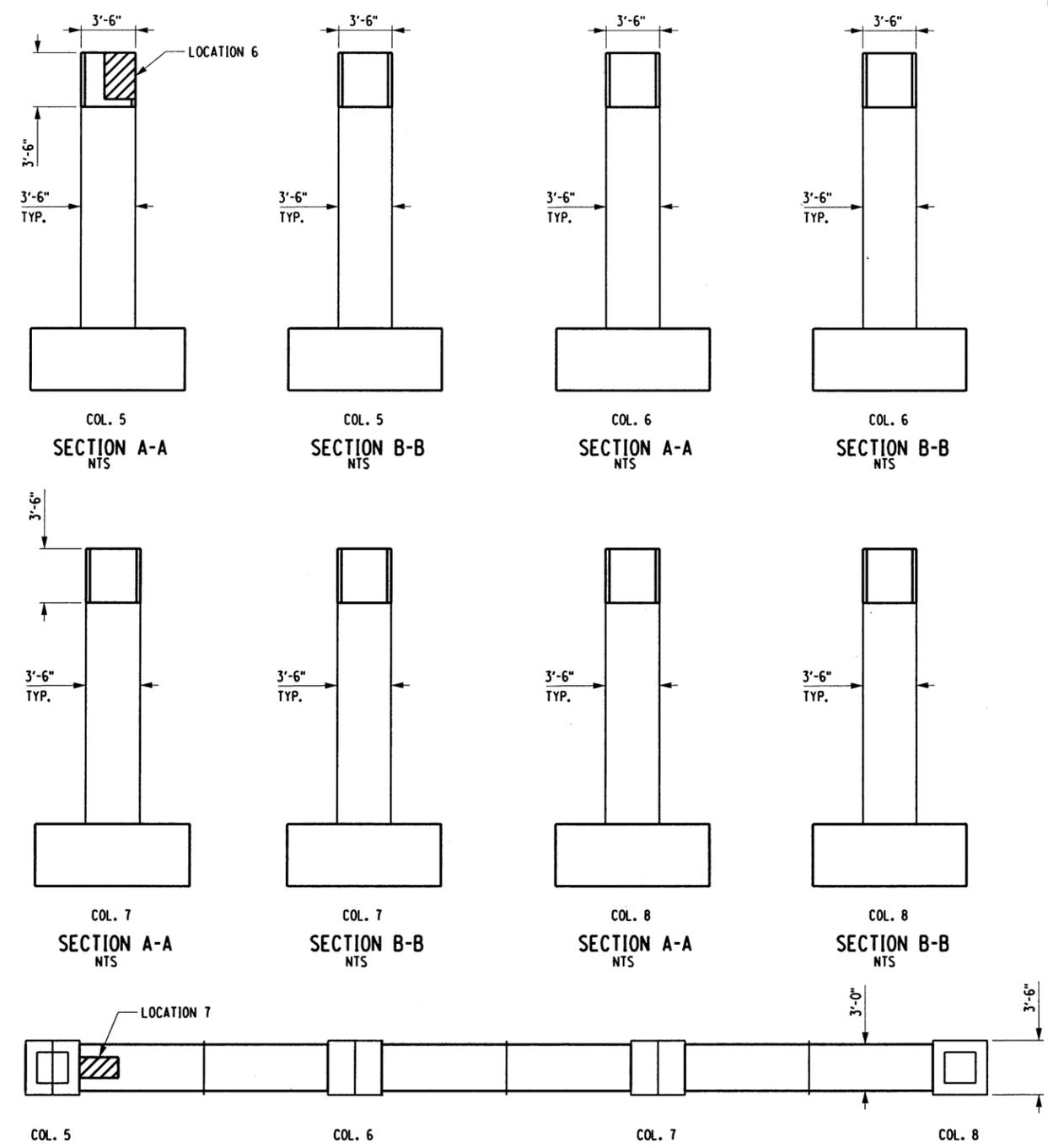
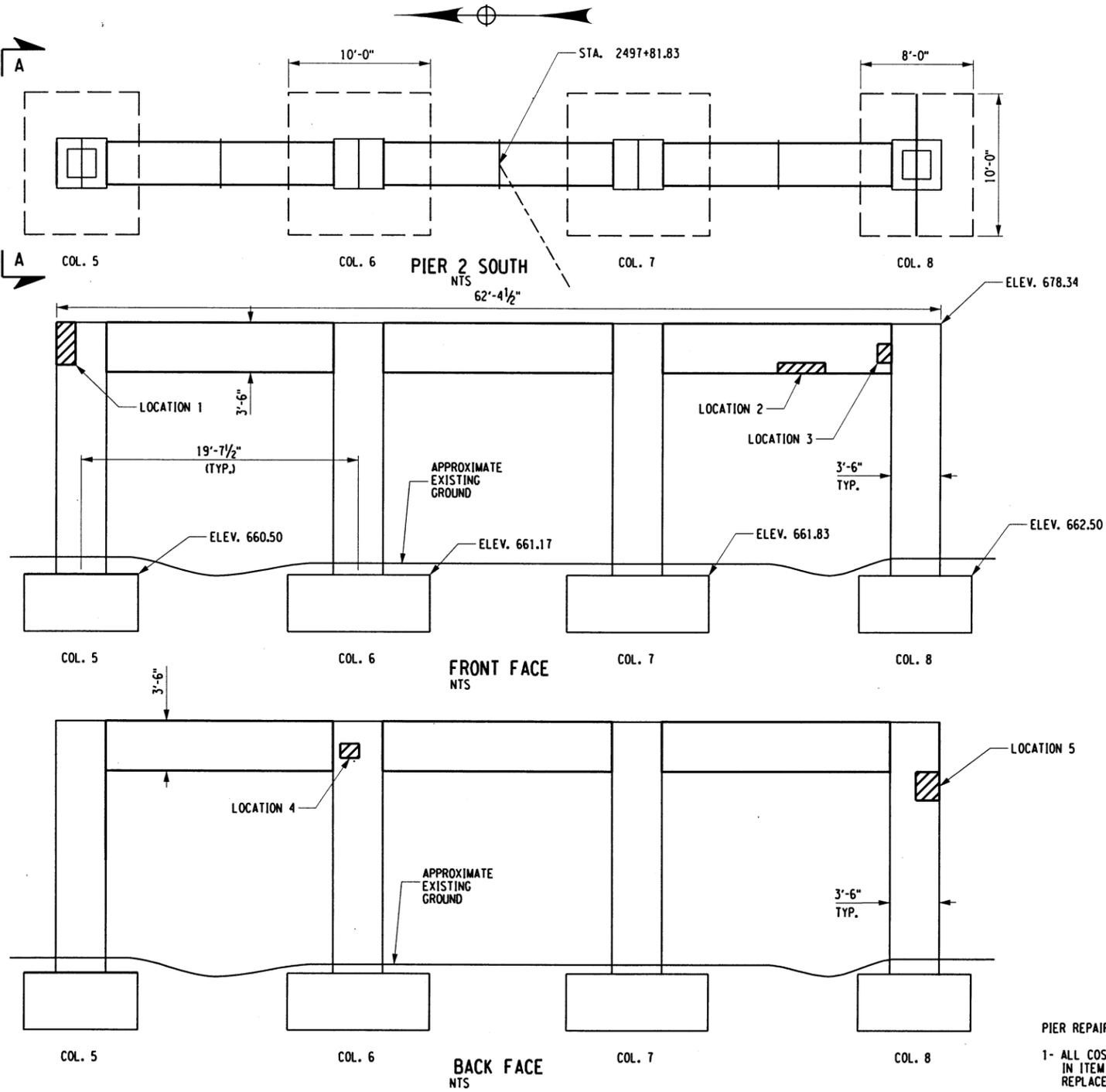
STAMP: \_\_\_\_\_ STAMP: \_\_\_\_\_

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/29/2016
TITLE OF DRAWING PIER 2 NORTH REPAIRS (STA. 2497+45.01)	DRAWING NUMBER: P2-NR



**PIER REPAIR NOTES:**

- 1- ALL COSTS FOR SUBSTRUCTURE REPAIR SHALL BE INCLUDED IN ITEM 582.06 REMOVAL OF STRUCTURAL CONCRETE AND REPLACEMENT WITH CLASS D.
- 2- TRAFFIC ON LOCAL ROADS SHALL BE MAINTAINED. THE CONTRACTOR SHALL USE STANDARD MAINTENANCE AND PROTECTION TRAFFIC DRAWINGS FOR LOCAL ROADS.
- 3- EXISTING REINFORCEMENT IS NOT SHOWN FOR CLARITY.
- 4- EXISTING REINFORCEMENT IS TO REMAIN.
- 5- SEE DWG. CRD-1 FOR TYPICAL REPAIR DETAILS.
- 6- APPLY SOLID COLOR PROTECTIVE SEALER TO ALL EXPOSED SUBSTRUCTURE SURFACES, EXCEPT THE UNDERSIDE OF CAP BEAM SURFACES, ITEM 559.1596--25 (TYP.)

**BOTTOM OF PIER CAP**  
NTS

ITEM 559.1596--25

PIER 2 SOUTH	
LOCATION:	AREA (SF)
COLUMNS	728
PIER CAP	654.94

- ITEM 582.06

PIER 2 SOUTH			
LOCATION	L	H	AREA (SF)
1	1.33	3.0	4.0
2	3.33	0.75	2.5
3	1.0	1.33	1.33
4	1.33	1.0	1.33
5	1.67	2.0	3.34
6	2.0	3.0	6.0
7	2.5	1.33	3.33

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 8/29/16

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

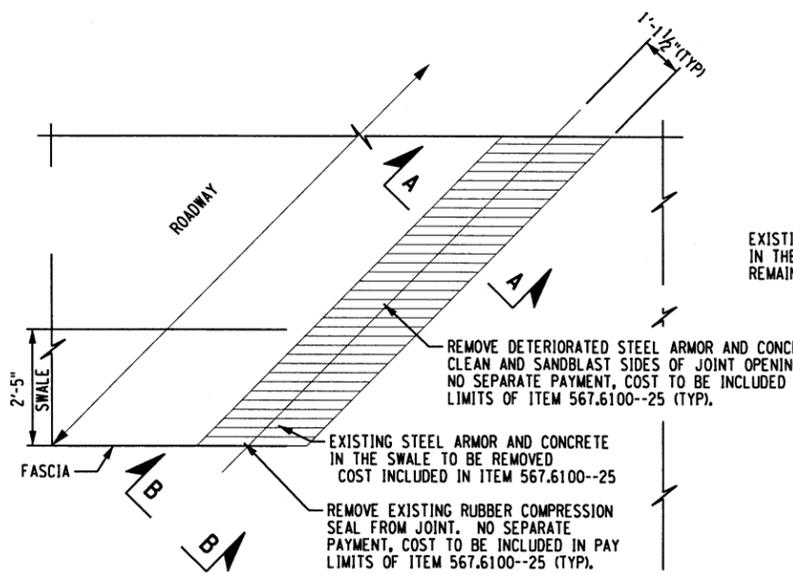
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

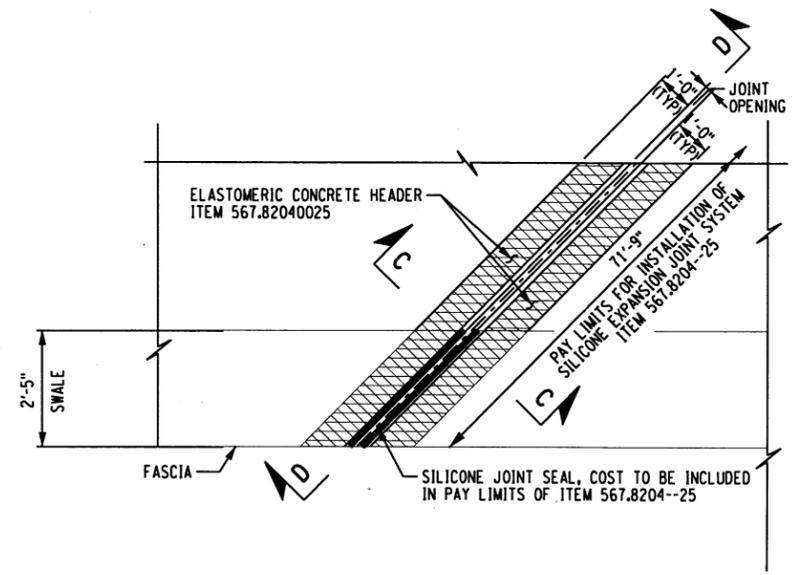
**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION PITTSFORD	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 MENDON CENTER RD., MONROE COUNTY	DATE: 06/29/2016
TITLE OF DRAWING PIER 2 SOUTH REPAIRS (STA. 2497+81.83)	DRAWING NUMBER: P2-SR

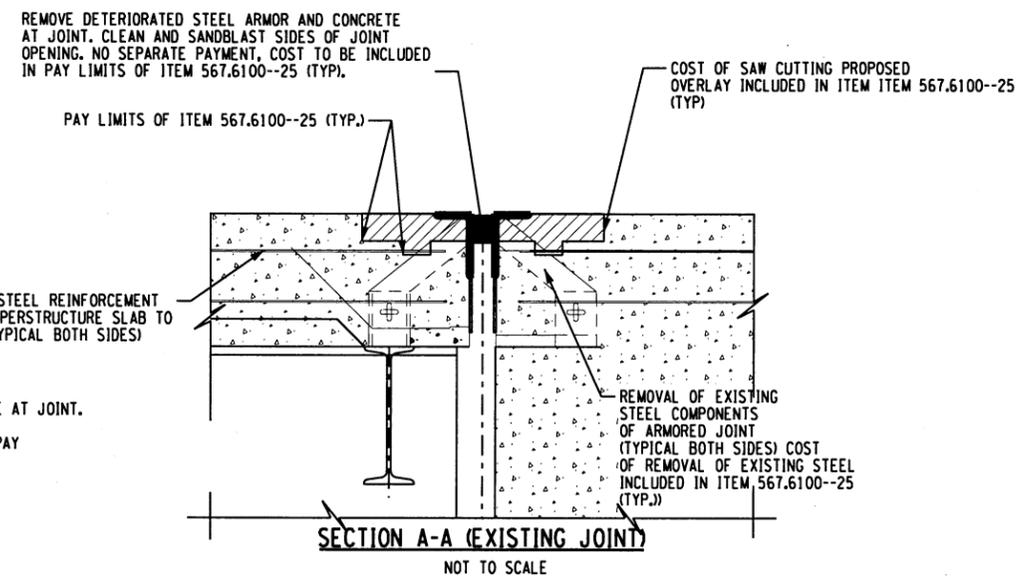
FILED BY: TA  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



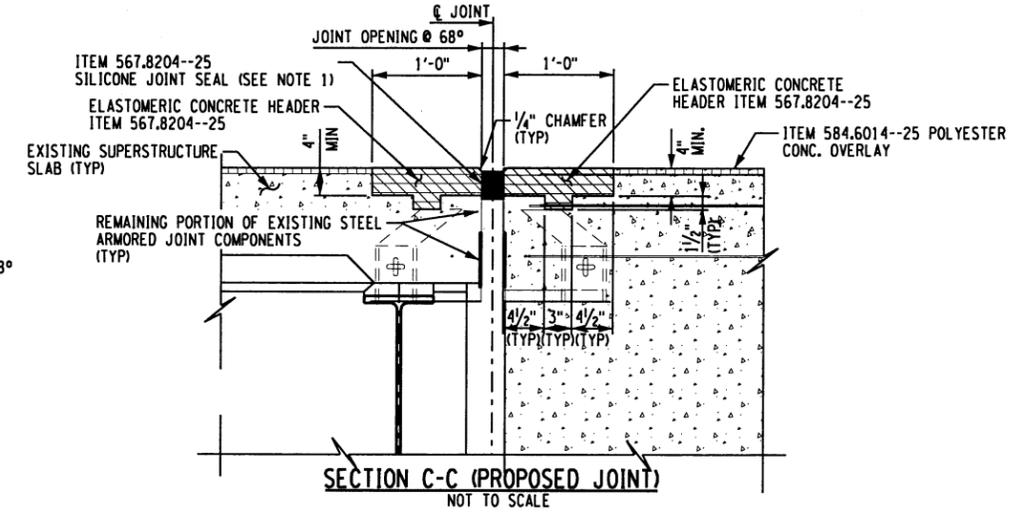
**PARTIAL PLAN EXISTING**  
(BOTH SIDES SIMILAR)  
NOT TO SCALE



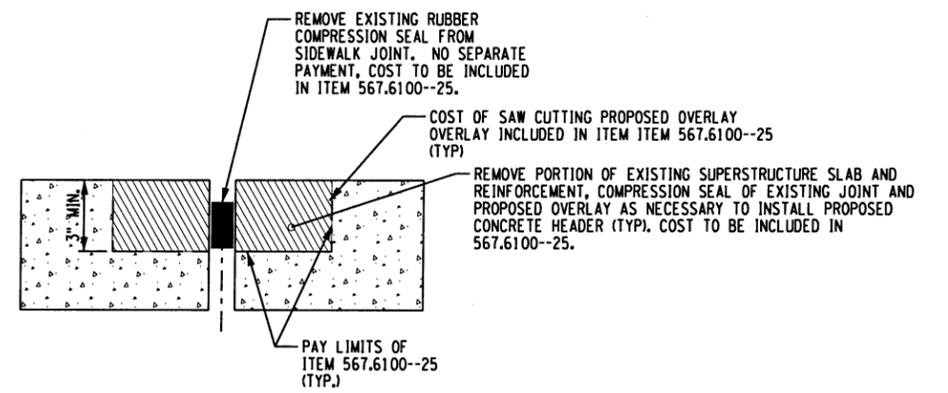
**PARTIAL PLAN PROPOSED**  
(BOTH SIDES SIMILAR)  
NOT TO SCALE



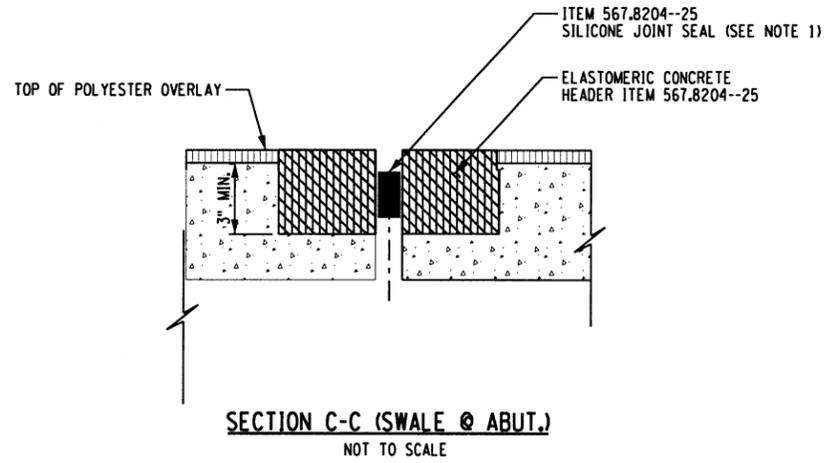
**SECTION A-A (EXISTING JOINT)**  
NOT TO SCALE



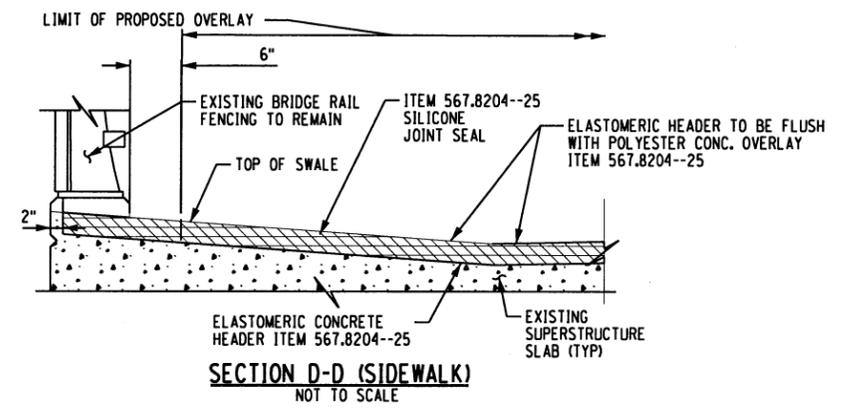
**SECTION C-C (PROPOSED JOINT)**  
NOT TO SCALE



**SECTION B-B (SWALE @ ABUT.)**  
NOT TO SCALE REINFORCEMENT  
AND HARDWARE NOT SHOWN FOR CLARITY



**SECTION C-C (SWALE @ ABUT.)**  
NOT TO SCALE



**SECTION D-D (SIDEWALK)**  
NOT TO SCALE

**NOTES**

- AREA OF EXISTING CONCRETE TO BE REMOVED, ITEM 567.6100--25.
- ELASTOMERIC CONCRETE HEADER TO ITEM 567.8204--25.
- ITEM 584.6014-25 POLYESTER CONC. OVERLAY

1- CONTRACTOR SHALL CONSULT WITH THE SEAL MANUFACTURER AND DETERMINE AND SUPPLY THE PROPER SIZE SILICONE SEAL WITH DIMENSIONS BASED UPON JOINT OPENING SIZE AND TOTAL EXPECTED MOVEMENT OF 1/8" AT PIER 1 AND 1/8" AT PIER 3. JOINT OPENING @ 68°F IS 2 3/8" PER THE RECORD PLANS. CONTRACTOR SHALL FIELD VERIFY THIS DIMENSION.

2- ELASTOMERIC CONCRETE HEADERS SHALL NOT OVERHANG THE CONCRETE SLAB UNDER ANY CIRCUMSTANCES. THE COST OF THE ELASTOMERIC CONCRETE AND INSTALLATION OF SILICONE SEAL SHALL BE INCLUDED IN ITEM 567.8204--25. THE CONTRACTOR SHALL NOTE THE PROPOSED BRIDGE JOINTS SHALL BE INSTALLED AFTER THE PROPOSED OVERLAY IS COMPLETED. THE PROPOSED JOINTS SHALL BE PLACED TO THE SAME GRADE AND ELEVATION AS THE EXISTING TOP WEARING SURFACE

3-AVOID DAMAGE TO EXISTING REINFORCEMENT. CLEAN AND COAT ALL EXPOSED REINFORCEMENT WITH ZINC RICH PRIMER BEFORE PLACING ELASTOMERIC CONCRETE HEADER, INCLUDED IN ITEM 567.6100--25

NOTE:  
EXISTING BRIDGE RAIL NOT SHOWN FOR CLARITY UNLESS NOTED OTHERWISE  
EXISTING BRIDGE RAIL TO REMAIN

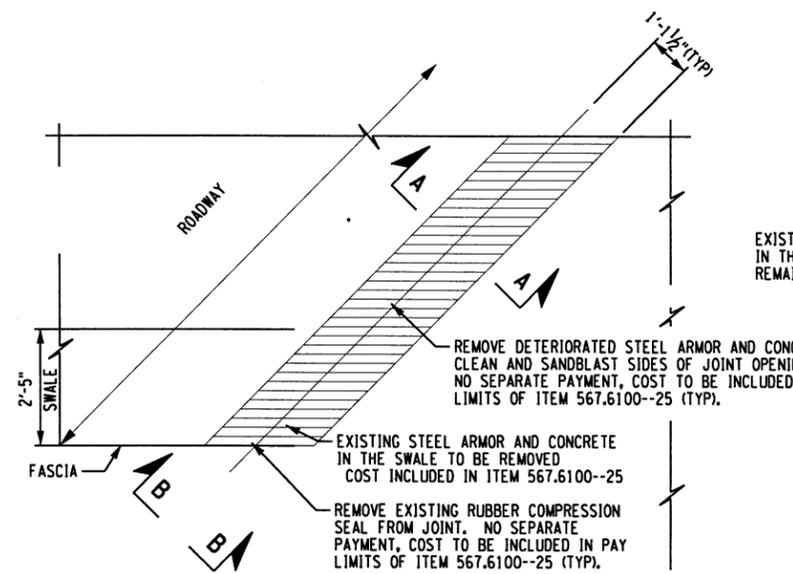
ALTERED ON:	AFFIXED ON: 9/15/16
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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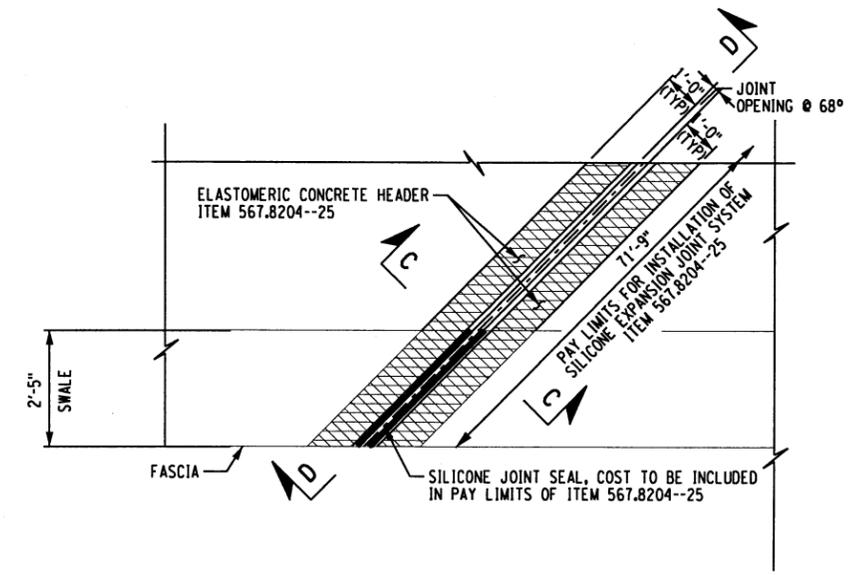
REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/29/2016
	TITLE OF DRAWING M.P. 356.69 EXISTING & PROPOSED JOINTS	DRAWING NUMBER: JD-1

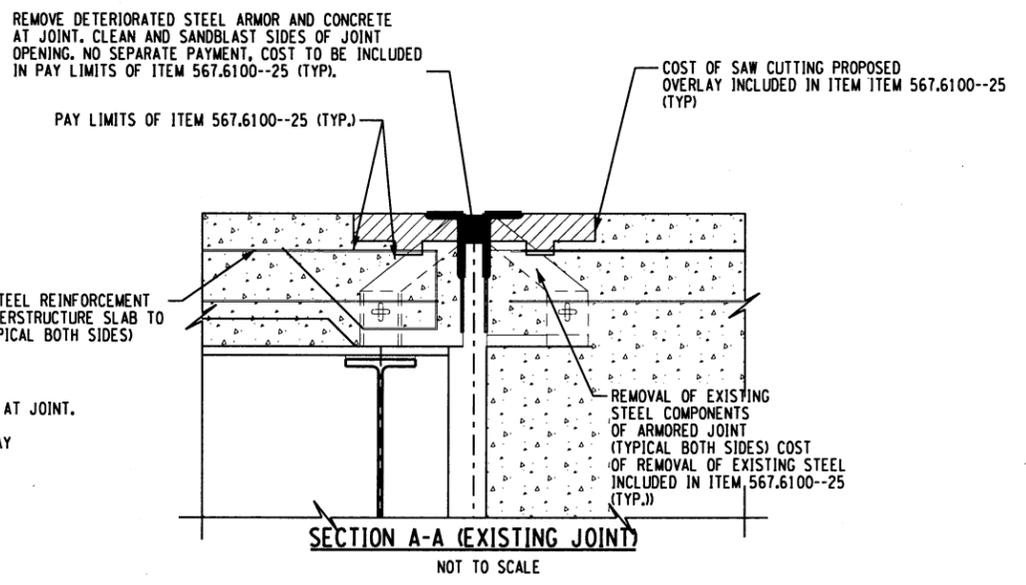
FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSSBURGH



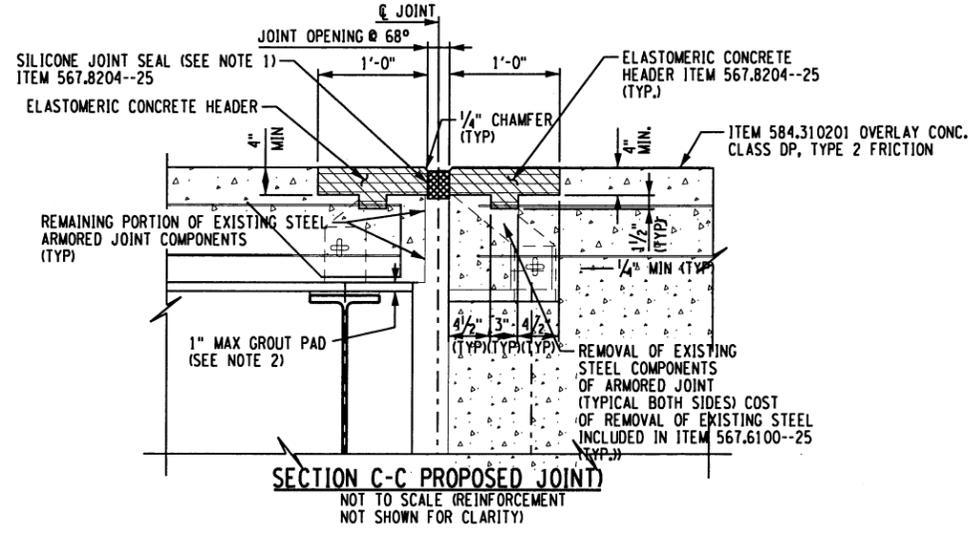
**PARTIAL PLAN EXISTING**  
(BOTH SIDES SIMILAR)  
NOT TO SCALE



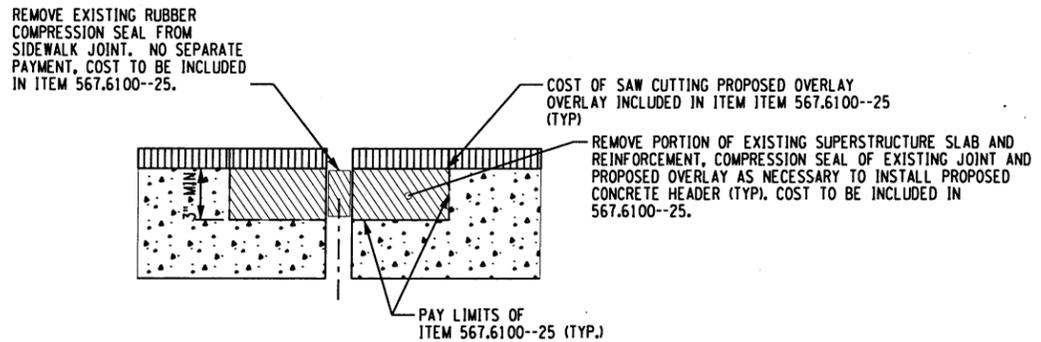
**PARTIAL PLAN PROPOSED**  
(BOTH SIDES SIMILAR)  
NOT TO SCALE



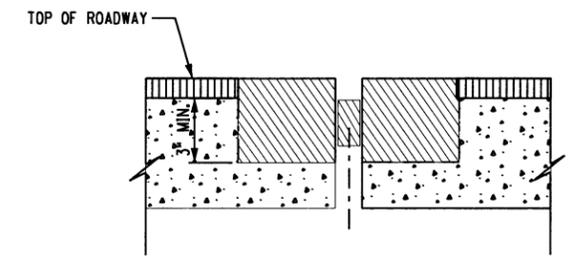
**SECTION A-A (EXISTING JOINT)**  
NOT TO SCALE



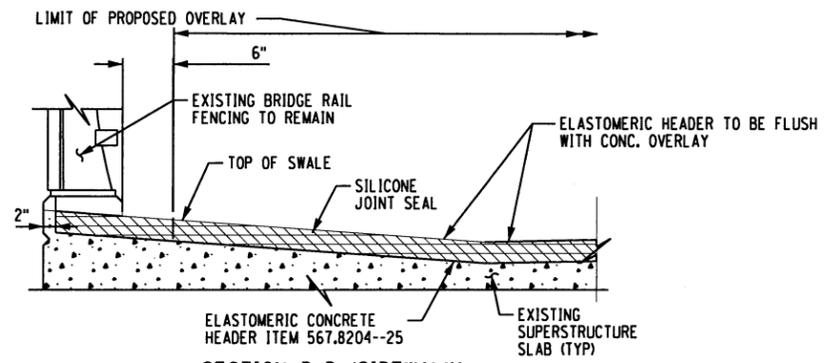
**SECTION C-C PROPOSED JOINT**  
NOT TO SCALE (REINFORCEMENT NOT SHOWN FOR CLARITY)



**SECTION B-B (SWALE @ ABUT.)**  
NOT TO SCALE REINFORCEMENT AND HARDWARE NOT SHOWN FOR CLARITY



**SECTION B-B (SWALE @ ABUT.)**  
NOT TO SCALE



**SECTION D-D (SIDEWALK)**  
NOT TO SCALE

**NOTES**

- AREA OF EXISTING CONCRETE TO BE REMOVED, ITEM 567.6100--25.
- ELASTOMERIC CONCRETE HEADER ITEM 567.8204--25.
- ITEM 581.01

- 1- CONTRACTOR SHALL CONSULT WITH THE SEAL MANUFACTURER AND DETERMINE AND SUPPLY THE PROPER SIZE SILICONE SEAL WITH DIMENSIONS BASED UPON JOINT OPENING SIZE AND TOTAL EXPECTED MOVEMENT OF 1 1/4" AT PIER 1 AND 1 3/8" AT PIER 3. JOINT OPENING @ 68°F IS 2 3/8" PER THE RECORD PLANS. CONTRACTOR SHALL FIELD VERIFY THIS DIMENSION.
- 2- ELASTOMERIC CONCRETE HEADERS SHALL NOT OVERHANG THE CONCRETE SLAB UNDER ANY CIRCUMSTANCES. THE COST OF THE ELASTOMERIC CONCRETE AND INSTALLATION OF SILICONE SEAL SHALL BE INCLUDED IN ITEM 567.8204--25. THE CONTRACTOR SHALL NOTE THE PROPOSED BRIDGE JOINTS SHALL BE INSTALLED AFTER THE PROPOSED OVERLAY IS COMPLETED. THE PROPOSED JOINTS SHALL BE PLACED TO THE SAME GRADE AND ELEVATION AS THE EXISTING TOP WEARING SURFACE
- 3-AVOID DAMAGE TO EXISTING REINFORCEMENT. CLEAN AND COAT ALL EXPOSED REINFORCEMENT WITH ZINC RICH PRIMER BEFORE PLACING ELASTOMERIC CONCRETE HEADER, INCLUDED IN ITEM 567.6100--25

NOTE:  
EXISTING BRIDGE RAIL NOT SHOWN FOR CLARITY UNLESS NOTED OTHERWISE

ALTERED ON:	AFFIXED ON: 9/15/16
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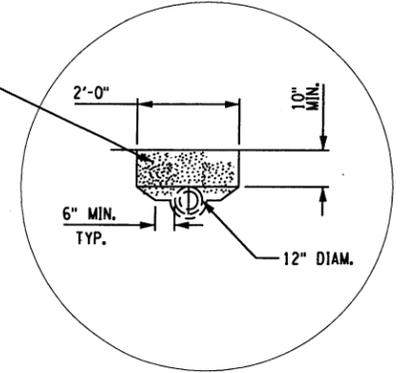
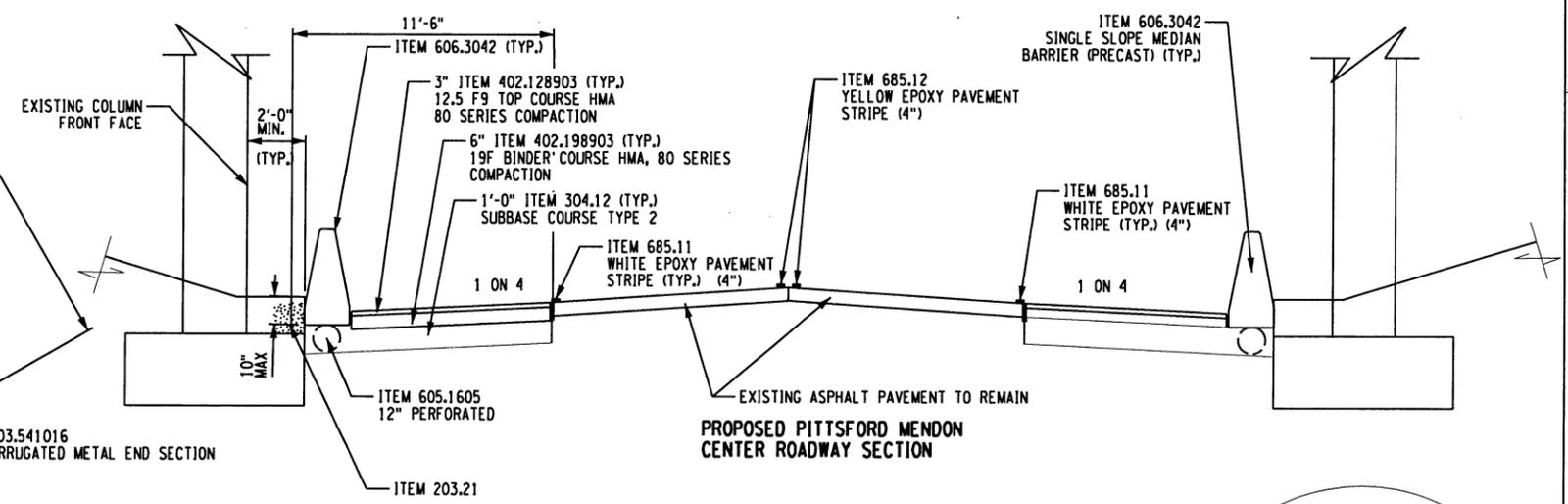
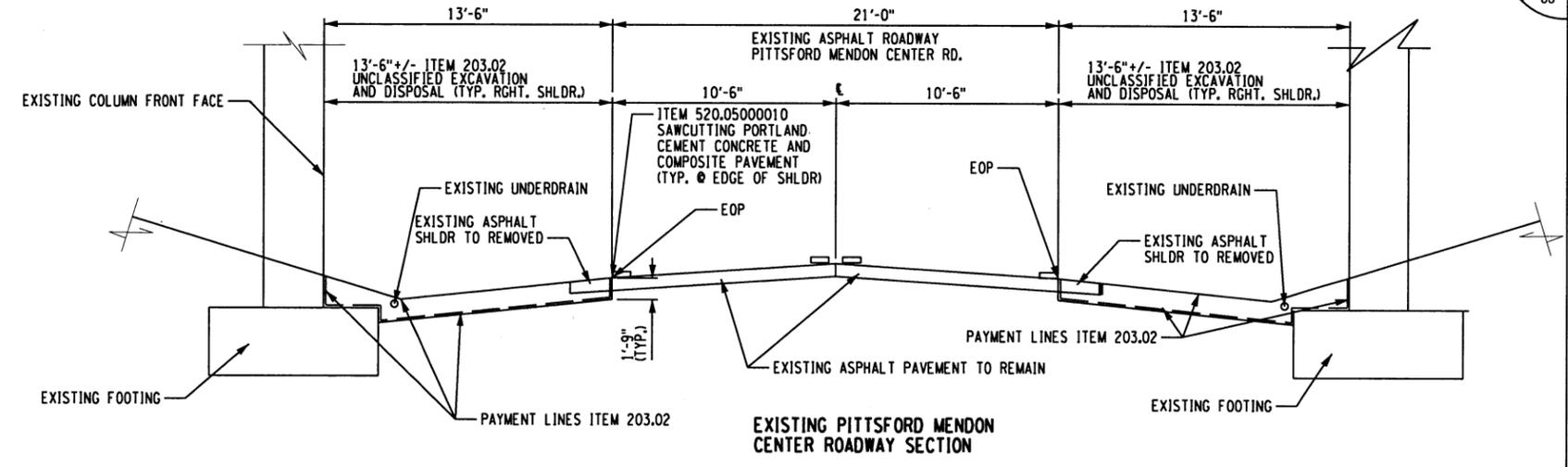
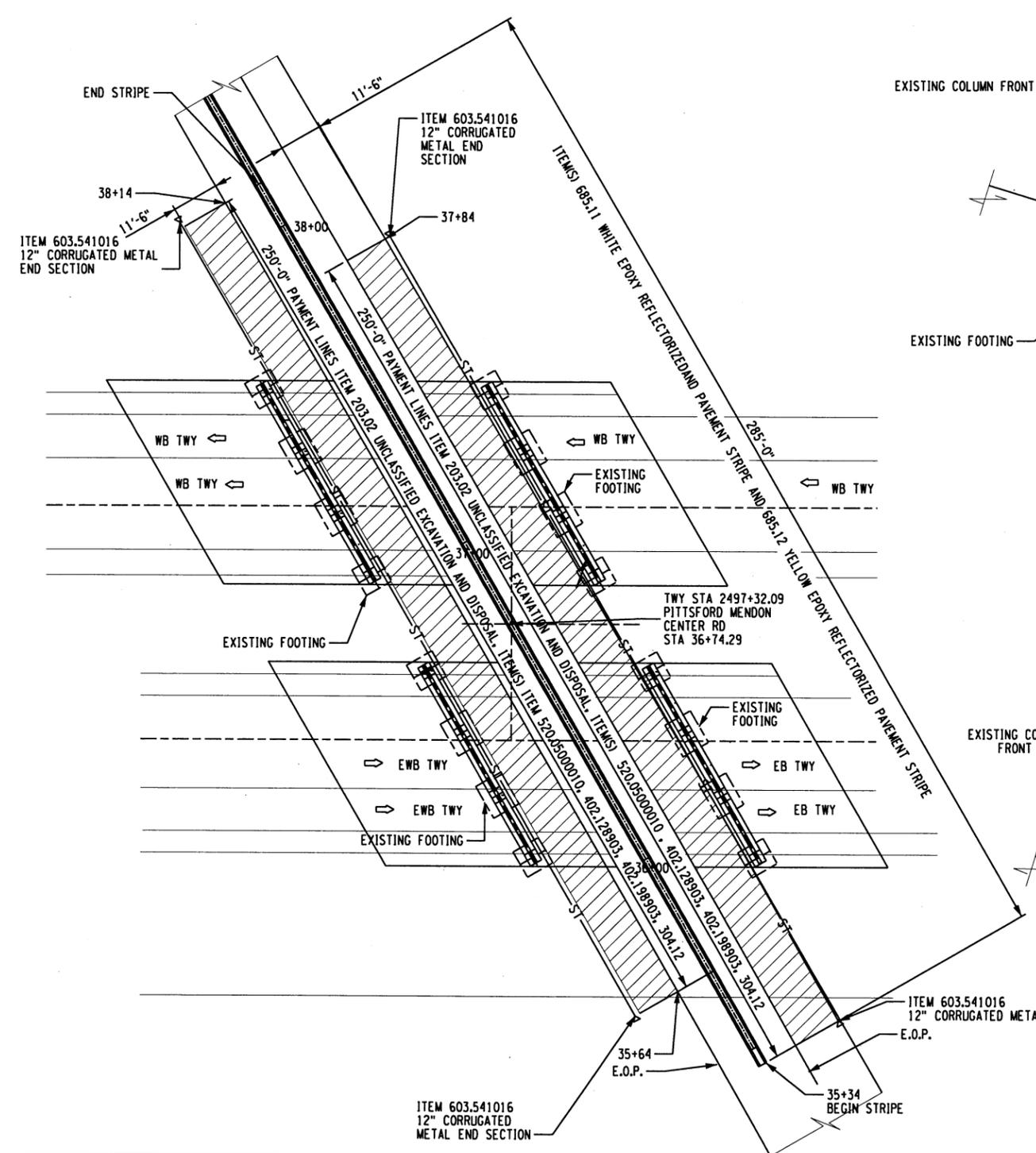
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/29/2016
TITLE OF DRAWING M.P. 356.70 EXISTING & PROPOSED JOINTS	DRAWING NUMBER: JD-2

FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



- NOTES:
- 1- THE CONTRACTOR SHALL ESTABLISH PROFILE FOR POSITIVE DRAINAGE BY SURVEY ITEM 625.01. A MINIMUM OF 1% SLOPE SHALL BE MAINTAINED
  - 2- COARSE AGGREGATE SHALL BE PLACED IN 6" LIFTS AND COMPACTED SUFFICIENTLY AS TO BE FIRM AND UNYIELDING.
  - 3- LIMITS OF SHOULDER RECONSTRUCTION AND DRAINAGE WORK
  - 4- DRAINAGE CULVERT PIPE ITEM 605.1605 PERFORATED POLYVINYL CHLORIDE UNDERDRAIN PIPE 12" DIAMETER.

ALTERED ON:	AFFIXED ON: 9/15/16
SIGNATURE: STAMP:	SIGNATURE: STAMP:

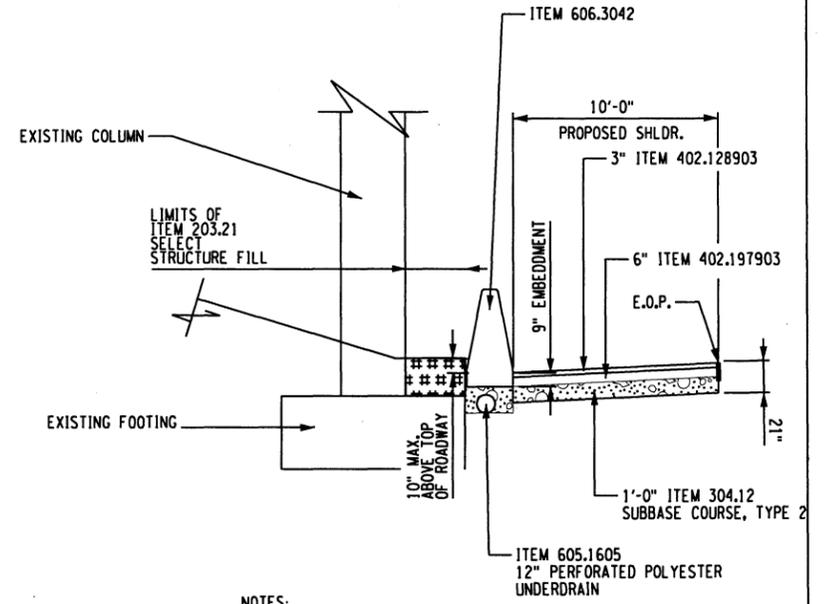
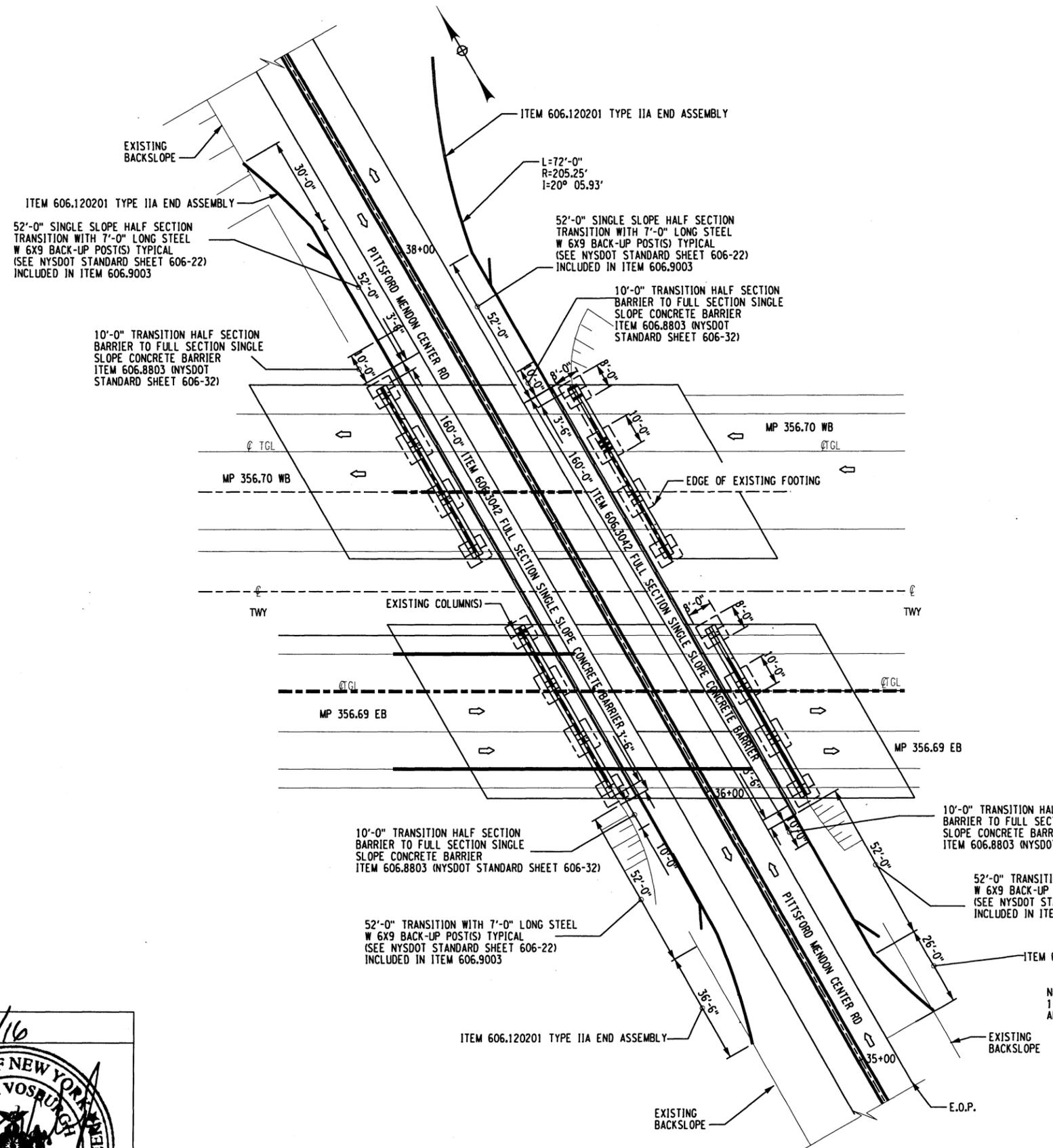
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT MP 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 07/2016
	TITLE OF DRAWING PROPOSED DRAINAGE PLAN	DRAWING NUMBER: PDP-1



FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



- NOTES:
- 1- PROPOSED GUIDERAIL AND CONCRETE BARRIER TO BE INSTALLED ONLY AFTER COMPLETION OF SUBSTRUCTURE REPAIRS
  - 2- THE CONTRACTOR SHALL SURVEY EXISTING CENTERLINE RIGHT AND LEFT EDGE OF PAVEMENT PROFILE OF PITTSFORD MENDON CENTER RD. ITEM 625.01. TO ENSURE PROPOSED PAVING DOES NOT DECREASE THE VERTICAL CLEARANCE.
  - 3- ITEM 304.12 SUBBASE COURSE, TYPE 2
  - 4- ITEM 203.21 SELECT STRUCTURE FILL
  - 5- ITEM 606.3042 & 203.21- BEGIN AT STA. 36+10.57 TO STA. 37+79.15

- NOTES:
- 1- SEE NYS DOT STANDARD SHEETS 606-04, 606-22, 606-32 AND 606-14 FOR DETAILS

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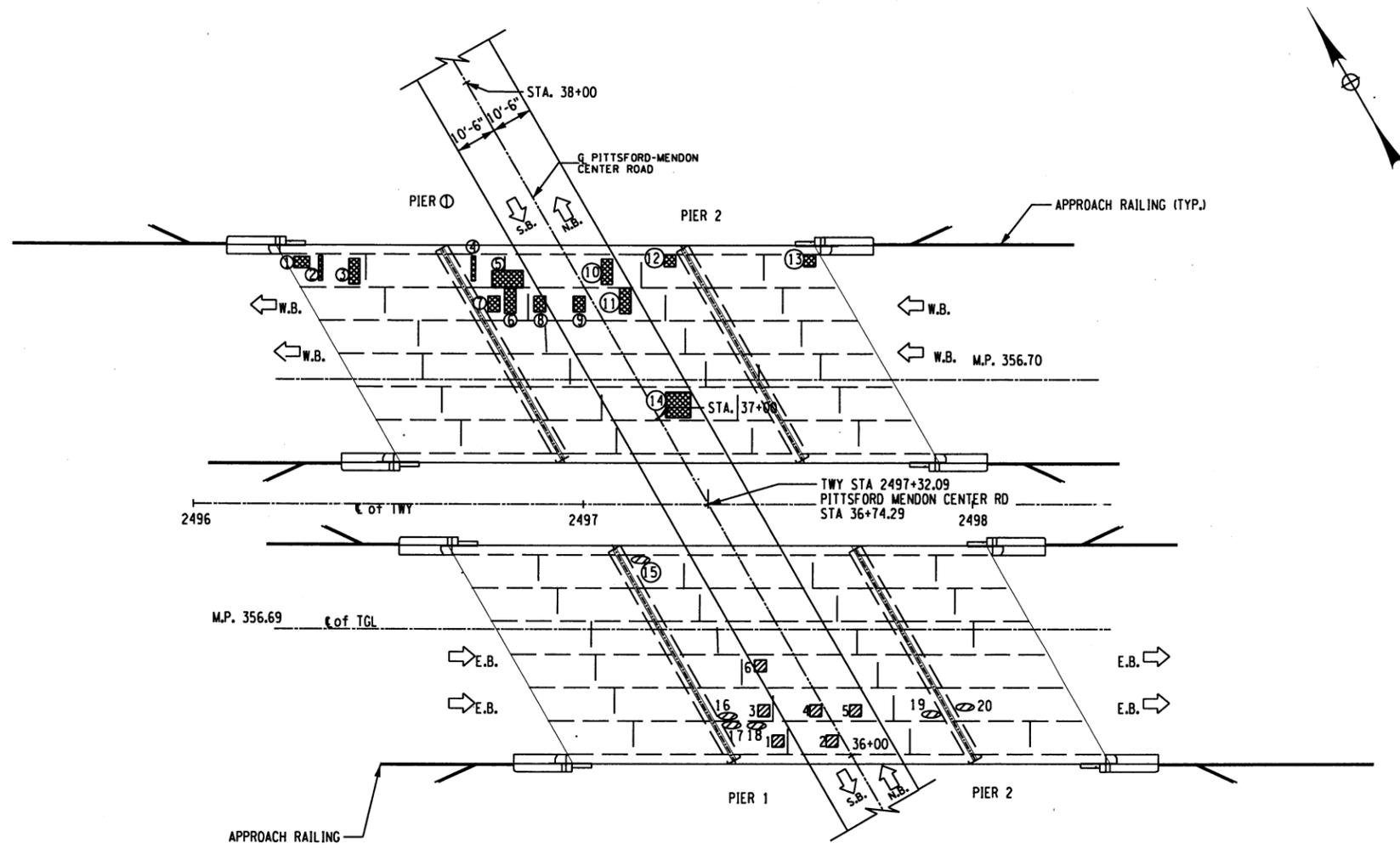
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT MP 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/29/2016
TITLE OF DRAWING PROPOSED PIER PROTECTION	DRAWING NUMBER: PP 1

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



**NOTES:**

1- FOR M.P. 356.70 THE FULL DEPTH REPAIR LOCATIONS INDICATED ARE ANTICIPATED AS A RESULT OF ITEM 579.02 REBAR EXPOSURE. IF AFTER EXPOSING THE REBAR THE CONCRETE IN THESE REPAIR LOCATIONS IS DETERMINED TO BE SOUND (I.E. "NO BLOW THROUGH" IF THE REPAIRS) SHALL BE ELIMINATED FROM THE WORK AS ORDERED BY THE ENGINEER.

2- FOR M.P. 356.69 REPAIRS AREA(S) SHALL BE DETERMINED BY SOUNDING THE TOP SURFACE OF THE EXISTING BRIDGE DECK.

- ITEM 584.55-25, PARTIAL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH RAPID HARDENING CONCRETE
- ITEM 584.56-25 FULL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH RAPID HARDENING CONCRETE
- ITEM 580.51030525 FULL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH CLASS D CONCRETE

**M.P. 356.69 (ITEM 584.56-25)**

FULL DEPTH REPAIR TABLE			
PLACEMENT	LENGTH	WIDTH	AREA SQFT
1	3'-0"	3'-0"	9.0
2	3'-0"	3'-0"	9.0
3	3'-0"	3'-0"	9.0
4	3'-0"	3'-0"	9.0
5	3'-0"	3'-0"	9.0
6	3'-0"	4'-0"	12'-0"

PARTIAL DEPTH REPAIR TABLE			
M.P. 356.69 (ITEM 584.55-25)			
PLACEMENT	LENGTH	WIDTH	AREA SQFT
15	3'-0"	3'-0"	9.0
16	3'-0"	3'-0"	9.0
17	3'-0"	3'-0"	9.0
18	3'-0"	3'-0"	9.0
19	3'-0"	3'-0"	9.0
20	3'-0"	3'-0"	9.0

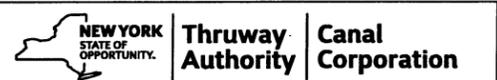
**M.P. 356.70 (ITEM 580.51030525)**

FULL DEPTH REPAIR TABLE			
PLACEMENT	LENGTH	WIDTH	AREA SQFT
1	4'-0"	2'-10"	11.33
2	1'-0"	6'-4"	6.33
3	2'-10"	6'-6"	18.4
4	1'-0"	6'-4"	6.33
5	8'-0"	4'-5"	35.33
6	2'-10"	6'-6"	18.4
7	3'-0"	4'-0"	12.0
8	3'-0"	4'-0"	12.0
9	3'-0"	4'-0"	12.0
10	2'-10"	6'-6"	18.4
11	2'-10"	6'-6"	18.4
12	3'-0"	3'-0"	9.0
13	3'-0"	3'-0"	9.0
14	6'-3"	6'-6"	40.6

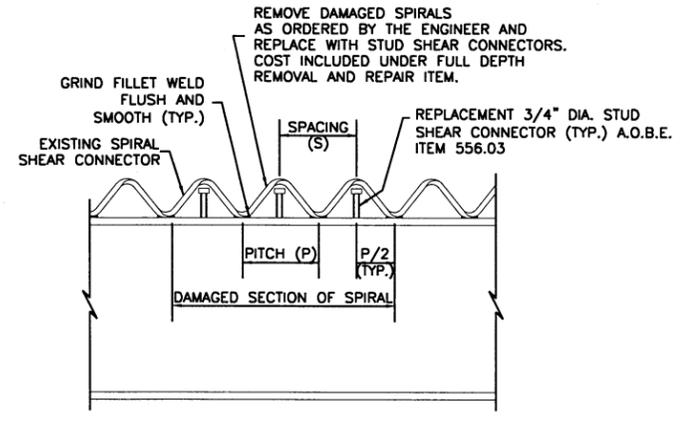
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

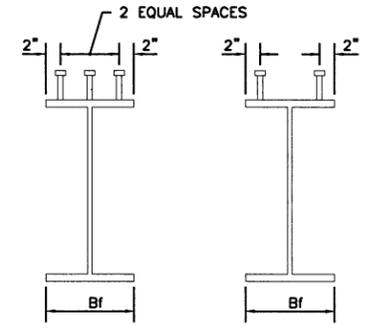


TITLE OF PROJECT <b>NYS THRUWAY BRIDGE REHABILITATION</b>	CONTRACT NUMBER: <b>TAB 16-33B</b>
LOCATION OF PROJECT <b>TWY MP. 356.69 &amp; 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY</b>	DATE: <b>07/2016</b>
TITLE OF DRAWING <b>BRIDGE DECK REPAIR PLAN</b>	DRAWING NUMBER: <b>BDRP-1</b>



NOTE: REPLACEMENT STUD SHEAR CONNECTORS - ITEM 556.03. THIS ITEM IS FOR BIDDING PURPOSES ONLY.

PARTIAL ELEVATION OF GIRDER



SECTION 3 STUDS PER ROW  
SECTION 2 STUDS PER ROW

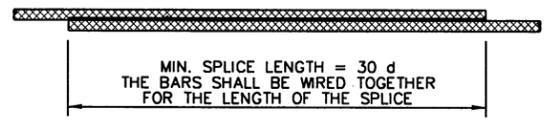
REPLACEMENT OF SPIRAL SHEAR CONNECTORS WITH STUD SHEAR CONNECTORS

N.T.S.

REPLACEMENT SHEAR STUD CONNECTOR SPACING		
PITCH	Bf < 13"	Bf ≥ 13"
< 7"	2 PER ROW, S = 4.5"	3 PER ROW, S = 4.5"
≥ 7"	2 PER ROW, S = 4.5"	3 PER ROW, S = 6"

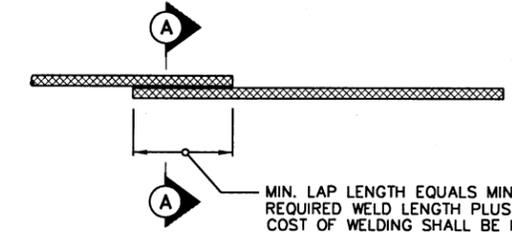
  

SHEAR STUD CONNECTOR LENGTH		
DECK THICKNESS	HAUNCH DEPTH	SHEAR STUD LENGTH
8.5" to 9.5"	1" and less	6"
	>1" to 4"	7"
	>4"	4" w/haunch reinf.
less than 8.5"	1" and less	4"
	>1" to 4"	6"
	>4"	4" w/haunch reinf.



LAPPED, TIED SPLICE

N.T.S.

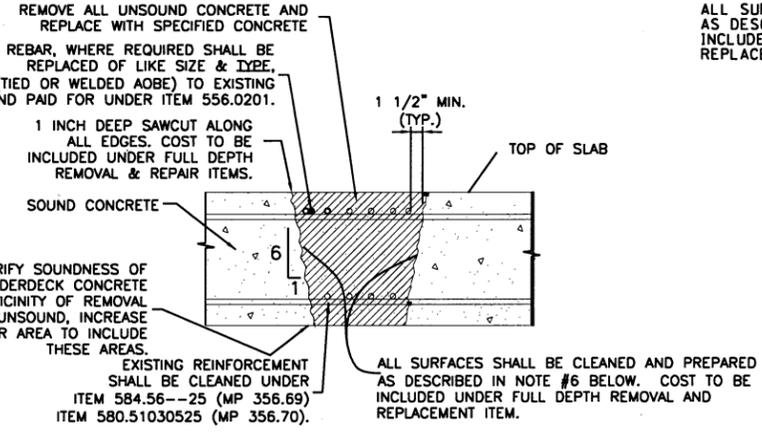


MIN. LAP LENGTH EQUALS MINIMUM REQUIRED WELD LENGTH PLUS 1 INCH. COST OF WELDING SHALL BE INCLUDED IN ITEMS 580.51030525, 584.55--25 & 584.56--25

WELDED SPLICE

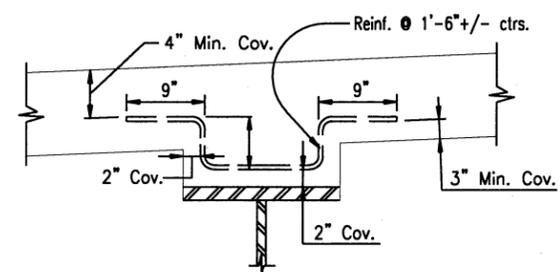
N.T.S.

MINIMUM WELD LENGTHS		
BAR SIZE	SINGLE FLARE L1 V-GROOVE WELD	DOUBLE FLARE L2 V-GROOVE WELD
5	4"	2 1/2"
6	4 1/2"	2 3/4"
7	5"	3"
8	5 1/2"	3 1/4"
9	6"	3 1/2"



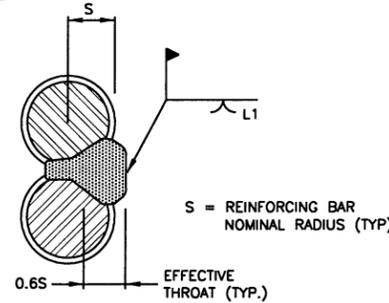
FULL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH CONCRETE (MISC. LOCATIONS, FORMWORK REQUIRED)

N.T.S.



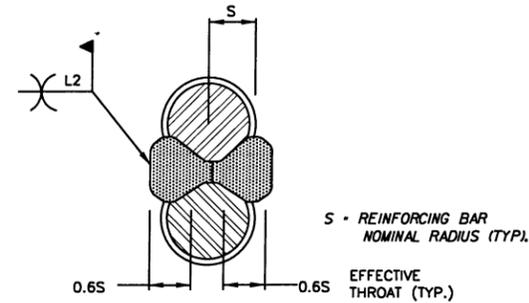
REINFORCED GIRDER HAUNCH DETAIL (FOR USE WHEN HAUNCH DEPTH > 4")

N.T.S.



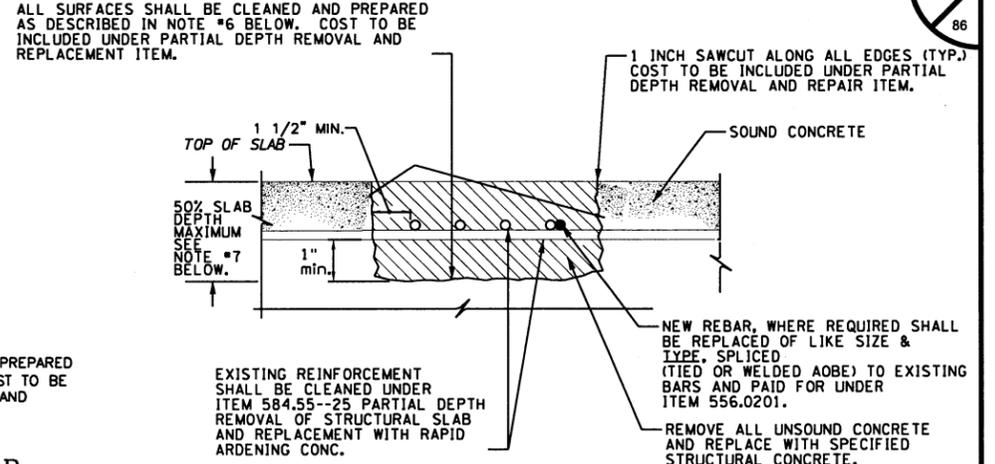
SINGLE FLARE V - GROOVE WELD

COST OF WELDING SHALL BE INCLUDED IN ITEMS 580.51030525, 584.55--25 & 584.56--25



SECTION A-A DOUBLE FLARE V - GROOVE WELD

COST OF WELDING SHALL BE INCLUDED IN ITEMS 580.51030525, 584.55--25 & 584.56--25



PARTIAL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH CONCRETE

N.T.S.

CONCRETE REPAIR NOTES:

- FOR GENERAL NOTES SEE DRAWING CN-1. FOR SPECIFIC CONCRETE REPAIR ITEMS SEE DRAWING NUMBERS TS-1 & TS-2
- LOCATION OF FULL DEPTH CONCRETE REPAIRS SHALL BE DETERMINED BY THE ENGINEER.
- THE PROJECT ENGINEER SHALL EXAMINE THE DECK SLAB CONCRETE AND ESTABLISH THOSE AREAS OF FULL AND PARTIAL DEPTH REPAIR TO BE MADE WITH THE SPECIFIED CONCRETE.
- DETERIORATED REINFORCEMENT SHALL BE REPLACED OF LIKE SIZE & TYPE, ITEM 556.0201, AND HAVE A MINIMUM LAP OF 30 BAR DIAMETERS OR A WELDED SPLICE SHALL BE PROVIDED (SEE WELDED SPLICE DETAILS ON THIS DRAWING). IF WELDED, WELDERS SHALL BE QUALIFIED IN ACCORDANCE WITH THE "NEW YORK STATE STEEL CONSTRUCTION MANUAL".
- WHEN JOINING FRESH CONCRETE TO NEW CONCRETE WHICH HAS ALREADY SET, OR TO EXISTING CONCRETE, THE CONCRETE IN PLACE SHALL HAVE ITS SURFACE SCOURED OR ABRADED WITH A SUITABLE TOOL TO REMOVE ALL LOOSE AND FOREIGN MATERIALS. THIS SURFACE SHALL BE SCRUBBED WITH WIRE BROOMS. AFTER THE SURFACE PREPARATION HAS BEEN ACCEPTED, THIS SURFACE SHALL BE THOROUGHLY WET PRIOR TO PLACING THE NEW CONCRETE. THIS MAY BE ACCOMPLISHED BY CONTINUOUS WETTING WITH SOAKER HOSES OR THE USE OF BURLAP/BURLENE/ETC. SO THAT MOISTURE CAN BE MAINTAINED. IF, IN THE OPINION OF THE ENGINEER, CONDITIONS OR THE SITUATION PROHIBITS THIS, THEN THE SURFACES SHALL BE WETTED FOR AS LONG AS POSSIBLE. THE CONTRACTOR SHALL REMOVE ANY PUDDLES OR FREE STANDING WATER WITH OIL-FREE COMPRESSED AIR, AND PROTECT THE SURFACES FROM DRYING, SO THAT THE EXISTING CONCRETE REMAINS IN A CLEAN, SATURATED SURFACE DRY CONDITION UNTIL PLACEMENT OF THE NEW CONCRETE. IMMEDIATELY BEFORE PLACING THE NEW CONCRETE, THE FORMS SHALL BE DRAWN TIGHT AGAINST THE CONCRETE ALREADY IN PLACE.
- AT M.P. 356.69 ALL PARTIAL DEPTH CONCRETE REMOVAL LOCATIONS, THE MINIMUM DEPTH OF REMOVAL SHALL BE NO LESS THAN 1 INCH BELOW THE TOP MAT REINFORCING BARS, OR TO SOUND CONCRETE, WHICHEVER IS GREATER. WHERE THE REMOVAL DEPTH EXCEEDS 50% OF THE SLAB THICKNESS, A FULL DEPTH REPAIR SHALL BE REQUIRED.
- WHERE CONCRETE REMOVAL AND REPLACEMENT NECESSITATES ADJACENT SEPARATE CONCRETE PLACEMENTS, CONCRETE REMOVAL SHALL NOT BE ALLOWED WITHIN 1 FOOT OF ADJACENT REPAIR AREAS. ADJACENT AREAS WHERE THIS 1 FOOT BUFFER IS NOT ATTAINABLE SHALL BE COMBINED INTO A SINGLE REPAIR AREA.
- IN SEQUENCING WORK, THE CONTRACTOR SHALL NOT BEGIN CONCRETE REMOVAL AT A LOCATION ADJACENT TO A COMPLETED REPAIR UNTIL A MINIMUM OF 2 CURING HOURS HAVE PASSED.
- A PROTECTIVE OR PENETRATING SEALER FOR CONCRETE SHALL BE APPLIED TO ALL EXPOSED CONCRETE SURFACES EXCEPT THE UNDERSIDE OF THE STRUCTURAL DECK BETWEEN THE DRIP EDGES.
- THE EXTENT OF CONCRETE REMOVAL FOR ANY COMPONENT SHALL NOT JEOPARDIZE ITS STRUCTURAL INTEGRITY. THE CONTRACTOR SHALL SUBMIT, PRIOR TO COMMENCEMENT OF THIS WORK, A REMOVAL PLAN AND SCHEDULE FOR PROJECT ENGINEER APPROVAL.
- MP 356.69 THE CONTRACTOR SHALL USE ITEM 584.55--25 PARTIAL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH RAPID HARDENING CONCRETE & ITEM 584.56--25 FULL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH RAPID HARDENING CONCRETE. MP 356.70 THE CONTRACTOR SHALL USE ITEM 580.51030525 FULL DEPTH REMOVAL OF STRUCTURAL SLAB AND REPLACEMENT WITH CLASS D CONCRETE.

CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 9/15/16

SIGNATURE: \_\_\_\_\_ STAMP: \_\_\_\_\_

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE. THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

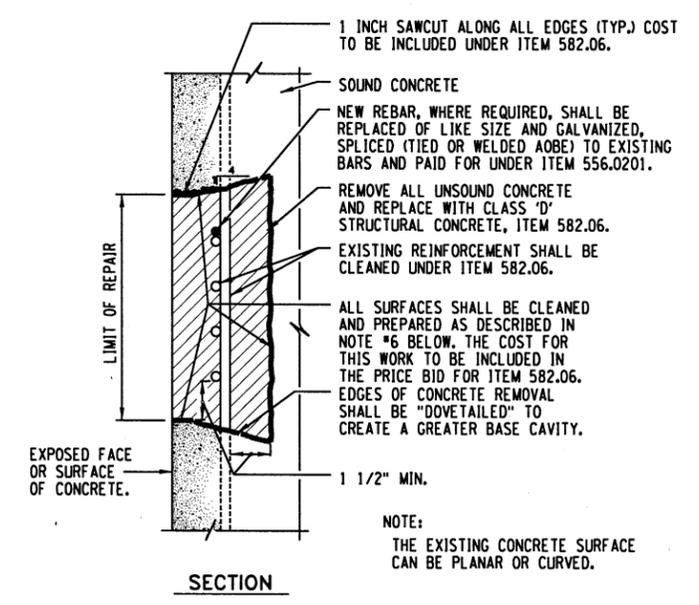
REVISIONS			
DATE	DESCRIPTION	BY	SYM

NEW YORK STATE OF OPPORTUNITY

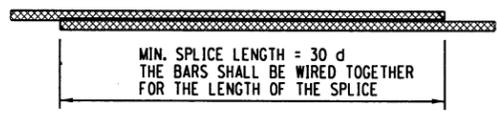
**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
TITLE OF DRAWING CONCRETE DECK REPAIR DETAILS	DRAWING NUMBER: CDR-1

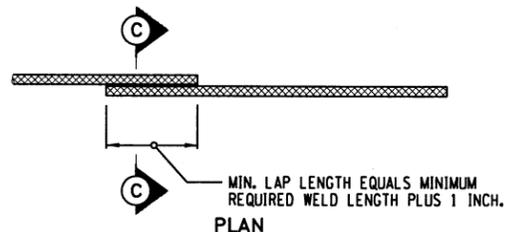
FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



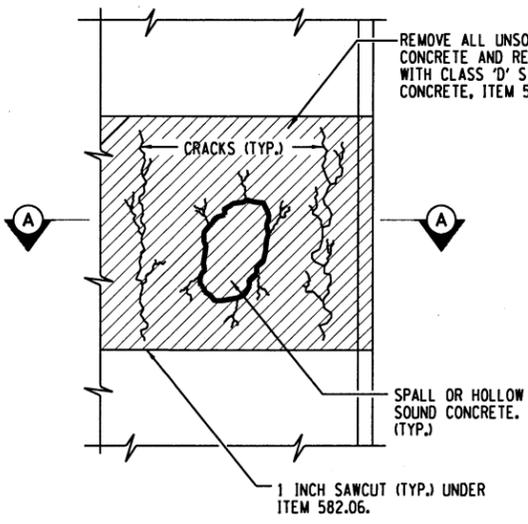
**TYPICAL CONCRETE SURFACE REPAIR SECTION**  
N.T.S.



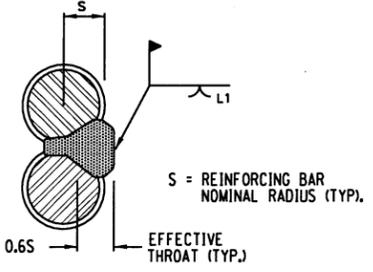
**LAPPED, TIED SPLICE**  
N.T.S.



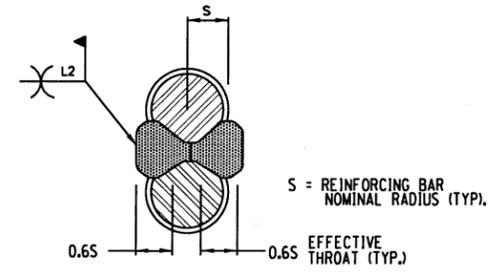
**WELDED SPLICE**  
N.T.S.



**CONCRETE DETERIORATION REPAIR**

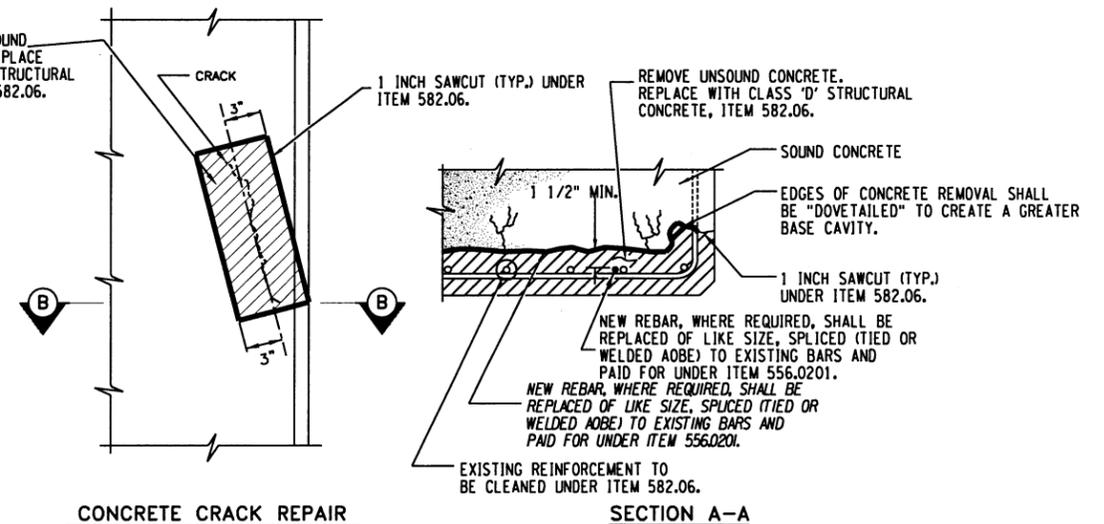


**SINGLE FLARE V - GROOVE WELD**



**DOUBLE FLARE V - GROOVE WELD**

**SECTION C-C**  
N.T.S.



**CONCRETE CRACK REPAIR**  
**CONCRETE SURFACE REPAIR**  
N.T.S.

**CONCRETE REPAIR NOTES:**

- FOR GENERAL NOTES SEE DRAWING GN-1.
- FOR LOCATION OF CONCRETE REPAIRS SEE DRAWING(S) AB-1 THRU AB-4 AND P1-NR, P1-SR, P2-NR AND P2-SR.
- SUBSTRUCTURE DETERIORATION REQUIRING REPAIR AS INDICATED ON THE PLANS HAS BEEN DETERMINED BY A FIELD INSPECTION. ALL OF THE MAJOR AREAS, KNOWN TO EXIST AT THE TIME, HAVE BEEN SHOWN TO INDICATE THE APPROXIMATE EXTENT OF DETERIORATION THAT WILL HAVE TO BE REPAIRED BY THE CONTRACTOR.
- THE ANTICIPATED EXTENT OF CONCRETE REPAIR HAS BEEN INDICATED ON THE CONTRACT PLANS. THE PROJECT ENGINEER SHALL EXAMINE THE SUBSTRUCTURE CONCRETE AND ESTABLISH THOSE AREAS OF REPAIR TO BE MADE WITH CLASS "D" CONCRETE. THE EXTENT OF CONCRETE REPAIR TO BE MADE SHALL BE APPROVED BY THE PROJECT ENGINEER.
- DETERIORATED REINFORCEMENT SHALL BE REPLACED OF LIKE SIZE, ITEM 556.0201, AND HAVE A MINIMUM LAP OF 30 BAR DIAMETERS OR A WELDED SPLICE SHALL BE PROVIDED (SEE WELDED SPLICE DETAILS ON THIS DRAWING). IF WELDED, WELDERS SHALL BE QUALIFIED IN ACCORDANCE WITH THE "NEW YORK STATE STEEL CONSTRUCTION MANUAL".
- WHEN JOINING FRESH CONCRETE TO NEW CONCRETE WHICH HAS ALREADY SET, OR TO EXISTING CONCRETE, THE CONCRETE IN PLACE SHALL HAVE ITS SURFACE SCOURED OR ABRADED WITH A SUITABLE TOOL TO REMOVE ALL LOOSE AND FOREIGN MATERIALS. THIS SURFACE SHALL BE SCRUBBED WITH WIRE BROOMS. AFTER THE SURFACE PREPARATION HAS BEEN ACCEPTED, THIS SURFACE SHALL BE THOROUGHLY WET AND KEPT SO FOR A PERIOD OF 12 HOURS IMMEDIATELY PRIOR TO PLACING THE NEW CONCRETE. THIS MAY BE ACCOMPLISHED BY CONTINUOUS WETTING WITH SOAKER HOSES OR THE USE OF BURLAP/BURLENE/ETC. SO THAT MOISTURE CAN BE MAINTAINED. IF, IN THE OPINION OF THE ENGINEER, CONDITIONS OR THE SITUATION PROHIBITS THIS, THEN THE SURFACES SHALL BE WETTED FOR AS LONG AS POSSIBLE. THE CONTRACTOR SHALL REMOVE ANY PUDDLES OR FREE STANDING WATER WITH OIL-FREE COMPRESSED AIR, AND PROTECT THE SURFACES FROM DRYING, SO THAT THE EXISTING CONCRETE REMAINS IN A CLEAN, SATURATED SURFACE DRY CONDITION UNTIL PLACEMENT OF THE NEW CONCRETE. IMMEDIATELY BEFORE PLACING THE NEW CONCRETE, THE FORMS SHALL BE DRAWN TIGHT AGAINST THE CONCRETE ALREADY IN PLACE.
- AT ALL CONCRETE REMOVAL LOCATIONS, THE MINIMUM DEPTH OF REMOVAL SHALL BE NO LESS THAN 1 1/2 INCHES BEHIND THE INNERMOST LAYER OF EXPOSED REINFORCING BARS, OR TO SOUND CONCRETE, WHICHEVER IS GREATER.
- WHERE CONCRETE REMOVAL AND REPLACEMENT NECESSITATES ADJACENT SEPARATE CONCRETE PLACEMENTS, CONCRETE REMOVAL SHALL NOT BE ALLOWED WITHIN 1 FOOT OF ADJACENT REPAIR AREAS. ADJACENT AREAS WHERE THIS 1 FOOT BUFFER IS NOT ATTAINABLE SHALL BE COMBINED INTO A SINGLE REPAIR AREA.
- IN SEQUENCING WORK, THE CONTRACTOR SHALL NOT BEGIN CONCRETE REMOVAL AT A LOCATION ADJACENT TO A COMPLETED REPAIR UNTIL A MINIMUM OF 7 CURING DAYS HAVE PASSED.
- PROTECTIVE SEALER FOR STRUCTURAL CONCRETE, ITEM(S) 559.1596--25 & 559.1860118, (A SOLID COLOR PROTECTIVE SEALER, WHITE) SHALL BE APPLIED TO ALL EXPOSED REPAIRED CONCRETE SURFACES OF THE SUBSTRUCTURES, EXCEPT THE BOTTOM SURFACE OF PIERCAPS (NONE SHALL BE APPLIED). A CLEAR SEALER SHALL BE APPLIED TO THE NEW CONCRET WEARING COURSE AFTER IT HAS BEEN PROPERLY CURED.
- THE EXTENT OF CONCRETE REMOVAL FOR ANY COMPONENT SHALL NOT JEOPARDIZE ITS STRUCTURAL INTEGRITY. THE CONTRACTOR SHALL SUBMIT, PRIOR TO COMMENCEMENT OF THIS WORK, A REMOVAL PLAN AND SCHEDULE FOR PROJECT ENGINEER APPROVAL.
- ALTHOUGH THE DEPTH OF REMOVAL TO SOUND CONCRETE MAY, AT SOME LOCATIONS, EXTEND TO DEPTHS GREATER THAN 5 INCHES, IT IS THE INTENT OF THIS PROJECT THAT THE CONTRACTOR SHALL MAKE ALL NECESSARY REPAIRS WITH CLASS "D" CONCRETE PER ITEM 582.06.

MINIMUM WELD LENGTHS		
BAR SIZE	SINGLE FLARE L1 V-GROOVE WELD	DOUBLE FLARE L2 V-GROOVE WELD
5	4"	2 1/2"
6	4 1/2"	2 3/4"
7	5"	3"
8	5 1/2"	3 1/4"
9	6"	3 1/2"

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/27/2016
TITLE OF DRAWING CONCRETE REPAIR DETAILS	DRAWING NUMBER: TCRD-1

ALTERED ON: 9/15/16

AFFIXED ON: 9/15/16

SIGNATURE: [Signature]

STAMP: [Professional Engineer Seal for David T. Vosburgh, No. 074259]

M.P. 356.70 REPAIR DRAWING

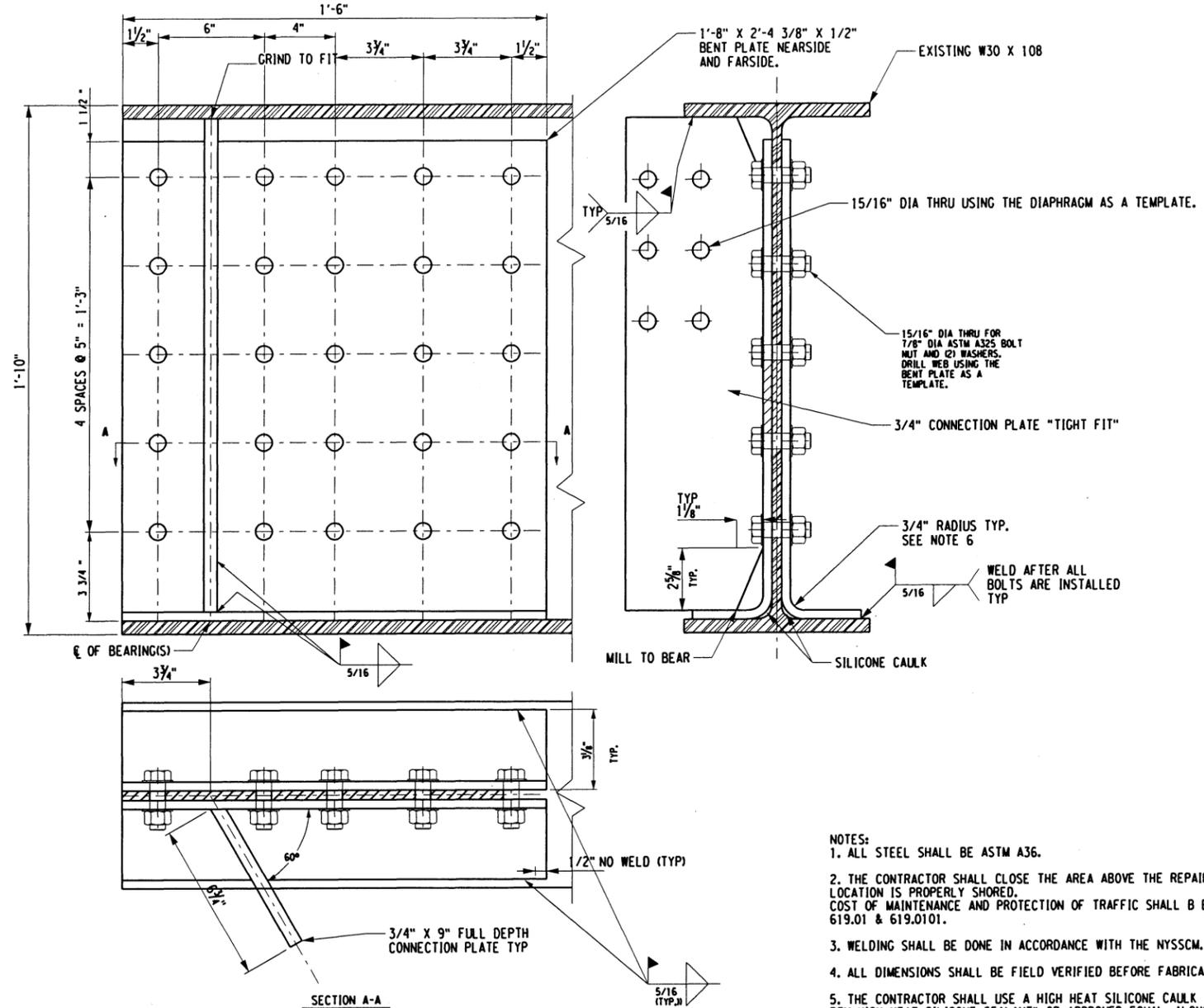
M.P. 356.70 REPAIR PROCEDURE

REPAIR PROCEDURE FOR GIRDER ENDS: ALL COST ASSOCIATED WITH THE REPAIR SHALL BE INCLUDED ITEM 564.85010125

- POWER TOOL CLEAN AND REMOVE ALL PAINT 2'-6" FROM END THE GIRDER(S) AND ADJOINING CONNECTIONS, FOR REPAIR LOCATIONS SEE LIST BELOW.
- REMOVE AND STORE THE DIAPHRAGM(S) CONNECTING THE GIRDERS LISTED BELOW. REMOVE ANY EXISTING PAINT AND CHECK THE FAYING SURFACES AND THRU HOLES OF THE EXISTING DIAPHRAGM WITH MAGNETIC PARTICLE TEST.
- AFTER REMOVING THE EXISTING PAINT THE CONTRACTOR SHALL INSTALL ANY SHORING AS ORDERED BY THE ENGINEER. ALL SHORING SHALL BE SUFFICIENT TO CARRY A MINIMUM OF AN HS20 LIVE LOAD AND ANY EXISTING DEAD LOAD. PRIOR TO INSTALLING THE SHORING THE CONTRACTOR SHALL SUBMIT DESIGN CALCULATIONS AND DRAWINGS STAMPED BY A NEW YORK STATE LICENSED PROFESSIONAL ENGINEER THE CONTRACTOR REMOVE THE EXISTING BEARING STIFFENER ON ONE SIDE OF THE GIRDER AT A TIME BY AIR CARBON ARC GOUGING AND/OR FLAME CUTTING THE STIFFENER TO WITHIN 1/8" OF THE WEB. THE REMAINING STIFFENER MATERIAL AND WELD METAL SHALL BE REMOVED BY GRINDING FLUSH AND SMOOTH WITH THE WEB SURFACE. ANY DAMAGE CAUSED BY THE REMOVAL OF THE EXISTING STIFFENER OR CONNECTIONS SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST(S).
- THE CONTRACTOR SHALL MAGNETIC PARTICLE TEST (M.T.) THE CLEANED SURFACE AREAS (WEBS) AND FLANGES AND DIAPHRAGM(S) PRIOR TO INSTALLING NEW WEB PLATE(S) ON THE GIRDER AND REPORT ANY DISCONTINUITIES TO THE ENGINEER.
- PRIOR TO INSTALLING NEW WEB PLATES THE CONTRACTOR SHALL PRIME THE SURFACE AREA. IF THE SURFACE AREA HAS "FLASH RUSTED" PRIOR TO PRIMING THE CONTRACTOR SHALL REMOVE ALL VISIBLE SIGNS OF RUST BY A METHOD APPROVED BY THE ENGINEER AND THEN PRIME THE AREA. PRIMER SHALL BE SUITABLE FOR A CLASS "A" SLIP COEFFICIENT. THE CONTRACTOR SHALL THEN INSTALL THE NEW WEB PLATE(S). DO NOT WELD THE BOTTOM OF THE NEW WEB PLATE(S) UNTIL BOTH WEB PLATE(S) AND ALL OF THE BOLTS ARE INSTALLED. THE CONTRACTOR MAY ELECT TO USE THE PROPOSED WEB PLATES AS A TEMPLATE. THE CONTRACTOR SHALL FIELD DRILL FULL SIZE ALL HOLES THROUGH EXISTING GIRDER WEB AND INSTALL A MINIMUM OF 50% OF THE BOLTS REQUIRED FOR THE ASSEMBLY. PRIOR TO REMOVING ANY NECESSARY SHORING.
- THE CONTRACTOR SHALL INSTALL THE PROPOSED WEB PLATE(S). 100% OF THE BOLTS ARE REQUIRED TO BE INSTALLED & TORQUED PRIOR TO WELDING THE BOTTOM EDGE OF THE BENT PLATE(S)
- INSTALL THE BEARING STIFFENER/CONNECTION PLATE(S) ON THE PROPOSED WEB PLATE(S).
- DRILL THE PROPOSED CONNECTION PLATE(S) USING THE EXISTING DIAPHRAGM REMOVED EARLIER AS A TEMPLATE. THE CONTRACTOR SHALL FIELD DRILL FULL SIZE ALL HOLES THROUGH EXISTING DIAPHRAGM.
- RE-INSTALL THE DIAPHRAGM(S) REMOVED EARLIER WITH NEW A325 BOLTS.

REPAIR LOCATIONS

- BEGIN OF GIRDER #7 - SPAN 1 - RIGHT AND LEFT SIDE
- END OF GIRDER #3 - SPAN 3 - RIGHT AND LEFT SIDE



- NOTES:
- ALL STEEL SHALL BE ASTM A36.
  - THE CONTRACTOR SHALL CLOSE THE AREA ABOVE THE REPAIR TO TRAFFIC UNTIL THE LOCATION IS PROPERLY SHORED. COST OF MAINTENANCE AND PROTECTION OF TRAFFIC SHALL BE INCLUDED IN ITEMS 619.01 & 619.0101.
  - WELDING SHALL BE DONE IN ACCORDANCE WITH THE NYSSCM.
  - ALL DIMENSIONS SHALL BE FIELD VERIFIED BEFORE FABRICATION BEGINS.
  - THE CONTRACTOR SHALL USE A HIGH HEAT SILICONE CAULK SUCH AS "500 DEGREE F RTV HIGH HEAT SILICONE SEALANT" OR APPROVED EQUAL. ALONG ALL EDGES OF PROPOSED PLATE(S) NOT WELDED AND FILL ALL VOIDS BEHIND PLATE ASSEMBLIES WITH AN APPROVED SILICONE CAULK. COST INCLUDE ITEM 564.85010125
  - ALL FIELD WELDS SHALL BE M.T. TESTED.
  - COLD BENDING IS ALLOWED. M.T. TEST BENT AREAS AFTER BENDING IS PERFORMED.
  - BENT PLATES TO BE MATCH DRILLED AND SUPPLIED IN SETS.
  - BENT PLATE SHALL BE INSTALLED BEFORE REMOVING THE STIFFENER/CONNECTION PLATE ON THE INSIDE OF THE GIRDER.
  - ALL COST FOR REMOVING THE EXISTING DAMAGED STEEL, BOLTS, RIVETS) AND REPAIRS (INCLUDING PAINT REMOVAL & PAINTING) SHALL BE INCLUDED IN ITEM 564.85010125 REPAIR OF STRUCTURAL STEEL.
  - ALL TESTING SHALL BE INCLUDED IN ITEM 564.85010125 .

FILED BY: TA  
 CHECKED BY: M. COX  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH

ALTERED ON:	AFFIXED ON: 8/29/16
SIGNATURE: STAMP:	SIGNATURE: STAMP:

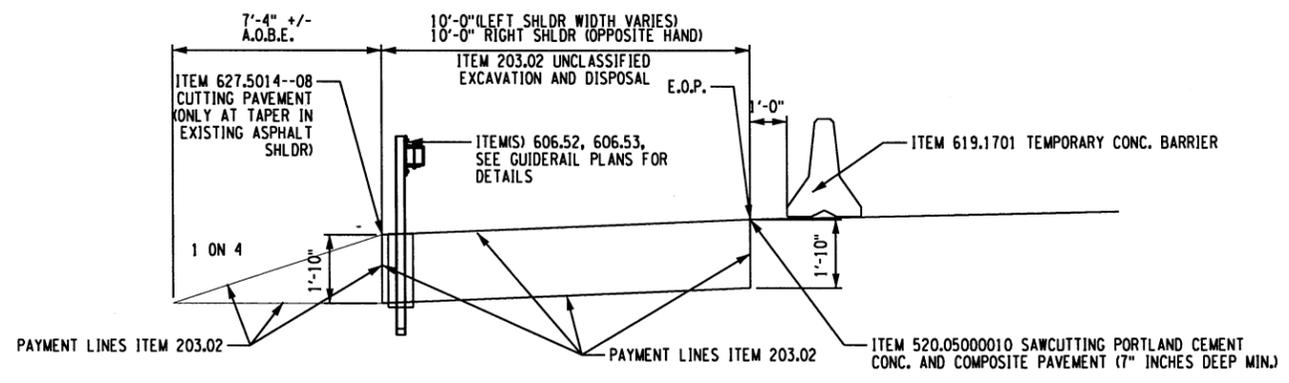
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE. THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

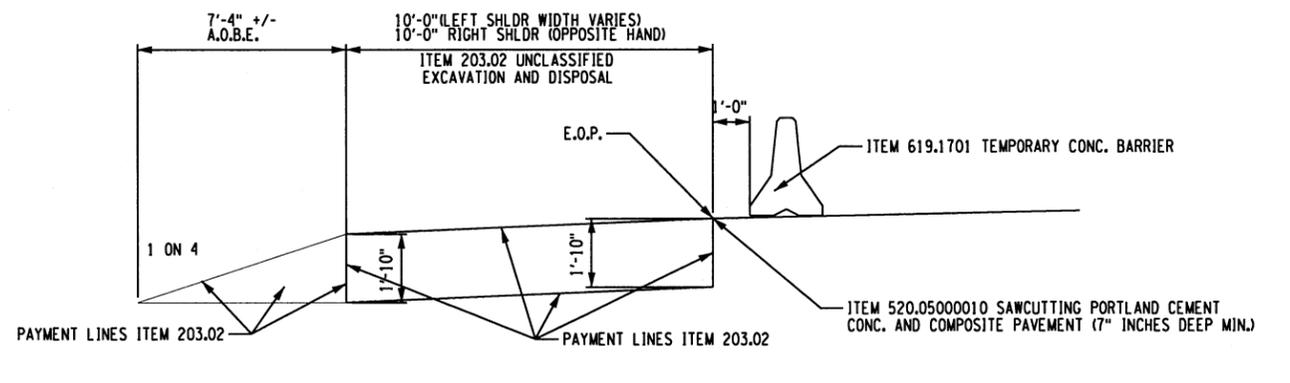
**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/29/2016
TITLE OF DRAWING STEEL REPAIRS	DRAWING NUMBER: SR-1

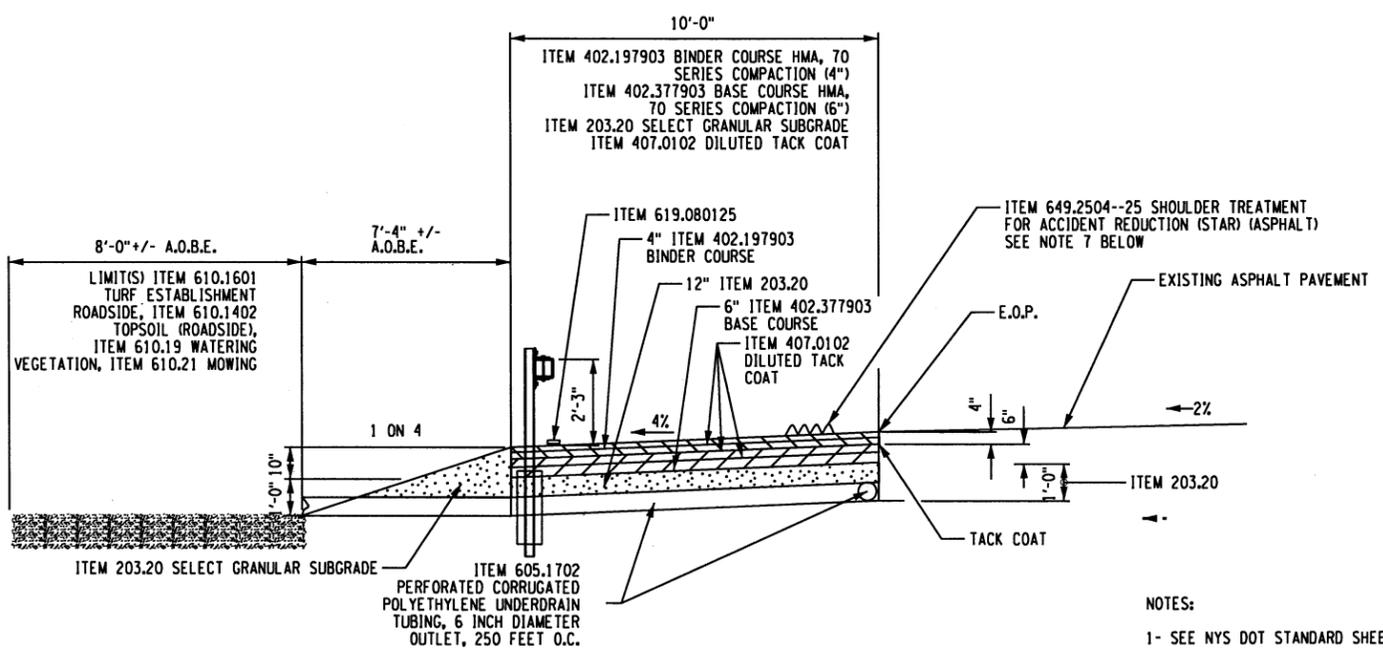
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 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



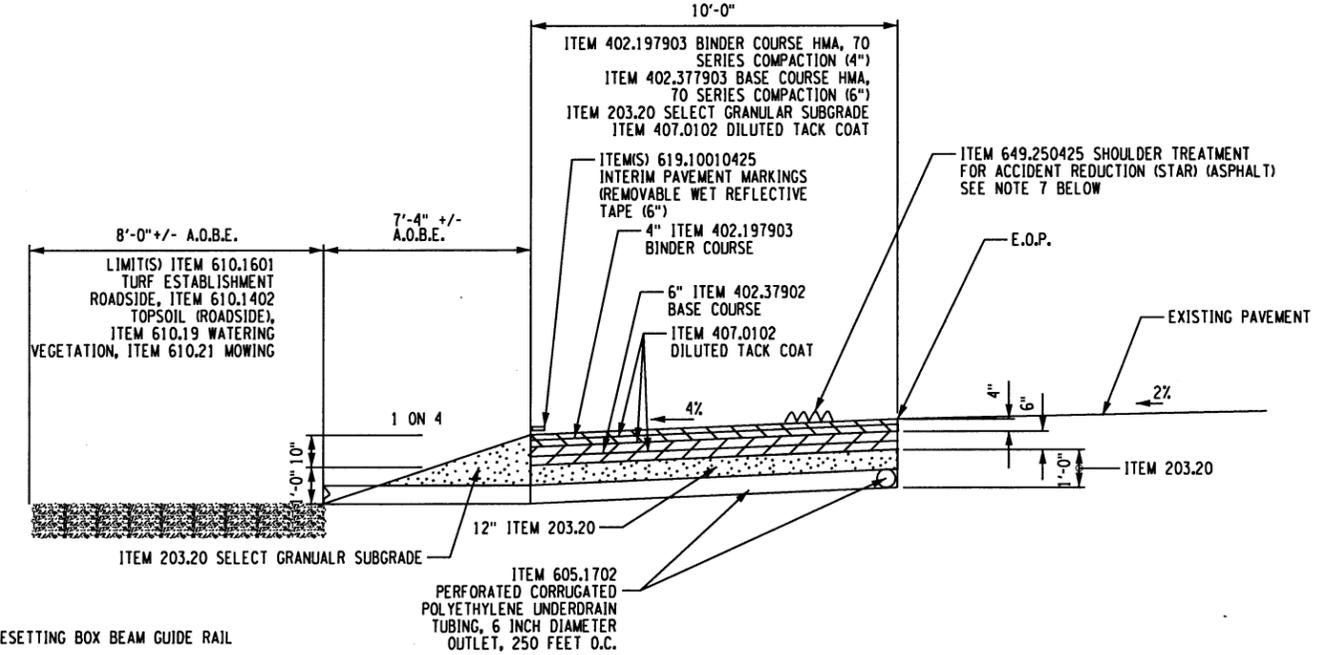
**THRUWAY SHOULDER REMOVAL DETAILS (WITH GUIDERAIL)**  
(RIGHT SHOULDER OPPOSITE HAND)



**THRUWAY SHOULDER REMOVAL DETAILS (WITHOUT GUIDERAIL)**  
(RIGHT SHOULDER OPPOSITE HAND)



**THRUWAY SHOULDER RECONSTRUCTION DETAILS (WITH GUIDERAIL)**  
(RIGHT SHOULDER OPPOSITE HAND)



**THRUWAY SHOULDER RECONSTRUCTION DETAILS (WITHOUT GUIDERAIL)**  
(RIGHT SHOULDER OPPOSITE HAND)

- NOTES:**
- 1- SEE NYS DOT STANDARD SHEET(S) 606-04 FOR RESETTING BOX BEAM GUIDE RAIL
  - 2- SHOULDER RECONSTRUCTION SHALL BE COMPLETED PRIOR TO REMOVAL OF EXISTING BRIDGE WEARING COURSE OF M.P. 356.70. SEE WZTC FOR LIMITS OF SHOULDER RECONSTRUCTION.
  - 3- WORK SHALL CONSIST OF FURNISHING THE NECESSARY TRAFFIC CONTROL EQUIPMENT AND MAINTAINING PROTECTION FOR EDGE DROP-OFFS ADJACENT TO THE PAVEMENT OR SHOULDER COST INCLUDED IN ITEM 619.0101.
  - 4- THE CONTRACTOR SHALL ONLY BEGIN REMOVAL OF THE EXISTING GUIDE RAIL OR RECONSTRUCTION OF THE EXISTING SHOULDER AFTER PLACING TEMPORARY CONCRETE BARRIER. ALL GUIDE RAIL SHALL BE IN PLACE PRIOR TO REMOVAL OF THE TEMPORARY BARRIER.
  - 5- AFTER COMPLETION OF ALL WORK THE CONTRACTOR SHALL RE- ESTABLISH TURF IN THE MEDIAN AND RIGHT SHOULDER (ITEMS) 610.1402 TOP SOIL - ROADSIDE AND 610.1601 TURF ESTABLISHMENT, 610.19, 610.21 (A.O.B.E.). FOR LIMITS SEE DWG FINAL STRIPING FS 1-6 THRU FS 6-6.
  - 6 - ITEM 407.0102 DILUTED TACK COAT ON THE CONTACT SURFACES BETWEEN ALL H.M.A. PAVEMENT LIFTS.

- ITEM 610.1402 TOP SOIL - ROADSIDE
- ITEM 402.19793 BINDER COURSE HMA, 70 SERIES COMPACTION (4")
- ITEM 402.37793 BASE COURSE HMA, 70 SERIES COMPACTION (6")
- ITEM 203.20 SELECT GRANULAR SUBGRADE
- ITEM 649.2504-25 SHOULDER TREATMENT FOR ACCIDENT REDUCTION (STAR) (ASPHALT) (SEE NOTE 7)

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 9/15/16

SIGNATURE: \_\_\_\_\_ SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_ STAMP: \_\_\_\_\_

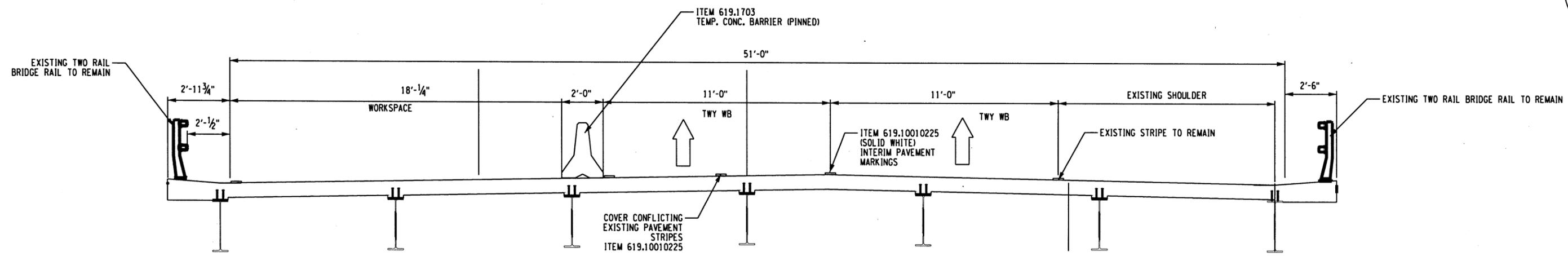
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

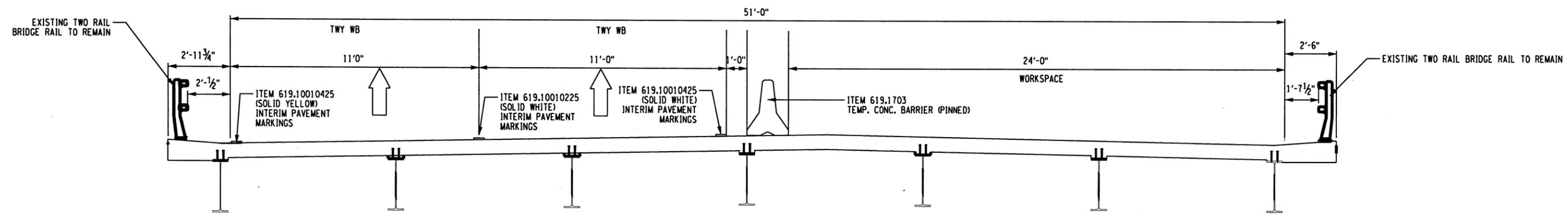
**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/29/2016
TITLE OF DRAWING SHOULDER RECONSTRUCTION DETAILS W.B. THRUWAY	DRAWING NUMBER: SRD-1

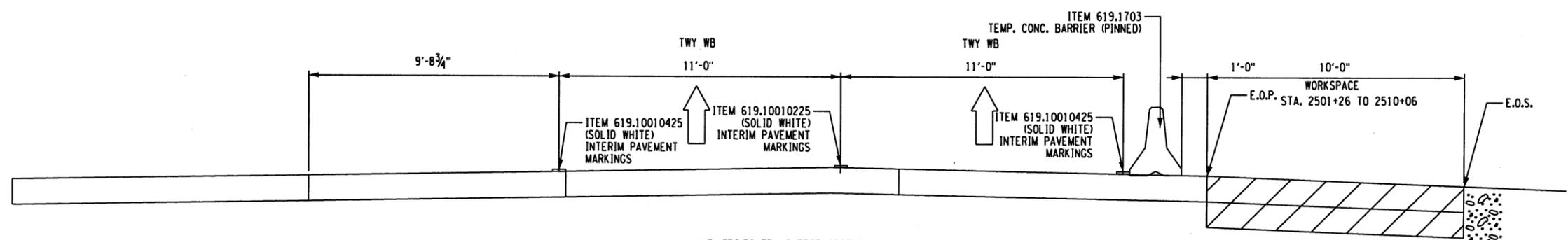
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 DESIGN SUPERVISOR: D. VOSBURGH



M.P. 356.70 TRANSVERSE SECTION STAGE 1



M.P. 356.70 TRANSVERSE SECTION STAGE 2



M.P. 356.70 TRANSVERSE SECTION STAGE 2A

- NOTES:
- 1- STAGES 1 AND 2 ARE SHOULDER RE-CONSTRUCTION.
  - 2- TRAFFIC IS TO BE MAINTAINED ON THE EXISTING STRUCTURE UTILIZING STAGE CONSTRUCTION.
  - 3- SEE LANE CLOSURE CHARTS FOR AVAILABILITY OF LANES.
  - 4- POSTED SPEED LIMIT SHALL BE REDUCED TO 55 MPH DURING STAGE(S) 1, 2, 3.
  - 5- STAGE 1, REMOVE CONFLICTING EXISTING EPOXY PAVEMENT MARKINGS- ITEM 619.08010225 STAGE REMOVE CONFLICTING STAGE 1 PAVEMENT MARKINGS ITEM 619.08010225 AND/OR ITEM 619.08010425 AS NEEDED, AS ORDERED BY THE ENGINEER ITEM 619.10010425

ALTERED ON:	AFFIXED ON: 8/29/16
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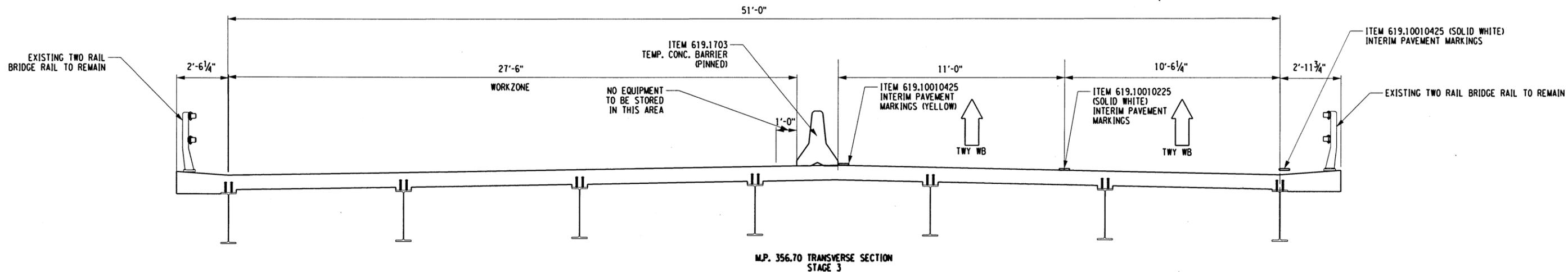
REVISIONS			
DATE	DESCRIPTION	BY	SYM.



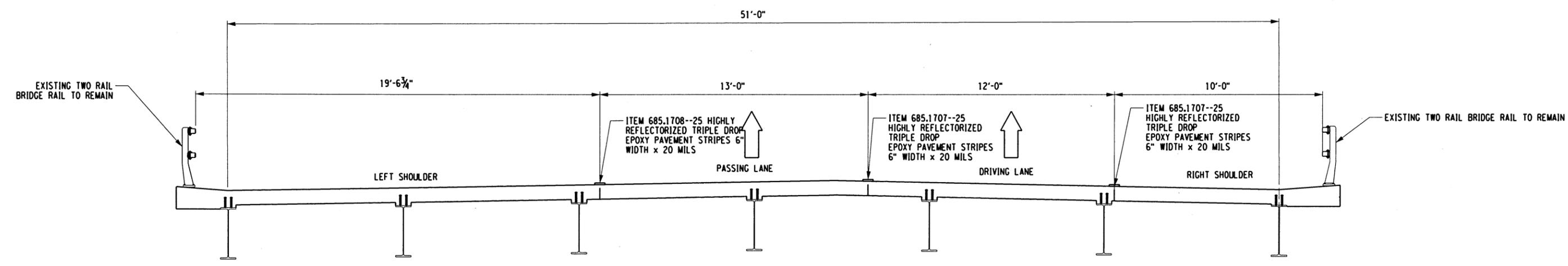
TITLE OF PROJECT: NYS THRUWAY BRIDGE REHABILITATION  
 LOCATION OF PROJECT: TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY  
 TITLE OF DRAWING: TYPICAL TRAFFIC CONTROL M.P. 356.70

CONTRACT NUMBER: TAB 16-33B  
 DATE: 06/29/2016  
 DRAWING NUMBER: TC-1

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 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



M.P. 356.70 TRANSVERSE SECTION  
STAGE 3



M.P. 356.70 TRANSVERSE SECTION  
FINAL STRIPING

- NOTES:
- 1- TRAFFIC IS TO BE MAINTAINED ON THE EXISTING STRUCTURE UTILIZING STAGE CONSTRUCTION.
  - 2- SEE LANE CLOSURE CHARTS FOR AVAILABILITY OF LANES.
  - 3- POSTED SPEED LIMIT SHALL BE REDUCED TO 55 MPH DURING STAGE(S) 1, 2, 3.
  - 4- STAGE 1, REMOVE CONFLICTING EXISTING EPOXY PAVEMENT MARKINGS- ITEM 619.08010225  
STAGE 2 REMOVE CONFLICTING STAGE 1 PAVEMENT MARKINGS ITEM 619.08010225 AND/OR ITEM 619.08010425 AS NEEDED, AS ORDERED BY THE ENGINEER ITEM 619.10010425

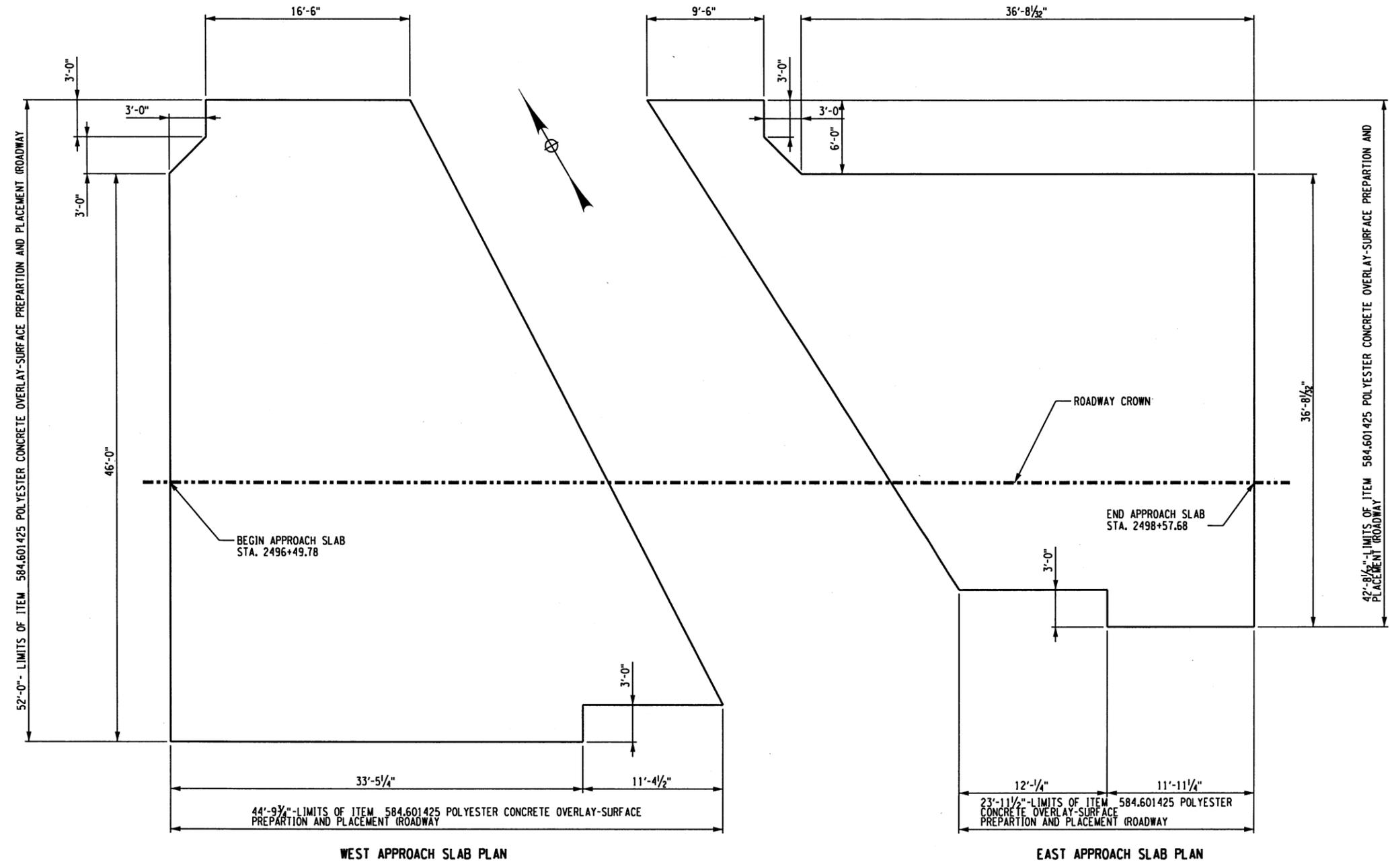
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY MP. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 07/2016
	TITLE OF DRAWING TRAFFIC CONTROL MP. 356.70	DRAWING NUMBER: TC-2

DESIGNED BY: M. COX  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 File  
 DESIGN SUPERVISOR: D. VOSBURGH  
 FILED BY: TA

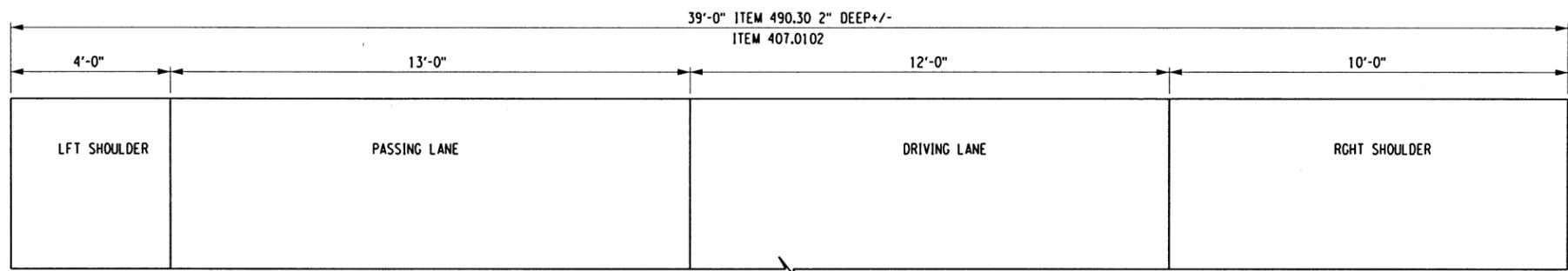


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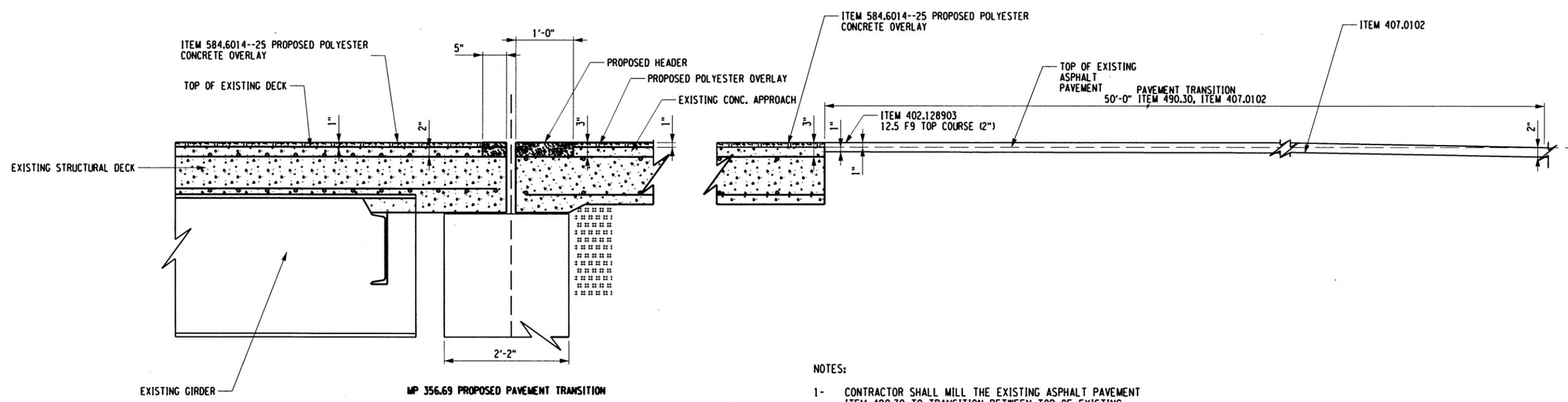
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DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT	NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER:	TAB 16-33B
LOCATION OF PROJECT	TWY MP 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE:	07/2016
TITLE OF DRAWING	MP.356.69 APPROACH SLAB OVERLAY PLAN	DRAWING NUMBER:	AS-1



PLAN VIEW M.P. 356.69



-  ITEM 584.6014--25 POLYESTER CONCRETE OVERLAY
-  EXISTING CONCRETE
-  EXISTING SUBBASE

NOTES:

- 1- CONTRACTOR SHALL MILL THE EXISTING ASPHALT PAVEMENT ITEM 490.30 TO TRANSITION BETWEEN TOP OF EXISTING ASPHALT TO TOP OF PROPOSED APPROACH OVERLAY
- 2- DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY.
- 3- AFTER COMPLETION OF PAVING THE CONTRACTOR RE-ESTABLISH PERMANENT STRIPES ITEMS 685.1707--25 AND 685.1708--25

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 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MOMROE COUNTY	DATE: 07/2016
TITLE OF DRAWING PAVEMENT TRANSITION	DRAWING NUMBER: PT -1

1. PURPOSE

THE PURPOSE OF THIS DOCUMENT IS TO ESTABLISH AND PROMOTE UNIFORM POLICIES FOR THE PLACEMENT, IDENTIFICATION AND REMOVAL OF STEEL PLATES OVER OPEN EXCAVATIONS WITHIN THE AUTHORITY R.O.W. FOR THE NEW YORK STATE THRUWAY AUTHORITY.

2. OBJECTIVE:

THESE REQUIREMENTS ARE INTENDED TO PROVIDE A HIGH LEVEL OF SERVICE OF BY ENSURING THAT THE THRUWAY AND ROADWAYS ASSOCIATED ARE LEFT IN THE SAME CONDITION IN WHICH THEY WERE FOUND BEFORE THE EXCAVATION AND TO ESTABLISH CRITERIA FOR THE USE OF STEEL PLATES DURING THE CUTTING OF PAVEMENT, EXCAVATING, BACK FILLING, REPLACEMENT OF BRIDGE JOINTS AND REPAIRING OF PAVEMENTS CUTS. THIS WILL REDUCE CONGESTION AND HAZARDS ALONG WITH INCREASING PUBLIC SERVICE AND SAFETY IN AND OUT OF CONSTRUCTION SITES.

3. BACKGROUND

STEEL PLATES ARE USED PRIMARILY IN WHERE AN EXCAVATION IS MADE IN THE ROADWAY FOR INSTALLATION OF BRIDGE JOINTS, UTILITY WORK, SUCH AS INSTALLING OR REPLACING UTILITY LINES, PIPES AND CONDUITS. STEEL PLATES ARE POSITIONED ON THE ROADWAY TO COVER THE EXCAVATION AND ALLOW TRAFFIC TO PROCEED WHEN WORK IS NOT UNDERWAY. THE USE OF STEEL PLATE(S) AS A TEMPORARY COVER OVER AN EXCAVATED AREA IN A VEHICULAR TRAVEL LANE CAN PRESENT SAFETY HAZARDS AND OTHER PROBLEMS IF ALLOWED TO REMAIN IN PLACE FOR EXTENDED PERIODS AND NOT PROPERLY SECURED. SOME OF THE PROBLEMS INCLUDE:

- 1- PLATE(S) BEING INSTALLED INCORRECTLY RESULTING IN THE LOUD RATTLING OF THE PLATES.
- 2- PLATE(S) NOT CLEARLY MARKED IN A LOCATION EASILY VISIBLE WITH NO WAY OF DETERMINING THE OWNER OF THE PLATE(S).
- 3- MOTORCYCLISTS ARE CHALLENGED WHEN THEY TRAVERSE AN UNEXPECTED PLATE(S).
- 4- PLATE(S) BEING USED AS A PERMANENT ROADWAY FIX.

THE CONTRACTOR SHALL PROVIDE A WRITTEN REQUEST FOR PERMISSION TO UTILIZE STEEL PLATE(S) AS A TEMPORARY COVER OVER AN EXCAVATED AREA FROM THE CHIEF ENGINEER OF THE NEW YORK STATE THRUWAY AUTHORITY

4. PERMIT APPLICATION AND NON -CONFORMANCE

4.1. PERMITTING

ANY WORK ON AUTHORITY R.O.W. OR MAINTAINED STREET, OR OTHER RIGHT-OF-WAY WITHIN THE CONTRACT DOCUMENTS REQUIRES A PERMIT FROM THE CHIEF ENGINEER. THIS PERMIT ALLOWS A CONTRACTOR OR AN INDIVIDUAL TO CONDUCT THE WORK WITHIN THE RIGHT-OF-WAY. APPROVAL OF THE AUTHORITY IS REQUIRED PRIOR TO THE STARTING THE WORK. CONSTRUCTION DRAWINGS AND A SITE SPECIFIC TRAFFIC CONTROL PLAN IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND MUST BE SUBMITTED WITH THE PERMIT REQUEST.

4.2. NON -CONFORMANCE

FAILURE TO COMPLY WITH APPLICABLE PERMITTING REQUIREMENTS WOULD BE CONSIDERED A VIOLATION OF THE CONTRACT DOCUMENTS. STRUCTURAL STEEL PLATE(S) THAT ARE KEPT OR MAINTAINED FOR AN UNREASONABLE LENGTH OF TIME AFTER THE PURPOSE THEREOF HAS BEEN ACCOMPLISHED SHALL ALSO BE CONSIDER IN NON -CONFORMANCE.

5. USE OF STEEL PLATE(S) IN THE TRAVELED WAY

WHEN COMPLETION OF THE SPECIFIED WORK OR BACK FILL OPERATIONS OF AN EXCAVATION IN THE TRAVELED WAY, WHETHER TRANSVERSE OR LONGITUDINAL, CANNOT BE PROPERLY COMPLETED WITHIN THE AVAILABLE TIME AS SHOWN IN THE CONTRACT DOCUMENTS, STEEL PLATE BRIDGING WILL BE REQUIRED TO PRESERVE UNOBSTRUCTED TRAFFIC FLOW IN AUTHORITY R.O.W. OR OTHER STREETS AND ROADWAYS. IN SUCH INSTANCES THE FOLLOWING APPLIES:

5.1. STEEL PLATES REQUIREMENTS:

- 1- STEEL PLATES MUST BE ABLE TO WITHSTAND HS-20 TRAFFIC LOADING WITHOUT ANY MOVEMENT.
- 2- STEEL PLATES SHALL BE FABRICATED TO MEET ASTM A36 STEEL REQUIREMENTS.
- 3- WHEN TWO OR MORE OF PLATES ARE USED, THE PLATES SHALL BE TACK WELDED TOGETHER AT EACH CORNER TO REDUCE OR ELIMINATE VERTICAL MOVEMENT. ALTERNATIVE METHODS TO ACCOMPLISH THIS, SUCH AS METAL CONNECTORS, WILL BE CONSIDERED FOR APPROVAL ON CASE BY CASE BASIS.
- 4- STEEL PLATES SHALL BE INSTALLED TO RESIST BENDING, VIBRATIONS, ETC., UNDER TRAFFIC LOADS AND SHALL BE ANCHORED SECURELY TO PREVENT MOVEMENT. IF THESE CONDITIONS ARE NOT MET, THE APPLICANT WILL BE REQUIRED TO BACK FILL AND PAVE THE EXCAVATION DAILY, OR USE ALTERNATIVE METHODS SUCH AS PLATE LOCK WHICH ARE DESIGNED TO SECURE THE PLATES WITH MINIMUM NOISE AND VIBRATION.

ALL STEEL PLATES SHALL BE PROPERLY MARKED WITH THE UTILITY AND OR CONTRACTOR NAME. AFTER -HOURS CONTACT PHONE NUMBER IN THE EVENT THE PLATES NEED TO BE SECURED. ALL STEEL PLATES WITHIN THE RIGHT-OF-WAY, WHETHER USED IN OR OUT OF THE TRAVELED WAY, SHALL BE WITHOUT DEFORMATION. THE PLATE SURFACE MUST NOT DEVIATE MORE THAN 1/4 INCH WHEN MEASURED WITH A 10-FOOT STRAIGHT EDGE ALONG THE LENGTH OF THE PLATE.

IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PERFORM AND DOCUMENT DAILY INSPECTIONS OF ALL ACTIVE PLATE(S) OR UNATTENDED PLATE(S) LOCATION(S), AND WHERE NECESSARY TAKE APPROPRIATE MEASURES TO PROTECT THE PUBLIC SAFETY UNTIL WORK IS COMPLETED. THIS DOCUMENTATION SHALL BE AVAILABLE TO THE AUTHORITY INSPECTOR UPON REQUEST. NO UN- PLATED EXCAVATION SHALL BE LEFT UNATTENDED OVERNIGHT.

IN THE EVENT OF IMPROPER INSTALLATION OF THE STEEL PLATE(S) THAT PRESENTS A NUISANCE OR A PUBLIC SAFETY PROBLEM, THE CONTRACTOR SHALL RESPOND TO ALL EXCAVATION RESTORATION REQUESTS BY THE AUTHORITY IMMEDIATELY UPON NOTIFICATION. NON -RESPONSES WILL RESULT IN THE REQUIRED RESTORATION WORK BEING DONE BY THE AUTHORITY WITH ALL EXPENSES TO BE PAID BY THE CONTRACTOR. STEEL PLATES MUST EXTEND A MINIMUM OF 18-INCHES BEYOND THE EDGES OF THE EXCAVATION. BEFORE STEEL PLATES ARE INSTALLED, THE EXCAVATION SHALL BE ADEQUATELY SHORED TO SUPPORT THE BRIDGING AND TRAFFIC LOADS.

TEMPORARY PAVING WITH A ASPHALT MIX TO FEATHER THE EDGES OF THE PLATE TO FORM A WEDGED TAPER TO COVER THE EDGES OF THE STEEL PLATE. OTHER ALTERNATIVE METHODS TO ACCOMPLISH THIS WILL BE CONSIDERED FOR APPROVAL. WEDGES OR OTHER NON -ASPHALTIC DEVICES SHALL BE USED FOR LEVELING AS REQUIRED TO ELIMINATE ROCKING OF THE PLATES. COMPACTED TEMPORARY ASPHALT SHALL BE USED TO FILL ALL GAPS BETWEEN THE PLATES AND EXISTING PAVEMENT SURFACES.

5.2. TYPICAL STEEL PLATES DETAILS

TEMPORARY STEEL PLATES INSTALLED ON ROADWAYS OPEN TO VEHICULAR TRAFFIC SHOULD HAVE A RIDING SURFACE WITH A SKID RESISTANT SURFACE EQUAL TO OR GREATER THAN ADJACENT ROADWAY.

THE STEEL PLATE(S) SHALL BE ANCHORED SECURELY TO PREVENT MOVEMENT. THE DRILLING AND USE OF MECHANICAL ANCHORING/OR ADHESIVE ANCHORING OF THE STEEL PLATE(S) INTO THE SURFACE BELOW SHALL NOT BE ALLOWED. TEMPORARY PAVING WITH A COLD ASPHALT MIX, OR APPROVED EQUAL, SHOULD ALSO BE USED TO FEATHER THE EDGES OF THE PLATE TO FORM A WEDGED TAPER TO COVER THE EDGES OF THE STEEL PLATE.

ALL STEEL PLATE SHALL BE A36 STEEL.

TYPE 1 INSTALLATION

TYPE 1 INSTALLATION SHOWN, SHALL BE USED IN AREAS WHERE COMPLETION OF THE WORK OR BACK FILLING OPERATIONS OF AN EXCAVATION IN THE TRAVELED WAY, WHETHER TRAVERSE OR LONGITUDINAL CANNOT BE PROPERLY COMPLETED WITHIN THE SAME DAY AND THE POSTED SPEED LIMIT IS LESS THAN 35 MPH. THE STEEL PLATE SHALL BE ANCHORED SECURELY TO PREVENT MOVEMENT.

IF AN LANE ADJACENT TO THE ROADWAY PLATE IS NOT AT THE SAME ELEVATION THE CONTRACTOR SHALL PROVIDE A SLOPED TRANSITION OF A MINIMUM WIDTH OF 12 INCHES (12") THE FULL LENGTH OF THE ADJACENT STEEL PLATE. THE DRILLING AND USE OF MECHANICAL ANCHORING/OR ADHESIVE ANCHORING OF THE STEEL PLATE(S) INTO THE SURFACE BELOW SHALL NOT BE ALLOWED. THE GAP BETWEEN THE EDGE OF THE PLATE AND THE ADJACENT EXISTING ASPHALT PAVEMENT MUST BE FILLED WITH TEMPORARY ASPHALT PATCH (COLD MIX). WEDGES OR OTHER NON -ASPHALTIC DEVICES SHALL BE USED FOR LEVELING AS REQUIRED TO ELIMINATE ROCKING OF THE PLATES. COMPACTED TEMPORARY ASPHALT SHALL BE USED TO FILL ALL GAPS BETWEEN THE PLATES AND EXISTING PAVEMENT SURFACES.

TYPE 2 INSTALLATION

TYPE 2 INSTALLATION SHOWN SHALL BE USED IN AREAS WHERE COMPLETION OF THE WORK OR BACK FILLING OPERATIONS OF AN EXCAVATION IN THE TRAVELED WAY, WHETHER TRAVERSE OR LONGITUDINAL CANNOT BE PROPERLY COMPLETED WITHIN THE SAME DAY AND THE POSTED SPEED LIMIT IS GREATER THAN 35 MPH. THE PAVEMENT SHALL BE TO A DEPTH EQUAL TO THE THICKNESS OF THE PLATE AND TO A WIDTH AND LENGTH EQUAL TO THE DIMENSIONS OF THE PLATE. IF AN LANE ADJACENT TO THE ROADWAY PLATE IS NOT AT THE SAME ELEVATION THE CONTRACTOR SHALL PROVIDE A SLOPED TRANSITION OF A MINIMUM WIDTH OF 12 INCHES (12") THE FULL LENGTH OF THE ADJACENT STEEL PLATE. THE STEEL PLATE SHALL BE ANCHORED SECURELY TO PREVENT MOVEMENT, THE DRILLING AND USE OF MECHANICAL ANCHORING/OR ADHESIVE ANCHORING OF THE STEEL PLATE(S) INTO THE SURFACE BELOW SHALL NOT BE ALLOWED. THE GAP BETWEEN THE EDGE OF THE PLATE AND THE ADJACENT EXISTING ASPHALT PAVEMENT MUST BE FILLED WITH TEMPORARY ASPHALT PATCH (COLD MIX). WEDGES OR OTHER NON -ASPHALTIC DEVICES SHALL BE USED FOR LEVELING AS REQUIRED TO ELIMINATE ROCKING OF THE PLATES. COMPACTED TEMPORARY ASPHALT SHALL BE USED TO FILL ALL GAPS BETWEEN THE PLATES AND EXISTING PAVEMENT SURFACES.

FIGURE 3 TYPE 2 INSTALLATION DETAIL

PROVISIONS SHOULD BE MADE FOR THE SAFETY AND PROTECTION OF VEHICULAR AND PEDESTRIAN TRAFFIC DURING THE CONSTRUCTION PERIOD AS FOLLOWS:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE FURNISHING, ERECTION AND MAINTENANCE OF ALL REQUIRED TRAFFIC CONTROL DEVICES. ALL SIGNS AND DEVICES SHALL CONFORM TO THE REQUIREMENTS OF THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD). WHEN IN THE OPINION OF THE AUTHORITY, THE WORK CONSTITUTES A HAZARD TO TRAFFIC IN ANY AREA OF THE WORK, THE CONTRACTOR MAY BE REQUIRED TO SUSPEND OPERATIONS DURING CERTAIN HOURS AND TO REMOVE ANY EQUIPMENT FROM THE AREA OF WORK THE ROADWAY SURFACE SHALL BE KEPT CLEAN OF DEBRIS AT ALL TIMES AND SHOULD BE THOROUGHLY CLEANED AT THE COMPLETION OF THE WORK. THE CONTRACTOR SHALL BE RESPONSIBLE TO REPLACE ALL PAVEMENT MARKINGS IN KIND WHICH HAVE BEEN DISTURBED AS A RESULT OF THE UTILITY WORK

6.1 SIGNAGE

IN ADDITION OF THE TRAFFIC CONTROL DEVICES REQUIRED BY THE ENGINEERING, WARNING SIGNS ADVISING MOTORIST THAT THEY SHOULD EXPECT TO ENCOUNTER STEEL PLATES, SHALL BE PLACED AT APPROXIMATELY 100 FEET IN ADVANCE OF THE STEEL PLATE LOCATION. THE WARNING SIGN, STEEL PLATES AHEAD SHALL MEET THE REQUIREMENTS OF THE MUTCD. PLATES LEFT OVERNIGHT MAY REQUIRE, AT THE DISCRETION OF THE ENGINEERING DIVISION INSPECTOR,

6.2 NIGHT VISIBILITY

TEMPORARY STEEL PLATES INSTALLED ON ROADWAYS OPEN TO VEHICULAR TRAFFIC SHOULD BE MARKED WITH A DURABLE AND HIGHLY REFLECTIVE PAVEMENT MARKING TAPE NO LESS THAN 4 INCHES IN WIDTH, THE MARKING PATTERN USED SHOULD, AT A MINIMUM, INCLUDE ALL FOUR CORNERS OF THE PLATE, SIMILAR TO THE PATTERN SHOWN EXISTING ROADWAY.

7. UTILITY CUT RESTORATION

PAVEMENT SETTLEMENT OCCURRING IN AND AROUND UTILITY CUTS IN THE AUTHORITY R.O.W. IS A COMMON PROBLEM, RESULTING IN UNEVEN PAVEMENT SURFACES, ANNOYANCE TO DRIVERS, AND ULTIMATELY, FURTHER MAINTENANCE BY THE AUTHORITY R.O.W. FACTORS INFLUENCING THE PERFORMANCE OF A PATCH INCLUDE:

PAVEMENT MATERIAL, SOIL CONDITIONS, CLIMATE, TRAFFIC AND REPAIR TECHNIQUES. POOR CONSTRUCTION TECHNIQUES WHICH DAMAGE THE AREA ADJACENT TO THE CUT AND FURTHER DEGRADE THE PATCH AND SURROUNDING PAVEMENT. PAVEMENT CUT REPAIRS MADE USING QUALITY MATERIALS AND SOUND ENGINEERING AND CONSTRUCTION TECHNIQUES TEND TO PERFORM AS WELL AS THE SURROUNDING PAVEMENT. POOR PERFORMANCE OF THE PATCH TENDS TO BE A RESULT OF INADEQUATE COMPACTION OF THE MATERIALS, INSUFFICIENT THICKNESS OF MATERIALS, POOR QUALITY OF MATERIALS, AND DAMAGE TO THE SIDE OF THE CUT.

7.1 EXCAVATION/BACK FILL

THE SURFACE OF THE ROADWAY TO BE EXCAVATED FOR THE UTILITY WORK SHALL BE SAW-CUT IN REASONABLY STRAIGHT AND PARALLEL LINES. THE CUTTING EXCAVATION SHOULD SHALL BE CUT AND BROUGHT TO A NEAT LINE.

EXPANSION JOINTS REMOVED SHALL BE REPLACED. THE CUTTING AND REPLACEMENT OF CONCRETE CURBS/GUTTERS AND SIDEWALKS SHALL BE FROM JOINT TO JOINT AND OF COMPLETE PANELS. BACK FILL MATERIAL SHALL MEET THE APPLICABLE REQUIREMENTS OF THE CURRENT EDITION OF THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION, STANDARD SPECIFICATIONS UNSUITABLE MATERIAL WILL NOT BE ALLOWED FOR BACK FILL.

7.2 PAVEMENT RESTORATION

THE CONTRACTOR SHALL BE RESPONSIBLE TO REPLACE ALL PAVEMENT DISTURBED WITH A HOMOGENEOUS AND IN -KIND MATERIAL.

ALL EXISTING PAVEMENT SURFACE SHOULD BE SWEEP CLEAN OF DIRT, DUST AND DEBRIS PRIOR OF PATCHING. IF NECESSARY THE EXISTING VERTICAL PAVEMENT SHALL BE TACK COATED WITH AN APPROPRIATE TACKING MATERIAL PRIOR TO PATCHING WHEN THE PAVEMENT REMAINING BETWEEN THE EXCAVATION AND THE EDGE OF THE ROADWAY IS LESS THAN TO 2 FEET, THE REMAINING AREA SHALL BE REMOVED IN CONJUNCTION WITH THE PERMANENT REPAIR. THE WORK PERFORMED SHALL BE FREE FROM WORKMANSHIP DEFECTS. IF PERMANENT PAVEMENT RESTORATION CANNOT BE COMPLETED WITHIN THREE (3) DAYS, THEN TEMPORARY PATCH MAY BE UTILIZED WITH APPROVAL OF THE ENGINEER.

ANY DISTURBED PAVEMENT MARKINGS MUST BE RESTORED TO MATCH ADJACENT STRIPING. CRACK SEAL AROUND ASPHALT PATCH MAY BE REQUIRED AT THE DISCRETION OF THE INSPECTOR WHEN THERE IS A GAP PRESENT. SPOIL PILES MUST BE REMOVED AND THE AREA MUST BE CLEANED AND RESTORED TO LIKE KIND OR BETTER CONDITION. TRAFFIC CONTROL DEVICES REMOVED.

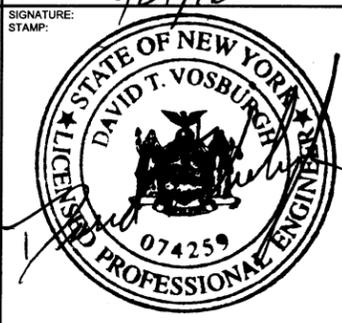
8.0 PAYMENT

ALL COSTS ASSOCIATED WITH THE WORK DESCRIBED ABOVE INCLUDING THE FABRICATION, TRANSPORTATION, INSTALLATION, MAINTENANCE, MONITORING AND REMOVAL, AND ALL MATERIALS, LABOR, EQUIPMENT NECESSARY TO PERFORM THE WORK SHALL BE INCLUDED IN ITEM 567.6100--25 REMOVAL OF EXISTING BRIDGE JOINT SYSTEM. ROADWAY PLATING ASSOCIATED WITH DECK REPAIRS FOR MP 356.69 SHALL BE INCLUDED IN ITEM 584.55--25 & 584.56--25

FILED  
CHECKED BY: TA  
DRAFTED BY: M. COX  
CHECKED BY: TA  
DESIGNED BY: M. COX  
DESIGN SUPERVISOR: D. VOSBURGH

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8/29/16

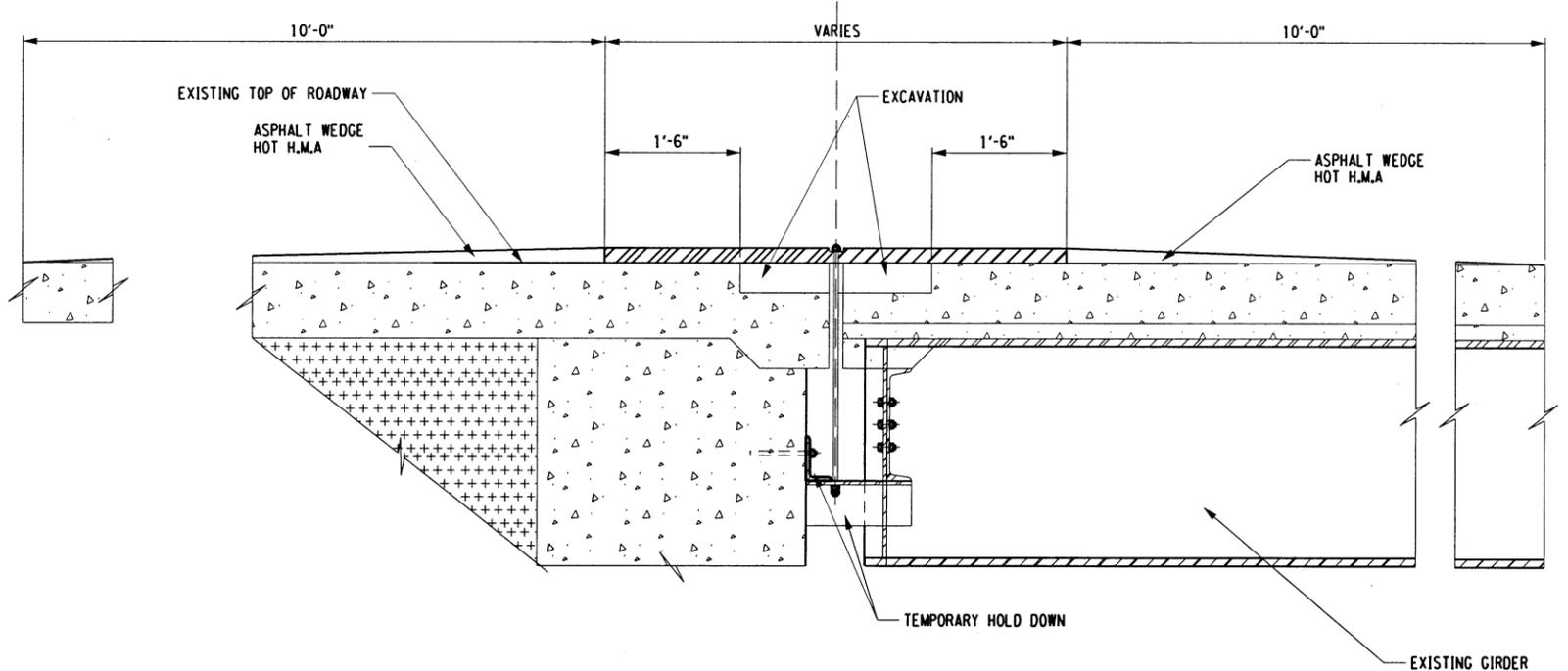
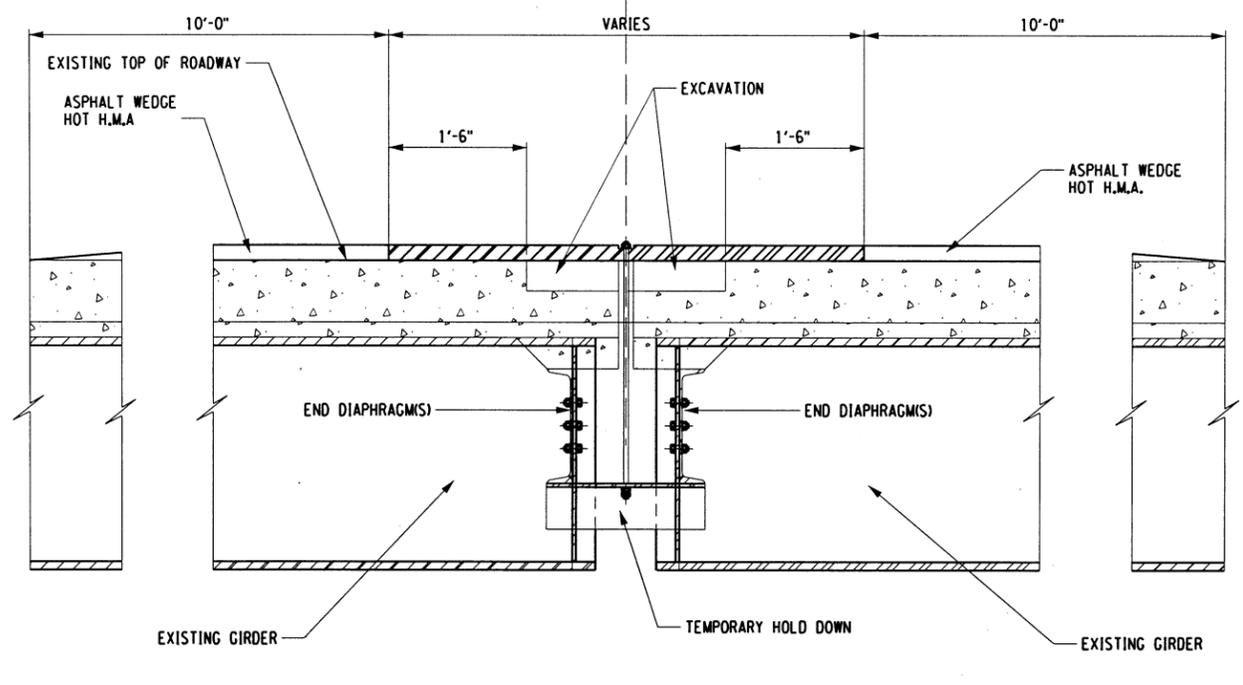


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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
	TITLE OF DRAWING ROADWAY PLATES GENERAL NOTES	DRAWING NUMBER: RPGN-1

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSEBURGH



- TYPE 2 INSTALLATION:**
- 1- SHALL BE USED ON ROADS WITH A POSTED SPEED LIMIT GREATER THAN 35 MPH.
  - 2- STEEL PLATE(S) SHALL BE SECURELY ANCHORED
  - 3- ALL STEEL SHALL BE ASTM A36, ALL BOLT(S) SHALL BE A325, THREADED ROD(S) SHALL BE A354
  - 4- HEADS OF ANCHOR ROD SHALL NOT PROTRUDE GREATER THEN 1/2" HIGHER THEN THE STEEL PLATE
  - 5- ROADWAY PLATE(S) SHALL NOT BE UTILIZED BETWEEN THE MONTHS OF OCTOBER 1 THRU APRIL 1.
  - 6- THE DETAILS ABOVE ARE NOT TO SCALE OR REPRESENTATIVE OF WHAT EXISTS, THE CONTRACTOR SHALL CAREFULLY EXAMINE THE WORK LOCATION AND DETERMINE THE EXISTING CONDITIONS. THE DETAILS ABOVE ARE A NOT AN ACTUAL DESIGN THE CONTRACTOR SHALL DETERMINE THEIR OWN MEANS AND METHOD
  - 7- ALL COST ASSOCIATED WITH ROADWAY PLATING SHALL FOR BRIDGE JOINTS SHALL BE INCLUDED IN ITEM 567.6100--25 REMOVAL OF EXISTING BRIDGE JOINT SYSTEM. ROADWAY PLATING ASSOCIATED WITH DECK REPAIRS FOR MP 356.69 SHALL BE INCLUDED IN ITEM 584.55--25 & 584.56--25

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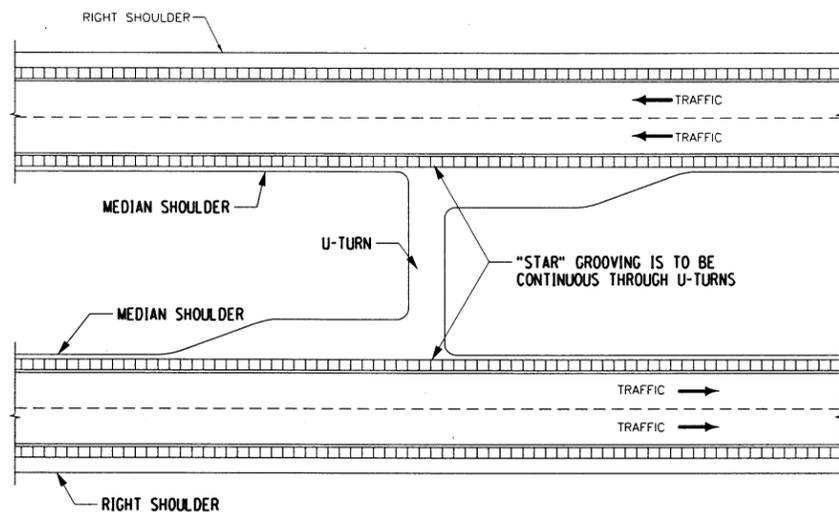
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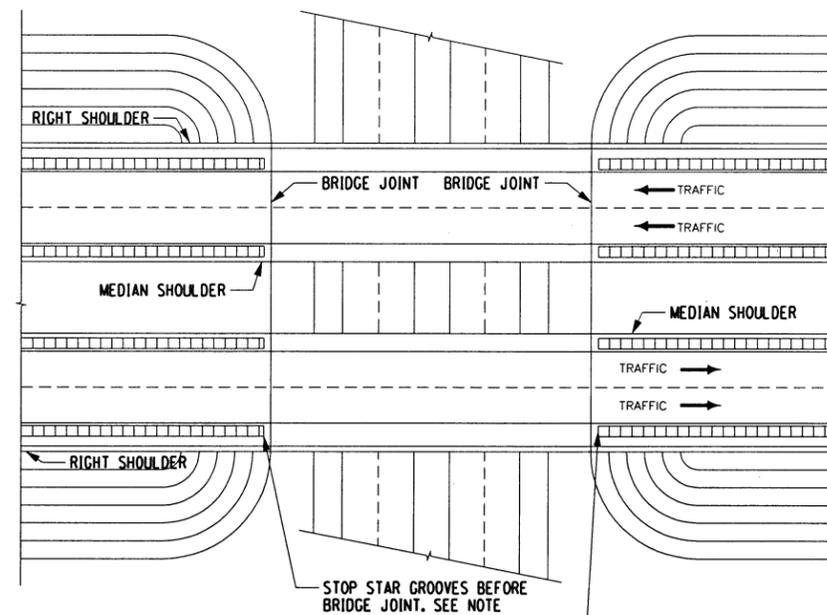
TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/29/2016
TITLE OF DRAWING ROADWAY PLATES DETAILS	DRAWING NUMBER: RPD-2



"STAR" GROOVE CONSTRUCTION PLAN  
U-TURN DETAIL  
N.T.S.

STAR LIMITS			
LFT SHOULDER	BEGIN STA. (FT)	END STA.	ESTIMATED LENGTH (FT)
	2481+84	2496+50	1466
	2497+44	2516+50	1906
RGHT SHOULDER			
	2488+45	2496+08	763
	2497+65	2510+00	1235

NOTES:  
STAR GROOVES ARE NOT TO BE PLACED ON OR NEAR LONGITUDINAL CONSTRUCTION JOINTS. THE CONTRACTOR SHALL ADJUST PLACEMENT OF THE "STARS" SO THAT THE EDGE OF THE STAR GROOVES ARE NO CLOSER THAN 6 INCHES FROM ANY LONGITUDINAL JOINT.

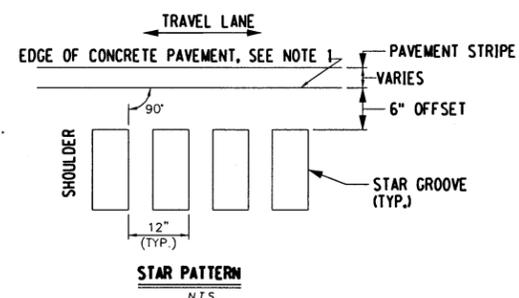


NOTE:  
STAR GROOVE SHALL NOT BE PLACED CLOSER THAN 2FT TO ANY BRIDGE JOINT.

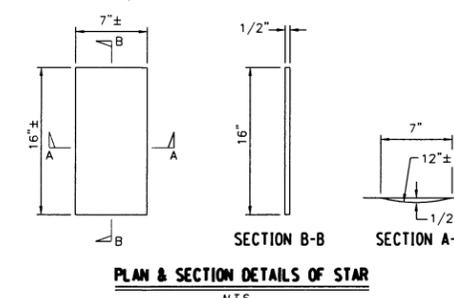
RESUME STAR GROOVES AFTER BRIDGE JOINT

NOTE:  
2 LANE PAVEMENT SHOWN-3 AND 4 LANE PAVEMENT AREAS ARE SIMILAR.

"STAR" GROOVE CONSTRUCTION PLAN  
BRIDGE DETAIL  
N.T.S.



STAR PATTERN  
N.T.S.



PLAN & SECTION DETAILS OF STAR  
N.T.S.

NOTES:

- STAR ALIGNMENT SHALL GENERALLY BE STRAIGHT AND OFFSET APPROX. 6" FROM THE ORIGINAL EDGE OF PAVEMENT FOR REHABILITATION PROJECTS AND 6" FROM THE NEW EDGE OF PAVEMENT FOR RECONSTRUCTION PROJECTS, UNLESS SHOWN OTHERWISE ON THE TYPICAL SECTION. THE EDGE OF CONCRETE PAVEMENT WILL NOT BE VISIBLE ON COMPOSITE PAVEMENTS.
- THE 1/2" DEPTH SHALL GENERALLY APPLY FOR THE ENTIRE 16" LENGTH. SOME VARIATION TO SUIT SHOULDER SLOPE BREAKS MAY BE NECESSARY.
- STAR TREATMENT SHALL BE COMPLETED AFTER COMPLETION OF THE BRIDGE DECK OVERLAY

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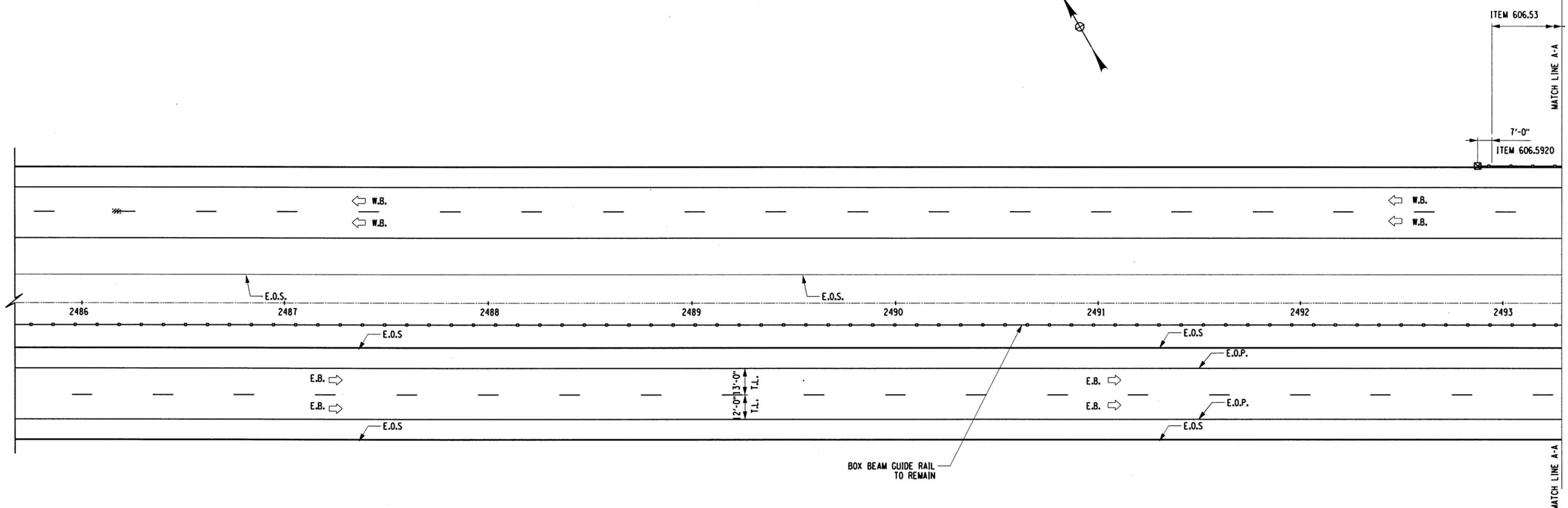
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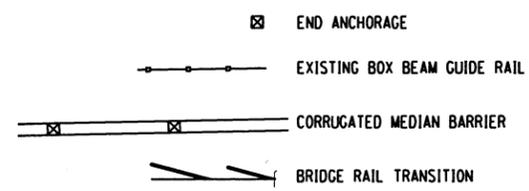
REVISIONS			
DATE	DESCRIPTION	BY	SYM



TITLE OF PROJECT PITTSFORD MENDON CENTER RD. MONROE COUNTY	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 BUFFALO DIVISION	DATE: 06/29/2016
TITLE OF DRAWING STAR GROOVE DETAILS	DRAWING NUMBER: SGD -1

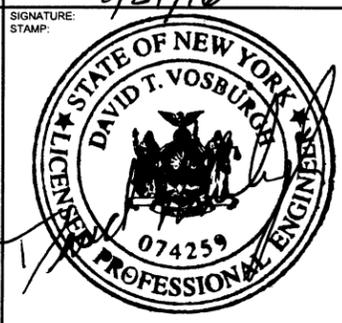


### THRUWAY GUIDERAIL PLAN



- NOTES:  
 1- EXISTING GUIDE RAIL SHALL NOT BE REMOVED UNTIL TEMPORARY CONCRETE BARRIER HAS BEEN INSTALLED  
 2- AFTER SHOULDER RECONSTRUCTION THE CONTRACTOR SHALL RESET/INSTALL ALL GUIDERAIL AND ANCHORS PRIOR TO REMOVING TEMPORARY CONCRETE BARRIER

\*RAIL RUNS ARE ROUNDED UP TO EVEN LENGTHS

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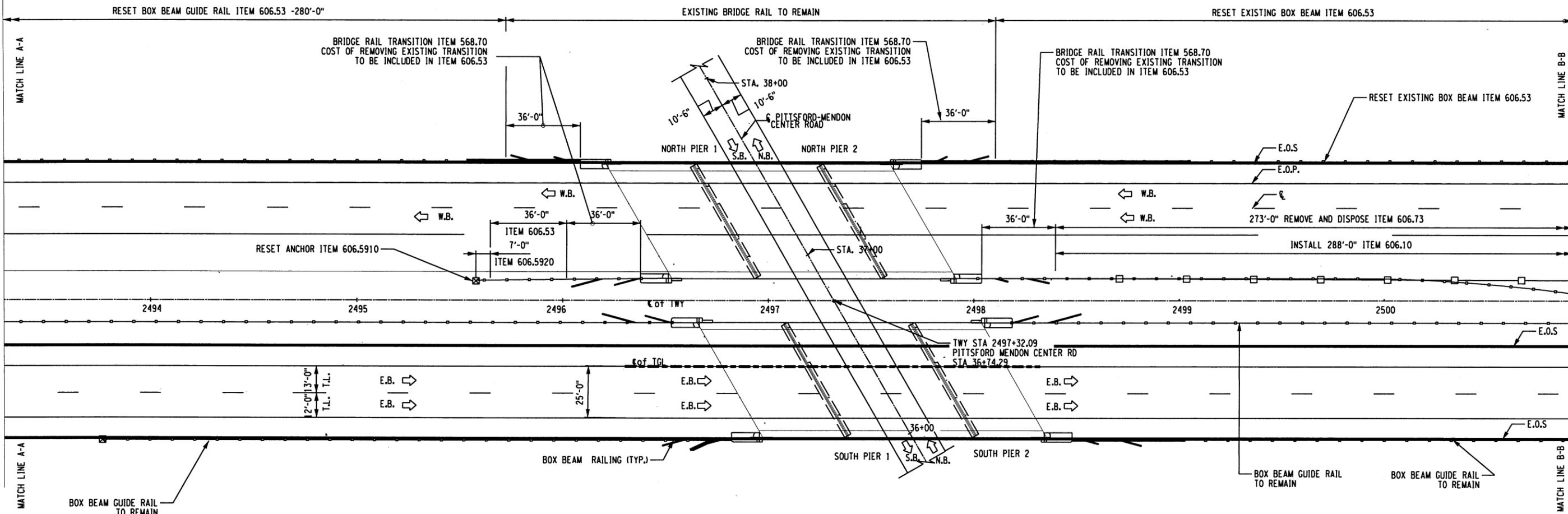
REVISIONS			
DATE	DESCRIPTION	BY	SYM



**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 07/2016
TITLE OF DRAWING GUIDERAIL PLAN	DRAWING NUMBER: GR 1-6

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 CHECKED BY: TA  
 DRAFTED BY: MACOX  
 CHECKED BY: TA  
 DESIGNED BY: MACOX  
 DESIGN SUPERVISOR: DAVOSBURGH



**THRUWAY GUIDERAIL PLAN**

- ☒ END ANCHORAGE
- EXISTING BOX BEAM GUIDE RAIL
- ▬— CORRUGATED MEDIAN BARRIER
- ▬▬ BRIDGE RAIL TRANSITION

NOTES:  
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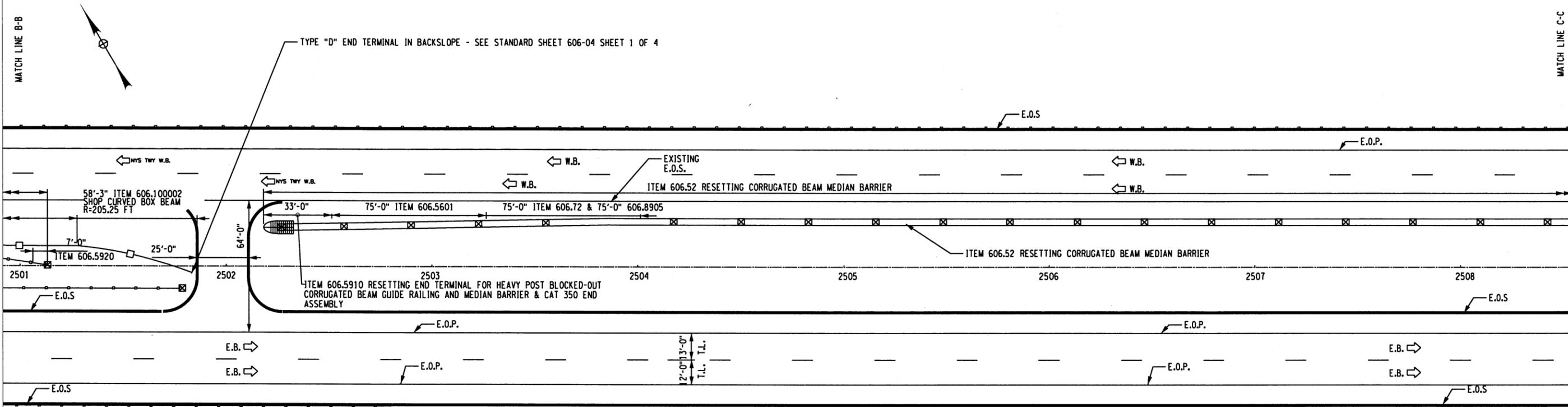
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**Thruway Authority** | **Canal Corporation**

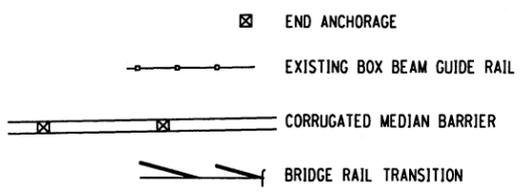
TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/27/2016
TITLE OF DRAWING GUIDERAIL PLAN	DRAWING NUMBER: GR 2-6

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 CHECKED BY: TA  
 DRAFTED BY: TA  
 CHECKED BY: TA  
 DESIGNED BY: TA  
 DESIGN SUPERVISOR: TA

RESET EXISTING BOX BEAM 1126'-0" ITEM 606.53



**THRUWAY GUIDERAIL PLAN**



- NOTES:**
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  - 2- AFTER SHOULDER RECONSTRUCTION THE CONTRACTOR SHALL RESET/INSTALL ALL GUIDERAIL AND ANCHORS PRIOR TO REMOVING TEMPORARY CONCRETE BARRIER
- ITEM 606.72- REMOVE & DISPOSE CORRUGATED BEAM MEDIAN BARRIER  
 ITEM 606.5601- RESETTING HPBO CORRUGATED BEAM MEDIAN BARRIER (NEW 12 INCH BLOCKOUTS)  
 ITEM 606.8905- TRANSITION HEAVY POST BLOCK-OUT (MOD) CORRUGATED BEAM MEDIAN BARRIER TO WEAK POST CORRUGATED BEAM MEDIAN BARRIER

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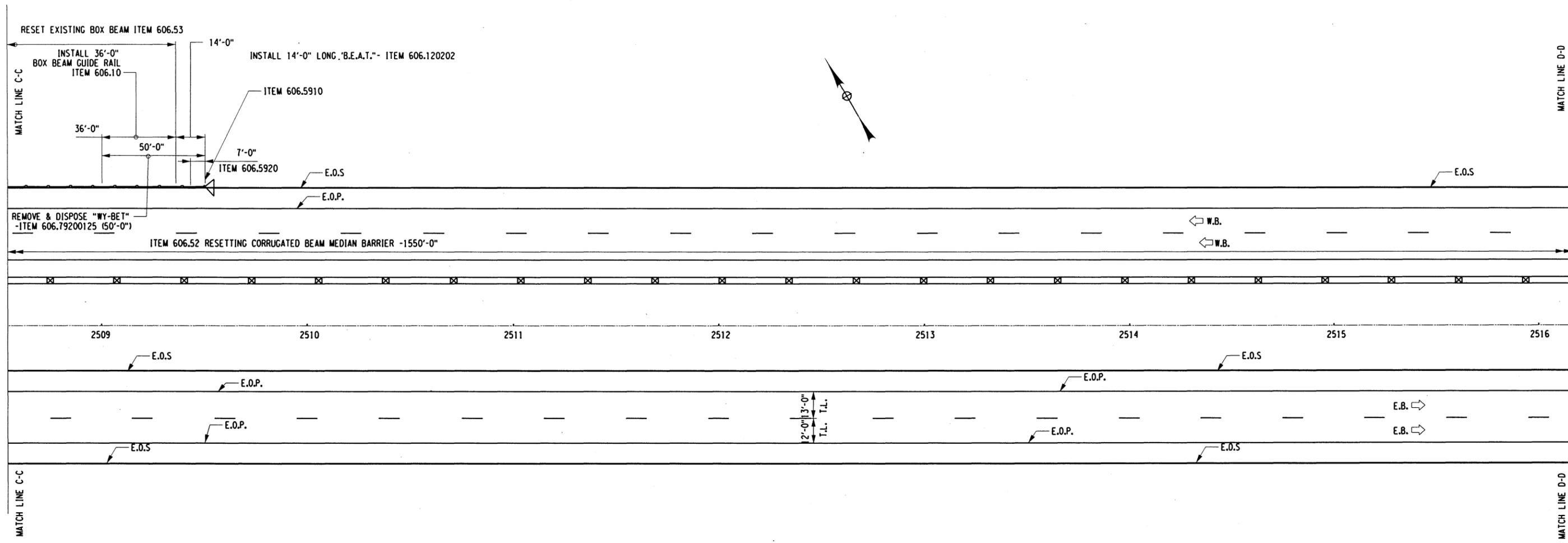
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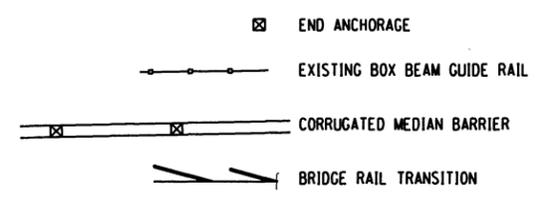


TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/27/2016
TITLE OF DRAWING GUIDERAIL PLAN	DRAWING NUMBER: GR 3-6

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



THRUWAY GUIDERAIL PLAN



NOTES:  
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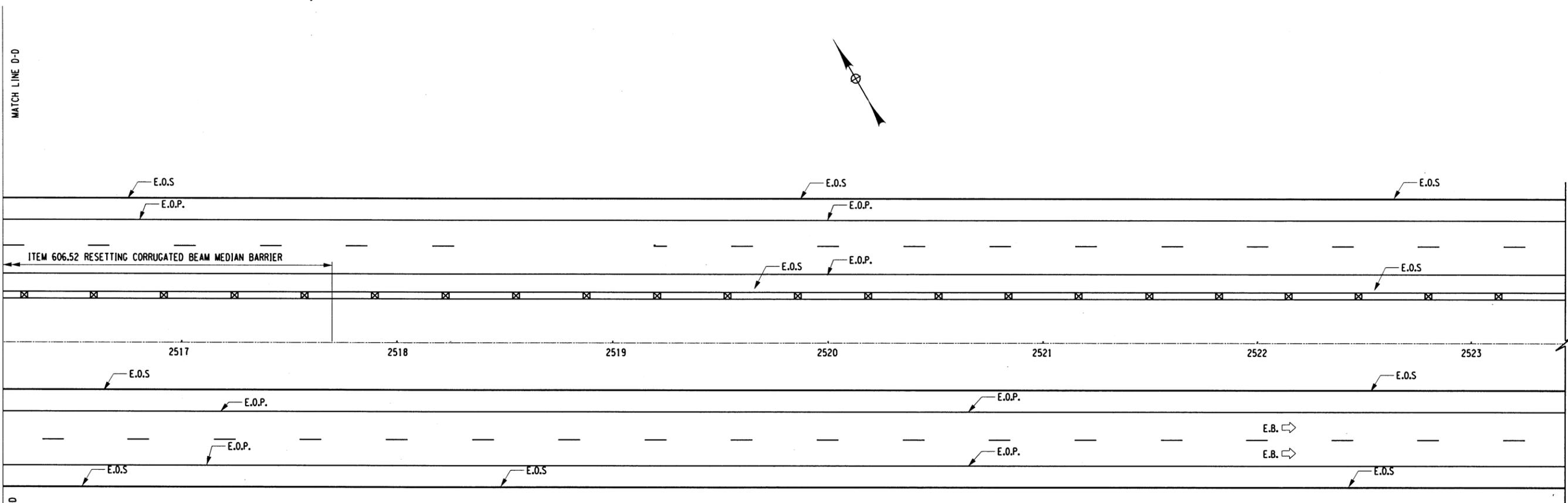
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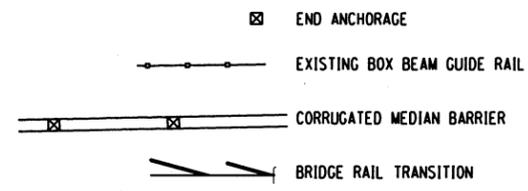
REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/27/2016
TITLE OF DRAWING GUIDERAIL PLAN	DRAWING NUMBER: GR 4-6	

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 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



**THRUWAY GUIDERAIL PLAN**



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 <b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT: NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT: TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER ROAD, MONROE COUNTY	
	DATE: 06/27/2016	
TITLE OF DRAWING: GUIDERAIL PLAN		DRAWING NUMBER: GR-5-6

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 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH

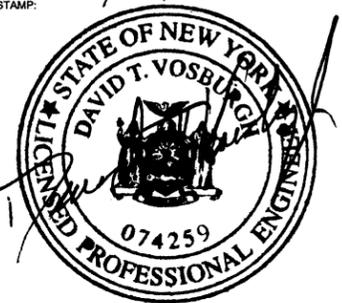
GUIDE RAILING - INSTALLATION TWY MAINLINE																
STATION TO STATION	SIDE			ITEM (L.F.) 568.70	ITEM (L.F.) 606.10	ITEM (L.F.) 606.100002	ITEM (EACH) 606.120202	ITEM (L.F.) 606.53	ITEM (L.F.) 606.5601		ITEM (EACH) 606.5910	ITEM (EACH) 606.5920	ITEM (L.F.) 606.72	ITEM (L.F.) 606.73	ITEM (EACH) 606.79200125	ITEM (EACH) 606.8905
WB 2492+87.5 TO WB 2492+94.5	RT SHLDR											1				
WB 2492+94.5 TO WB 2495+72.4	RT SHLDR							280.0								
WB 2495+72.4 TO WB 2496+08	RT SHLDR			36.0				36.0								
WB 2497+74.3 TO WB 2498+10.3	RT SHLDR			36.0				36.0								
WB 2498+10.3 TO WB 2509+36.3	RT SHLDR							1126.0								
WB 2509+00.3 TO WB 2509+36.3	RT SHLDR				36.0											
WB 2509+00.3 TO WB 2509+50.3	RT SHLDR														1	
WB 2509+43.3 TO WB 2509+50.3	RT SHLDR											1				
WB 2509+43.3											1					
WB 2509+36 TO 2509+50	RT SHLDR						1									
<b>TOTAL</b>				72.0	36.0		1	1478.0			1	2			1	

NOTE: STATION TO STATION DISTANCE MAY NOT EQUAL TRUE DISTANCE DUE TO RADIUS

GUIDE RAILING - INSTALLATION TWY MAINLINE																
STATION TO STATION	SIDE			ITEM (L.F.) 568.70	ITEM (L.F.) 606.10	ITEM (L.F.) 606.100002	ITEM (L.F.) 606.52	ITEM (L.F.) 606.53	ITEM (L.F.) 606.5601		ITEM (EACH) 606.5910	ITEM (EACH) 606.5920	ITEM (L.F.) 606.72	ITEM (L.F.) 606.73	ITEM (EACH) 606.79200125	ITEM (EACH) 606.8905
WB 2495+57.9	LFT SHLDR										1.0					
WB 2495+57.5 TO 2495+64.5	LFT SHLDR											1.0				
WB 2501+06.3 TO 2501+13.3	LFT SHLDR											1.0				
WB 2495+64.9 TO 2496+01.9	LFT SHLDR							36.0								
WB 2496+00.9 TO 2496+36.9	LFT SHLDR			36.0				36.0								
WB 2496+01.9 TO 2496+37.9	LFT SHLDR															
WB 2498+03.7 TO 2498+39.7	LFT SHLDR			36.0				36.0								
WB 2498+39.7 TO 2501+13.1	LFT SHLDR													273.0		
WB 2501+27.5 TO 2501+85.8	LFT SHLDR					58.3										
WB 2502+18.2 TO 2517+69.9	LFT SHLDR						1551.7									
WB 2502+51.2 TO 2503+26.2	LFT SHLDR								75.0							
WB 2502+18.9 TO 2502+51.9	LFT SHLDR										1.0					
WB 2503+26.2 TO 2504+01.2	LFT SHLDR												75.0			1
WB 2498+39.7 TO 2501+27.6	LFT SHLDR				288.0											
<b>TOTAL</b>				72.0	288.0	58.3	1551.7	108.0	75.0		2.0	2.0	75.0	273.0		1

NOTE: STATION TO STATION DISTANCE MAY NOT EQUAL TRUE DISTANCE DUE TO RADIUS

GUIDE RAILING - INSTALLATION PITTSFORD MENDON CENTER RD.									
STATION TO STATION	SIDE					ITEM (EACH) 606.120201	ITEM (L.F.) 606.3042	ITEM (EACH) 606.8803	ITEM (EACH) 606.9003
STA 35+22 TO 35+58	LFT					1			
STA 35+58.5 TO 36+10	LFT								1
STA 36+01.10 TO 36+10	LFT							1	
STA 36+10 TO 37+70	LFT						160.0		
STA 37+70 TO 37+80	LFT							1	
STA 37+70 TO 38+22	LFT								1
STA 38+22 TO 38+51	LFT					1			
STA 35+02 TO 35+27.5	RT					1			
STA 35+27.5 TO 35+79	RT								1
STA 35+69 TO 35+79	RT							1	
STA 35+79 TO 37+39	RT						160.0		
STA 37+39 TO 37+49	RT							1	
STA 37+39 TO 37+91	RT								1
STA 37+91 TO 38+63	RT					1			
<b>TOTAL</b>						4	320	4	4

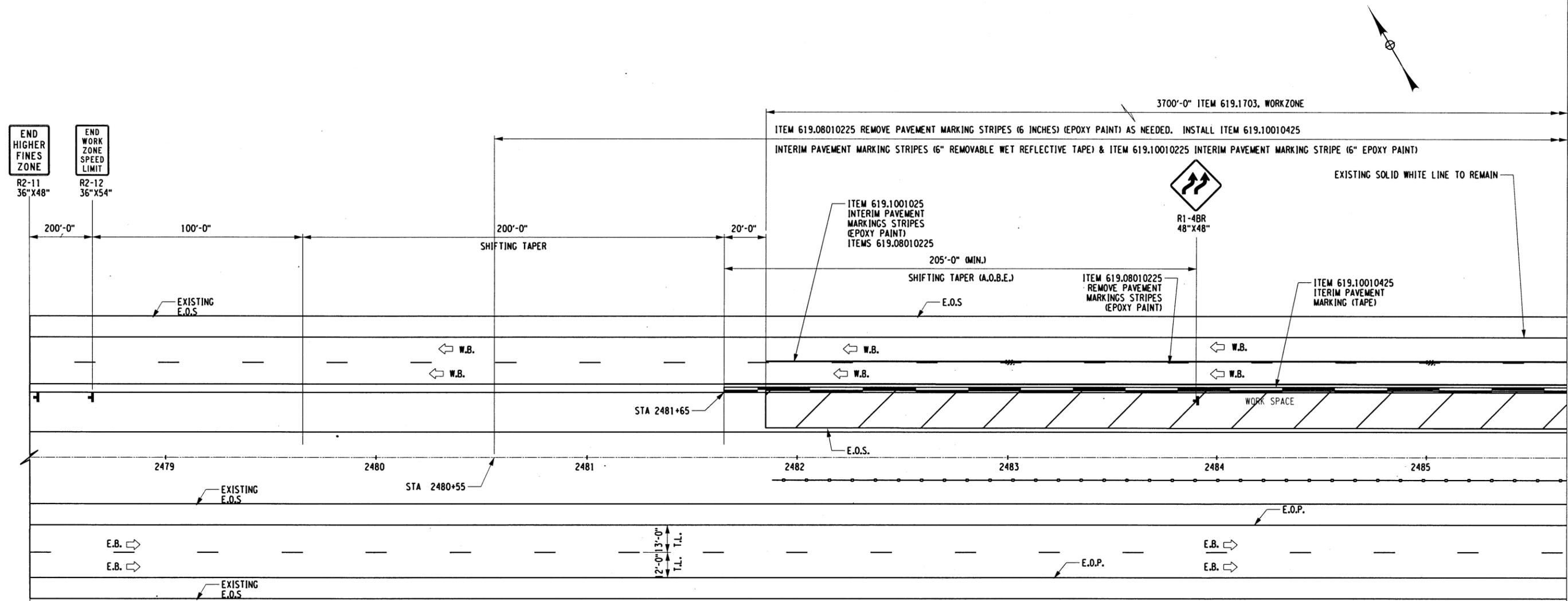
ALTERED ON:	AFFIXED ON: 9/15/16
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

 <b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 07/2016
	TITLE OF DRAWING GUIDERAIL PLAN	DRAWING NUMBER: GR 6-6

FILE NO. TA  
 CHECKED BY: TA  
 DRAFTED BY: WACOX  
 CHECKED BY: TA  
 DESIGNED BY: JASHINSKI  
 DESIGN SUPERVISOR: RACOURNOYER



**STAGE 1 WORKZONE TRAFFIC CONTROL PLAN**  
**WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

- 1- SHOULDER RECONSTRUCTION LIMITS.
- 2- THE CONTRACTOR SHALL RECONSTRUCT THE SHOULDER PRIOR TO SHIFTING TRAFFIC ONTO THE SHOULDER.
- 3- SEE SRD-1 FOR RECONSTRUCTION DETAILS
- 4- DIRECTION OF TRAFFIC
- 5- TEMPORARY SIGNS) COST INCLUDED IN ITEM 619.01
- 6- CONFLICTING PAVEMENT MARKING TO BE REMOVED/COVERED
- 7- DRUMS COST INCLUDED ITEM 619.01 BASIC WORK ZONE TRAFFIC CONTROL
- 8- TEMPORARY CONCRETE BARRIER (AS NOTED ABOVE)

ALTERED ON:	AFFIXED ON: 8/30/16
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

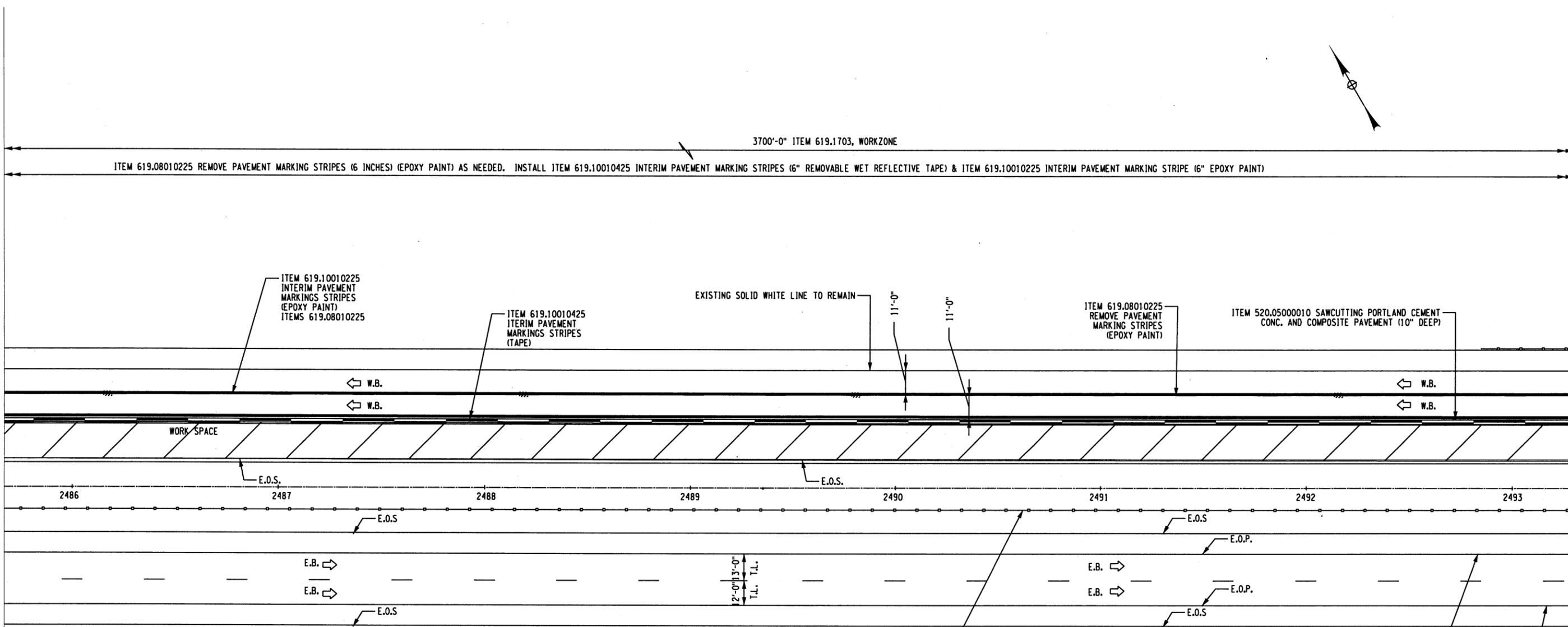
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REVISIONS			
DATE	DESCRIPTION	BY	SYM

<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
	TITLE OF DRAWING STAGE 1 WZTC	DRAWING NUMBER: WZTC 1-6

MATCH LINE A-A

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: J. SHINSKI  
 DESIGN SUPERVISOR: R. COURNOYER



**STAGE 1 WORKZONE TRAFFIC CONTROL PLAN**  
**WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

-  1- SHOULDER RECONSTRUCTION LIMITS.
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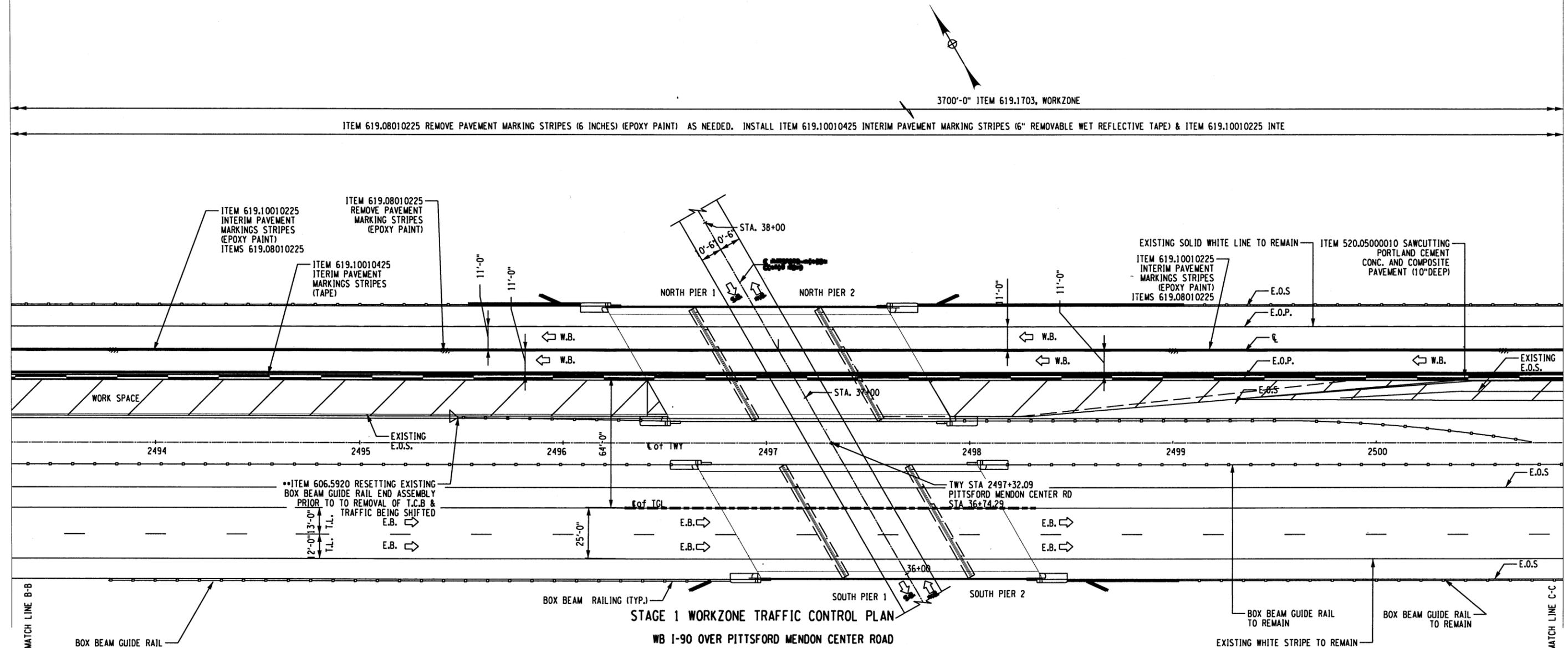


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	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
	TITLE OF DRAWING STAGE 1 WZTC	DRAWING NUMBER: WZTC 2-6

FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: J. HINSKI  
 DESIGN SUPERVISOR: R. COURNOYER



**STAGE 1 WORKZONE TRAFFIC CONTROL PLAN**

**WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

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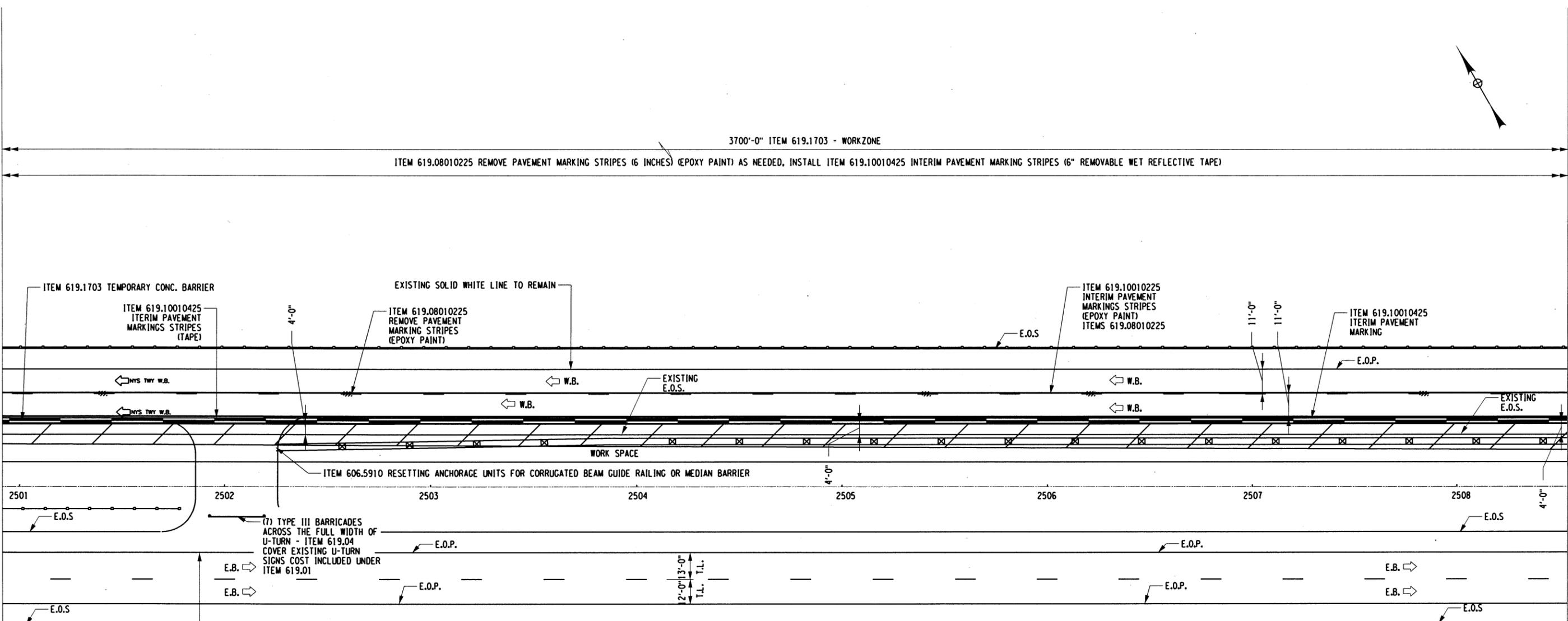
REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT: NYS THRUWAY BRIDGE REHABILITATION  
 LOCATION OF PROJECT: TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY  
 TITLE OF DRAWING: STAGE 1 WZTC

CONTRACT NUMBER: TAB 16-33B  
 DATE: 06/28/2016  
 DRAWING NUMBER: WZTC 3-6

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: J. SHINSKI  
 DESIGN SUPERVISOR: R. COURNOYER



**STAGE 1 WORKZONE TRAFFIC CONTROL PLAN**  
**WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

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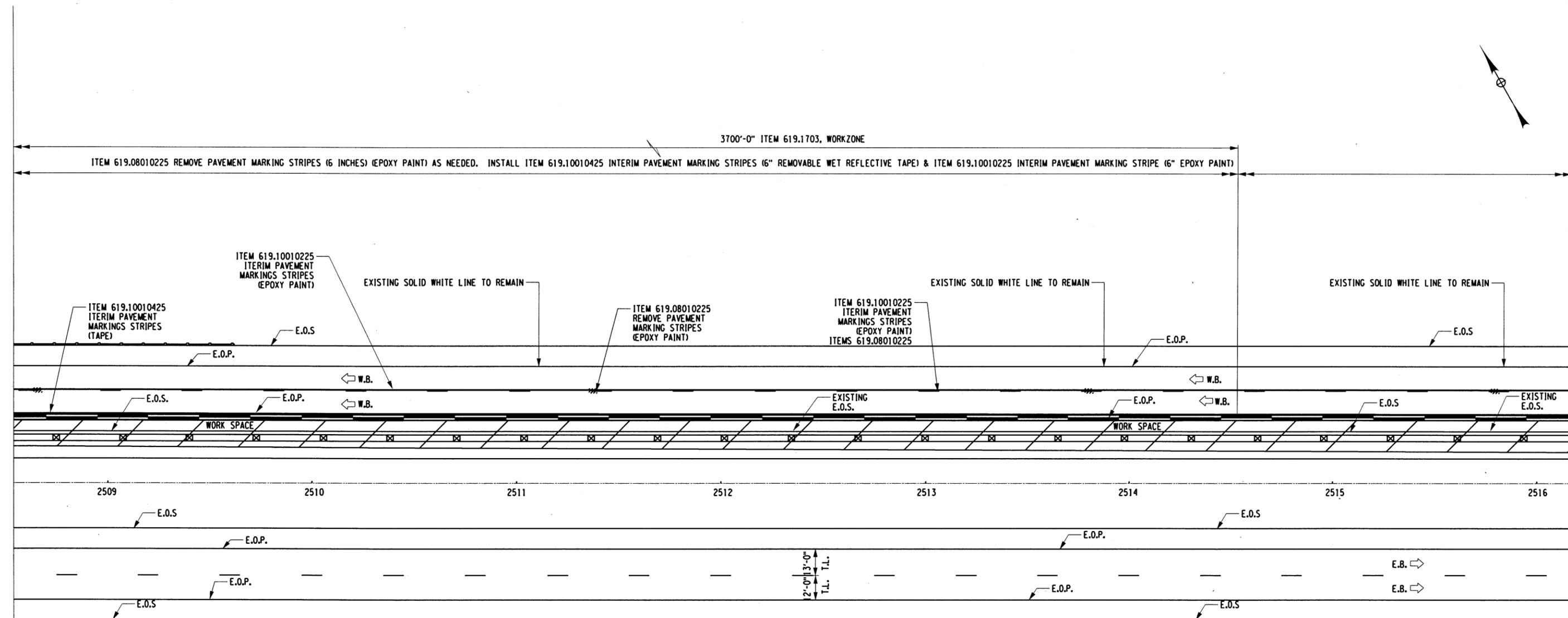
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	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
	TITLE OF DRAWING STAGE 1 WZTC	DRAWING NUMBER: WZTC 4-6

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: J. SHINSKI  
 DESIGN SUPERVISOR: R. COURNOYER



**STAGE 1 WORKZONE TRAFFIC CONTROL PLAN**  
**WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

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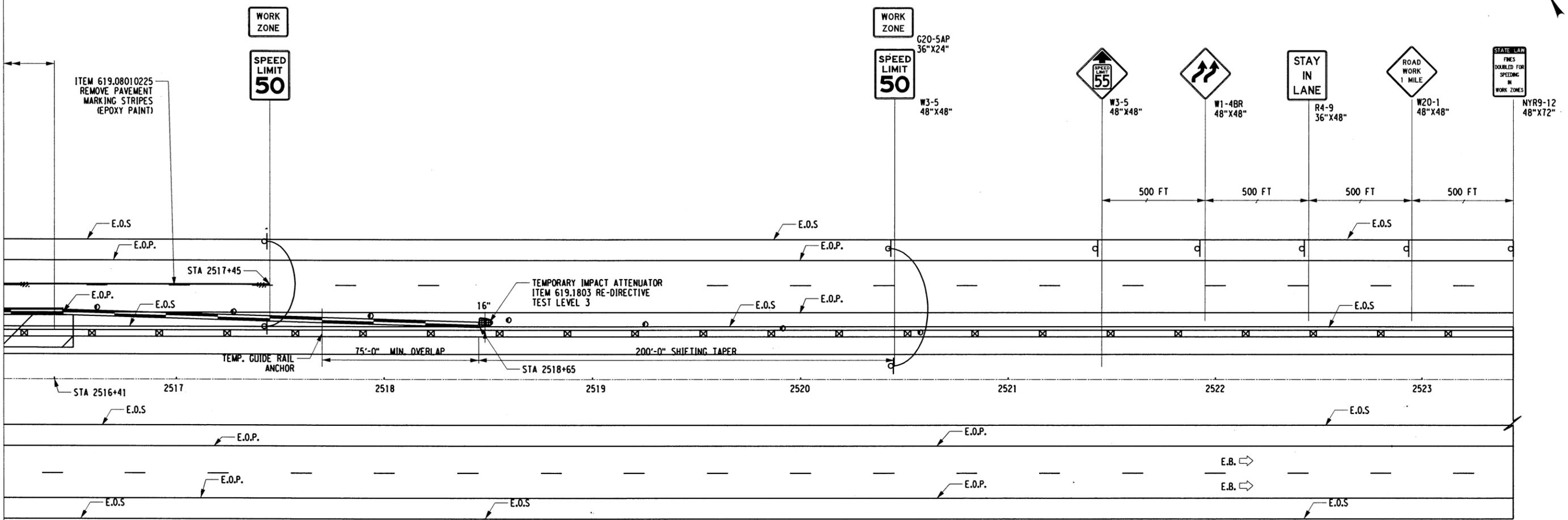
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LOCATION OF PROJECT TWY M.P., 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
TITLE OF DRAWING STAGE 1 WZTC	DRAWING NUMBER: WZTC 5-6



**STAGE 1 WORKZONE TRAFFIC CONTROL PLAN**  
**WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

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	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
	TITLE OF DRAWING STAGE 1 WZTC	DRAWING NUMBER: WZTC 6-6

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: J. SHINSKI  
 DESIGN SUPERVISOR: R. COURNOYER

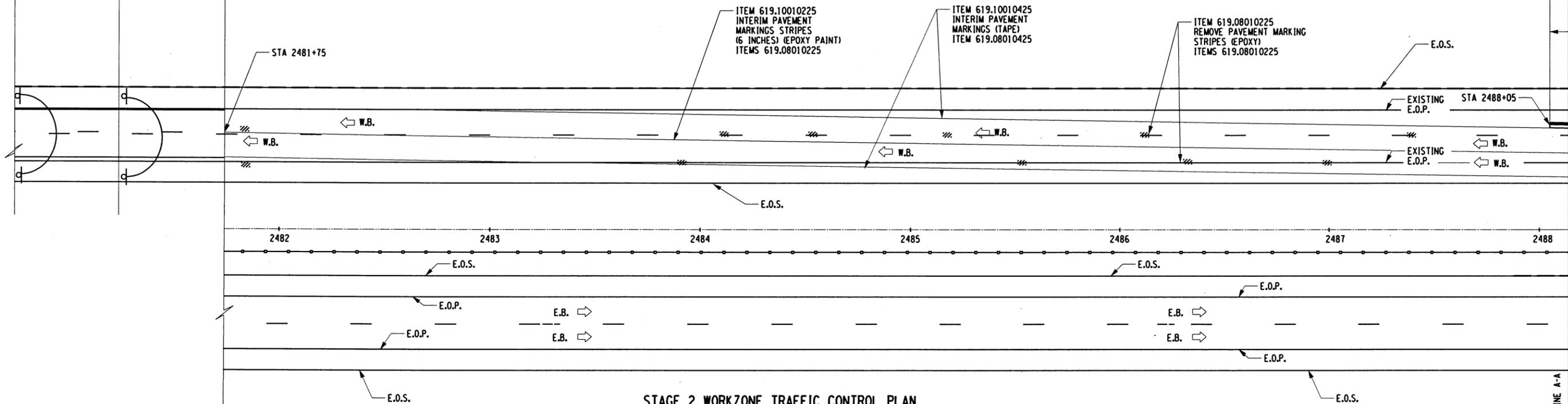
ITEM 619.08010225 REMOVE PAVEMENT MARKING STRIPES (6 INCHES) (EPOXY PAINT) & 619.08010425 (6" REMOVABLE WET REFLECTIVE TAPE) AS NEEDED, INSTALL ITEM 619.10010425 INTERIM PAVEMENT MARKING STRIPES (6" REMOVABLE WET REFLECTIVE TAPE)

END HIGHER FINES ZONE  
 END WORK ZONE SPEED LIMIT

R2-11 (36"x48")  
 R2-12 (36"x54")

200'-0"  
 100'-0"

1280'-0" SHIFTING TAPER



**STAGE 2 WORKZONE TRAFFIC CONTROL PLAN  
 WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

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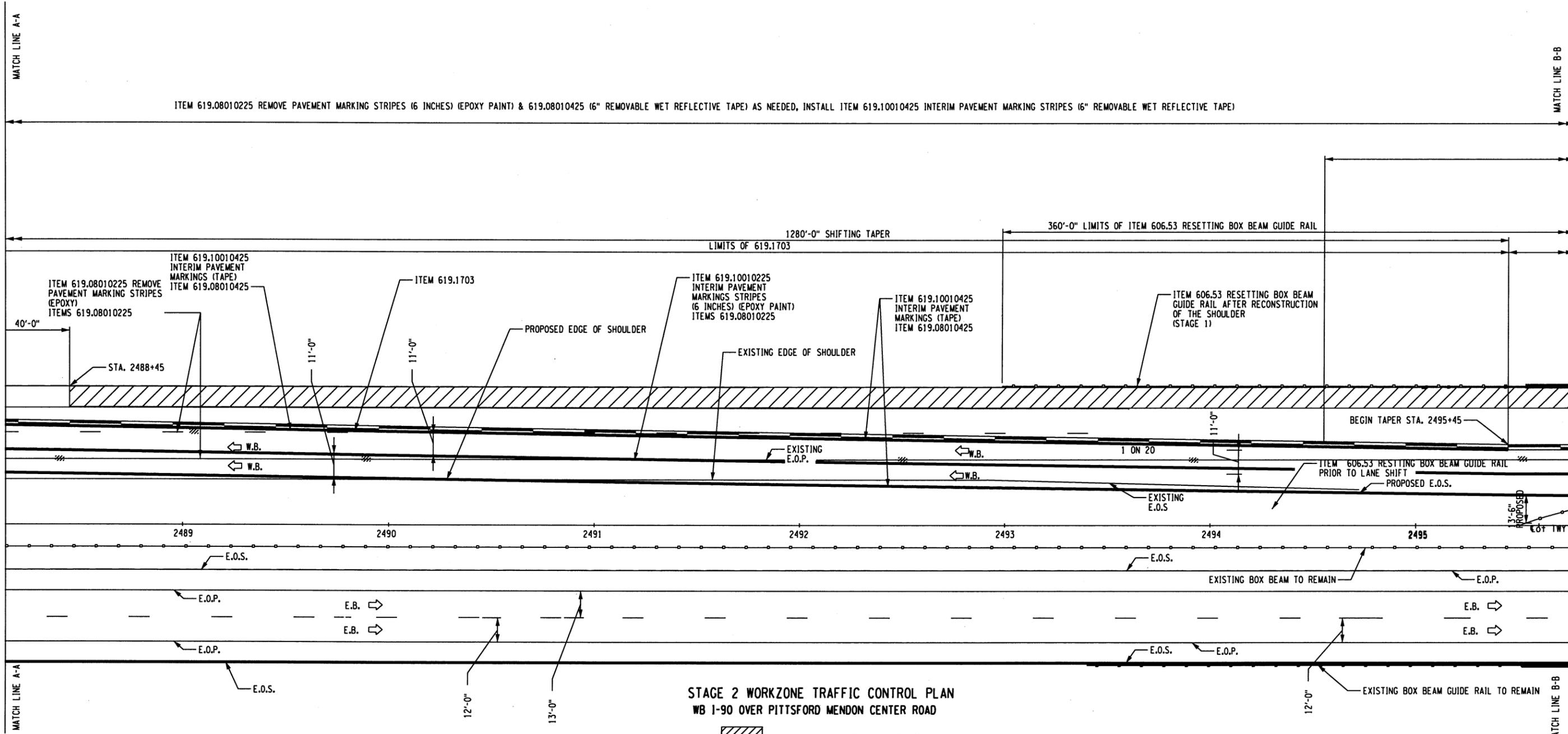
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**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
TITLE OF DRAWING STAGE 2 WZTC	DRAWING NUMBER: WZTC2 1-6

FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: J. SHINSKI  
 DESIGN SUPERVISOR: R. COURNOYER



**STAGE 2 WORKZONE TRAFFIC CONTROL PLAN  
WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

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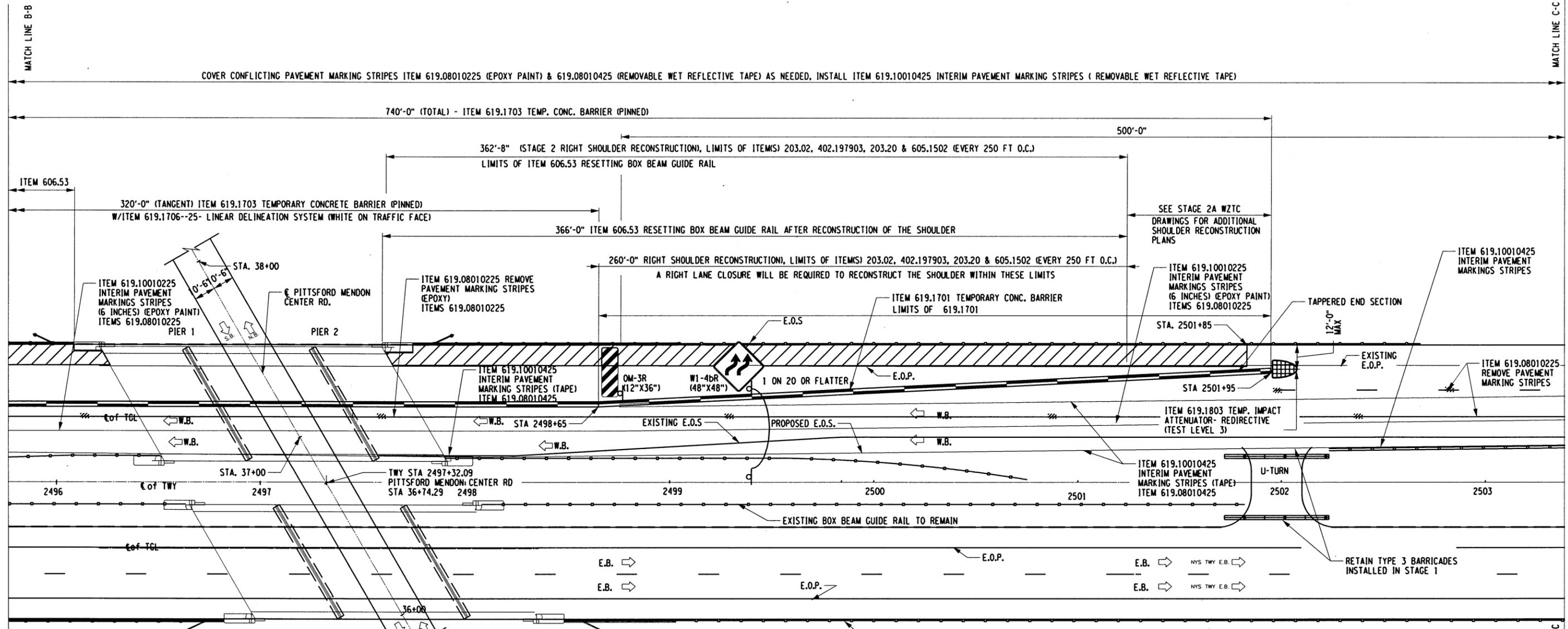
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	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
	TITLE OF DRAWING STAGE 2 WZTC	DRAWING NUMBER: WZTC2 2-6

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: J. SHINSKI  
 DESIGN SUPERVISOR: R. COURNOYER



**STAGE 2 WORKZONE TRAFFIC CONTROL PLAN  
WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

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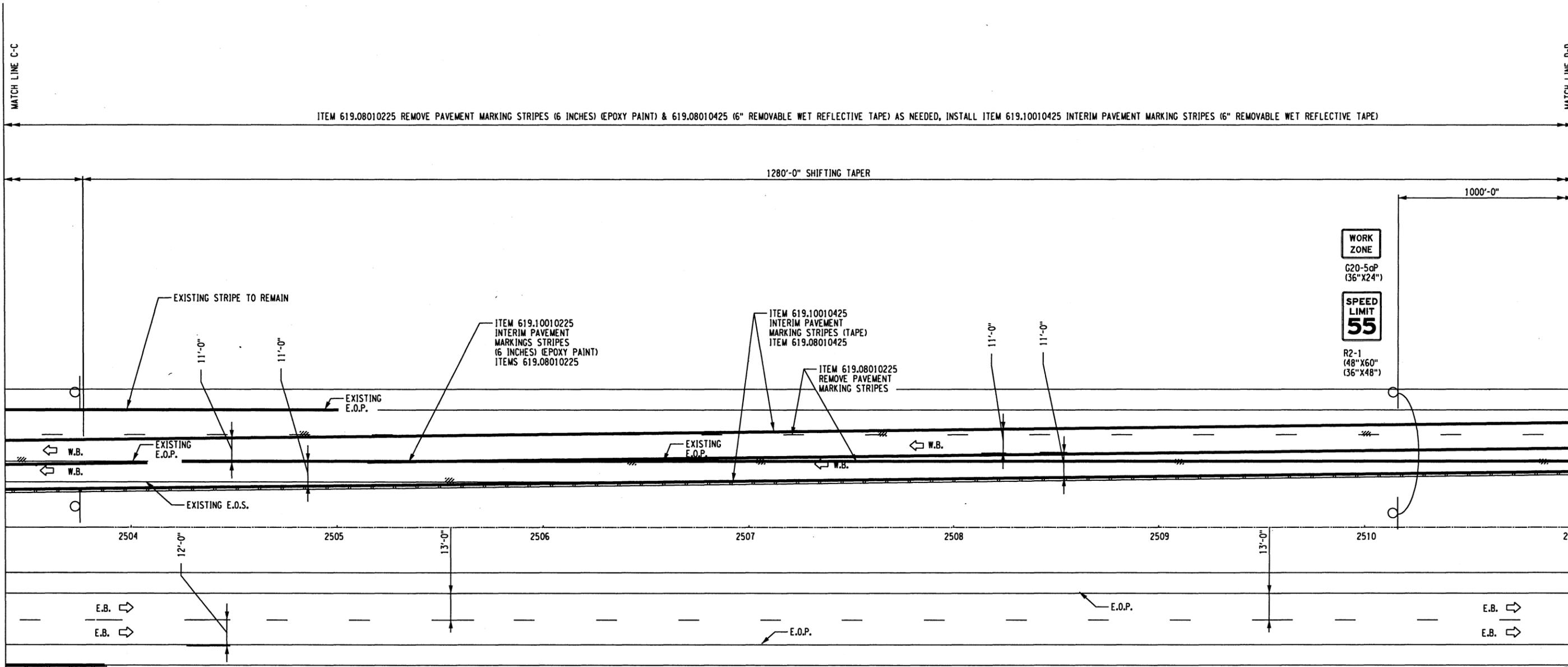
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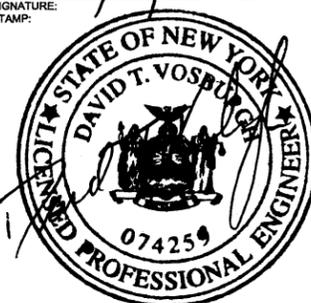
<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TYW M. P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
	TITLE OF DRAWING STAGE 2 WZTC	DRAWING NUMBER: WZTC2 3-6

FILE  
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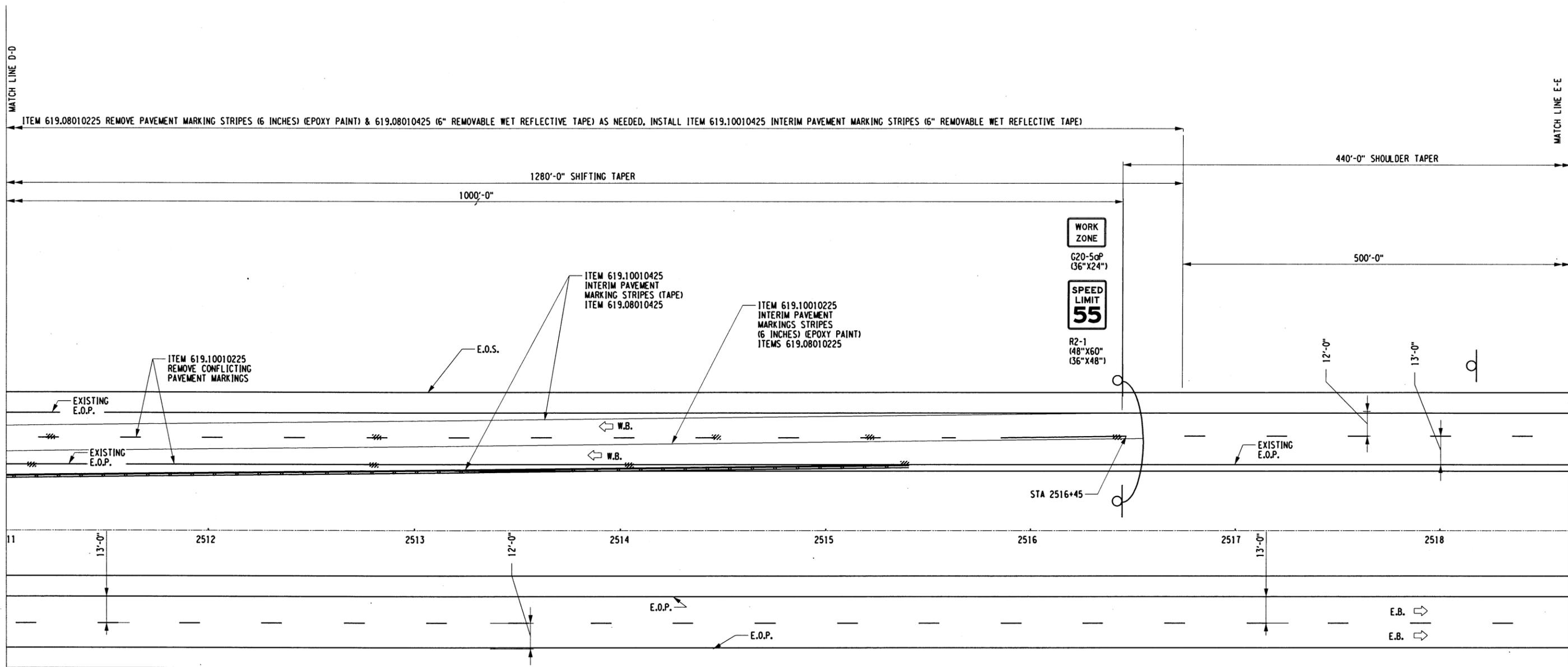


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	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
	TITLE OF DRAWING STAGE 2 WZTC	DRAWING NUMBER: WZTC2 4-6

FILED: TA  
 CHECKED BY: M. COX  
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 DESIGNED BY: J. SHINSKI  
 DESIGN SUPERVISOR: R. COURNOYER



**STAGE 2 WORKZONE TRAFFIC CONTROL PLAN  
WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

- 1- SHOULDER RECONSTRUCTION LIMITS.
- 2- THE CONTRACTOR SHALL RECONSTRUCT THE SHLDR PRIOR TO SHIFTING TRAFFIC ONTO THE SHLDR.
- 3- SEE SRD-1 FOR RECONSTRUCTION DETAILS
- 4- DIRECTION OF TRAFFIC
- 5- TEMPORARY SIGN(S) COST INCLUDED IN ITEM 619.01
- 6- CONFLICTING PAVEMENT MARKING TO BE REMOVED
- 7- DRUMS COST INCLUDED ITEM 619.01 BASIC WORK ZONE TRAFFIC CONTROL

ALTERED ON: \_\_\_\_\_

AFFIXED ON: 8/30/16

SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_

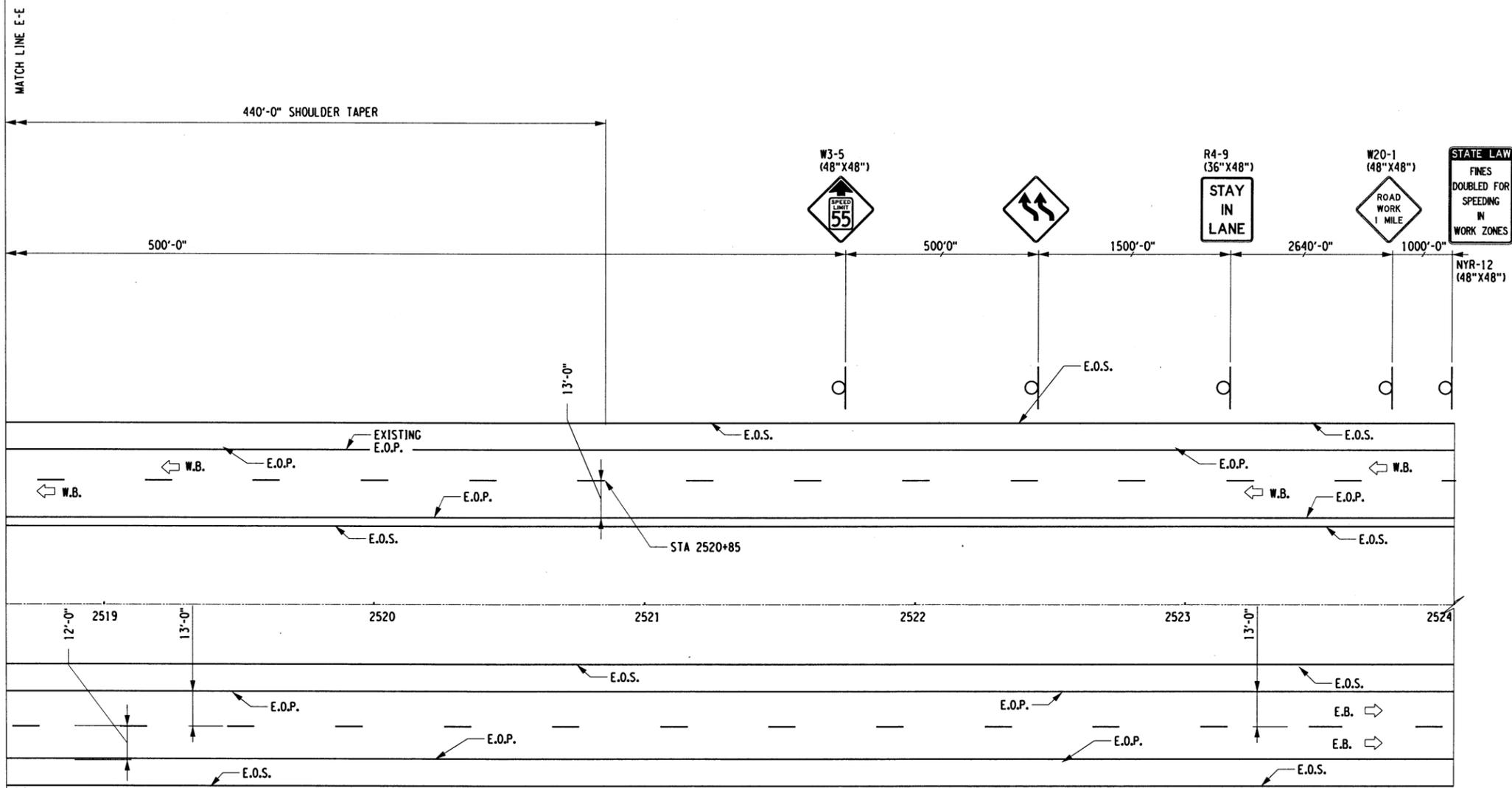
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
TITLE OF DRAWING STAGE 2 WZTC	DRAWING NUMBER: WZTC2 5-6

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: J. SHINSKI  
 DESIGN SUPERVISOR: R. COURNOYER



**STAGE 2 WORKZONE TRAFFIC CONTROL PLAN  
WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

-  1- SHOULDER RECONSTRUCTION LIMITS.
-  4- DIRECTION OF TRAFFIC
-  5- TEMPORARY SIGNS COST INCLUDED IN ITEM 619.01
-  6- CONFLICTING PAVEMENT MARKING TO BE REMOVED
-  7- DRUMS COST INCLUDED ITEM 619.01 BASIC WORK ZONE TRAFFIC CONTROL

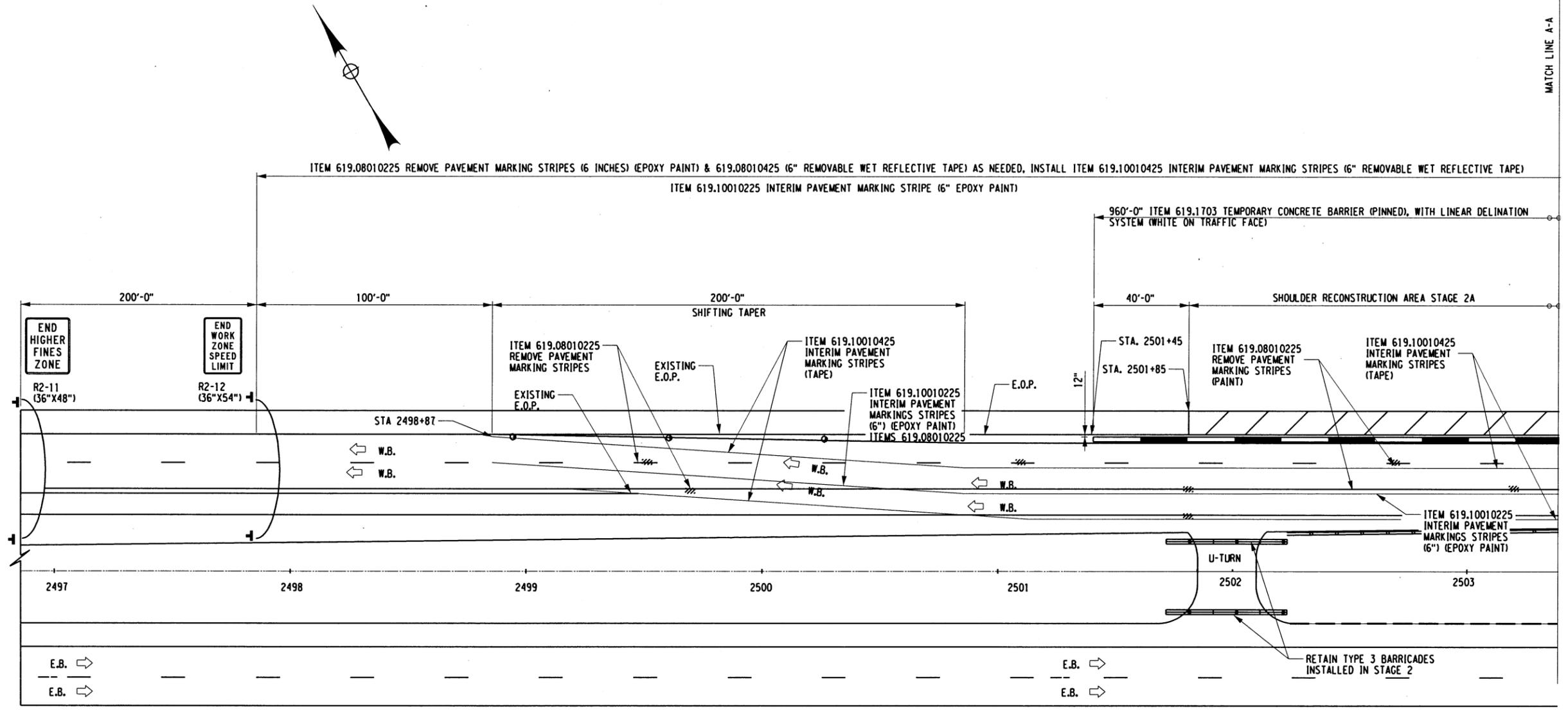
ALTERED ON:	AFFIXED ON: 8/20/16
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM

 <b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
	TITLE OF DRAWING STAGE 2 WZTC	DRAWING NUMBER: WZTC2 6-6

FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSEBURGH



**STAGE 2A WORKZONE TRAFFIC CONTROL PLAN**  
**WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

- 1- SHOULDER RECONSTRUCTION LIMITS.
- 2- THE CONTRACTOR SHALL RECONSTRUCT THE SHLDR PRIOR TO SHIFTING TRAFFIC ONTO THE SHLDR.
- 3- SEE SRD-1 FOR RECONSTRUCTION DETAILS
- 4- DIRECTION OF TRAFFIC
- 5- TEMPORARY SIGN(S) COST INCLUDED IN ITEM 619.01
- 6- CONFLICTING PAVEMENT MARKING TO BE REMOVED
- 7- DRUMS COST INCLUDED ITEM 619.01 BASIC WORK ZONE TRAFFIC CONTROL

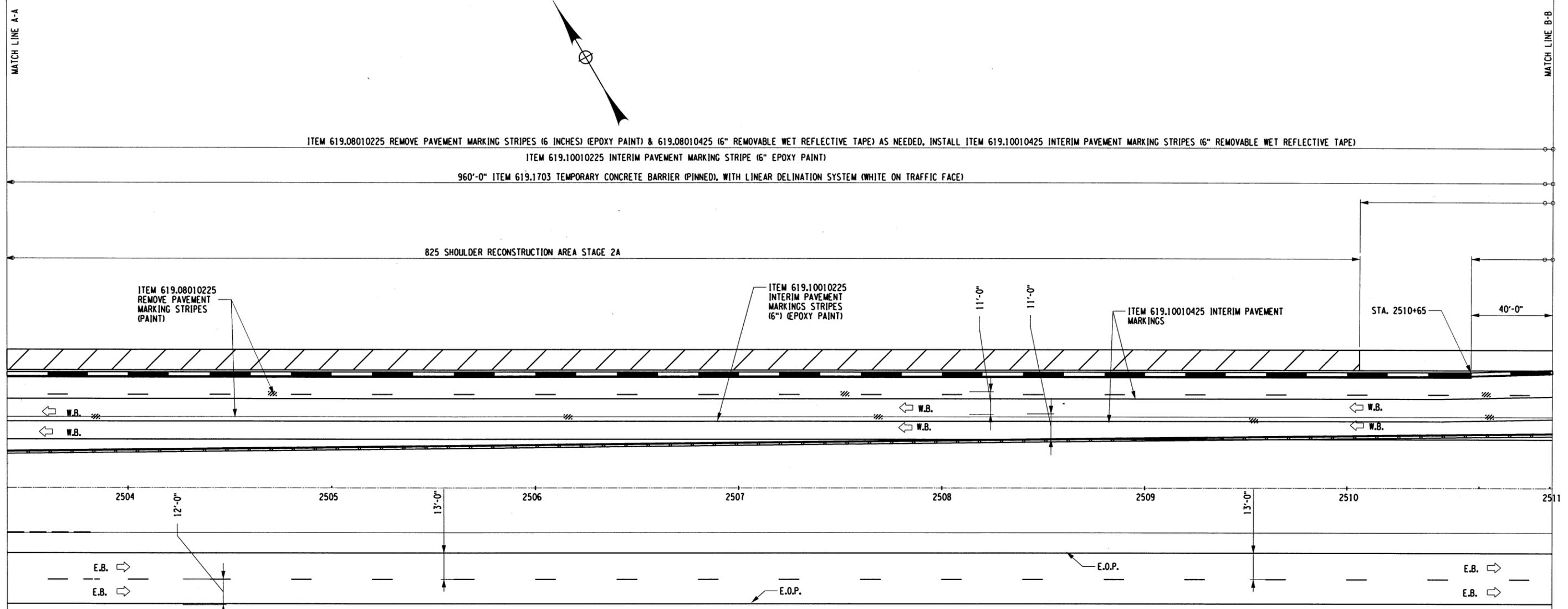
ALTERED ON:	AFFIXED ON: 8/30/16
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY MP 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 08/2016
	TITLE OF DRAWING STAGE 2A WZTC	DRAWING NUMBER: WZTC 2A 1-4

FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSSBURGH



**STAGE 2A WORKZONE TRAFFIC CONTROL PLAN**  
**WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

- 1- SHOULDER RECONSTRUCTION LIMITS.
- 2- THE CONTRACTOR SHALL RECONSTRUCT THE SHLDR PRIOR TO SHIFTING TRAFFIC ONTO THE SHLDR.
- 3- SEE SRD-1 FOR RECONSTRUCTION DETAILS
- 4- DIRECTION OF TRAFFIC
- 5- TEMPORARY SIGN(S) COST INCLUDED IN ITEM 619.01
- 6- CONFLICTING PAVEMENT MARKING TO BE REMOVED
- 7- DRUMS COST INCLUDED ITEM 619.01 BASIC WORK ZONE TRAFFIC CONTROL

ALTERED ON: SIGNATURE: STAMP:	AFFIXED ON: 8/20/16 SIGNATURE: STAMP:
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REVISIONS			
DATE	DESCRIPTION	BY	SYM

<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 07/2016
	TITLE OF DRAWING STAGE 2A WZTC	DRAWING NUMBER: WZTC 2A 2-4

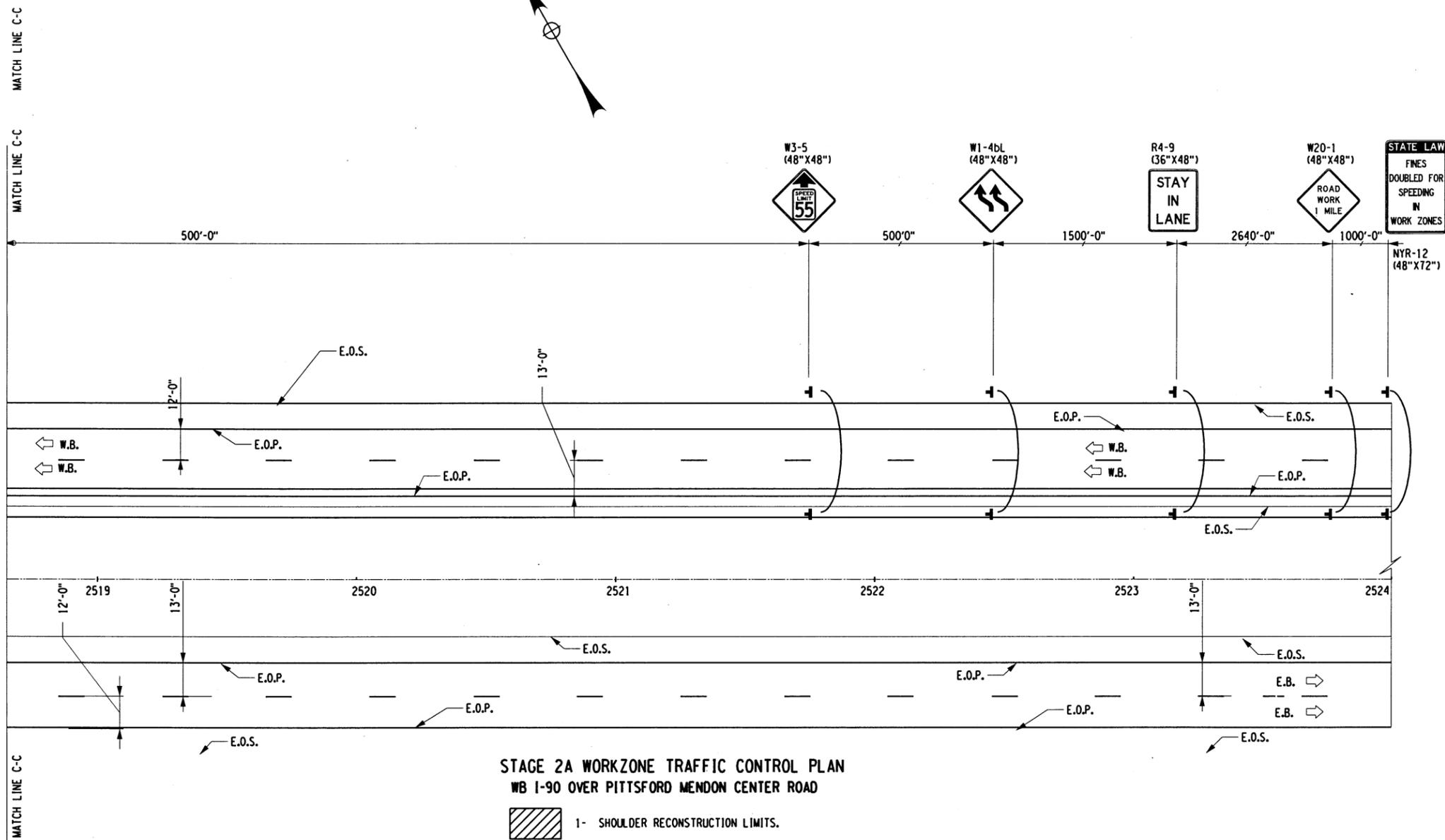


CHECKED BY: TA

DRAFTED BY: M. COX

CHECKED BY: TA

DESIGNED BY: M. COX



**STAGE 2A WORKZONE TRAFFIC CONTROL PLAN**  
**WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

-  1- SHOULDER RECONSTRUCTION LIMITS.
-  2- THE CONTRACTOR SHALL RECONSTRUCT THE SHLDR PRIOR TO SHIFTING TRAFFIC ONTO THE SHLDR.
- 3- SEE SRD-1 FOR RECONSTRUCTION DETAILS
-  4- DIRECTION OF TRAFFIC
-  5- TEMPORARY SIGN(S) COST INCLUDED IN ITEM 619.01
-  6- CONFLICTING PAVEMENT MARKING TO BE REMOVED
-  7- DRUMS COST INCLUDED ITEM 619.01 BASIC WORK ZONE TRAFFIC CONTROL

ALTERED ON:	AFFIXED ON: 8/30/16
SIGNATURE:	SIGNATURE:
STAMP:	STAMP:



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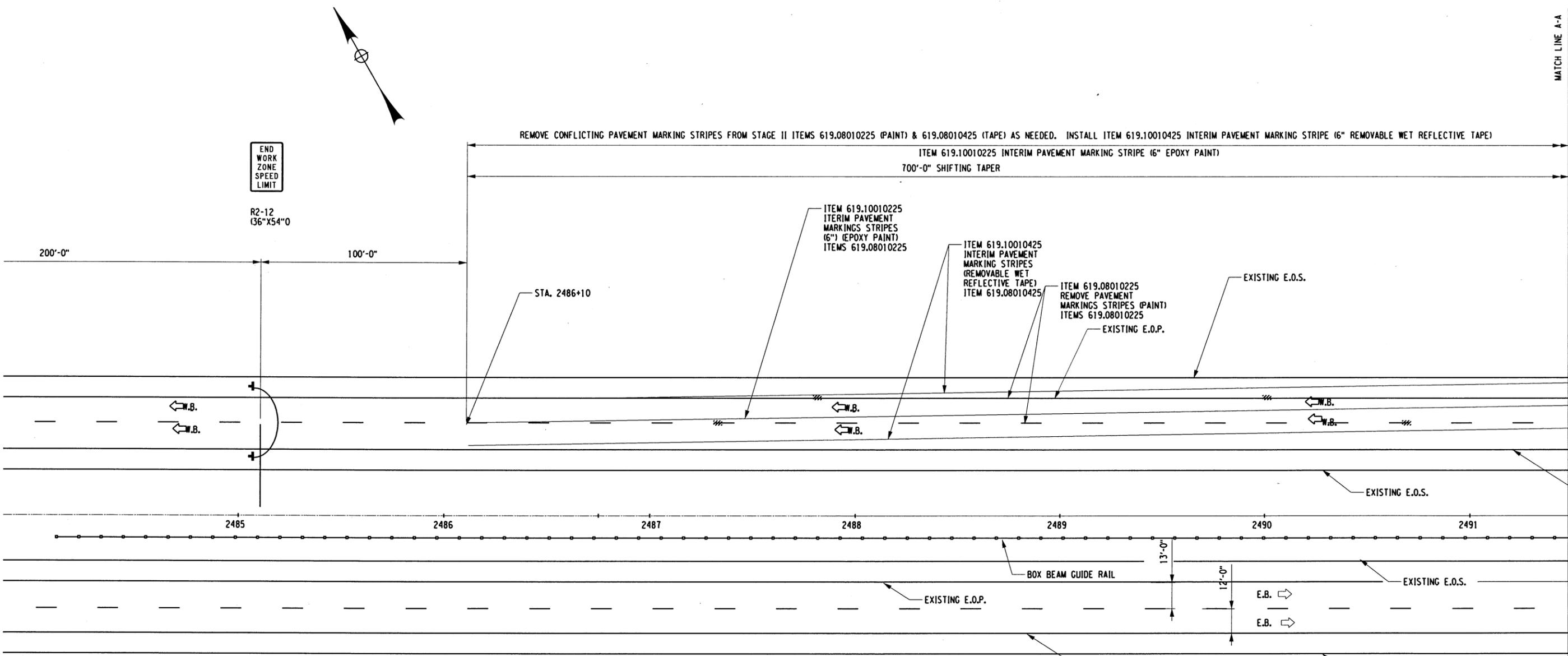
REVISIONS			
DATE	DESCRIPTION	BY	SYM.



**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT: NYS THRUWAY BRIDGE REHABILITATION  
 LOCATION OF PROJECT: TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY  
 TITLE OF DRAWING: STAGE 2A WZTC

CONTRACT NUMBER: TAB 16-33B  
 DATE: 07/2016  
 DRAWING NUMBER: WZTC 2A 4-4



**STAGE 3 WORKZONE TRAFFIC CONTROL PLAN**

**WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

-  1- SHOULDER RECONSTRUCTION LIMITS.
-  2- THE CONTRACTOR SHALL RECONSTRUCT THE SHLDR PRIOR TO SHIFTING TRAFFIC ONTO THE SHLDR.
-  3- SEE SRD-1 FOR RECONSTRUCTION DETAILS
-  4- DIRECTION OF TRAFFIC
-  5- TEMPORARY SIGN(S) COST INCLUDED IN ITEM 619.01
-  6- CONFLICTING PAVEMENT MARKING TO BE REMOVED
-  7- DRUMS COST INCLUDED ITEM 619.01 BASIC WORK ZONE TRAFFIC CONTROL

ALTERED ON: \_\_\_\_\_

AFFIXED ON: **8/30/16**

SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_



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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT: NYS THRUWAY BRIDGE REHABILITATION

LOCATION OF PROJECT: TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY

TITLE OF DRAWING: STAGE 3 WZTC

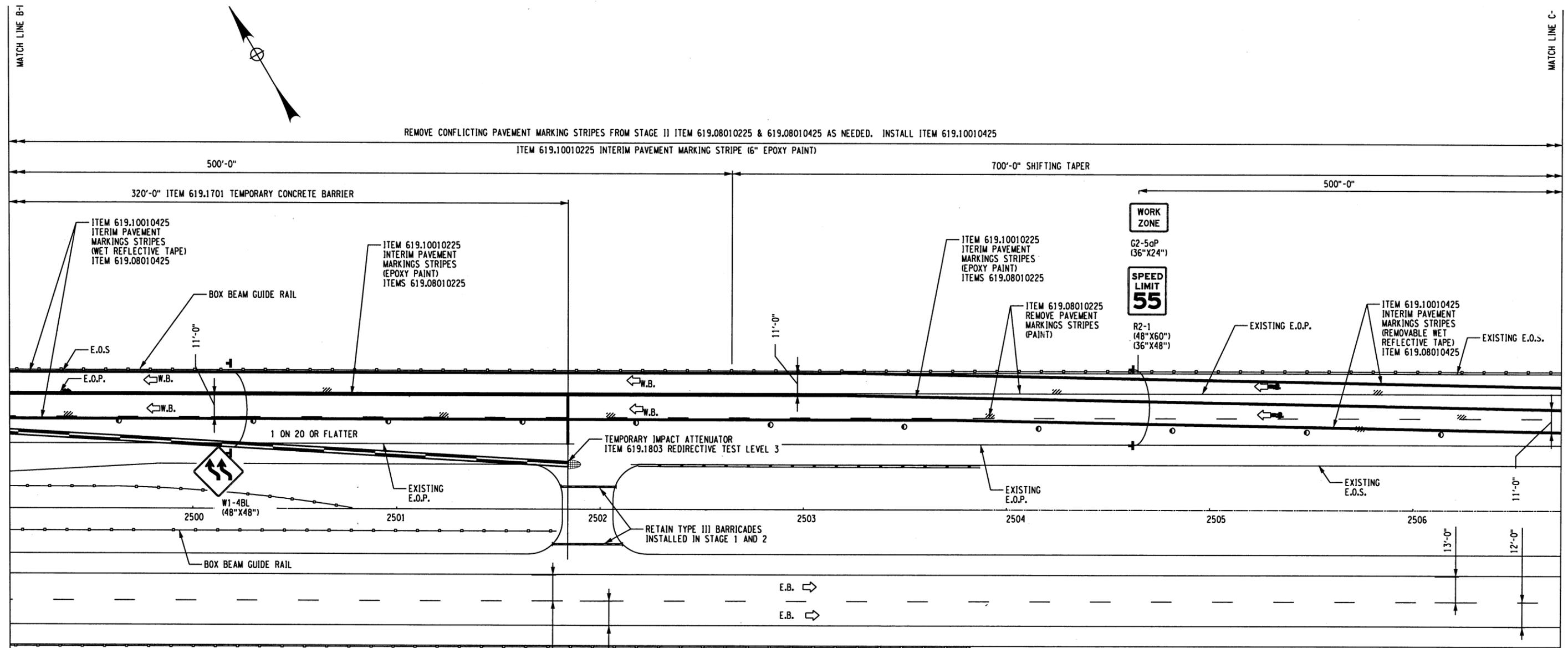
CONTRACT NUMBER: TAB 16-33B

DATE: 06/28/2016

DRAWING NUMBER: WZTC3 1-4



FILE  
 CHECKED BY: TA  
 DRAFTED BY: MACOX  
 CHECKED BY: TA  
 DESIGNED BY: J. SHINSKI  
 DESIGN SUPERVISOR: R. COURNOYER



**STAGE 3 WORKZONE TRAFFIC CONTROL PLAN**

**WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

- 1- SHOULDER RECONSTRUCTION LIMITS.
- 2- THE CONTRACTOR SHALL RECONSTRUCT THE SHLDR PRIOR TO SHIFTING TRAFFIC ONTO THE SHLDR.
- 3- SEE SRD-1 FOR RECONSTRUCTION DETAILS
- 4- DIRECTION OF TRAFFIC
- 5- TEMPORARY SIGN(S) COST INCLUDED IN ITEM 619.01
- 6- CONFLICTING PAVEMENT MARKING TO BE REMOVED
- 7- DRUMS COST INCLUDED ITEM 619.01 BASIC WORK ZONE TRAFFIC CONTROL

ALTERED ON:  SIGNATURE: STAMP:	AFFIXED ON: <b>9/15/16</b>  
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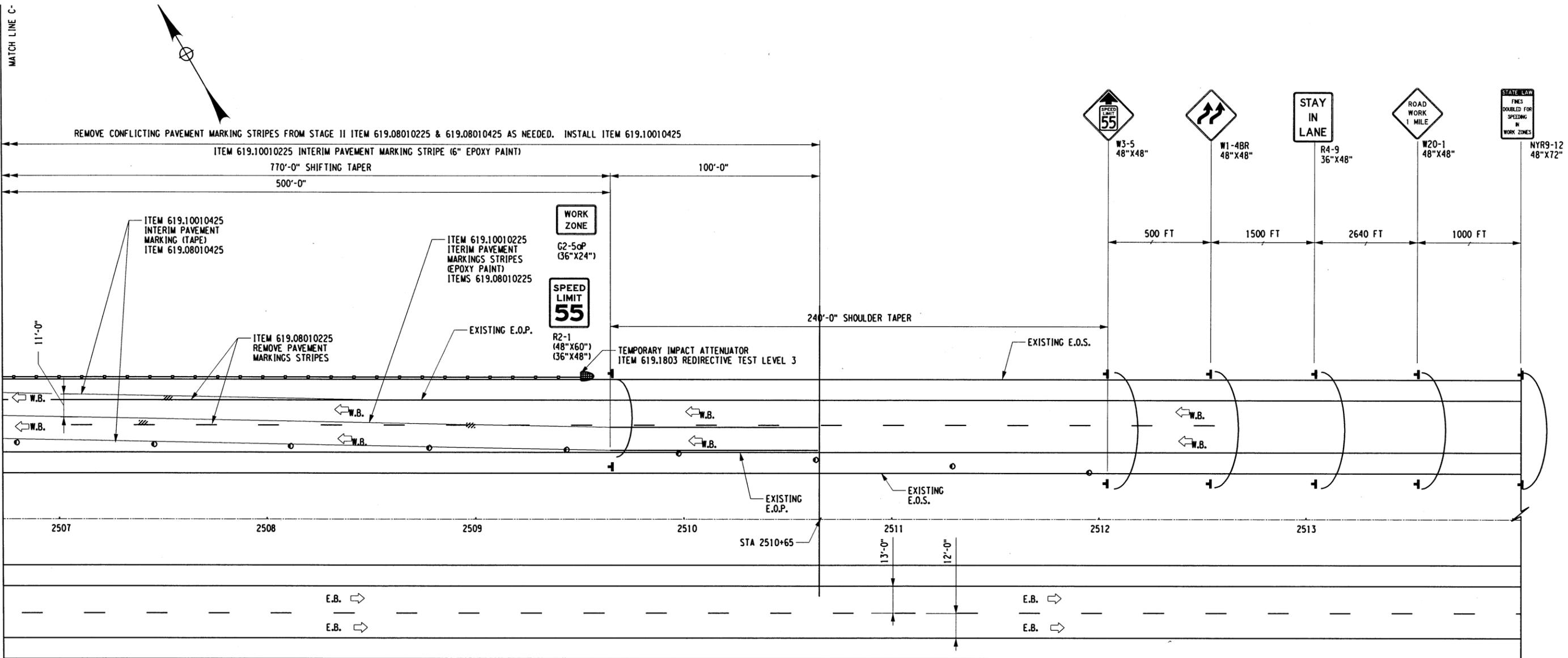
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DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT <b>NYS THRUWAY BRIDGE REHABILITATION</b>	CONTRACT NUMBER: <b>TAB 16-33B</b>
LOCATION OF PROJECT <b>TWY M.P. 356.69 7 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY</b>	DATE: <b>06/28/2016</b>
TITLE OF DRAWING <b>STAGE 3 WZTC</b>	DRAWING NUMBER: <b>WZTC 3-4</b>

FILED  
 CHECKED BY: TA  
 DRAFTED BY: WACOX  
 CHECKED BY: TA  
 DESIGNED BY: JASHINSKI  
 DESIGN SUPERVISOR: R. COURNOYER



**STAGE 3 WORKZONE TRAFFIC CONTROL PLAN**

**WB I-90 OVER PITTSFORD MENDON CENTER ROAD**

- 1- SHOULDER RECONSTRUCTION LIMITS.
- 2- THE CONTRACTOR SHALL RECONSTRUCT THE SHLDR PRIOR TO SHIFTING TRAFFIC ONTO THE SHLDR.
- 3- SEE SRD-1 FOR RECONSTRUCTION DETAILS
- 4- DIRECTION OF TRAFFIC
- 5- TEMPORARY SIGNS COST INCLUDED IN ITEM 619.01
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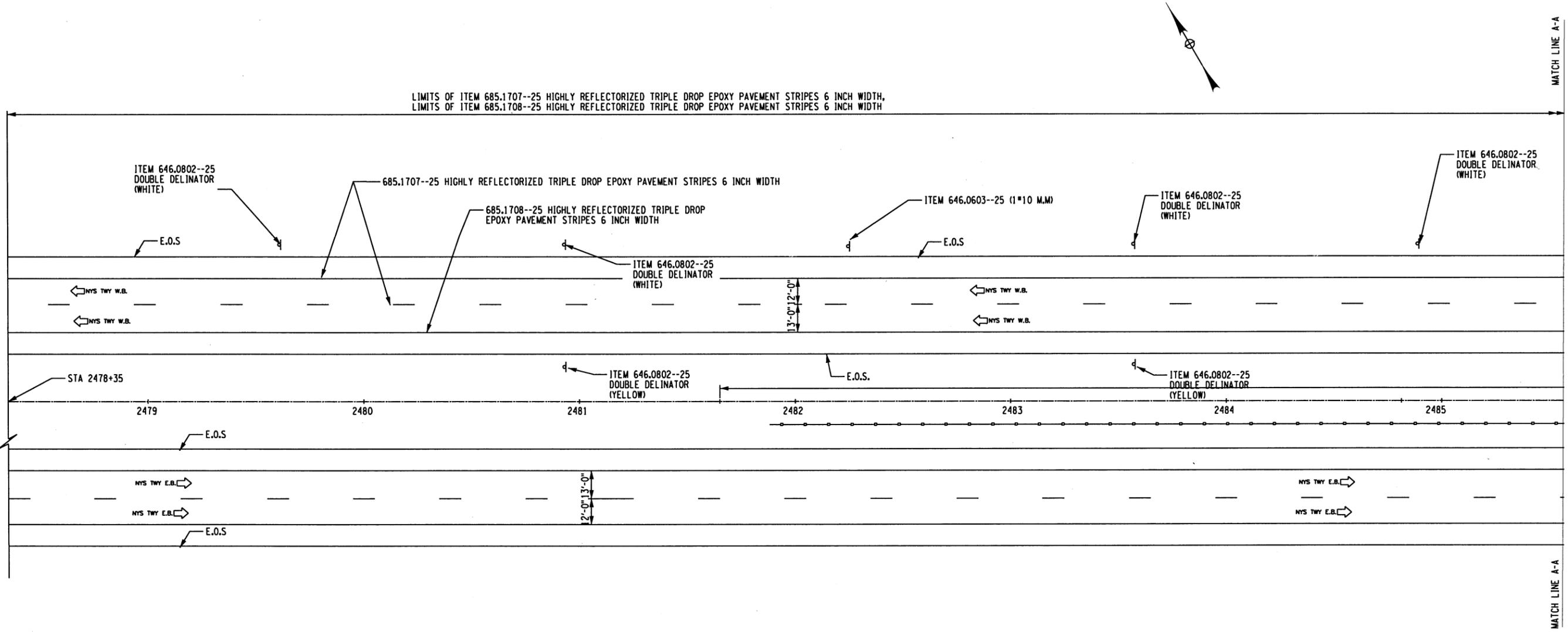
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SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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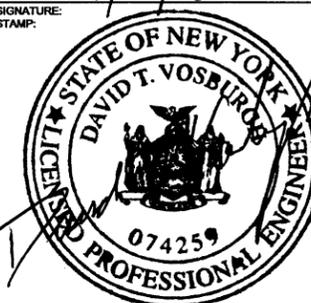
<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
	TITLE OF DRAWING STAGE 3 WZTC	DRAWING NUMBER: WZTC3 4-4

FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSSBURGH



LIMITS OF ITEM 685.1707--25 HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH,  
 LIMITS OF ITEM 685.1708--25 HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH

- 1- ITEM 646.0802--25 INSTALL SNOWPLOW MARKERS, SINGLE UNIT  
 ITEM 646.50--25 REMOVE AND DISPOSAL OF DELINEATORS AND MILE MARKERS
- 2- W.B. MP. 356.70 STRUCTURE (ONLY) LIMITS OF ITEMS) 610.1402, 610.1601, 610.19, 610.21  
 STA. 2484+81 TO 2496+38 LFT. & RT. SHOULDER  
 STA. 2498+00 TO 2510+00 LFT. & RT. SHOULDER

ALTERED ON:  SIGNATURE: STAMP:	AFFIXED ON: <div style="text-align: center; font-size: 1.2em;">9/15/16</div> 
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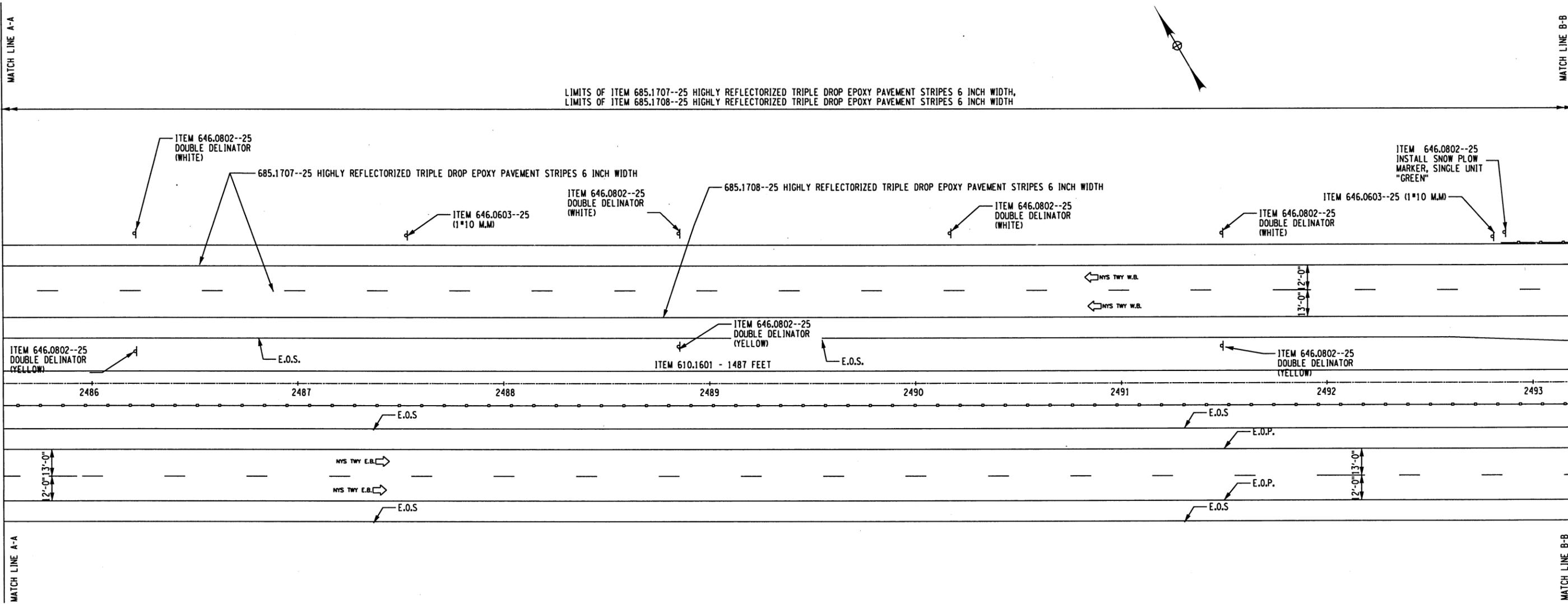
REVISIONS			
DATE	DESCRIPTION	BY	SYM.



**Thruway Authority | Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
TITLE OF DRAWING FINAL STRIPING PLAN	DRAWING NUMBER: FS 1-6

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: MACOX  
 DESIGN SUPERVISOR: D. VOSSBURGH



- 1- ITEM 646.0802--25 INSTALL SNOWPLOW MARKERS, SINGLE UNIT  
 ITEM 646.50--25 REMOVE AND DISPOSAL OF DELINEATORS AND MILE MARKERS
- 2- W.B. MP. 356.70 STRUCTURE (ONLY) LIMITS OF ITEMS) 610.1402, 610.1601, 610.19, 610.21  
 STA. 2484+81 TO 2496+38 LFT. & RT. SHOULDER  
 STA. 2498+00 TO 2510+00 LFT. & RT. SHOULDER

ALTERED ON:	AFFIXED ON: 9/15/16
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

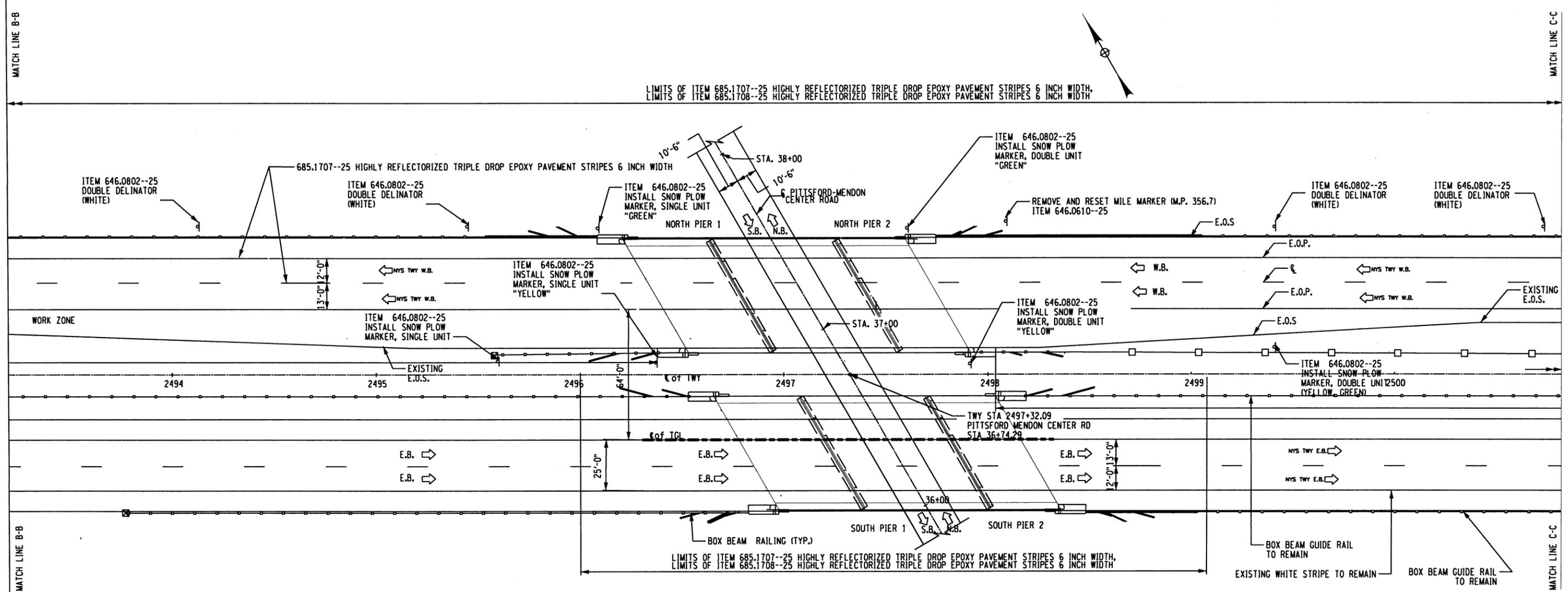
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 7 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
TITLE OF DRAWING FINAL STRIPING PLAN	DRAWING NUMBER: FS 2-6

CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



LIMITS OF ITEM 685.1707--25 HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH,  
 LIMITS OF ITEM 685.1708--25 HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH

LIMITS OF ITEM 685.1707--25 HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH,  
 LIMITS OF ITEM 685.1708--25 HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH

- 1- ITEM 646.0802--25 INSTALL SNOWPLOW MARKERS, SINGLE UNIT  
 ITEM 646.50--25 REMOVE AND DISPOSAL OF DELINEATORS AND MILE MARKERS
- 2- ITEM 645.5102 GROUND-MOUNTED SIGN PANEL LESS THAN 30 SF WITH Z BARS  
 ITEM 646.50--25 REMOVE AND DISPOSAL OF DELINEATORS AND MILE MARKERS
- 3- W.B. M.P. 356.70 STRUCTURE (ONLY) LIMITS OF ITEM(S) 610.1402, 610.1601, 610.19, 610.21  
 STA. 2484+81 TO 2496+38 LFT. & RT. SHOULDER  
 STA. 2498+00 TO 2510+00 LFT. & RT. SHOULDER

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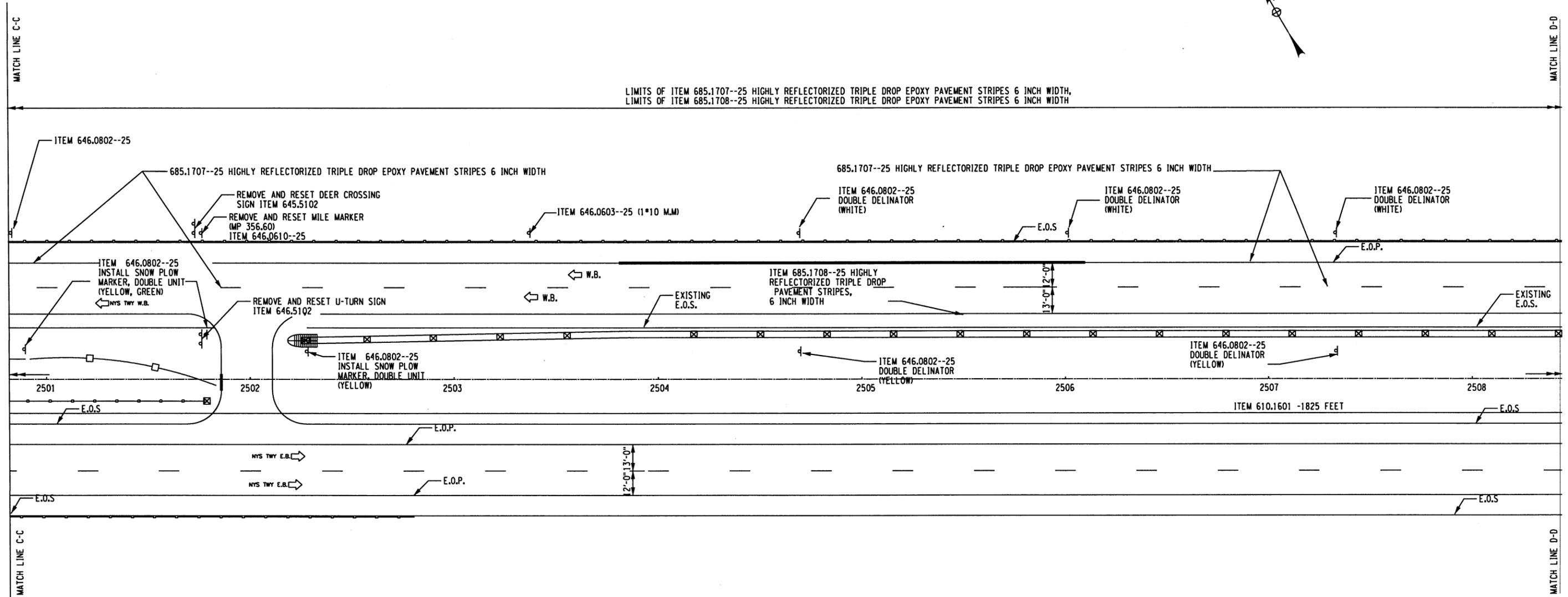
REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**Thruway Authority**  
**Canal Corporation**

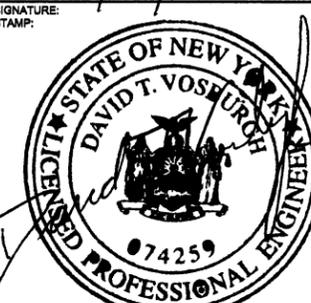
TITLE OF PROJECT: NYS THRUWAY BRIDGE REHABILITATION  
 LOCATION OF PROJECT: TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY  
 TITLE OF DRAWING: FINAL STRIPING PLAN

CONTRACT NUMBER: TAB 16-33B  
 DATE: 06/28/2016  
 DRAWING NUMBER: FS 3-6

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSEBURGH



- 1- ITEM 646.0802--25 INSTALL SNOWPLOW MARKERS, SINGLE UNIT  
ITEM 646.50--25 REMOVE AND DISPOSAL OF DELINEATORS AND MILE MARKERS
- 2- ITEM 645.5102 GROUND-MOUNTED SIGN PANEL LESS THAN 30 SF WITH 2 BARS  
ITEM 646.50--25 REMOVE AND DISPOSAL OF DELINEATORS AND MILE MARKERS
- 3- W.B. MP. 356.70 STRUCTURE (ONLY) LIMITS OF ITEMS) 610.1402, 610.1601, 610.19, 610.21  
STA. 2484+81 TO 2496+38 LFT. & RT. SHOULDER  
STA. 2498+00 TO 2510+00 LFT. & RT. SHOULDER

ALTERED ON:  SIGNATURE: STAMP:	AFFIXED ON: 9/15/16  
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IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

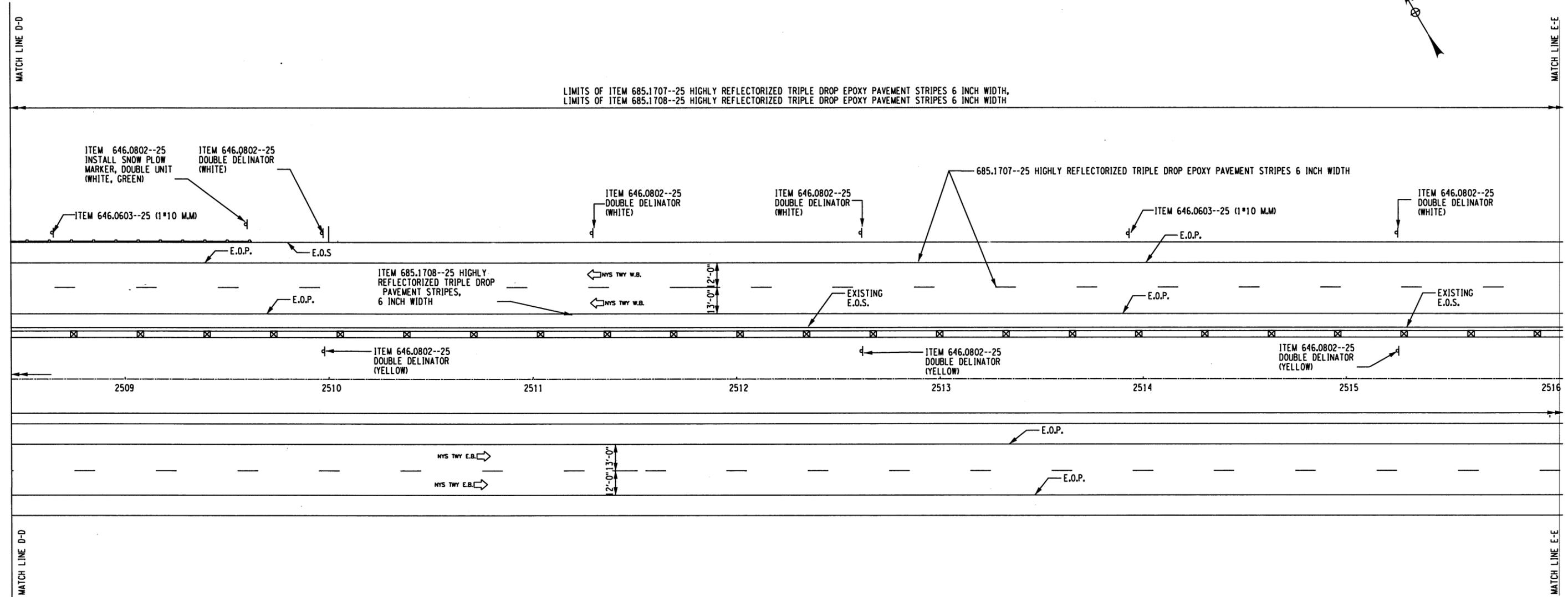
REVISIONS			
DATE	DESCRIPTION	BY	SYM.



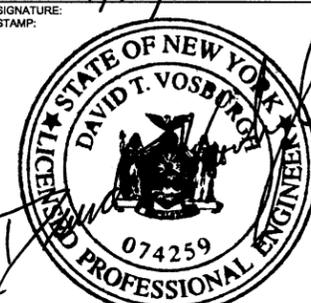
**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
TITLE OF DRAWING FINAL STRIPING PLAN	DRAWING NUMBER: FS 4-6

FILE  
 CHECKED BY: TA  
 DRAFTED BY: WACOX  
 CHECKED BY: TA  
 DESIGNED BY: WACOX  
 DESIGN SUPERVISOR: DAVOSSBURGH



- 1- ITEM 646.0802--25 INSTALL SNOWPLOW MARKERS, SINGLE UNIT  
 ITEM 646.50--25 REMOVE AND DISPOSAL OF DELINEATORS AND MILE MARKERS
- 2- W.B. MP. 356.70 STRUCTURE (ONLY) LIMITS OF ITEM(S) 610.1402, 610.1601, 610.19, 610.21  
 STA. 2484+81 TO 2496+38 LFT. & RT. SHOULDER  
 STA. 2498+00 TO 2510+00 LFT. & RT. SHOULDER

ALTERED ON:  SIGNATURE: STAMP:	AFFIXED ON: <div style="text-align: center; font-size: 1.2em; font-weight: bold;">9/15/16</div> 
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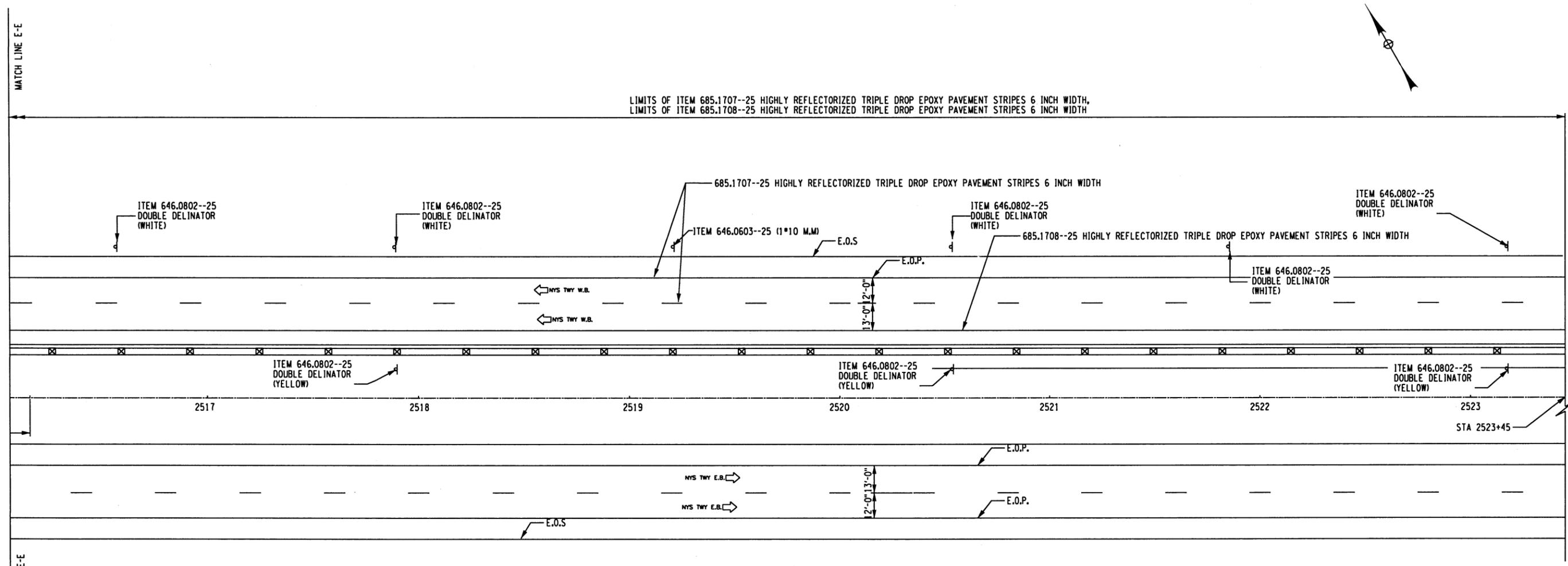
REVISIONS			
DATE	DESCRIPTION	BY	SYM.



Thruway Authority | Canal Corporation

TITLE OF PROJECT <b>NYS THRUWAY BRIDGE REHABILITATION</b>	CONTRACT NUMBER: <b>TAB 16-33B</b>
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUTNY	DATE: <b>06/28/2016</b>
TITLE OF DRAWING <b>FINAL STRIPING PLAN</b>	DRAWING NUMBER: <b>FS 5-6</b>

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: TA  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSSBURGH



LIMITS OF ITEM 685.1707--25 HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH,  
 LIMITS OF ITEM 685.1708--25 HIGHLY REFLECTORIZED TRIPLE DROP EPOXY PAVEMENT STRIPES 6 INCH WIDTH

- 1- ITEM 646.0802--25 INSTALL SNOWPLOW MARKERS, SINGLE UNIT  
 ITEM 646.50--25 REMOVE AND DISPOSAL OF DELINEATORS AND MILE MARKERS
- 2- W.B. MP. 356.70 STRUCTURE (ONLY) LIMITS OF ITEMS) 610.1402, 610.1601, 610.19, 610.21  
 STA. 2484+81 TO 2496+38 LFT. & RT. SHOULDER  
 STA. 2498+00 TO 2510+00 LFT. & RT. SHOULDER

ALTERED ON:	AFFIXED ON: 9/15/16
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.



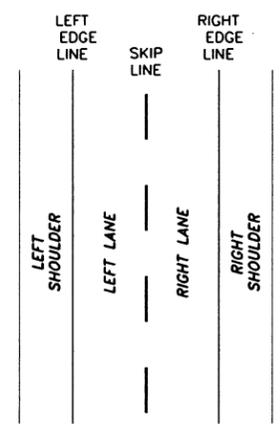
TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 06/28/2016
TITLE OF DRAWING FINAL STRIPING PLAN	DRAWING NUMBER: FS 6-6

FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: J. SHINKI  
 DESIGNED BY: R. COURNOYER  
 DESIGN SUPERVISOR: T. MAHAR

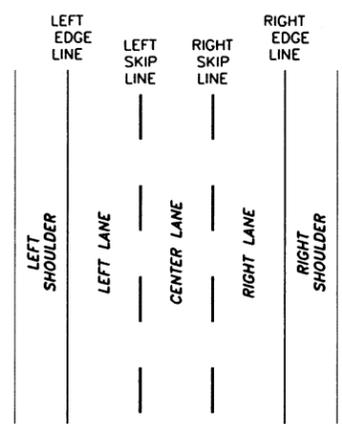
TRAFFIC CONTROL GUIDE FOR PAVEMENT STRIPING OPERATIONS	THRUWAY MAINLINE		
	TWO LANE SECTION	THREE LANE SECTION	FOUR LANE SECTION
LEFT EDGE LINE ONLY	DRAWING MLC-PS STRIPING OPERATION (LEFT EDGE LINE ONLY)	DRAWING MLC-PS STRIPING OPERATION (LEFT EDGE LINE ONLY)	DRAWING MLC-PS STRIPING OPERATION (LEFT EDGE LINE ONLY)
LEFT EDGE LINE WITH LEFT SKIP LINE	DRAWING MLC-PS STRIPING OPERATION LEFT LANE CLOSURE	DRAWING MLC-PS STRIPING OPERATION LEFT LANE CLOSURE	DRAWING MLC-PS STRIPING OPERATION LEFT LANE CLOSURE
LEFT SKIP LINE ONLY	DRAWING MLC-PS STRIPING OPERATION LEFT LANE CLOSURE	DRAWING MLC-PS STRIPING OPERATION LEFT LANE CLOSURE	DRAWING MLC-PS STRIPING OPERATION LEFT LANE CLOSURE
CENTER SKIP LINE	_____	_____	DRAWING MLC-PS STRIPING OPERATION LEFT DOUBLE LANE CLOSURE
RIGHT SKIP LINE ONLY	_____	DRAWING MLC-PS STRIPING OPERATION LEFT DOUBLE LANE CLOSURE	DRAWING INT TYPICAL DECELERATION AND ACCELERATION LANES
RIGHT EDGE LINE	DRAWING MLC-PS STRIPING OPERATION (RIGHT EDGE LINE ONLY)	DRAWING MLC-PS STRIPING OPERATION (RIGHT EDGE LINE ONLY)	DRAWING MLC-PS STRIPING OPERATION (RIGHT EDGE LINE ONLY)
RIGHT EDGE LINE AT DECELERATION AND ACCELERATION LANES	DRAWING MLC-PS STRIPING OPERATION (RIGHT EDGE LINE ONLY)	DRAWING MLC-PS STRIPING OPERATION (RIGHT EDGE LINE ONLY)	DRAWING MLC-PS STRIPING OPERATION (RIGHT EDGE LINE ONLY)
EDGE LINES AT EXIT AND ENTRANCE RAMP	DRAWING INT-PS	DRAWING INT-PS	DRAWING INT-PS
GORES	DRAWING INT TYPICAL ACCELERATION AND DECELERATION LANES  DRAWING SLC-55 OR SLC-65	DRAWING INT TYPICAL ACCELERATION AND DECELERATION LANES  DRAWING SLC-55 OR SLC-65	DRAWING INT TYPICAL ACCELERATION AND DECELERATION LANES  DRAWING SLC-55 OR SLC-65

**NOTES:**

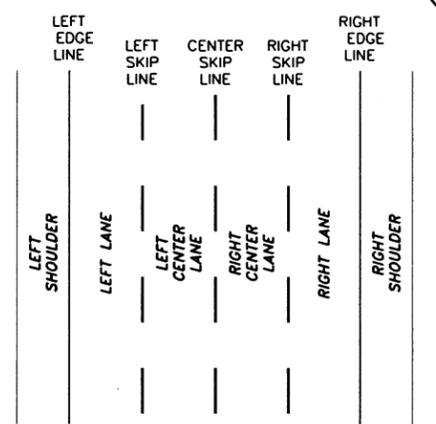
- THE TABLE SHOWN IS INTENDED TO BE A GUIDE IN SELECTING THE APPROPRIATE WORK ZONE TRAFFIC CONTROL PLAN FOR PAVEMENT STRIPING OPERATIONS.
- THE FOLLOWING STANDARD TRAFFIC CONTROL DRAWINGS ARE REFERENCED IN THE TABLE:  
 MLC-PS - MOBILE LANE CLOSURE FOR PAVEMENT STRIPING OPERATIONS  
 INT-PS - WORK ZONE TRAFFIC CONTROL FOR PAVEMENT STRIPING OPERATIONS AT INTERCHANGES, SERVICE AREAS, AND PARKING AREAS  
 INT - WORK ZONE TRAFFIC CONTROL AT INTERCHANGES, SERVICE AREAS, AND PARKING AREAS  
 SLC - SINGLE LANE CLOSURE (SHORT- OR INTERMEDIATE-TERM STATIONARY)
- DRAWINGS REFERENCED ON THIS SHEET THAT HAVE NOT BEEN INCLUDED IN THE CONTRACT ARE NOT APPLICABLE.



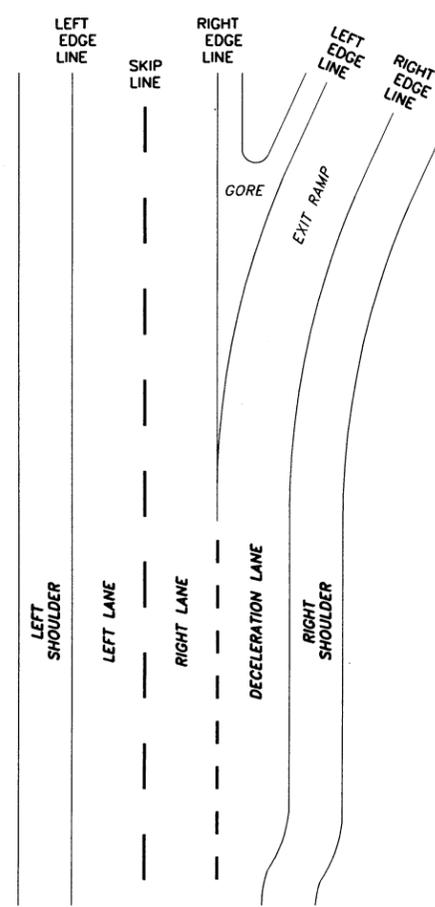
SCHEMATIC TWO LANE SECTION  
N.T.S.



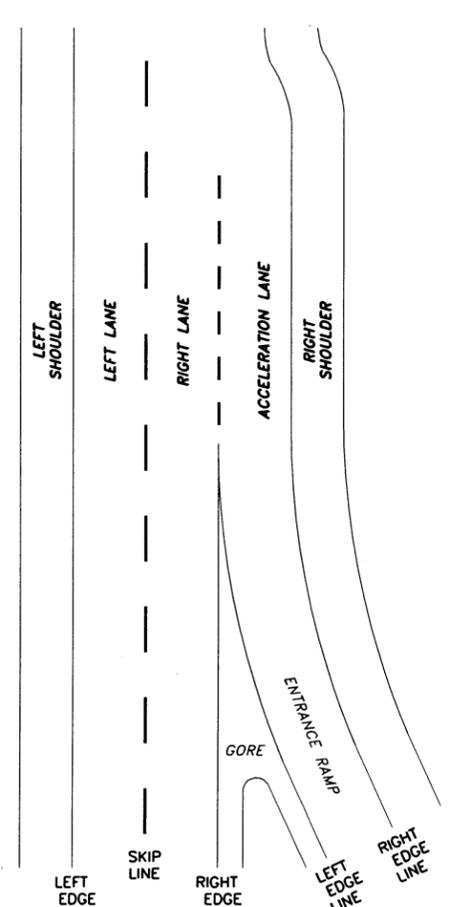
SCHEMATIC THREE LANE SECTION  
N.T.S.



SCHEMATIC FOUR LANE SECTION  
N.T.S.



SCHEMATIC DECELERATION LANE AND EXIT RAMP  
N.T.S.



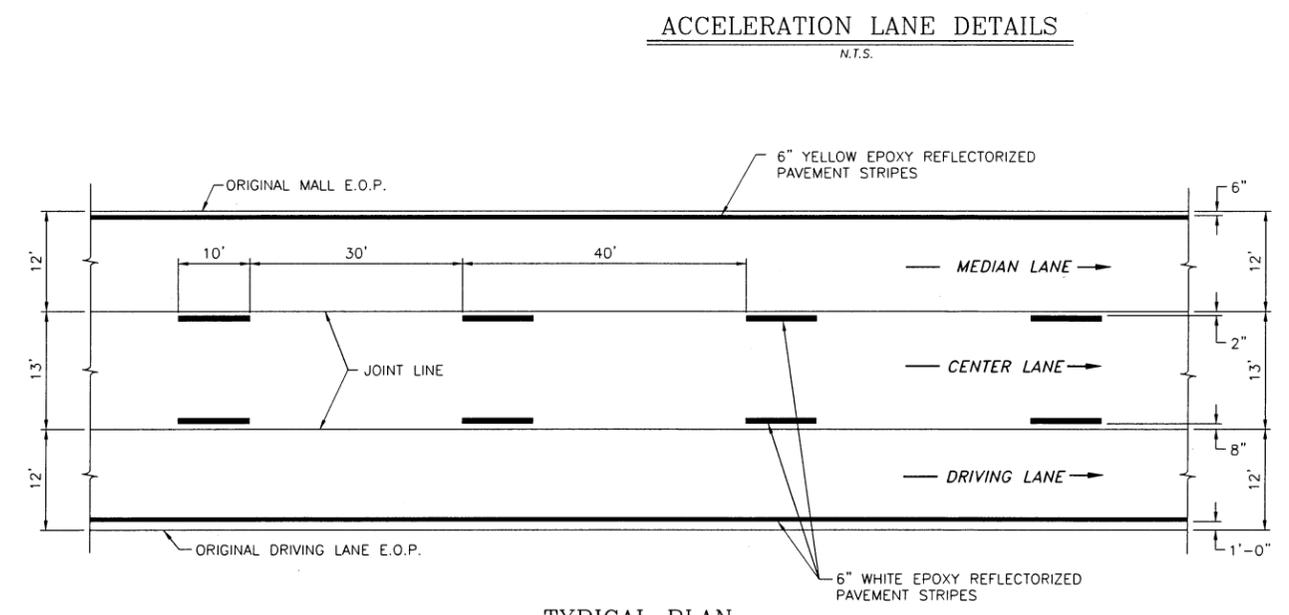
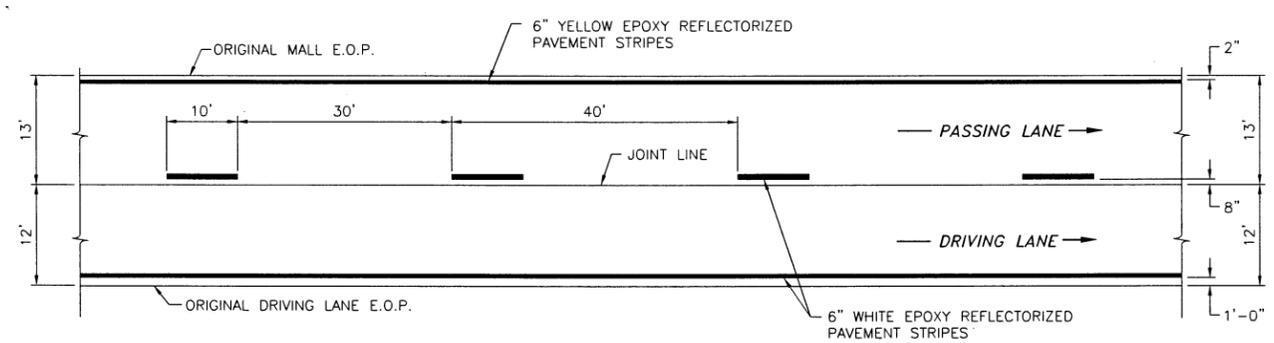
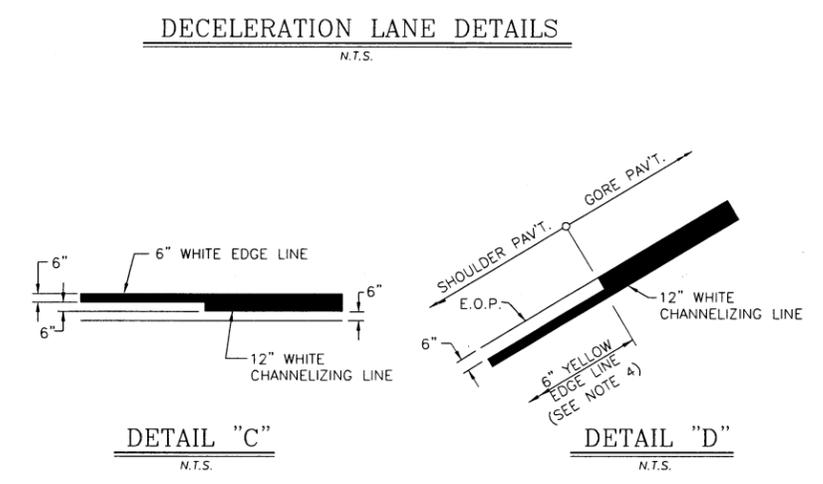
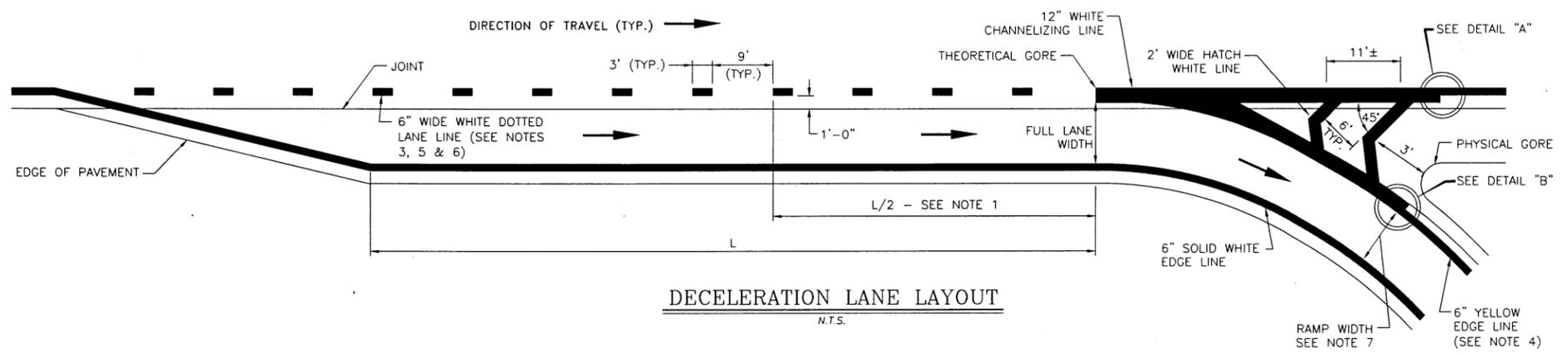
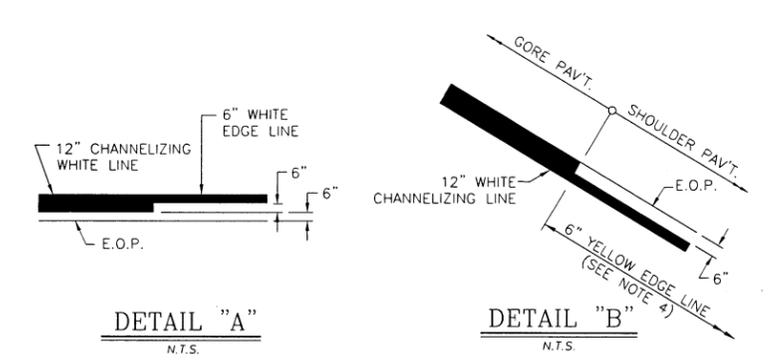
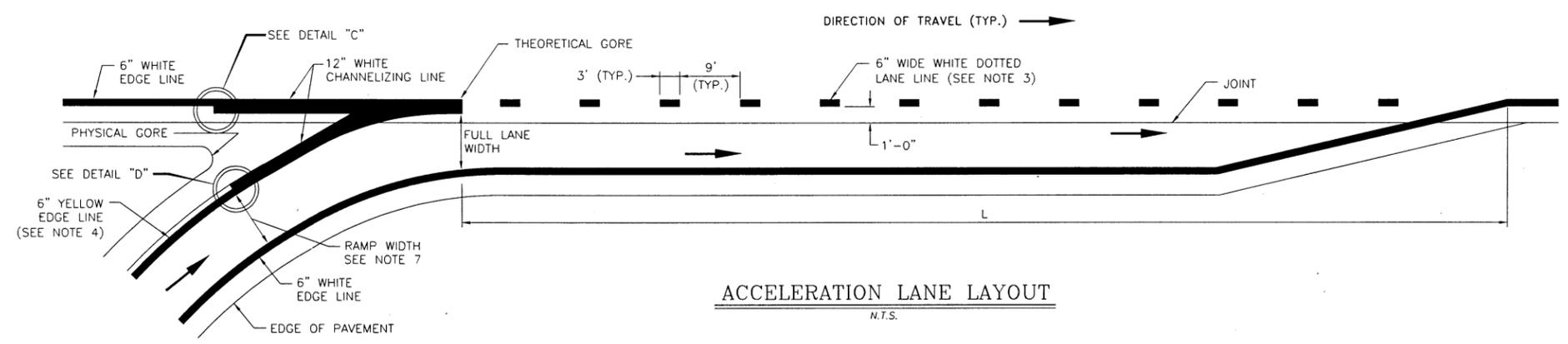
SCHEMATIC ACCELERATION LANE AND ENTRANCE RAMP  
N.T.S.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 02/13
TITLE OF DRAWING WORK ZONE TRAFFIC CONTROL GUIDE FOR PAVEMENT STRIPING OPERATIONS	DRAWING NUMBER: PM-G

FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: J. SHINSKI  
 DESIGNED BY: R. COURNOYER  
 DESIGN SUPERVISOR: T. MAHAR



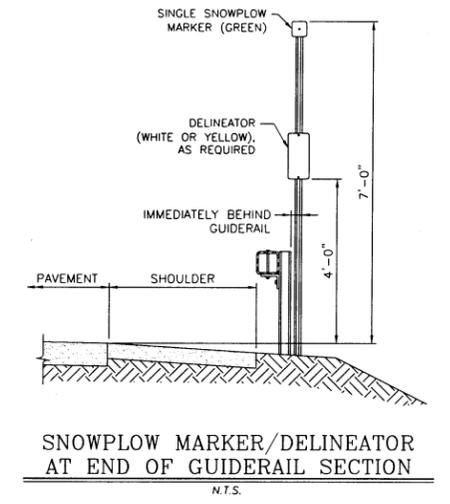
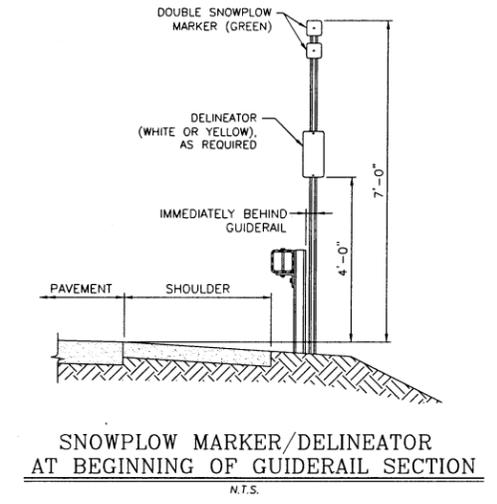
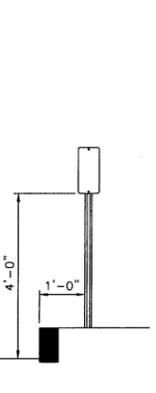
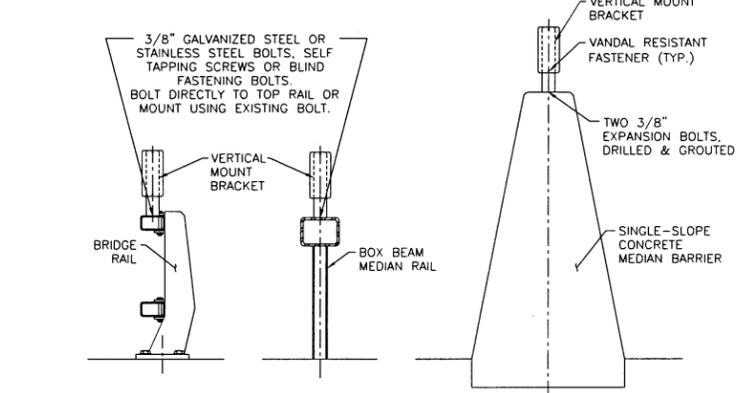
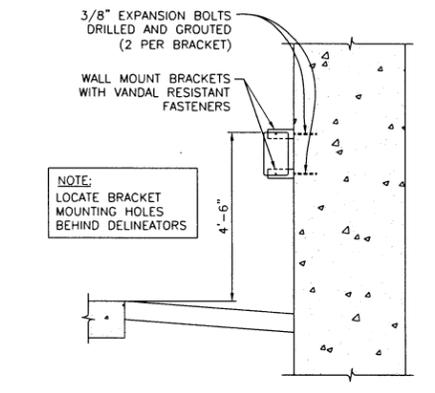
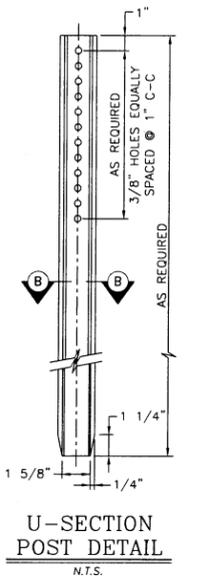
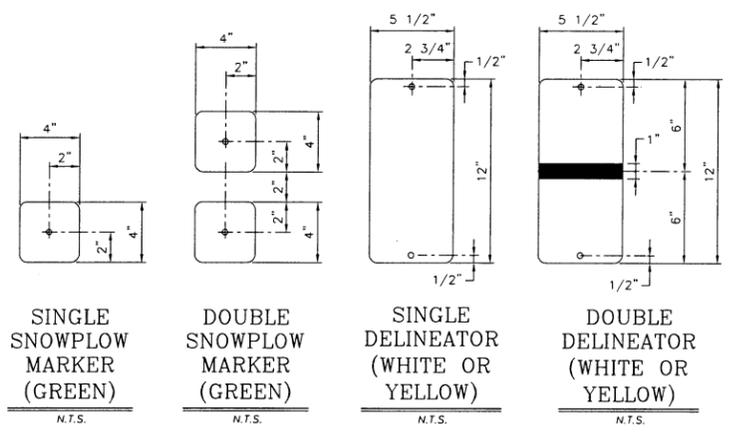
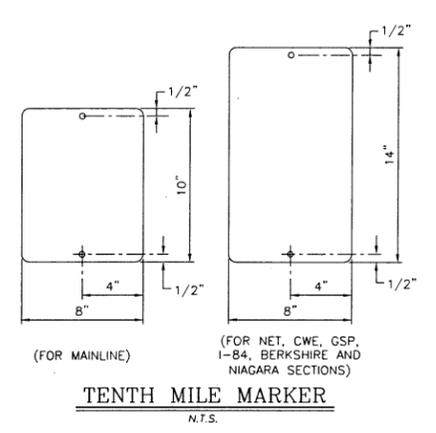
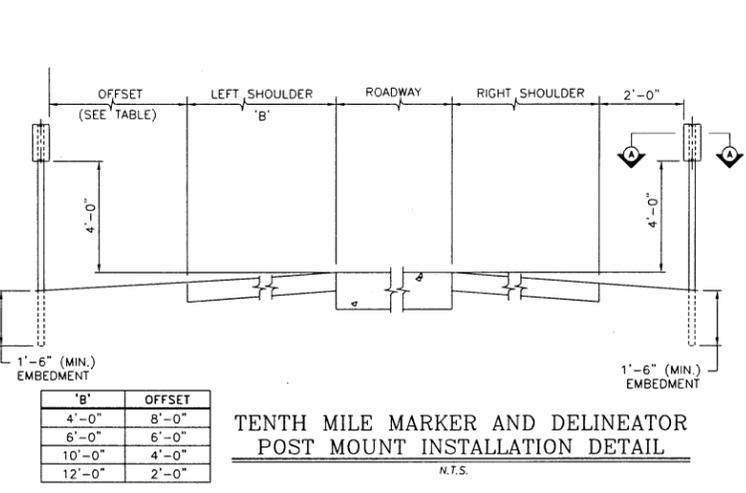
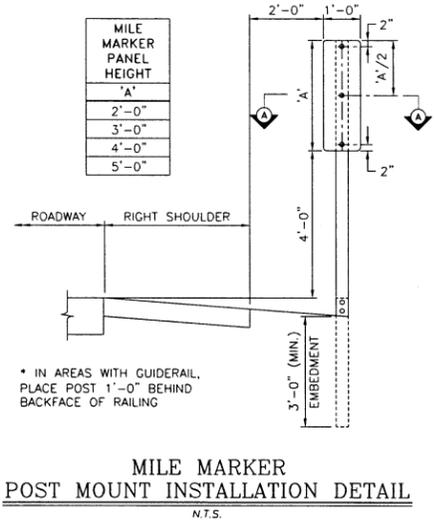
**NOTES:**

1. WHEN THE CONTRACT DOES NOT INCLUDE REPAVING THE GORE, THE EXISTING STRIPING PATTERN IN THE DECELERATION RAMP GORE SHALL BE REPAINTED UNLESS OTHERWISE NOTED.
2. WHEN THE CONTRACT PROVIDES FOR REPAVING OF THE DECELERATION LANE GORE, A "CHEVRON" PATTERN SHALL BE INSTALLED IN THE NEWLY PAVED GORE AS SHOWN IN THE "DECELERATION LANE LAYOUT".
3. IF A PARTIAL BARRIER AND/OR SKIP LINES EXIST AT AN ACCELERATION OR DECELERATION LANE, THEY SHALL BE REMOVED AND THE LANE SHALL BE RESTRIPEDED AS SHOWN IN THE APPROPRIATE LANE LAYOUT.
4. YELLOW EDGE LINE ON RAMP SHALL EXTEND TO ACTUAL PHYSICAL GORE. THE PHYSICAL GORE IS A POINT WHERE A PHYSICAL BARRIER (I.E. SIGN POST, ATTENUATOR, OR OTHER FIXED OBJECT) OR LACK OF A PAVED SURFACE INHIBITS ROAD USER FROM CROSSING A RAMP TO A TRAVEL LANE OR VICE VERSA.
5. A 12" WIDE WHITE DOTTED LANE LINE SHALL BE USED AS A LANE DROP MARKING IN ADVANCE OF LANE DROPS AT EXIT RAMP TO DISTINGUISH A LANE DROP FROM A NORMAL EXIT RAMP.
6. WHITE DOTTED LANE LINES USED IN ADVANCE OF LANE DROPS SHOULD BEGIN AT LEAST 1/2 MILE IN ADVANCE OF THE THEORETICAL GORE.
7. WHEN STRIPES ARE REMOVED ON RAMP, THE CONTRACTOR SHALL NOTE THE EXISTING RAMP WIDTHS, STRIPE LOCATIONS AND REPLACE THE STRIPING IN THE SAME LOCATION UNLESS OTHERWISE NOTED IN THE CONTRACT DOCUMENTS.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT <b>NYS THRUWAY BRIDGE REHABILITATION</b>	CONTRACT NUMBER: <b>TAB 16-33B</b>
	LOCATION OF PROJECT <b>TWY M.P. 356.69 &amp; 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY</b>	DATE: <b>12/12</b>
	TITLE OF DRAWING <b>TEMPORARY PAVEMENT MARKING DETAILS</b>	DRAWING NUMBER: <b>PM-T</b>

FILE  
 CHECKED BY: TA  
 DRAFTED BY: TA  
 CHECKED BY: TA  
 DESIGNED BY: TA  
 DESIGN SUPERVISOR: TA



**TENTH MILE MARKER AND DELINEATOR WALL MOUNT INSTALLATION DETAIL**  
N.T.S.

**DELINEATOR VERTICAL MOUNT INSTALLATION DETAILS**  
N.T.S.

**CURB SECTION**  
N.T.S.

**SNOWPLOW MARKER/DELINEATOR AT BEGINNING OF GUIDERAIL SECTION**  
N.T.S.

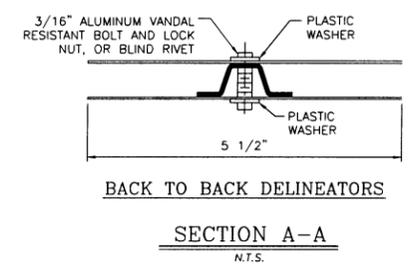
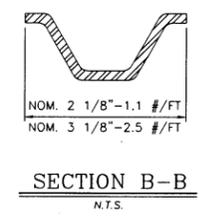
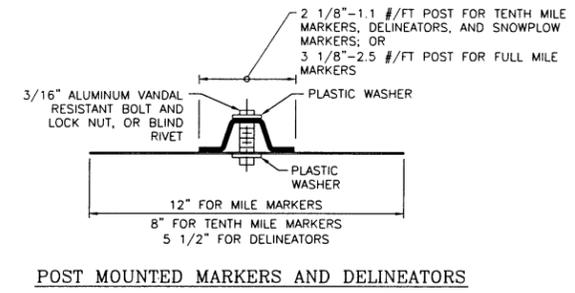
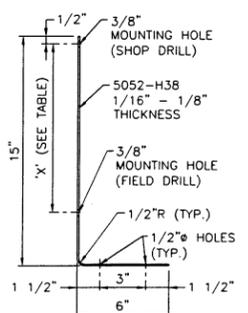
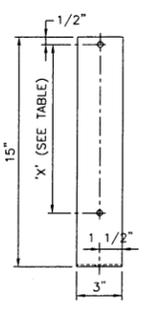
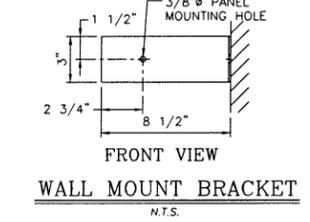
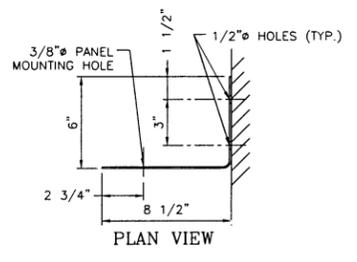
**SNOWPLOW MARKER/DELINEATOR AT END OF GUIDERAIL SECTION**  
N.T.S.

**U-SECTION POST DETAIL**  
N.T.S.

- VERTICAL MOUNT BRACKET NOTES:**
- WHEN GALVANIZED STEEL IS USED FOR BRACKETS, SPACERS THAT DO NOT INDUCE CORROSION MUST BE USED.
  - BRACKETS MAY BE USED FOR MOUNTING DELINEATORS OR REFERENCE MARKERS ON BRIDGE RAIL, GUIDE RAIL, OR AS OTHERWISE NEEDED.

**MOUNTING HOLE SPACING TABLE**

'X'	DESCRIPTION
11"	FOR 5 1/2" x 12" DELINEATOR
9"	FOR 8" x 10" TENTH MILE MARKER
13"	FOR 8" x 14" TENTH MILE MARKER



- NOTES:**
- MATERIAL SHALL CONFORM TO EITHER A.S.T.M. A-499 OR A.S.T.M. A-36.
  - GALVANIZING SHALL CONFORM TO A.S.T.M. A-123.
  - REMOVE ALL BURRS AND SHARP EDGES.

ALTERED ON:	AFFIXED ON:
SIGNATURE: STAMP:	SIGNATURE: STAMP:

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**REVISIONS**

DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY**

**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT PITTSFORD MENDON CENTER RD. MONROE COUNTY	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 3565.69 & 356.70 BUFFALO DIVISION	DATE: 06/28/2016
TITLE OF DRAWING MILE MARKER DETAILS	DRAWING NUMBER: MMD-1

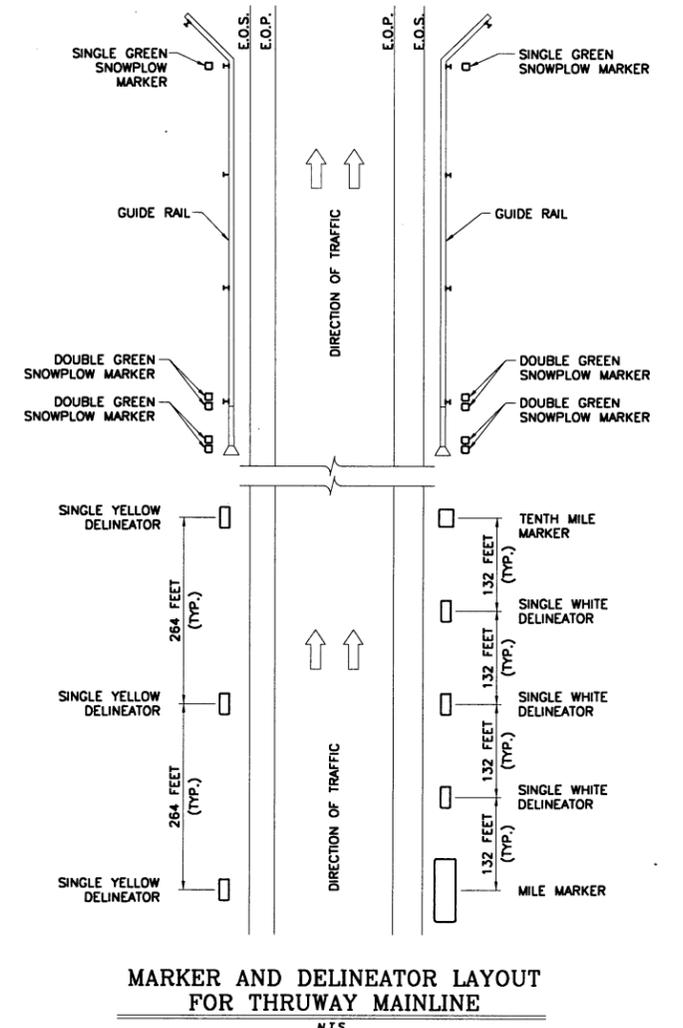
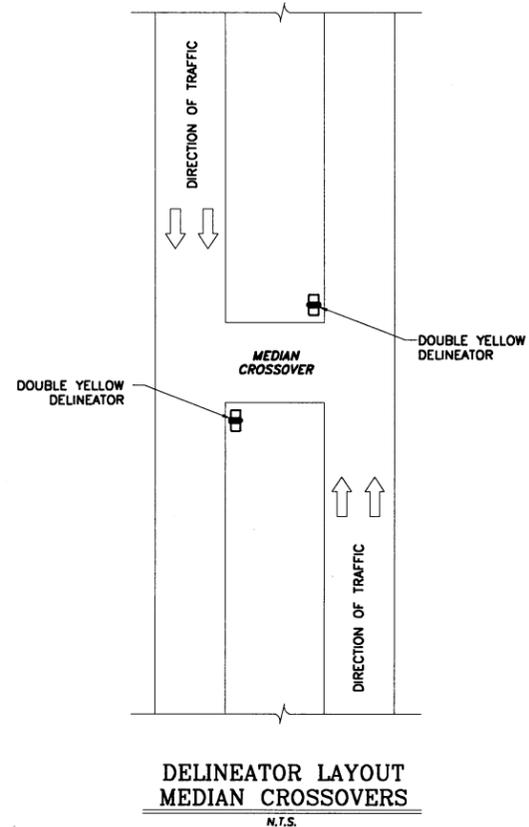
**TABLE OF SNOWPLOW MARKERS**

STATION	SIDE	WHITE	YELLOW
ITEM 646.50----25 REMOVAL AND DISPOSAL OF DELINEATORS			
ITEM 646.0802--25 INSTALL SNOWPLOW MARKER, DOUBLE UNIT			
WB 2478+83	LFT & RT	1	1
WB 2479+62	RT	1	
WB 2480+94	LFT & RT	1	1
WB 2482+26	RT	1	
WB 2483+58	LFT & RT	1	1
WB 2484+90	RT	1	
WB 2486+22	LFT & RT	1	1
WB 2487+54	RT	1	
WB 2488+86	LFT & RT	1	1
WB 2490+18	RT	1	
WB 2491+50	LFT & RT	1	1
WB 2492+82	RT	1	
WB 2494+14	LFT & RT	1	1
WB 2495+46	RT	1	
WB 2496+78	LFT & RT	1	1
WB 2498+10 (MP356.70)	RT	1	
WB 2499+42	LFT & RT	1	1
WB 2500+74	RT	1	
WB 2502+06	LFT & RT	1	1
WB 2503+38	RT	1	
WB 2504+70	LFT & RT	1	1
WB 2506+02	RT	1	
WB 2507+34	LFT & RT	1	1
WB 2508+66	RT	1	
WB 2509+98	LFT & RT	1	1
WB 2511+30	RT	1	
WB 2512+62	LFT & RT	1	1
WB 2513+94	RT	1	
WB 2515+26	LFT & RT	1	1
WB 2516+58	RT	1	
WB 2517+90	LFT & RT	1	1
WB 2519+22	RT	1	
WB 2520+54	LFT & RT	1	1
WB 2521+86	RT	1	
WB 2523+18	LFT & RT	1	1
<b>TOTAL</b>		<b>35</b>	<b>18</b>

**TABLE OF MILE MARKERS & DELINEATORS**

ITEM 646.50----25	REMOVAL AND DISPOSAL OF DELINEATORS AND TENTH MILE MARKER					
ITEM 646.0603--25	INSTALL DELINEATOR OR TENTH MILE MARKER					
LOCATION	EA	EA	EA	EA	EA	EA
ITEM 646.50----25 1/10+						
ITEM 646.0603--25 INSTALL 10TH MARKER						
STATION AND SIDE	EA	EA	EA	EA	EA	EA
WB 2482+26	1				1	
WB 2497+54	1				1	
WB 2492+82	1				1	
WB 2498+10 (MP356.70)	1				1	
WB 2501+79	1				1	
WB 2503+38	1				1	
WB 2508+66	1				1	
WB 2513+94	1				1	
<b>TOTAL</b>	<b>8</b>				<b>8</b>	

- NOTES:**
1. THE TYPICAL SPACING OF DELINEATORS FOR INTERCHANGES IS 100 FEET. FOR LOCATIONS WHERE THE ROADWAY HAS A DEGREE OF CURVE OF 5 DEGREES OR MORE (OR A RADIUS OF CURVE OF 1000 FEET OR LESS), THE SPACING SHALL BE 50 FEET.
  2. ON THRUWAY RAMP WHERE TWO-WAY TRAFFIC IS SEPARATED BY BARRIER (CONCRETE, CORRUGATED BEAM, ETC.), BACK-TO-BACK YELLOW DELINEATORS SHALL BE INSTALLED ALONG THE BARRIER AT A SPACING OF 50 FEET.
  3. SOME EXISTING MARKERS AND DELINEATORS WITHIN THE PROJECT LIMITS MAY NOT BE IN THE CORRECT LOCATIONS. THE CONTRACTOR SHALL INCLUDE IN THE PRICE BID FOR EACH RESPECTIVE MARKER AND/OR DELINEATOR THE COST TO ACCURATELY DETERMINE THE EXACT LOCATION PRIOR TO INSTALLATION. OVERHEAD AND MAINLINE STRUCTURES SHALL BE USED AS FIXED REFERENCE GUIDES FOR THE INSTALLATION. THE ENGINEER SHALL APPROVE THE EXACT LOCATIONS PRIOR TO INSTALLATION. ANY COST FOR THIS LAYOUT WORK SHALL BE INCLUDED IN THE PRICE BID FOR THE MARKERS AND/OR DELINEATORS.
  4. WHERE AN EXISTING TENTH MILE MARKER FALLS WITHIN A LINE OF DOUBLE WHITE DELINEATORS, THE TENTH MILE MARKER SHALL REMAIN AT ITS PRESENT LOCATION.
  5. POSTS, BANDS, BRACKETS, AND ALL NECESSARY HARDWARE ARE TO BE FURNISHED BY THE CONTRACTOR. DELINEATORS, MILE MARKERS, TENTH MILE MARKERS, AND SNOWPLOW MARKERS ARE TO BE FURNISHED BY THE AUTHORITY. THE DESIGN ENGINEER SHALL PROVIDE THE SIGN SHOP WITH DELINEATOR, MILE MARKER, TENTH MILE MARKER, AND SNOWPLOW MARKER REQUIREMENTS DURING PROJECT DESIGN AS WELL AS AN APPROXIMATE DATE WHEN NEEDED IN THE FIELD. DURING CONSTRUCTION, AT LEAST ONE MONTH PRIOR TO SCHEDULED INSTALLATION, THE ENGINEER-IN-CHARGE SHALL CONTACT THE SIGN SHOP TO CONFIRM THE DATE WHEN THE MATERIAL IS NEEDED IN THE FIELD. THE SIGN SHOP WILL PREPARE THE SHIPMENT AND ARRANGE FOR DIVISION HIGHWAY TO PICK UP THE DELINEATORS AND/OR MARKERS. THE CONTRACTOR SHALL THEN ARRANGE TO PICK UP THE DELINEATORS AND/OR MARKERS FROM DIVISION HIGHWAY. COSTS FOR PICKING UP THE DELINEATORS AND/OR MARKERS FROM DIVISION HIGHWAY ARE TO BE INCLUDED IN THE PRICE BID FOR EACH RESPECTIVE ITEM.
  6. IDENTICAL MILE MARKERS FOR OPPOSITE DIRECTIONS OF TRAVEL SHALL BE LOCATED DIRECTLY ACROSS FROM EACH OTHER.
  7. ON CURVES, DELINEATOR FACES SHALL BE ORIENTED TO PROVIDE OPTIMUM VISIBILITY AT NIGHT. EXACT ORIENTATION WILL VARY BASED UPON THE DEGREE OF CURVE.
  8. DELINEATORS, MILE MARKERS, TENTH MILE MARKERS, AND SNOWPLOW MARKERS SHALL BE ATTACHED TO POSTS AND BRACKETS USING VANDAL RESISTANT FASTENERS. THE FASTENERS SHALL BE ALUMINUM ALLOY 6061-T6 OR 2024-T4.
  9. BRACKETS SHALL BE ATTACHED TO GUIDE RAIL, BRIDGE RAIL, MEDIAN RAIL, AND MEDIAN BARRIER USING BOLTS, BLIND LOCK BOLTS, SELF-TAPPING SCREWS, EXPANDING ANCHOR BOLTS, ETC. MADE OF STAINLESS STEEL, GALVANIZED STEEL, OR ALUMINUM ALLOY 6061-T6 OR 2024-T4.
  10. ALUMINUM HARDWARE OF 2024-T4 SHALL BE ANODIZED OR FINISHED WITH TYPE 206 FINISH IN ACCORDANCE WITH THE SPECIFICATIONS OF ALUMINUM ANODIC COATINGS.



- DELINEATORS AND SNOWPLOWING MARKERS**
11. ON ROADWAY SECTIONS WITHOUT GUIDE RAIL, RIGHT SIDE DELINEATORS SHALL BE INSTALLED 2'-0" OUTSIDE THE USABLE RIGHT SHOULDER AND LEFT SIDE DELINEATORS SHALL BE INSTALLED 2 TO 8 FEET FROM THE LEFT EDGE OF SHOULDER. (REFER TO TABLE ON DMM-1.) WHERE THERE IS A CHANGE IN SHOULDER WIDTH, THE TRANSITION IN DELINEATOR PLACEMENT SHALL BE MADE GRADUALLY.
  12. ON ROADWAY SECTIONS HAVING GUIDE RAIL, DELINEATORS AND SNOWPLOW MARKERS SHALL BE INSTALLED IMMEDIATELY BEHIND THE GUIDE RAIL AND BEHIND THE FRONT FACE OF THE END TREATMENT. WHERE THERE IS A CHANGE IN THE GUIDE RAIL LATERAL OFFSET, THE TRANSITION IN DELINEATOR PLACEMENT SHALL BE MADE GRADUALLY.
- POST ERECTION**
13. POSTS MAY EITHER BE DRIVEN OR SET. POSTS SHALL BE ERECTED TO PROVIDE THE PROPER LOCATION, LINE AND GRADE, AND TRUE VERTICAL ALIGNMENT OF THE MARKERS AND/OR DELINEATORS.
  14. FOR POSTS THAT ARE DRIVEN, HAND OR MECHANICAL DEVICES MAY BE USED. A SUITABLE DRIVING CAP SHALL ALSO BE USED TO PREVENT EXCESSIVE DAMAGE TO THE TOP OF THE POSTS. AFTER DRIVING, THE TOP OF THE POSTS SHALL HAVE SUBSTANTIALLY THE SAME CROSS-SECTIONAL DIMENSIONS AS THE BODY OF THE POSTS. NO BATTERED HEADS WILL BE ACCEPTED. POSTS THAT ARE BENT OR OTHERWISE DAMAGED TO THE EXTENT THAT, IN THE OPINION OF THE ENGINEER, THEY ARE UNFIT FOR USE IN THE FINISHED WORK SHALL BE REMOVED FROM THE SITE AND REPLACED BY THE CONTRACTOR AT HIS/HER OWN EXPENSE.
  15. FOR POSTS THAT ARE SET, HOLES SHALL BE DUG TO THE FULL EMBEDMENT DEPTH SHOWN ON THE PLANS. AFTER SETTING THE POSTS AT THE FULL EMBEDMENT DEPTH, THE HOLES SHALL BE BACKFILLED WITH SUITABLE MATERIAL PLACED IN LAYERS OF NOT MORE THAN 6 INCHES IN DEPTH. EACH LAYER SHALL BE THOROUGHLY COMPACTED. CARE SHALL BE TAKEN DURING COMPACTION TO PRESERVE THE ALIGNMENT OF THE POST.
  16. WHEN SOUND ROCK IS ENCOUNTERED, POSTS SHALL BE FOUNDED A MINIMUM OF 12 INCHES INTO SOUND ROCK. COST SHALL BE INCLUDED IN THE APPROPRIATE MARKER OR DELINEATOR ITEM.
  17. FOR POSTS THAT ARE INSTALLED IN PAVED AREAS, SODDED AREAS, SIDEWALKS, ETC., DISTURBED AREAS SHALL BE RESTORED IN-KIND.

FILED  
 CHECKED BY: TA  
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 CHECKED BY: TA  
 DESIGNED BY: TA  
 DESIGN SUPERVISOR: TA

ALTERED ON: \_\_\_\_\_ AFFIXED ON: 9/15/16

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IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE. THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

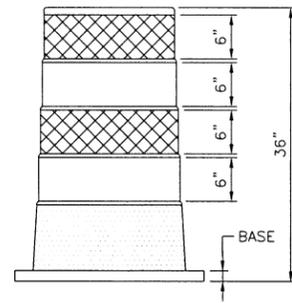
**NEW YORK STATE OF OPPORTUNITY**

**Thruway Authority Canal Corporation**

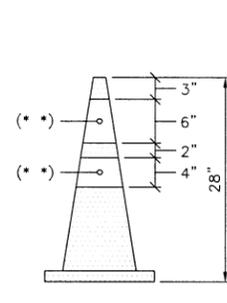
TITLE OF PROJECT: NYS THRUWAY BRIDGE REHABILITATION  
 LOCATION OF PROJECT: TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY  
 TITLE OF DRAWING: MILE MARKER LAYOUT

CONTRACT NUMBER: TAB 16-33B  
 DATE: 06/2016  
 DRAWING NUMBER: MML 1-2

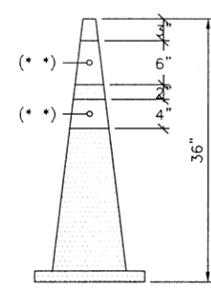
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 DESIGN SUPERVISOR: T. MAHAR



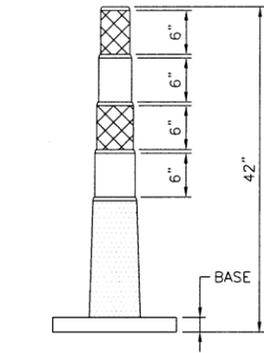
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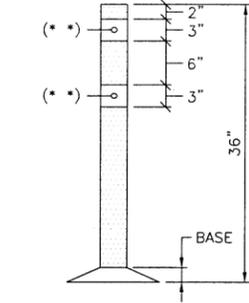
STANDARD CONE



TALL CONE



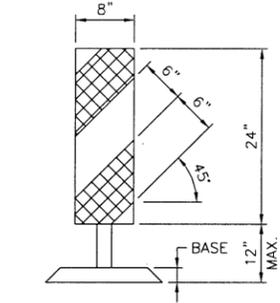
EXTRA TALL CONE



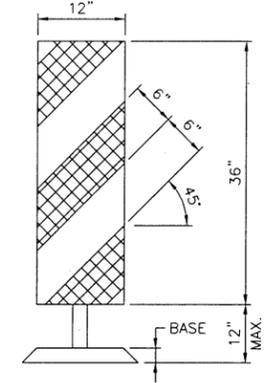
TUBULAR MARKER

**CHANNELIZING DEVICES**

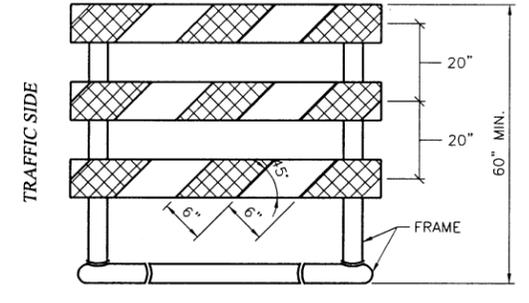
N.T.S.



STANDARD VERTICAL PANEL



OVERSIZED VERTICAL PANEL



TYPE III BARRICADE

**GENERAL NOTES:**

1. THE ENGINEER SHALL APPROVE THE CONDITION OF ALL TEMPORARY TRAFFIC CONTROL DEVICES PRIOR TO USE. THE ENGINEER SHALL ALSO REVIEW THE PROPOSED WORK ZONE TRAFFIC CONTROL PLAN FOR PRECISE DEVICE POSITIONING PRIOR TO INSTALLATION.
2. ALL SIGNS SHALL CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD). ORANGE SIGNS ON RIGID PANELS SHALL BE FLUORESCENT-ORANGE ASTM TYPE IX (CLASS E) RETROREFLECTIVE SHEETING. ALL OTHER COLORS OF CONSTRUCTION SIGN FACES ON RIGID PANELS SHALL BE ASTM TYPE III (CLASS B) REFLECTIVE SHEETING.
3. EXISTING SPEED LIMIT SIGNS WITHIN THE WORK ZONE SHALL BE COMPLETELY COVERED TO AVOID CONFLICT WITH THE WORK ZONE SPEED LIMIT SIGNS.
4. WHEN A SPEED DISPLAY TRAILER IS DEPLOYED, IT MAY BE USED AS A SUBSTITUTE FOR THE SECOND "WORK ZONE/SPEED LIMIT 45" SIGN (G20-50P/R2-1). THE UNIT SHALL BE PROVIDED IN ACCORDANCE WITH THE CONTRACT SPECIFICATIONS. WHEN NOT IN OPERATION, THE SPEED DISPLAY TRAILER SHALL BE REMOVED FROM THE WORK ZONE.
5. CHANNELIZING DEVICES SHALL CONFORM TO THE REQUIREMENTS OF THE MUTCD AND SECTION 729 OF THE STANDARD SPECIFICATIONS WITH THE EXCEPTION THAT SHEETING REQUIREMENTS SHALL BE AS SPECIFIED ON THIS DRAWING. CHANNELIZING DEVICE TYPE AND SPACING REQUIREMENTS SHALL BE IN ACCORDANCE WITH SECTION 619 OF THE STANDARD SPECIFICATIONS.
6. ARROW PANELS SHALL CONFORM TO SECTION 729-15 OF THE STANDARD SPECIFICATIONS. THE ADVANCE WARNING ARROW DISPLAY SHALL BE A FULL FLASHING ARROW ONLY. CHEVRONS AND SEQUENTIAL ARROW DISPLAYS SHALL NOT BE PERMITTED. THE CAUTION MODE DISPLAY SHALL BE FOUR FLASHING CORNERS. FLASHING BAR DISPLAYS SHALL NOT BE PERMITTED.
7. THERE SHALL BE NO WORK ACTIVITY, EQUIPMENT, VEHICLES AND/OR MATERIALS LOCATED WITHIN THE BUFFER SPACE. THE SAME SHALL ALSO APPLY TO THE SPACE BETWEEN THE BARRIER VEHICLE(S) AND THE ACTIVE WORK AREA (BARRIER VEHICLE PLACEMENT DISTANCE).

**CHANNELIZING DEVICE**

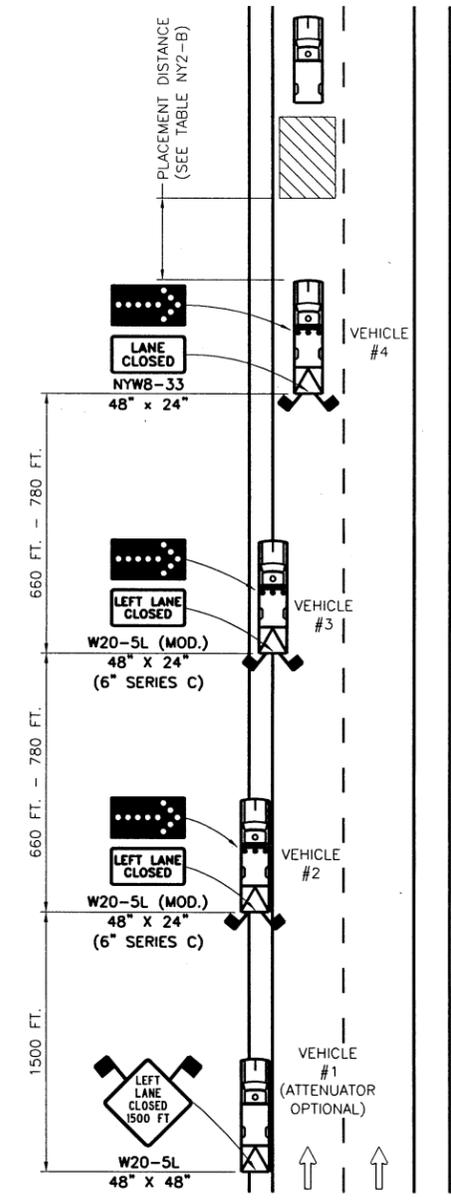
**LEGEND**

	WHITE RETROREFLECTORIZED SHEETING, ASTM TYPE IX (CLASS E)
	ORANGE RETROREFLECTORIZED SHEETING, ASTM TYPE IX (CLASS E)
	NON-REFLECTORIZED ORANGE
( * * )	WHITE REFLECTIVE SHEETING, ASTM TYPE III (CLASS B) OR ASTM TYPE V (CLASS C)

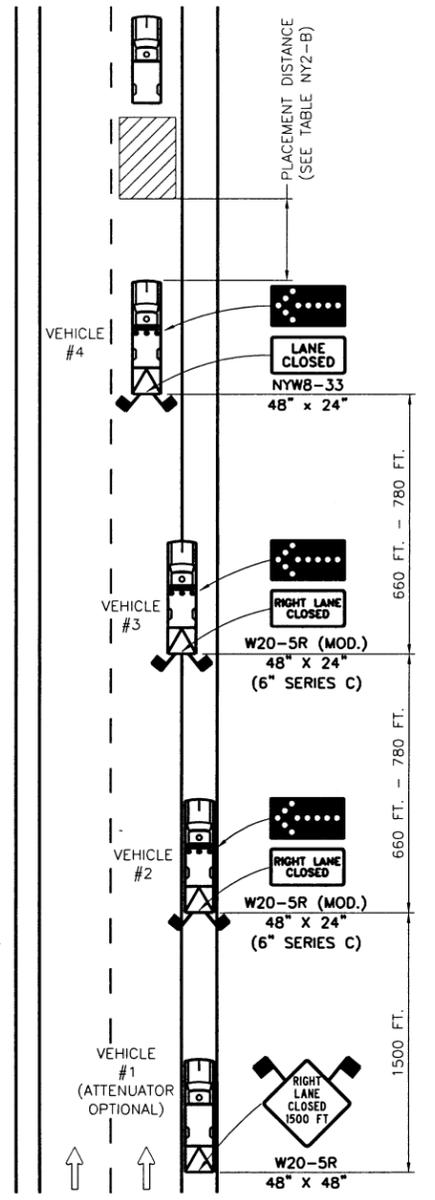
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DATE	DESCRIPTION	BY	SYM.



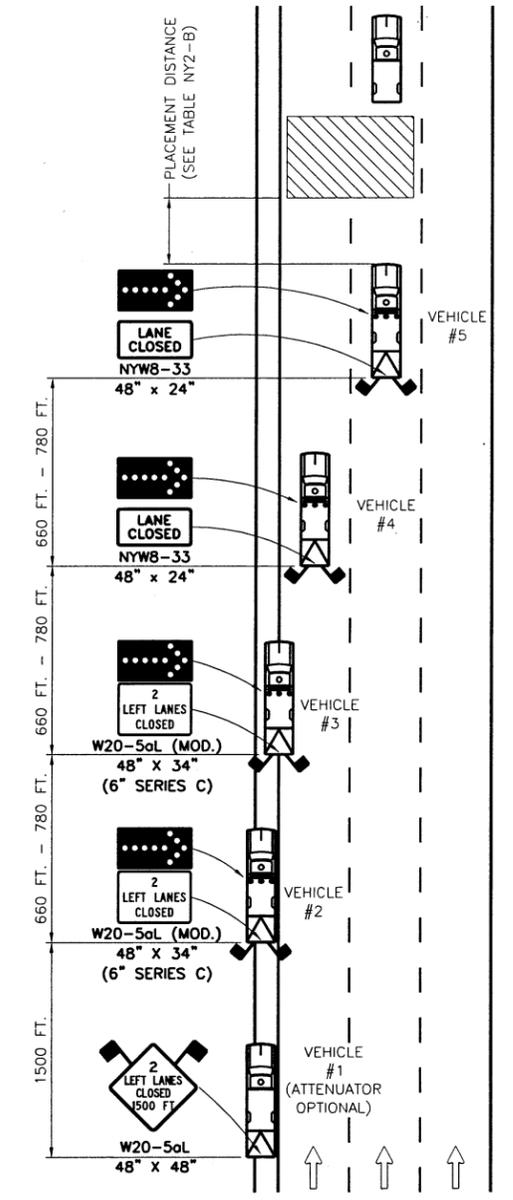
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LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 02/13
TITLE OF DRAWING GENERAL WZTC NOTES AND CHANNELIZING DEVICES	DRAWING NUMBER: GWZN



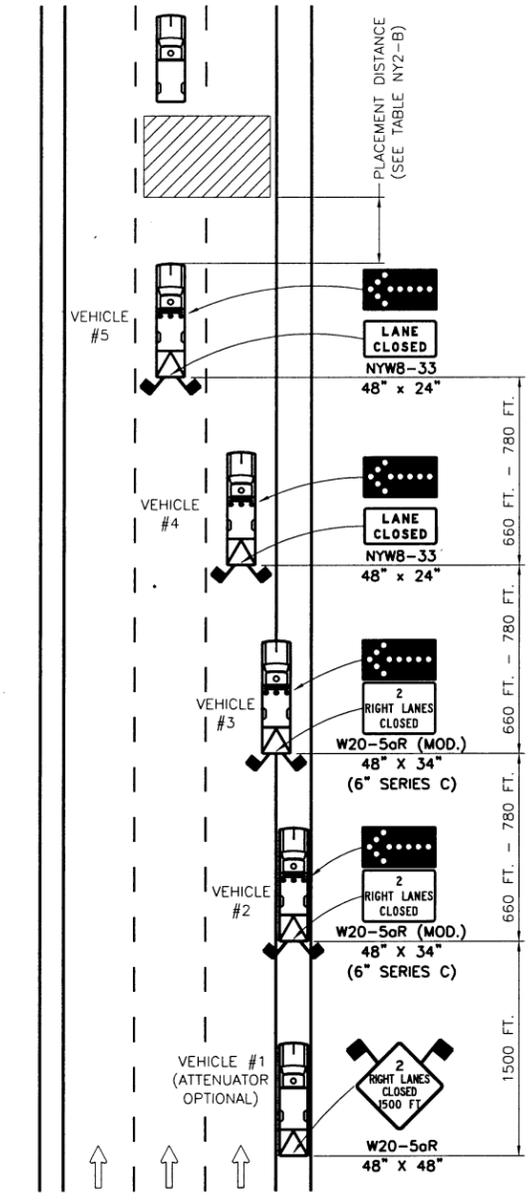
**MOBILE OPERATION  
LEFT LANE CLOSURE**  
N.T.S.



**MOBILE OPERATION  
RIGHT LANE CLOSURE**  
N.T.S.



**MOBILE OPERATION  
LEFT DOUBLE LANE CLOSURE**  
N.T.S.



**MOBILE OPERATION  
RIGHT DOUBLE LANE CLOSURE**  
N.T.S.

**NOTES:**

1. THESE PLANS ARE TO BE UTILIZED ONLY WHEN AUTHORIZED BY THE ENGINEER.
2. MOBILE OPERATIONS ARE WORK ACTIVITIES THAT MOVE CONTINUOUSLY OR STOP INTERMITTENTLY FOR SHORT PERIODS IN THE ROADWAY. THE DURATION FOR EACH INTERMITTENT STOP MAY BE APPROXIMATELY 15 MINUTES BEFORE MOVING TO A NEW LOCATION.
3. VEHICLE #4 (LEFT LANE CLOSURE AND RIGHT LANE CLOSURE PLANS) AND VEHICLE #5 (LEFT DOUBLE LANE CLOSURE AND RIGHT DOUBLE LANE CLOSURE PLANS) SHALL NOT BE USED TO TRANSPORT WORKERS, MATERIALS, AND/OR EQUIPMENT TO THE WORK SITE. A SEPARATE WORK VEHICLE(S) SHALL BE REQUIRED.
4. THESE TEMPORARY TRAFFIC CONTROL PLANS SHALL NOT BE ADVANCED THROUGH AN AREA WHERE THERE IS AN EXIT OR ENTRANCE RAMP.
5. WHERE THE LEFT LANE IS TO BE CLOSED, VEHICLE #1 AND VEHICLE #2 ARE TO BE LOCATED COMPLETELY ON THE LEFT SHOULDER, VEHICLE #3 STRADDLES THE LEFT SHOULDER AND THE LEFT LANE, AND VEHICLE #4 IS IN THE LEFT LANE.
6. WHERE THE RIGHT LANE IS TO BE CLOSED, VEHICLE #1 AND VEHICLE #2 ARE TO BE LOCATED COMPLETELY ON THE RIGHT SHOULDER, VEHICLE #3 STRADDLES THE RIGHT SHOULDER AND THE RIGHT LANE, AND VEHICLE #4 IS IN THE RIGHT LANE.
7. WHERE THE LEFT TWO LANES ARE TO BE CLOSED, VEHICLE #1 AND VEHICLE #2 ARE TO BE LOCATED COMPLETELY ON THE LEFT SHOULDER, VEHICLE #3 STRADDLES THE LEFT SHOULDER AND THE LEFT LANE, VEHICLE #4 IS IN THE LEFT LANE, AND VEHICLE #5 IS IN THE CENTER LANE.
8. WHERE THE RIGHT TWO LANES ARE TO BE CLOSED, VEHICLE #1 AND VEHICLE #2 ARE TO BE LOCATED COMPLETELY ON THE RIGHT SHOULDER, VEHICLE #3 STRADDLES THE RIGHT SHOULDER AND THE RIGHT LANE, VEHICLE #4 IS IN THE RIGHT LANE, AND VEHICLE #5 IS IN THE CENTER LANE.
9. FOR VEHICLE #2, A TRAILER-MOUNTED ARROW PANEL MAY BE SUBSTITUTED FOR THE TRUCK-MOUNTED ARROW PANEL.
10. FOR VEHICLE #2, THE ATTENUATOR IS OPTIONAL FOR DAYTIME OPERATIONS. FOR NIGHTTIME OPERATIONS, ALL VEHICLES, INCLUDING VEHICLE #1 AND VEHICLE #2 ON THE SHOULDER, SHALL BE EQUIPPED WITH AN ATTENUATOR.
11. FOR VEHICLE #1, A TRUCK-MOUNTED OR TRAILER-MOUNTED PORTABLE VARIABLE MESSAGE SIGN (PVMS) MAY BE USED IN LIEU OF THE SIGN SHOWN. THE PVMS UNIT SHALL BE COMPLETELY ON THE SHOULDER AND SHALL HAVE NO PORTION PROTRUDE OVER THE TRAVEL LANE AT ANY TIME. THE MESSAGE DISPLAYED SHALL BE THE SAME AS THAT SHOWN.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT MENDON ENTER RD., MONROE COUNTY TWY M.P. 356.69 & 356.70 PITTSFORD	DATE: 02/13
	TITLE OF DRAWING MOBILE LANE CLOSURE	DRAWING NUMBER: MLC

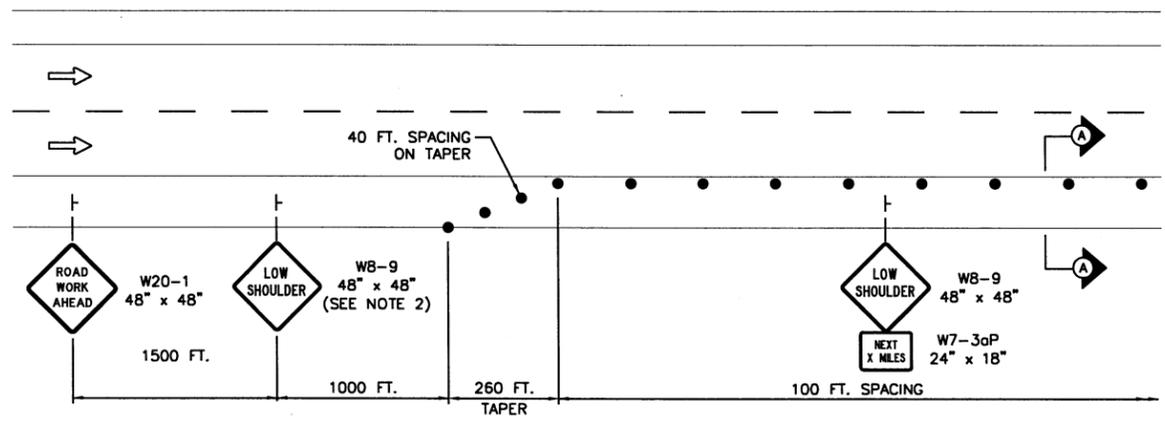
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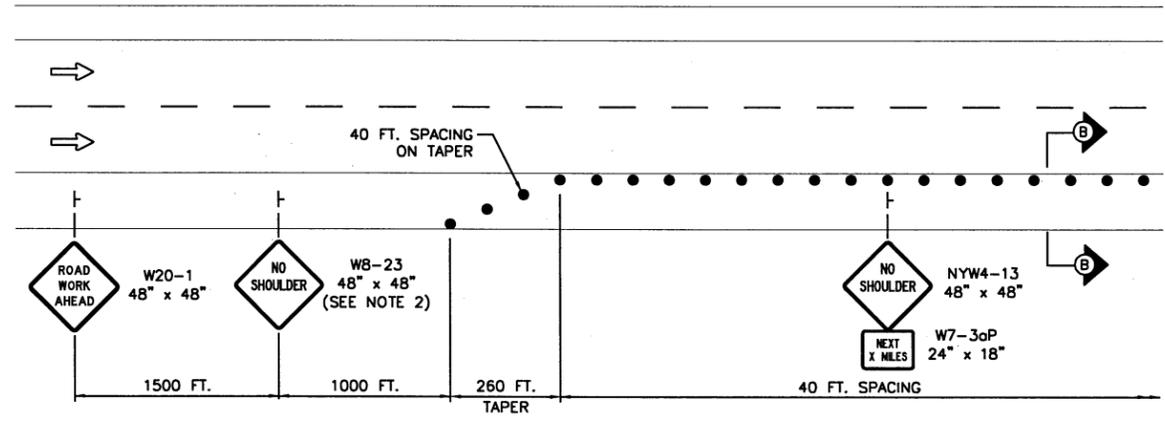
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DESIGNED BY: R. COURNOYER

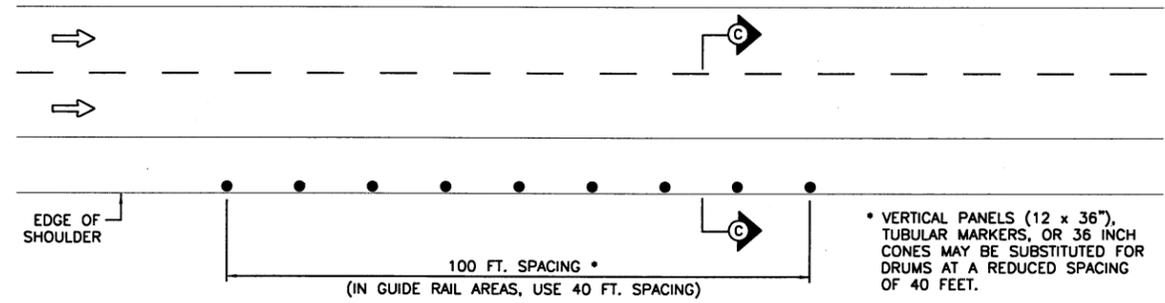
DESIGN SUPERVISOR: T. MAHAR



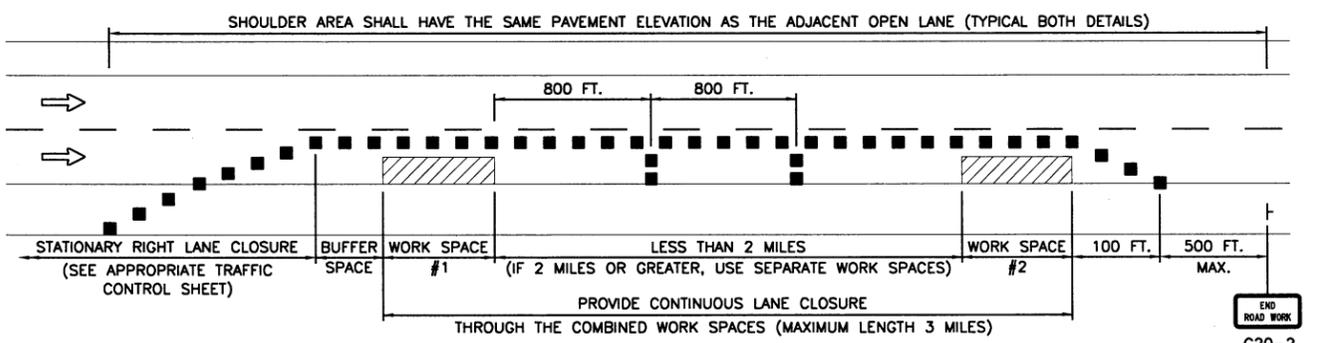
**DROP-OFF AT EDGE OF TRAVELED WAY  
DEPTH 2 INCHES TO 4 INCHES**  
N.T.S.



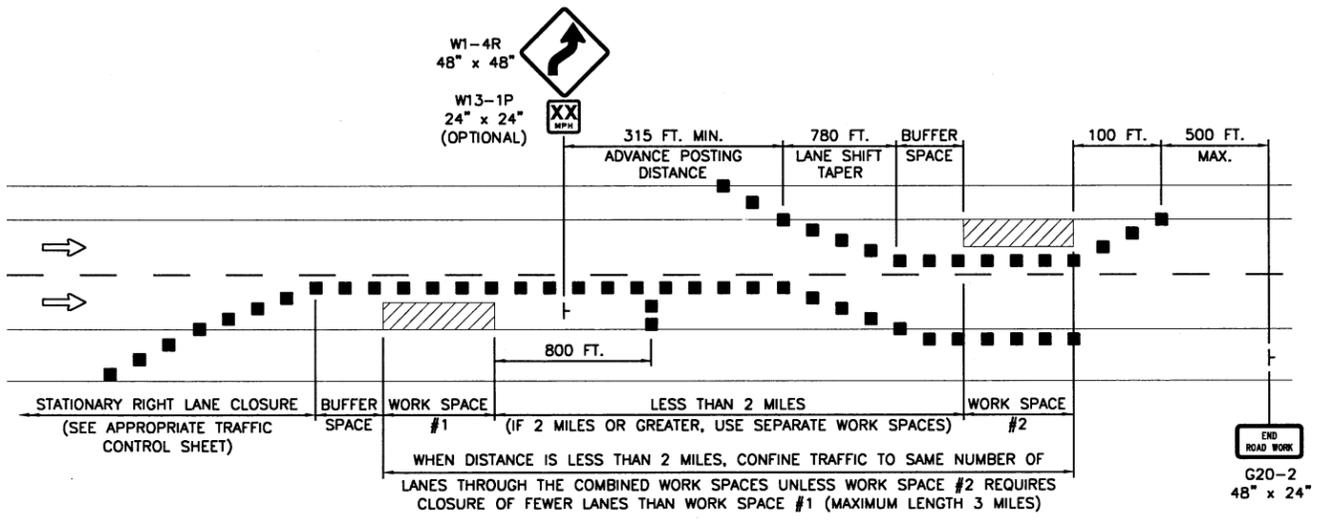
**DROP-OFF AT EDGE OF TRAVELED WAY  
DEPTH GREATER THAN 4 INCHES, BUT LESS THAN OR EQUAL TO 18 INCHES**  
N.T.S.



**DROP-OFF AT OUTSIDE EDGE OF SHOULDER  
DEPTH GREATER THAN 4 INCHES, BUT LESS THAN OR EQUAL TO 18 INCHES  
AND SHOULDER WIDTH 4 FEET OR GREATER**  
N.T.S.



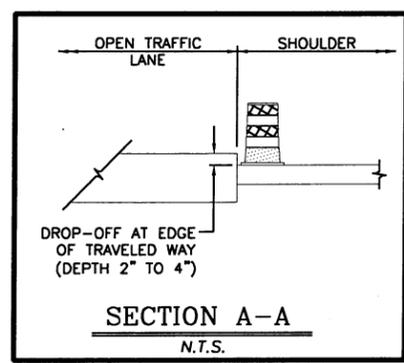
**SUCCESSIVE WORK SPACES OCCUPYING SAME LANE**  
N.T.S.



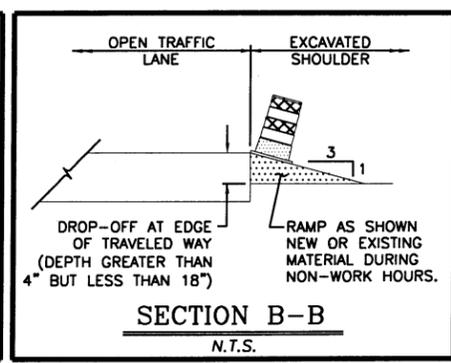
**SUCCESSIVE WORK SPACES OCCUPYING DIFFERENT LANES**  
N.T.S.

**DROP-OFF DELINEATION NOTES:**

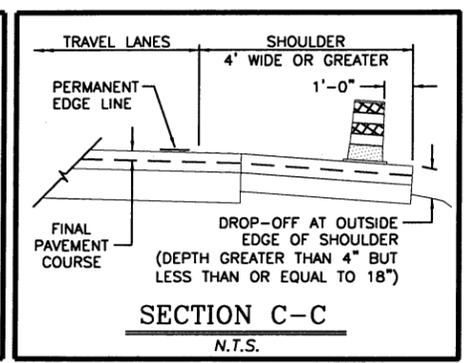
- SIGNING AND DELINEATION ARE SHOWN ALONG THE RIGHT SHOULDER. SIGNING AND DELINEATION FOR THE LEFT SHOULDER SHALL BE THE MIRROR IMAGE OF THE SAME DETAILS.
- THE "LOW SHOULDER" (WB-9) SIGN OR "NO SHOULDER" (WB-23) SIGN SHALL BE PLACED IN ADVANCE OF THE DRUM TAPER AS SHOWN. SIGNING SHALL BE REPEATED EVERY 1/2 MILE WITH "NEXT X MILES" (W7-3aP) SUPPLEMENTAL PLAQUES.
- VERTICAL PANELS (12 x 36 IN.) MAY BE SUBSTITUTED FOR DRUMS. DRUMS OR VERTICAL PANELS SHALL BE PLACED AND MAINTAINED SUCH THAT AT LEAST TWO-THIRDS OF THEIR HEIGHT IS EXPOSED ABOVE THE PAVEMENT.
- IF THE DEPTH OF EXCAVATION EXCEEDS 18 INCHES, THE ADJACENT LANE SHALL BE CLOSED OR TEMPORARY CONCRETE BARRIER SHALL BE USED TO PROTECT THE CONDITION.



**SECTION A-A**  
N.T.S.



**SECTION B-B**  
N.T.S.

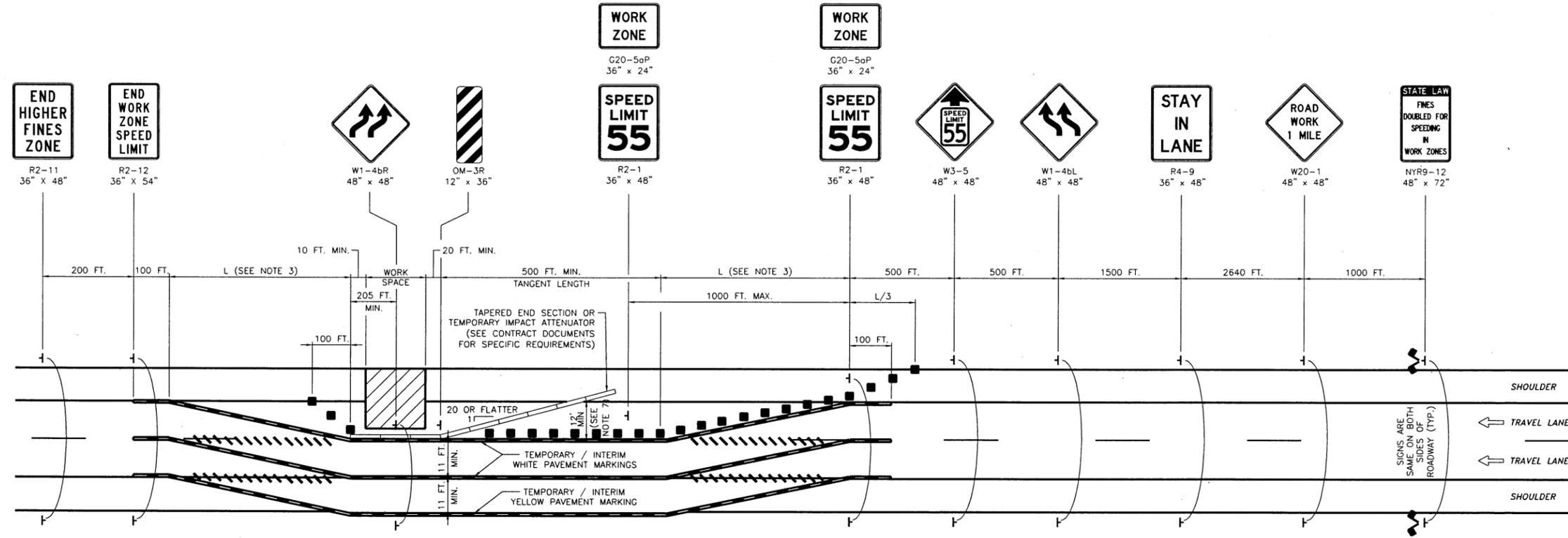


**SECTION C-C**  
N.T.S.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 02/13
TITLE OF DRAWING WORK ZONE TRAFFIC CONTROL FOR MISCELLANEOUS OPERATIONS	DRAWING NUMBER: MO



WORK ZONE TRAFFIC CONTROL PLAN  
N.T.S.

NOTES:

1. THE PLAN SHOWN IS FOR A DOUBLE LANE SHIFT TO THE LEFT. FOR A DOUBLE LANE SHIFT TO THE RIGHT, CHANGE REVERSE CURVE SIGNS W1-4bL TO W1-4bR AND W1-4bR TO W1-4bL, AND THE OM-3R SIGN TO OM-3L. THE DOUBLE LANE SHIFT PLAN TO THE RIGHT SHALL BE THE MIRROR IMAGE OF THE DOUBLE LANE SHIFT PLAN TO THE LEFT.
2. FOR LANE SHIFTS CONSISTING OF MORE THAN TWO LANES, THE NUMBER OF ARROWS DEPICTED ON THE REVERSE CURVE SIGNS SHALL BE THE SAME AS THE NUMBER OF THROUGH LANES AVAILABLE TO TRAFFIC.
3. TO DETERMINE THE TAPER LENGTH (L), SEE TABLE 6H-4 ON THE "LEGEND AND TABLES" DRAWING. IF CONSTRAINTS EXIST AND L CANNOT BE ACHIEVED, A REDUCTION IN THE TAPER LENGTH TO L/2 MAY BE USED WITH THE APPROVAL OF THE ENGINEER.
4. WHEN THE DISTANCE BETWEEN THE SECOND "WORK ZONE/SPEED LIMIT 55" SIGN (G20-5oP/R2-1) AND THE END OF THE LANE SHIFT PATTERN EXCEEDS 1/2 MILE, ADDITIONAL "WORK ZONE/SPEED LIMIT 55" SIGN(S) (G20-5oP/R2-1) SHALL BE INSTALLED ON BOTH SIDES OF THE ROADWAY TO MAINTAIN A MAXIMUM SPACING OF 1/2 MILE.
5. IN ADDITION TO THE SIGNING SHOWN, "ROAD WORK AHEAD" (W20-1) AND "WORK ZONE/SPEED LIMIT 55" (G20-5oP/R2-1) SIGNS SHALL BE PLACED ALONG ANY ENTRANCE RAMP THAT TERMINATES WITHIN THE WORK ZONE TRAFFIC CONTROL LIMITS. THE LOCATION OF THESE SIGNS SHALL BE DETERMINED BY THE ENGINEER.
6. TEMPORARY / INTERIM PAVEMENT MARKINGS SHALL BE USED IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. ANY EXISTING PAVEMENT MARKINGS THAT ARE CONFLICTING OR MISLEADING SHALL BE REMOVED OR COVERED.
7. WHEN TERMINATING THE APPROACH END OF TEMPORARY CONCRETE BARRIER (TCB) IN AREAS WITH A POSTED SPEED LIMIT OF 45 MPH OR HIGHER, AN APPROVED TEMPORARY IMPACT ATTENUATOR (REDIRECTIVE) SHALL BE USED WHEN THE BLUNT END OF THE TCB IS LESS THAN 12'-0" FROM THE WZTC EDGE OF TRAVELED WAY.

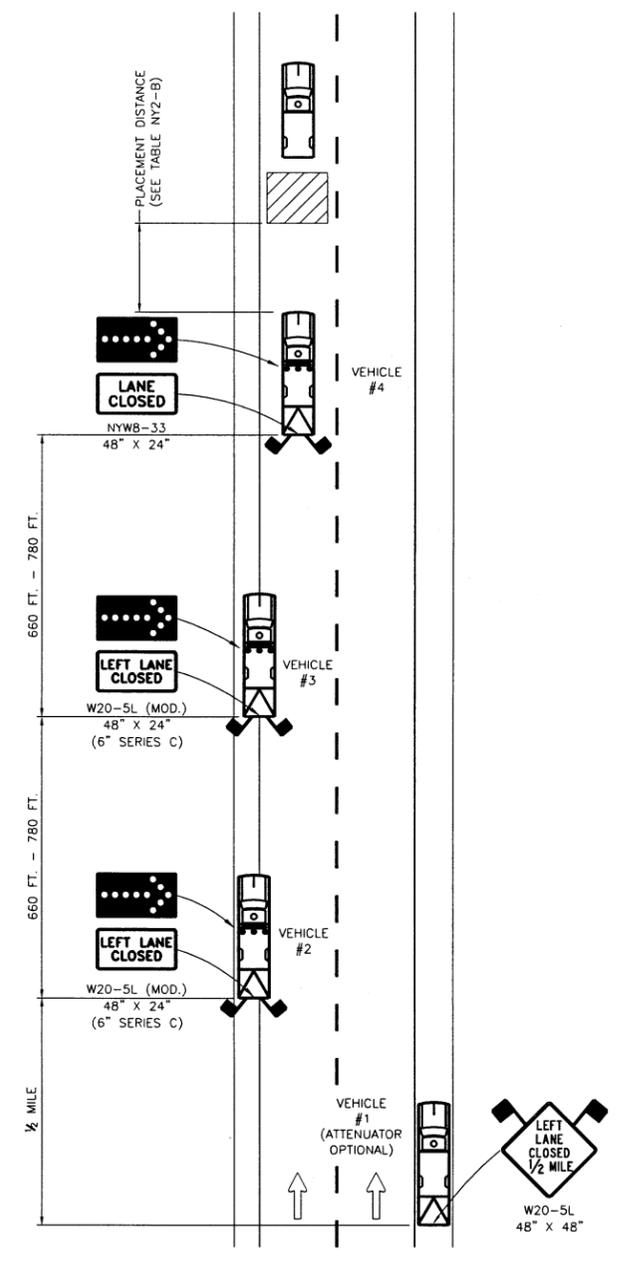
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IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE. THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

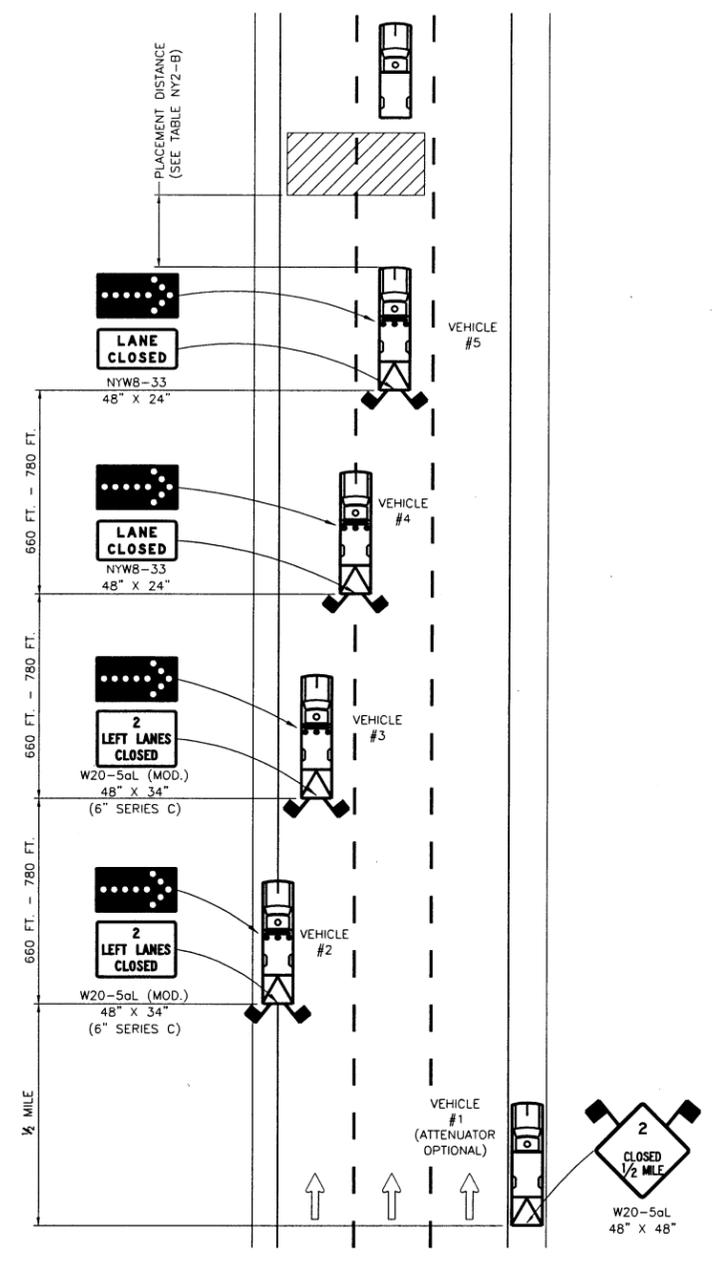
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DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 07/14
	TITLE OF DRAWING LANE SHIFT	DRAWING NUMBER: LS-55

FILE  
 CHECKED BY: TA  
 DRAFTED BY: TA  
 CHECKED BY: TA  
 DESIGNED BY: TA  
 DESIGN SUPERVISOR: TA



MOBILE OPERATION - SINGLE LANE CLOSURE  
IN NARROW SHOULDER AREA  
N.T.S.



MOBILE OPERATION - DOUBLE LANE CLOSURE  
IN NARROW SHOULDER AREA  
N.T.S.

**GENERAL NOTES:**

1. THESE PLANS ARE TO BE UTILIZED ONLY WHEN AUTHORIZED BY THE ENGINEER.
2. MOBILE OPERATIONS ARE WORK ACTIVITIES THAT MOVE CONTINUOUSLY OR STOP INTERMITTENTLY FOR SHORT PERIODS IN THE ROADWAY. THE DURATION FOR EACH INTERMITTENT STOP MAY BE APPROXIMATELY 15 MINUTES BEFORE MOVING TO A NEW LOCATION.
3. VEHICLE #4 (SINGLE LANE CLOSURE PLAN) AND VEHICLE #5 (DOUBLE LANE CLOSURE PLAN) SHALL NOT BE USED TO TRANSPORT WORKERS, MATERIALS, AND/OR EQUIPMENT TO THE WORK SITE. A SEPARATE WORK VEHICLE(S) SHALL BE REQUIRED.
4. THESE TEMPORARY TRAFFIC CONTROL PLANS SHALL NOT BE ADVANCED THROUGH AN AREA WHERE THERE IS AN EXIT OR ENTRANCE RAMP.
5. FOR VEHICLE #1, A TRUCK-MOUNTED OR TRAILER-MOUNTED PORTABLE VARIABLE MESSAGE SIGN (PVMS) MAY BE USED IN LIEU OF THE SIGN SHOWN. THE PVMS UNIT SHALL BE COMPLETELY ON THE SHOULDER AND SHALL HAVE NO PORTION PROTRUDE OVER THE TRAVEL LANE AT ANY TIME. THE MESSAGE DISPLAYED SHALL BE THE SAME AS THAT SHOWN.

**SINGLE LANE CLOSURE NOTES:**

1. THE PLAN SHOWN IS FOR A MOBILE OPERATION INVOLVING A LEFT SINGLE LANE CLOSURE IN AN AREA WHERE THE USABLE LEFT SHOULDER/MEDIAN WIDTH IS LESS THAN 8 FEET.
2. WHERE THE LEFT LANE IS TO BE CLOSED, VEHICLE #1 IS TO BE LOCATED COMPLETELY ON THE RIGHT SHOULDER, VEHICLE #2 IS AS FAR LEFT ON THE LEFT SHOULDER AS CONDITIONS PERMIT, VEHICLE #3 STRADDLES THE LEFT SHOULDER AND THE LEFT LANE, AND VEHICLE #4 IS IN THE LEFT LANE.
3. IN AREAS WHERE THERE IS LITTLE TO NO LEFT SHOULDER/MEDIAN WIDTH, VEHICLE #2 AND VEHICLE #3 SHALL BE IN THE LEFT LANE.

**DOUBLE LANE CLOSURE NOTES:**

1. THE PLAN SHOWN IS FOR A MOBILE OPERATION INVOLVING A LEFT DOUBLE LANE CLOSURE IN AN AREA WHERE THE USABLE LEFT SHOULDER/MEDIAN WIDTH IS LESS THAN 8 FEET.
2. WHERE THE LEFT TWO LANES ARE TO BE CLOSED, VEHICLE #1 IS TO BE LOCATED COMPLETELY ON THE RIGHT SHOULDER, VEHICLE #2 STRADDLES THE LEFT SHOULDER AND THE LEFT LANE, VEHICLE #3 IS IN THE LEFT LANE, VEHICLE #4 STRADDLES THE LEFT LANE AND THE CENTER LANE, AND VEHICLE #5 IS IN THE CENTER LANE.
3. IN AREAS WHERE THERE IS LITTLE TO NO LEFT SHOULDER/MEDIAN WIDTH, VEHICLE #2 SHALL BE IN THE LEFT LANE.

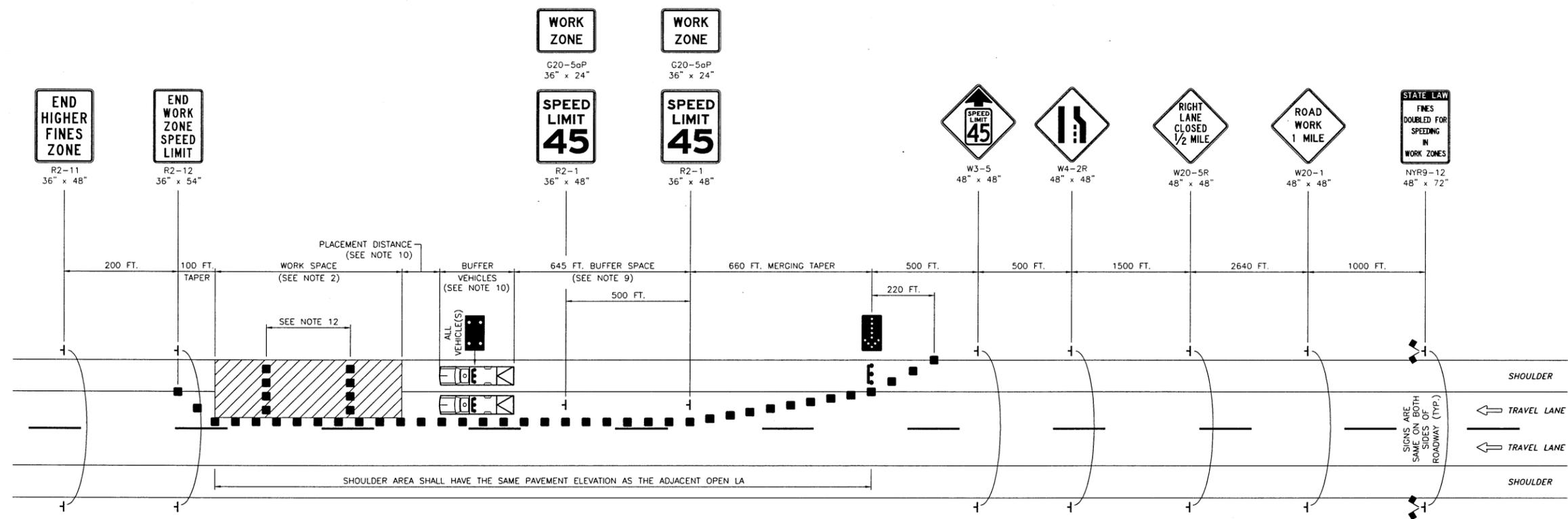
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 02/13
	TITLE OF DRAWING MOBILE LANE CLOSURE (NARROW SHOULDER AREA)	DRAWING NUMBER: MLC-NS

FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: J. SHINSKI  
 DESIGNED BY: R. COURNOYER  
 DESIGN SUPERVISOR: T. MAHAR



**WORK ZONE TRAFFIC CONTROL PLAN**  
N.T.S.

**NOTES:**

- THIS PLAN APPLIES TO TWO-, THREE-, FOUR-, AND FIVE-LANE SECTIONS.
- THE MAXIMUM LENGTH OF ANY CONTINUOUS WORK SPACE SHALL NOT EXCEED 3 MILES (2 MILES FOR MILLING AND PAVING OPERATIONS). ALL TRAFFIC SHALL BE RE-ESTABLISHED TO ITS NORMAL LANE CONFIGURATION FOR A MINIMUM 2 MILES PRIOR TO A SUCCESSIVE LANE CLOSURE. (THE SEPARATION BETWEEN SUCCESSIVE LANE CLOSURES IS MEASURED FROM THE LAST SIGN OF THE FIRST LANE CLOSURE TO THE BEGINNING OF THE MERGING TAPER OF THE SECOND LANE CLOSURE).
- THE PLAN SHOWN IS FOR A STATIONARY RIGHT LANE CLOSURE. FOR A STATIONARY LEFT LANE CLOSURE, SUBSTITUTE "LEFT LANE CLOSED 1/2 MILE" SIGN (W20-5L) FOR THE "RIGHT LANE CLOSED 1/2 MILE" SIGN (W20-5R) AND LEFT LANE ENDS SYMBOL SIGN (W4-2L) FOR THE RIGHT LANE ENDS SYMBOL SIGN (W4-2R). THE STATIONARY LEFT LANE CLOSURE PLAN SHALL BE THE MIRROR IMAGE OF THE PLAN SHOWN WITH THE EXCEPTION THAT THE "WORK ZONE/SPEED LIMIT 45" SIGNS (G20-5aP/R2-1) AND THE "SPEED LIMIT 55" SIGN (R2-1) AT THE END OF THE WORK ZONE SHALL BE INSTALLED ON THE RIGHT SIDE OF THE ROADWAY.
- FOR A STATIONARY LANE CLOSURE IN AN AREA WHERE THE USABLE LEFT SHOULDER/MEDIAN WIDTH IS LESS THAN 8 FEET, LEFT SIDE SIGNS SHALL BE MOUNTED ON THE MEDIAN BARRIER. IF THE TOTAL MEDIAN WIDTH IS LESS THAN 6 FEET, LEFT SIDE SIGNS SHALL NOT BE REQUIRED.
- ON ROADWAY SECTIONS WHERE THE USABLE SHOULDER IS LESS THAN 8 FEET, A MOBILE LANE CLOSURE PLAN SHALL BE USED TO INSTALL AND REMOVE TEMPORARY TRAFFIC CONTROL DEVICES. THE SAME SHALL ALSO APPLY WHEN COVERING AND UNCOVERING PREVIOUSLY INSTALLED SIGNS.
- WHEN TRAFFIC IS REDUCED TO A SINGLE LANE, THE "WORK ZONE/SPEED LIMIT 45" SIGNS (G20-5aP/R2-1) AND THE "SPEED LIMIT 55" SIGN (R2-1) AT THE END OF THE WORK ZONE SHALL BE INSTALLED ON THE RIGHT SIDE OF THE ROADWAY ONLY. IF MULTIPLE LANES ARE OPEN TO TRAFFIC, THE SIGNS SHALL BE INSTALLED ON BOTH SIDES OF THE ROADWAY.
- WHEN THE DISTANCE BETWEEN THE SECOND "WORK ZONE/SPEED LIMIT 45" SIGN (G20-5aP/R2-1) AND THE END OF THE WORK SPACE EXCEEDS 1/2 MILE, ADDITIONAL "WORK ZONE/SPEED LIMIT 45" SIGN(S) (G20-5aP/R2-1) SHALL BE INSTALLED ON THE RIGHT SIDE OF THE OPEN TRAVEL LANE, OR BOTH SIDES OF MULTIPLE OPEN TRAVEL LANES, TO MAINTAIN A MAXIMUM SPACING OF 1/2 MILE.
- IN ADDITION TO THE SIGNING SHOWN, "ROAD WORK AHEAD" (W20-1) AND "WORK ZONE/SPEED LIMIT 45" (G20-5aP/R2-1) SIGNS SHALL BE PLACED ALONG ANY ENTRANCE RAMP THAT TERMINATES WITHIN THE WORK ZONE TRAFFIC CONTROL LIMITS. THE LOCATION OF THESE SIGNS SHALL BE DETERMINED BY THE ENGINEER.
- THE LENGTH OF THE BUFFER SPACE SHALL BE EXTENDED, AS ORDERED BY THE ENGINEER, TO ENSURE ADEQUATE SIGHT DISTANCE FOR VEHICLES APPROACHING THE LANE CLOSURE TAPER. IN CASES WHERE RESTRICTIVE FEATURES ARE PRESENT, A REDUCTION IN THE BUFFER SPACE LENGTH MAY BE PERMITTED WITH THE APPROVAL OF THE ENGINEER.
- BARRIER VEHICLES SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 619 OF THE STANDARD SPECIFICATIONS. FOR BARRIER VEHICLE USE AND PLACEMENT REQUIREMENTS, SEE TABLES NY1-A AND NY2-A ON THE "WORK ZONE TRAFFIC CONTROL TABLES AND LEGEND" DRAWING.
- EXISTING PAVEMENT MARKINGS SHALL BE MAINTAINED BY THE CONTRACTOR WITHIN THE WORK ZONE TRAFFIC CONTROL LIMITS.
- IN LONG WORK SPACES (1500 FEET AND GREATER), TWO DRUMS, TWO TALL CONES, OR TWO OVERSIZED VERTICAL PANELS SHALL BE PLACED TRANSVERSELY ACROSS EACH CLOSED LANE (AND SHOULDER IF WIDTH IS 8 FEET OR GREATER) AT MAXIMUM INTERVALS OF 800 FEET. IN ADDITION, TWO DRUMS, TWO TALL CONES, OR TWO OVERSIZED VERTICAL PANELS SHALL BE SIMILARLY PLACED IN ADVANCE OF ANY TRANSVERSE DROP-OFF OF 1 1/2 INCHES OR GREATER.

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REVISIONS			
DATE	DESCRIPTION	BY	SYM

<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 02/13
	TITLE OF DRAWING SINGLE LANE CLOSURE (SHORT- OR INTERMEDIATE- TERM STATIONARY)	DRAWING NUMBER: SLC-55

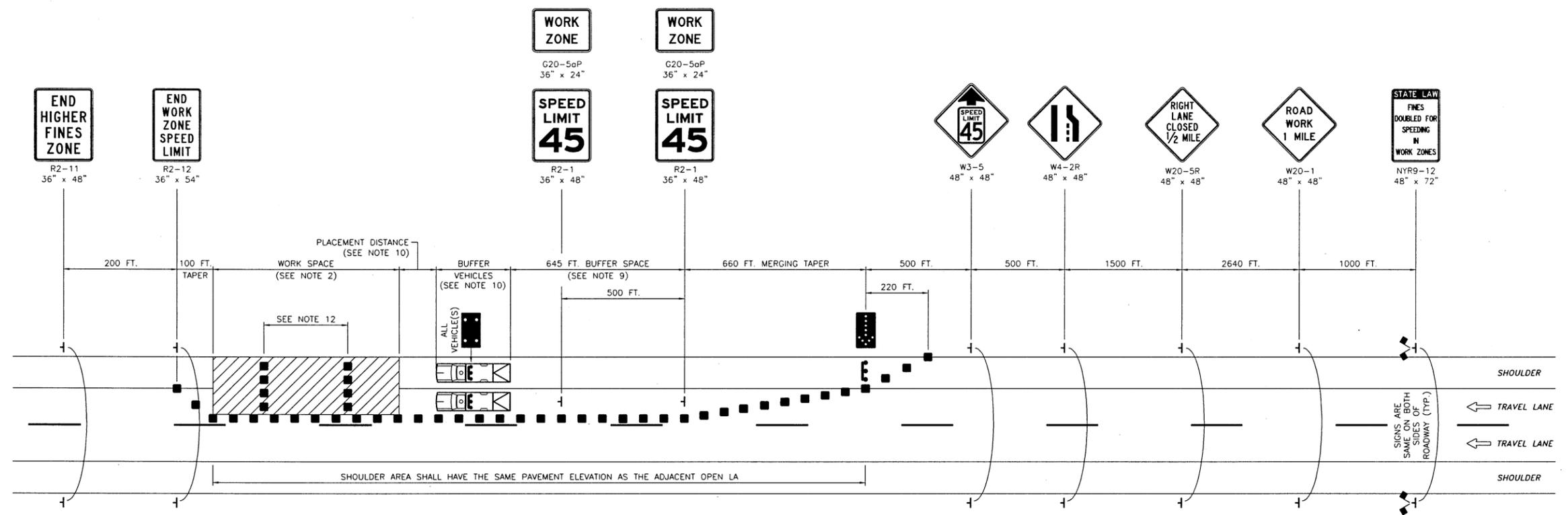
CHECKED BY: TA

DRAFTED BY: M. COX

CHECKED BY: J. SHINSKI

DESIGNED BY: R. COURNOYER

DESIGN SUPERVISOR: T. MAHAR



WORK ZONE TRAFFIC CONTROL PLAN  
N.T.S.

NOTES:

- THIS PLAN APPLIES TO TWO-, THREE-, FOUR-, AND FIVE-LANE SECTIONS.
- THE MAXIMUM LENGTH OF ANY CONTINUOUS WORK SPACE SHALL NOT EXCEED 3 MILES (2 MILES FOR MILLING AND PAVING OPERATIONS). ALL TRAFFIC SHALL BE RE-ESTABLISHED TO ITS NORMAL LANE CONFIGURATION FOR A MINIMUM 2 MILES PRIOR TO A SUCCESSIVE LANE CLOSURE. (THE SEPARATION BETWEEN SUCCESSIVE LANE CLOSURES IS MEASURED FROM THE LAST SIGN OF THE FIRST LANE CLOSURE TO THE BEGINNING OF THE MERGING TAPER OF THE SECOND LANE CLOSURE).
- THE PLAN SHOWN IS FOR A STATIONARY RIGHT LANE CLOSURE. FOR A STATIONARY LEFT LANE CLOSURE, SUBSTITUTE "LEFT LANE CLOSED 1/2 MILE" SIGN (W20-5L) FOR THE "RIGHT LANE CLOSED 1/2 MILE" SIGN (W20-5R) AND LEFT LANE ENDS SYMBOL SIGN (W4-2L) FOR THE RIGHT LANE ENDS SYMBOL SIGN (W4-2R). THE STATIONARY LEFT LANE CLOSURE PLAN SHALL BE THE MIRROR IMAGE OF THE PLAN SHOWN WITH THE EXCEPTION THAT THE "WORK ZONE/SPEED LIMIT 45" SIGNS (G20-50P/R2-1) AND THE "SPEED LIMIT 55" SIGN (R2-1) AT THE END OF THE WORK ZONE SHALL BE INSTALLED ON THE RIGHT SIDE OF THE ROADWAY.
- FOR A STATIONARY LANE CLOSURE IN AN AREA WHERE THE USABLE LEFT SHOULDER/MEDIAN WIDTH IS LESS THAN 6 FEET, LEFT SIDE SIGNS SHALL BE MOUNTED ON THE MEDIAN BARRIER. IF THE TOTAL MEDIAN WIDTH IS LESS THAN 6 FEET, LEFT SIDE SIGNS SHALL NOT BE REQUIRED.
- ON ROADWAY SECTIONS WHERE THE USABLE SHOULDER IS LESS THAN 8 FEET, A MOBILE LANE CLOSURE PLAN SHALL BE USED TO INSTALL AND REMOVE TEMPORARY TRAFFIC CONTROL DEVICES. THE SAME SHALL ALSO APPLY WHEN COVERING AND UNCOVERING PREVIOUSLY INSTALLED SIGNS.
- WHEN TRAFFIC IS REDUCED TO A SINGLE LANE, THE "WORK ZONE/SPEED LIMIT 45" SIGNS (G20-50P/R2-1) AND THE "SPEED LIMIT 55" SIGN (R2-1) AT THE END OF THE WORK ZONE SHALL BE INSTALLED ON THE RIGHT SIDE OF THE ROADWAY ONLY. IF MULTIPLE LANES ARE OPEN TO TRAFFIC, THE SIGNS SHALL BE INSTALLED ON BOTH SIDES OF THE ROADWAY.
- WHEN THE DISTANCE BETWEEN THE SECOND "WORK ZONE/SPEED LIMIT 45" SIGN (G20-50P/R2-1) AND THE END OF THE WORK SPACE EXCEEDS 1/2 MILE, ADDITIONAL "WORK ZONE/SPEED LIMIT 45" SIGN(S) (G20-50P/R2-1) SHALL BE INSTALLED ON THE RIGHT SIDE OF THE OPEN TRAVEL LANE, OR BOTH SIDES OF MULTIPLE OPEN TRAVEL LANES, TO MAINTAIN A MAXIMUM SPACING OF 1/2 MILE.
- IN ADDITION TO THE SIGNING SHOWN, "ROAD WORK AHEAD" (W20-1) AND "WORK ZONE/SPEED LIMIT 45" (G20-50P/R2-1) SIGNS SHALL BE PLACED ALONG ANY ENTRANCE RAMP THAT TERMINATES WITHIN THE WORK ZONE TRAFFIC CONTROL LIMITS. THE LOCATION OF THESE SIGNS SHALL BE DETERMINED BY THE ENGINEER.
- THE LENGTH OF THE BUFFER SPACE SHALL BE EXTENDED, AS ORDERED BY THE ENGINEER, TO ENSURE ADEQUATE SIGHT DISTANCE FOR VEHICLES APPROACHING THE LANE CLOSURE TAPER. IN CASES WHERE RESTRICTIVE FEATURES ARE PRESENT, A REDUCTION IN THE BUFFER SPACE LENGTH MAY BE PERMITTED WITH THE APPROVAL OF THE ENGINEER.
- BARRIER VEHICLES SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 619 OF THE STANDARD SPECIFICATIONS. FOR BARRIER VEHICLE USE AND PLACEMENT REQUIREMENTS, SEE TABLES NY1-A AND NY2-A ON THE "WORK ZONE TRAFFIC CONTROL TABLES AND LEGEND" DRAWING.
- EXISTING PAVEMENT MARKINGS SHALL BE MAINTAINED BY THE CONTRACTOR WITHIN THE WORK ZONE TRAFFIC CONTROL LIMITS.
- IN LONG WORK SPACES (1500 FEET AND GREATER), TWO DRUMS, TWO TALL CONES, OR TWO OVERSIZED VERTICAL PANELS SHALL BE PLACED TRANSVERSELY ACROSS EACH CLOSED LANE (AND SHOULDER IF WIDTH IS 8 FEET OR GREATER) AT MAXIMUM INTERVALS OF 800 FEET. IN ADDITION, TWO DRUMS, TWO TALL CONES, OR TWO OVERSIZED VERTICAL PANELS SHALL BE SIMILARLY PLACED IN ADVANCE OF ANY TRANSVERSE DROP-OFF OF 1 1/2 INCHES OR GREATER.

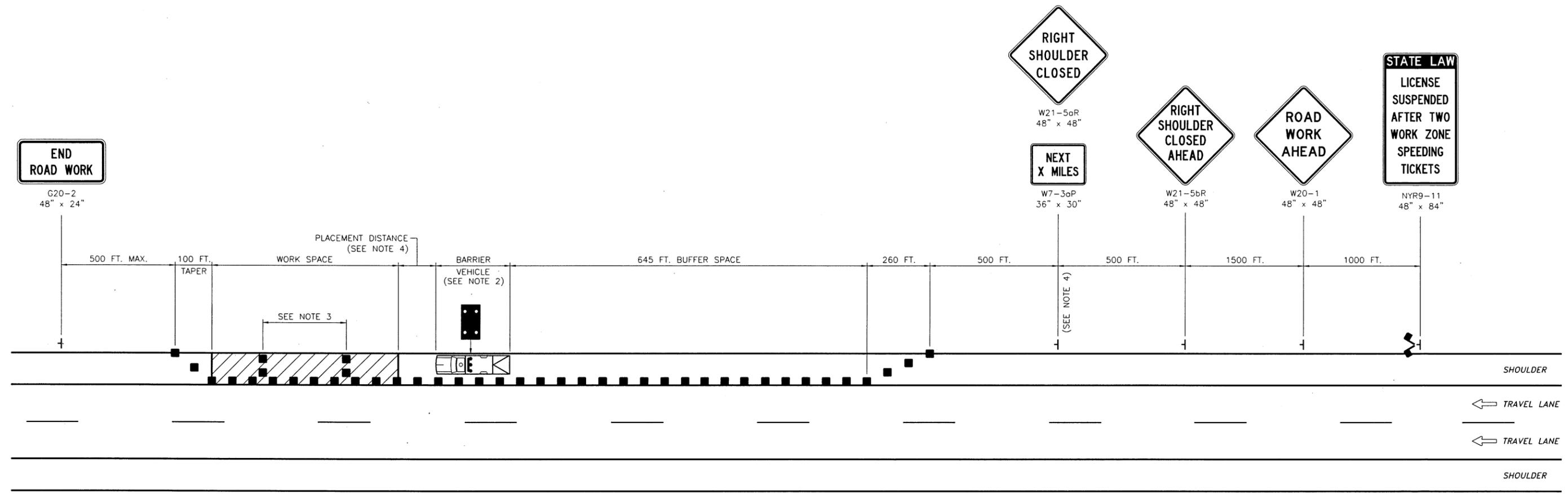
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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b> <b>Canal Corporation</b>	TITLE OF PROJECT BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD - MENDON CENTER RD., MONROE COUNTY	DATE: 02/13
	TITLE OF DRAWING SINGLE LANE CLOSURE (SHORT- OR INTERMEDIATE-TERM STATIONARY)	DRAWING NUMBER: SLC-65

FILE  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: J. SHINSKI  
 DESIGNED BY: R. COURNOYER  
 DESIGN SUPERVISOR: T. MAHAR



**WORK ZONE TRAFFIC CONTROL PLAN**  
N.T.S.

**NOTES:**

1. THE PLAN SHOWN IS FOR A SHORT- OR INTERMEDIATE-TERM STATIONARY RIGHT SHOULDER CLOSURE. FOR A SHORT- OR INTERMEDIATE-TERM STATIONARY LEFT SHOULDER CLOSURE, SUBSTITUTE "LEFT SHOULDER CLOSED AHEAD" SIGN (W21-5bL) FOR THE "RIGHT SHOULDER CLOSED AHEAD" SIGN (W21-5bR) AND "LEFT SHOULDER CLOSED" SIGN (W21-5aL) FOR THE "RIGHT SHOULDER CLOSED" SIGN (W21-5aR). THE SHORT- OR INTERMEDIATE-TERM STATIONARY LEFT SHOULDER CLOSURE PLAN SHALL BE THE MIRROR IMAGE OF THE PLAN SHOWN.
2. THE "NEXT X MILES" SUPPLEMENTAL SIGN (W7-3aP) IS REQUIRED WHEN THE SHOULDER IS CLOSED FOR A DISTANCE GREATER THAN 2 MILES.
3. IN LONG WORK SPACES (1500 FEET AND GREATER) ON PAVED SHOULDERS HAVING A WIDTH OF 8 FEET OR GREATER, TWO DRUMS, TWO TALL CONES, OR TWO OVERSIZED VERTICAL PANELS SHALL BE PLACED TRANSVERSELY ACROSS THE CLOSED SHOULDER AT MAXIMUM INTERVALS OF 800 FEET. IN ADDITION, TWO DRUMS, TWO TALL CONES, OR TWO OVERSIZED VERTICAL PANELS SHALL BE SIMILARLY PLACED IN ADVANCE OF ANY TRANSVERSE DROP-OFF OF 1 1/2 INCHES OR GREATER.
4. THE BARRIER VEHICLE SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 619 OF THE STANDARD SPECIFICATIONS. FOR BARRIER VEHICLE USE AND PLACEMENT REQUIREMENTS, SEE TABLES NY1-A AND NY2-A ON THE "WORK ZONE TRAFFIC CONTROL TABLES AND LEGEND" DRAWING. THE BARRIER VEHICLE SHALL BE LOCATED COMPLETELY ON THE SHOULDER.

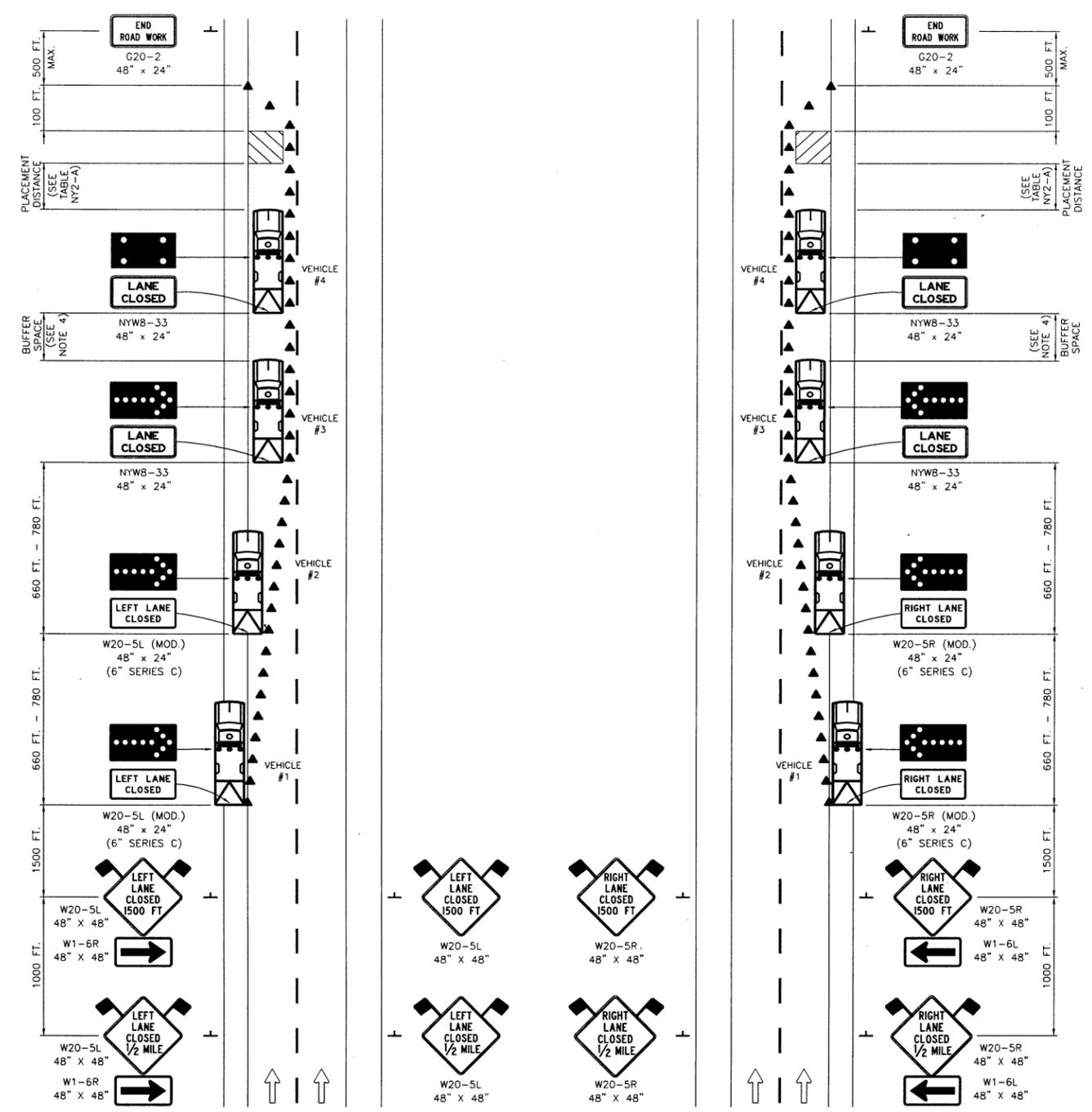
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<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 02/13
	TITLE OF DRAWING WZTC PLAN	DRAWING NUMBER: WZTCP 1

FILED  
 CHECKED BY: TA  
 DRAFTED BY: TA  
 CHECKED BY: TA  
 DESIGNED BY: TA  
 DESIGN SUPERVISOR: TA



SHORT DURATION LEFT LANE CLOSURE  
N.T.S.

SHORT DURATION RIGHT LANE CLOSURE  
N.T.S.

**GENERAL NOTES:**

1. USE OF THESE PLANS SHALL BE LIMITED TO A WORK SPACE LENGTH OF 1/2 MILE OR LESS AND A DAYTIME WORK DURATION OF UP TO 2 HOURS, OR NIGHTTIME WORK DURATION UP TO 1 HOUR.
2. TRAILER-MOUNTED ARROW PANELS MAY BE SUBSTITUTED FOR VEHICLES #1, #2, AND #3. TOW VEHICLES SHALL NOT REMAIN ATTACHED TO TRAILER-MOUNTED ARROW PANELS.
3. FOR VEHICLE #1, THE ATTENUATOR IS OPTIONAL FOR DAYTIME OPERATIONS. FOR NIGHTTIME OPERATIONS, ALL VEHICLES, INCLUDING VEHICLE #1 ON THE SHOULDER, SHALL BE EQUIPPED WITH AN ATTENUATOR.
4. THE MINIMUM BUFFER SPACE LENGTH SHALL BE THE PLACEMENT DISTANCE REQUIRED FOR BARRIER VEHICLES (SEE TABLE NY2-A). THE LENGTH OF THE BUFFER SPACE SHALL BE EXTENDED, AS DETERMINED BY THE ENGINEER, TO ENSURE ADEQUATE SIGHT DISTANCE FOR VEHICLES APPROACHING THE LANE CLOSURE TAPER.

**LEFT LANE CLOSURE NOTES:**

1. WHERE THE LEFT LANE IS TO BE CLOSED, VEHICLE #1 IS TO BE LOCATED COMPLETELY ON THE LEFT SHOULDER, VEHICLE #2 STRADDLES THE LEFT SHOULDER AND THE LEFT LANE, AND VEHICLE #3 IS IN THE LEFT LANE.
2. FOR A LEFT LANE CLOSURE IN AN AREA WHERE THE USABLE LEFT SHOULDER/MEDIAN WIDTH IS LESS THAN 6 FEET, LEFT SIDE SIGNS SHALL BE MOUNTED ON THE MEDIAN BARRIER. IF THE TOTAL MEDIAN WIDTH IS LESS THAN 6 FEET, LEFT SIDE SIGNS IN ADVANCE OF THE LANE CLOSURE TAPER AND THE "END ROAD WORK" (G20-2) SIGN SHALL NOT BE REQUIRED.
3. FOR A LEFT LANE CLOSURE IN AN AREA WHERE THE USABLE LEFT SHOULDER/MEDIAN WIDTH IS LESS THAN 8 FEET, VEHICLE #1 IS AS FAR LEFT ON THE LEFT SHOULDER AS CONDITIONS PERMIT, VEHICLE #2 STRADDLES THE LEFT SHOULDER AND THE LEFT LANE, AND VEHICLE #3 IS IN THE LEFT LANE.
4. IN AREAS WHERE THERE IS LITTLE TO NO LEFT SHOULDER/MEDIAN WIDTH, VEHICLE #1 AND VEHICLE #2 SHALL BE IN THE LEFT LANE. FOR THESE SITUATIONS, CONSULT THE ENGINEER FOR APPROPRIATE TEMPORARY TRAFFIC CONTROL UPGRADES.

**RIGHT LANE CLOSURE NOTES:**

1. WHERE THE RIGHT LANE IS TO BE CLOSED, VEHICLE #1 IS TO BE LOCATED COMPLETELY ON THE RIGHT SHOULDER, VEHICLE #2 STRADDLES THE RIGHT SHOULDER AND THE RIGHT LANE, AND VEHICLE #3 IS IN THE RIGHT LANE.
2. FOR A RIGHT LANE CLOSURE IN AN AREA WHERE THE USABLE LEFT SHOULDER/MEDIAN WIDTH IS LESS THAN 6 FEET, LEFT SIDE SIGNS SHALL BE MOUNTED ON THE MEDIAN BARRIER. IF THE TOTAL MEDIAN WIDTH IS LESS THAN 6 FEET, LEFT SIDE SIGNS SHALL NOT BE REQUIRED.

**LEGEND**

▲ TALL TRAFFIC CONES  
 @ 40 FT. SPACING ON TAPER AND TANGENT

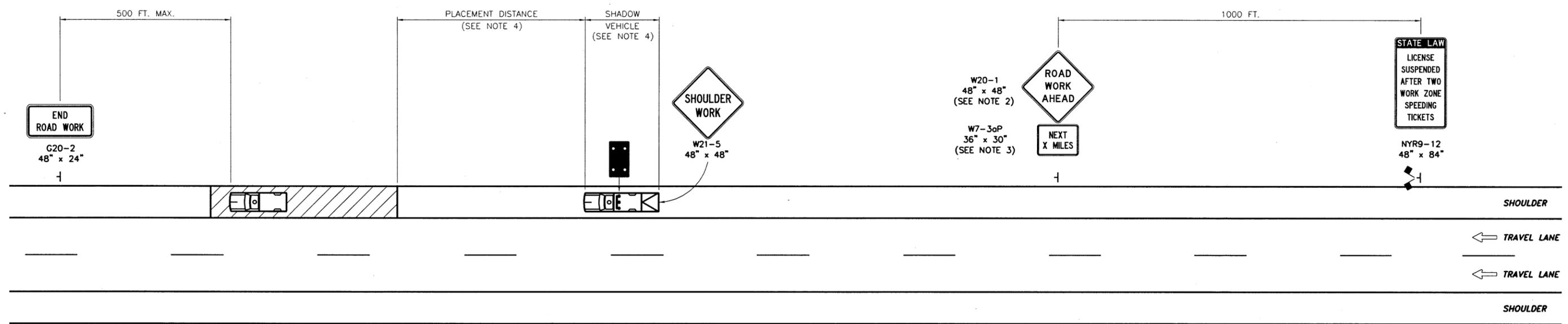
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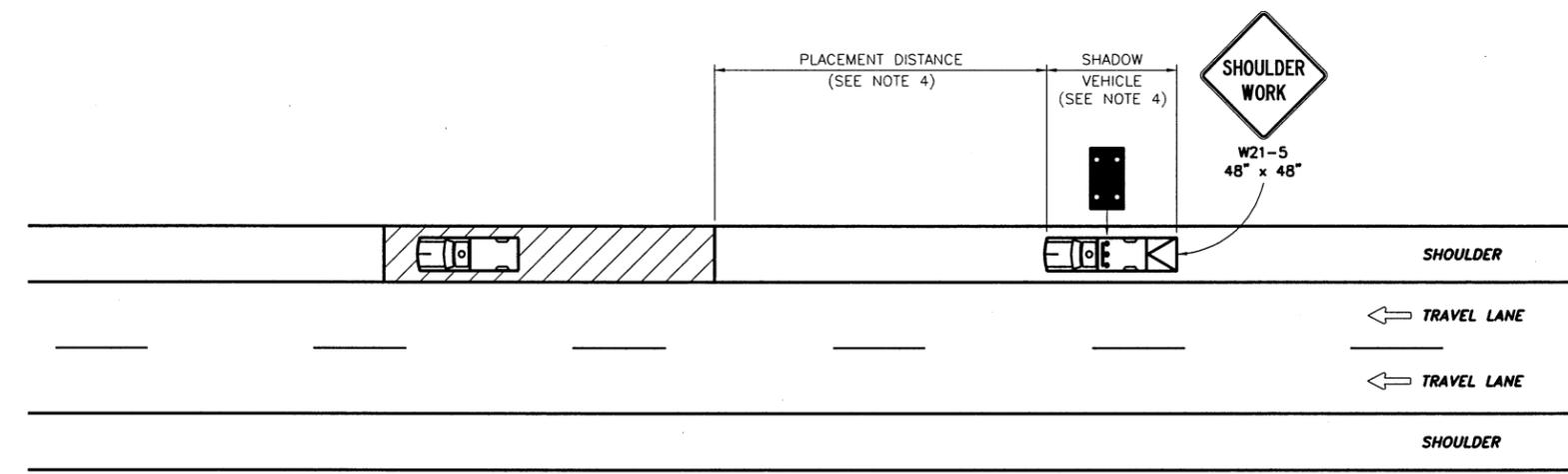
REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 02/13
	TITLE OF DRAWING SHORT TERM LANE CLOSURE	DRAWING NUMBER: STLC -1

FILE  
 CHECKED BY: TA  
 DRAFTED BY: TA  
 CHECKED BY: J. SHINSKI  
 DESIGNED BY: R. COURNOYER  
 DESIGN SUPERVISOR: T. MAHAR



**SHORT-DURATION STATIONARY OR MOBILE SHOULDER CLOSURE  
 (MULTIPLE WORK LOCATIONS)**  
 N.T.S.



**MOBILE OPERATION ON SHOULDER**  
 N.T.S.

**NOTES:**

1. THE PLANS SHOWN ARE FOR SHORT-DURATION STATIONARY WORK OR MOBILE OPERATIONS ON THE RIGHT SHOULDER. THE LEFT SHOULDER PLANS SHALL BE THE MIRROR IMAGE OF THOSE SHOWN.
2. IN SITUATIONS WHERE MULTIPLE WORK LOCATIONS WITHIN A LIMITED DISTANCE MAKE IT PRACTICAL TO PLACE STATIONARY SIGNS, THE DISTANCE BETWEEN THE ADVANCE WARNING SIGN AND THE WORK SHALL NOT EXCEED 5 MILES.
3. IN SITUATIONS WHERE THE DISTANCE BETWEEN THE ADVANCE WARNING SIGNS AND THE WORK IS 2 MILES TO 5 MILES, THE "NEXT X MILES" SUPPLEMENTAL SIGN (W7-3aP) SHALL BE USED WITH THE "ROAD WORK AHEAD" SIGN (W20-1).
4. THE SHADOW VEHICLE SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 619 OF THE STANDARD SPECIFICATIONS. FOR SHADOW VEHICLE USE AND PLACEMENT REQUIREMENTS, SEE TABLES NY1-B AND NY2-B ON THE "WORK ZONE TRAFFIC CONTROL TABLES AND LEGEND" DRAWING. THE SHADOW VEHICLE SHALL BE LOCATED COMPLETELY ON THE SHOULDER.

REVISIONS			
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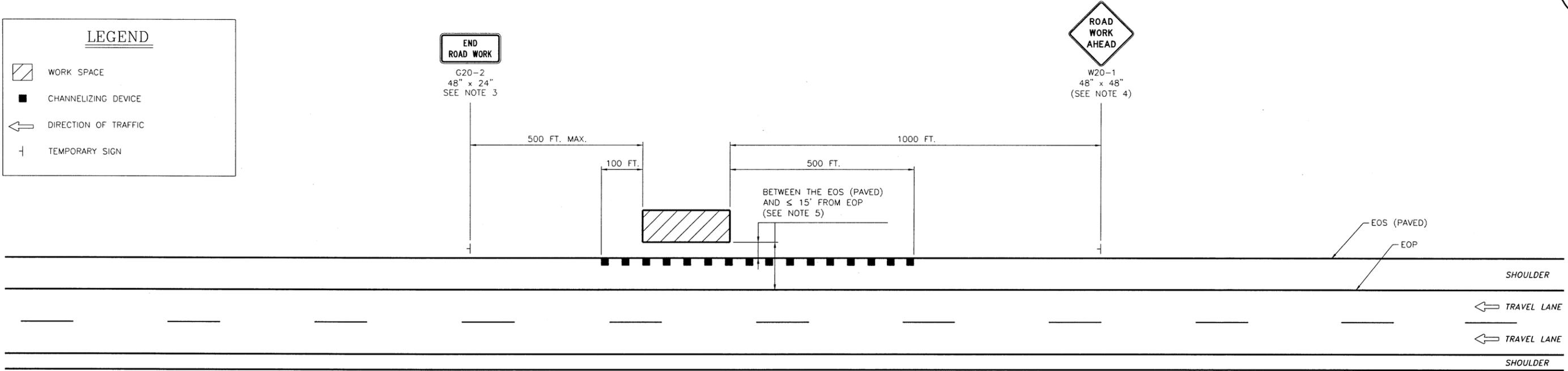


TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 02/13
TITLE OF DRAWING SHOULDER CLOSURE (SHORT-DURATION STATIONARY AND MOBILE)	DRAWING NUMBER: SCM

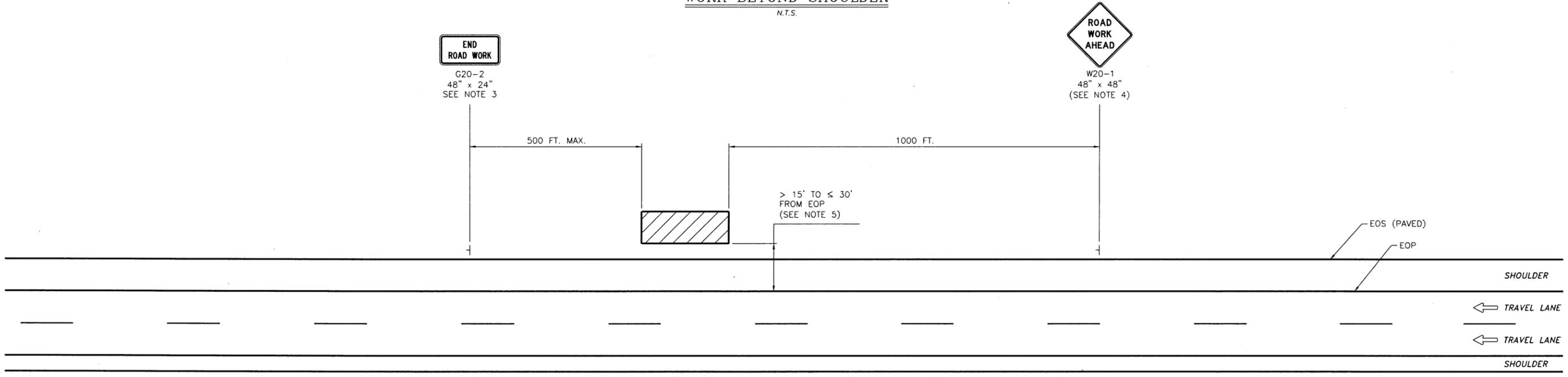
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 DRAFTED BY: TA  
 CHECKED BY: J. SHINSKI  
 DESIGNED BY: R. COURNOYER  
 DESIGN SUPERVISOR: T. MAHAR

**LEGEND**

- WORK SPACE
- CHANNELIZING DEVICE
- DIRECTION OF TRAFFIC
- TEMPORARY SIGN



**OPTION A  
WORK BEYOND SHOULDER**  
N.T.S.



**OPTION B  
WORK BEYOND SHOULDER**  
N.T.S.

**NOTES:**

1. THE PLAN SHOWN IS FOR STATIONARY WORK BEING PERFORMED OFF THE ROADWAY (BEYOND THE SHOULDER, BUT WITHIN THE R.O.W.). NO TRAFFIC CONTROL IS REQUIRED WHERE THE WORK SPACE IS BEHIND A BARRIER OR CONFINED TO AN AREA MORE THAN 30' FROM THE EDGE OF TRAVEL WAY.
2. WHEN THE WORK SPACE IS IN THE MEDIAN AND AN ADVANCE "ROAD WORK AHEAD" SIGN IS REQUIRED, THE SIGN SHALL BE PLACED ON THE LEFT SIDE OF THE DIRECTIONAL ROADWAY.
3. THE "END ROAD WORK" SIGN IS OPTIONAL IF THE WORK DURATION IS TWO HOURS OR LESS.
4. A "SHOULDER WORK" W21-5 (48" x 48") MAY BE SUBSTITUTED FOR THE "ROAD WORK AHEAD" SIGN.
5. DURING NON-WORKING HOURS, ALL EQUIPMENT AND MATERIALS SHALL BE STORED AT LEAST THIRTY (30) FEET FROM THE EDGE OF PAVEMENT (BOTH MAINLINE AND RAMP) OR BE PROTECTED BY A PHYSICAL BARRIER AS APPROVED BY THE ENGINEER.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

TITLE OF PROJECT <b>NYS THRUWAY BRIDGE REHABILITATION</b>	CONTRACT NUMBER: <b>TAB 16-33B</b>
LOCATION OF PROJECT <b>TWY M.P. 356.69 &amp; 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY</b>	DATE: <b>02/13</b>
TITLE OF DRAWING <b>WORK BEYOND SHOULDER</b>	DRAWING NUMBER: <b>WBS</b>

TABLE NY1-A BARRIER VEHICLE USE REQUIREMENTS (LONG TERM, INTERMEDIATE TERM & SHORT TERM STATIONARY CLOSURES)					
CLOSURE TYPE	EXPOSURE CONDITION <sup>1</sup>	USE REQUIREMENTS <sup>4, 5</sup>			
		FREEWAY	NON-FREEWAY (PRECONSTRUCTION POSTED SPEED LIMIT)		
			≥ 45 MPH	35-40 MPH	≤ 30 MPH
LANE CLOSURE	WORKERS ON FOOT OR IN VEHICLES EXPOSED TO TRAFFIC	REQUIRED <sup>3</sup>	REQUIRED <sup>3</sup>	REQUIRED <sup>3</sup>	OPTIONAL <sup>2</sup>
	NON-TRAVERSABLE HAZARD (IE. EQUIPMENT, MATERIALS, EXCAVATION) ONLY NO WORKERS EXPOSED	REQUIRED <sup>3</sup>	REQUIRED <sup>3</sup>	OPTIONAL <sup>2</sup>	OPTIONAL <sup>2</sup>
SHOULDER CLOSURE	WORKERS ON FOOT OR IN VEHICLES EXPOSED TO TRAFFIC	REQUIRED <sup>3</sup>	REQUIRED <sup>3</sup>	OPTIONAL <sup>2</sup>	OPTIONAL <sup>2</sup>
	NON-TRAVERSABLE HAZARD (IE. EQUIPMENT, MATERIALS, EXCAVATION) ONLY NO WORKERS EXPOSED	REQUIRED <sup>3</sup>	OPTIONAL <sup>2</sup>	OPTIONAL <sup>2</sup>	OPTIONAL <sup>2</sup>

1. THE EXPOSURE CONDITIONS DESCRIBED IN TABLE NY1-A ASSUMES THERE IS NO POSITIVE PROTECTION (TEMPORARY TRAFFIC BARRIER) PRESENT. WHERE WORKERS OR HAZARDS ARE PROTECTED BY A TEMPORARY TRAFFIC BARRIER, BARRIER VEHICLES ARE NOT REQUIRED.
2. WHERE THE REQUIREMENT IS "OPTIONAL", EITHER A BARRIER VEHICLE OR THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE 6C-2) SHALL BE PROVIDED.
3. REQUIREMENTS SHALL INCLUDE PROVIDING A SEPARATE BARRIER VEHICLE FOR EACH CLOSED LANE AND EACH CLOSED SHOULDER 8 FEET OR GREATER IN WIDTH. IF THE WORK SPACE MOVES WITHIN THE STATIONARY CLOSURE, THE BARRIER VEHICLE SHALL BE REPOSITIONED ACCORDINGLY. BARRIER VEHICLES PROTECTING NON-TRAVERSABLE HAZARDS SHALL REMAIN IN PLACE DURING BOTH WORKING AND NON-WORKING HOURS UNTIL THE HAZARD NO LONGER EXISTS. EXCEPTIONS TO THESE REQUIREMENTS MAY BE MADE, AS APPROVED BY THE ENGINEER, WHERE BARRIER VEHICLE PLACEMENT WOULD BE INEFFECTIVE OR WOULD INTERFERE WITH THE SAFE OPERATION OF TRAFFIC.
4. BARRIER VEHICLES ARE NOT REQUIRED FOR MILLING AND/OR PAVING OPERATIONS, BUT THE STANDARD LONGITUDINAL BUFFER SPACE (TABLE 6C-2) SHALL BE PROVIDED.
5. BARRIER VEHICLES ARE NOT REQUIRED FOR FLAGGING OPERATIONS.

TABLE NY1-B SHADOW VEHICLE USE REQUIREMENTS (MOBILE CLOSURES <sup>1</sup> )					
CLOSURE TYPE	EXPOSURE CONDITION	USE REQUIREMENTS			
		FREEWAY	NON-FREEWAY (PRECONSTRUCTION POSTED SPEED LIMIT)		
			≥ 45 MPH	35-40 MPH	≤ 30 MPH
LANE CLOSURE	WHEN ANY WORKERS, VEHICLES, OR OTHER HAZARD IS EXPOSED TO TRAFFIC	REQUIRED <sup>2,4</sup>	REQUIRED <sup>2,4</sup>	REQUIRED <sup>2,4</sup>	REQUIRED <sup>3,4</sup>
	WHEN ANY WORKERS, VEHICLES, OR OTHER HAZARD IS EXPOSED TO TRAFFIC	REQUIRED <sup>2,4</sup>	REQUIRED <sup>2,4</sup>	REQUIRED <sup>3,4</sup>	REQUIRED <sup>3,4</sup>

1. A MOBILE CLOSURE SHALL BE USED FOR ANY WORK ACTIVITY THAT MOVES CONTINUOUSLY OR INTERMITTENTLY ALONG THE TRAVELED WAY OR SHOULDER SLOWER THAN THE PREVAILING SPEED OF TRAFFIC. CHANNELIZING DEVICES ARE NOT USED FOR MOBILE CLOSURES.
2. SHADOW VEHICLES SHALL BE EQUIPPED WITH AN APPROVED REAR MOUNTED ATTENUATOR (TRUCK-MOUNTED OR TRAILER MOUNTED) FOR THE FOLLOWING MOBILE CLOSURES: LANE CLOSURES ON FREEWAYS, LANE CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION POSTED SPEED LIMIT OF 35 MPH OR MORE, SHOULDER CLOSURES ON FREEWAYS, AND SHOULDER CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION SPEED LIMIT OF 45 MPH OR MORE.
3. FOR MOBILE LANE CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION POSTED SPEED LIMIT OF 30 MPH OR LESS AND MOBILE SHOULDER CLOSURES ON NON-FREEWAY ROADWAYS HAVING A PRE-CONSTRUCTION SPEED LIMIT OF 40 MPH OR LESS, SHADOW VEHICLES ARE NOT REQUIRED TO BE EQUIPPED WITH A REAR MOUNTED ATTENUATOR.
4. A SHADOW VEHICLE IS USED TO PROTECT EXPOSED WORKERS (ON FOOT OR IN A VEHICLE) AND SHALL BE REQUIRED FOR ALL MOBILE CLOSURES. SHADOW VEHICLE REQUIREMENTS SHALL INCLUDE PROVIDING A SEPARATE SHADOW VEHICLE FOR EACH CLOSED LANE AND EACH CLOSED SHOULDER 8 FEET OR GREATER IN WIDTH. ADDITIONAL SHADOW VEHICLES MAY BE REQUIRED TO PROMOTE THE SAFE OPERATION OF TRAFFIC AND THE INCREASED PROTECTION OF EXPOSED WORKERS, AS DIRECTED BY THE ENGINEER.

TABLE NY2-A PLACEMENT DISTANCE FOR BARRIER VEHICLES				
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	PLACEMENT DISTANCE (FT)			
	BARRIER VEHICLES			
	18000 LBS.		24000 LBS.	
	MINIMUM	MAXIMUM	MINIMUM	MAXIMUM
> 55	100 FT	200 FT	100 FT	200 FT
45 - 55	100 FT	200 FT	80 FT	160 FT
< 45	80 FT	160 FT	50 FT	100 FT

AS DEFINED IN NYS DOT STANDARD SPECIFICATION 619:  
**BARRIER VEHICLE:** VEHICLE USED FOR STATIONARY SHOULDER CLOSURES, LANE CLOSURES AND OTHER STATIONARY WORK ZONES.  
 MINIMUM DISTANCE SHOWN REFLECTS THE ACTUAL ROLL AHEAD DISTANCE FROM MANUFACTURER.

TABLE NY2-B PLACEMENT DISTANCE FOR SHADOW VEHICLES				
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	PLACEMENT DISTANCE (FT)			
	SHADOW VEHICLES			
	18000 LBS.		24000 LBS.	
	MINIMUM	MAXIMUM	MINIMUM	MAXIMUM
> 55	230 FT	325 FT	180 FT	280 FT
45 - 55	180 FT	280 FT	150 FT	250 FT
< 45	100 FT	200 FT	100 FT	200 FT

AS DEFINED IN NYS DOT STANDARD SPECIFICATION 619:  
**SHADOW VEHICLE:** VEHICLE USED FOR MOBILE OR SHORT DURATION WORK OPERATIONS.  
 MINIMUM DISTANCE SHOWN REFLECTS THE ACTUAL ROLL AHEAD DISTANCE FROM MANUFACTURER.

TABLE NY6H-3 ADVANCE WARNING SIGN SPACING					
ROAD TYPE	DISTANCE BETWEEN SIGNS			SIGN LEGEND	
	A (FT)	B (FT)	C (FT)	XX	YY
URBAN (130 MPH*)	100	100	100	AHEAD	AHEAD
URBAN (35-40 MPH*)	200	200	200	AHEAD	AHEAD
URBAN (w45 MPH*)	350	350	350	1000 FT	AHEAD
RURAL	500	500	500	1500 FT	1000 FT
EXPRESSWAY/FREEWAY	1,000	1,500	2,640	1 MILE	1/2 MILE

\* PRECONSTRUCTION POSTED SPEED LIMIT

**URBAN:** ANY AREA EXHIBITING AT LEAST TWO OF THE FOLLOWING CHARACTERISTICS: SIDEWALKS, BICYCLE USAGE, CURBING, CLOSED DRAINAGE SYSTEMS, DRIVEWAY DENSITIES GREATER THAN 24 DRIVEWAYS PER MILE, MINOR COMMERCIAL DRIVEWAY DENSITIES OF 10 DRIVEWAYS PER MILE OR GREATER, MAJOR COMMERCIAL DRIVEWAYS, NUMEROUS RIGHT-OF-WAY CONSTRAINTS, HIGH DENSITY OF CROSS STREETS, OPERATING SPEEDS OF 45 MPH OR LESS.

**RURAL:** ANY AREA EXHIBITING NO MORE THAN ONE OF ABOVE CHARACTERISTICS.

**EXPRESSWAY:** DIVIDED HIGHWAYS FOR THROUGH TRAFFIC WITH FULL OR PARTIAL CONTROL OF ACCESS AND GENERALLY WITH GRADE SEPARATIONS AT MAJOR CROSSROADS.

**FREEWAY/INTERSTATE:** LOCAL OR INTERREGIONAL HIGH-SPEED, DIVIDED, HIGH-VOLUME FACILITIES WITH FULL OR PARTIAL CONTROL OF ACCESS.

THRUWAY WORK DURATION DEFINITIONS
<b>LONG-TERM STATIONARY</b> IS WORK THAT OCCUPIES A LOCATION MORE THAN 3 CONSECUTIVE DAYS.
<b>INTERMEDIATE-TERM STATIONARY</b> IS WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.
<b>SHORT-TERM STATIONARY</b> IS DAYTIME WORK THAT OCCUPIES A LOCATION WITHIN A SINGLE DAYLIGHT PERIOD.
<b>SHORT DURATION</b> IS DAYTIME WORK THAT OCCUPIES A LOCATION UP TO 2 HOURS, OR NIGHTTIME WORK LASTING UP TO 1 HOUR.
<b>MOBILE</b> IS WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY.

TABLE 619-4 FLARE RATES FOR POSITIVE BARRIER					
TYPE OF POSITIVE BARRIER	PRECONSTRUCTION POSTED SPEED LIMIT				
	30 MPH	40 MPH	50 MPH	55 MPH	65 MPH
TEMPORARY CONCRETE BARRIER	8:1	11:1	14:1	16:1	20:1
BOX BEAM OR HEAVY POST CORRUGATED BEAM	7:1	9:1	11:1	12:1	15:1

TABLE 6C-2 LONGITUDINAL BUFFER SPACE	
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	DISTANCE
25	155 FT
30	200 FT
35	250 FT
40	305 FT
45	360 FT
50	425 FT
55	495 FT
60	570 FT
65	645 FT

TABLE 6H-4 FORMULAS FOR DETERMINING TAPER LENGTHS											
SPEED LIMIT (S) (MPH)	TAPER LENGTH (L) (FEET)		L = TAPER LENGTH (FEET) W = WIDTH OF OFFSET (FEET) S = PRE-CONSTRUCTION POSTED SPEED LIMIT (MPH)								
40 MPH OR LESS	L = WS <sup>2</sup> /60										
45 MPH OR MORE	L = WS										
STANDARD TAPER LENGTHS											
LATERAL SHIFT OF TRAFFIC FLOW PATH	WORK ZONE PRE-CONSTRUCTION POSTED SPEED LIMIT										
	25 MPH	30 MPH	35 MPH	40 MPH	45 MPH	50 MPH	55 MPH	60 MPH	65 MPH		
4 FT	40 FT	60 FT	80 FT	105 FT	180 FT	200 FT	220 FT	240 FT	260 FT		
5 FT	50 FT	75 FT	100 FT	135 FT	225 FT	250 FT	275 FT	300 FT	325 FT		
6 FT	65 FT	90 FT	125 FT	160 FT	270 FT	300 FT	330 FT	360 FT	390 FT		
7 FT	75 FT	105 FT	145 FT	185 FT	315 FT	350 FT	385 FT	420 FT	455 FT		
8 FT	85 FT	120 FT	165 FT	215 FT	360 FT	400 FT	440 FT	480 FT	520 FT		
9 FT	95 FT	135 FT	185 FT	240 FT	405 FT	450 FT	495 FT	540 FT	585 FT		
10 FT	105 FT	150 FT	205 FT	265 FT	450 FT	500 FT	550 FT	600 FT	650 FT		
11 FT	115 FT	165 FT	225 FT	295 FT	495 FT	550 FT	605 FT	660 FT	715 FT		
12 FT	125 FT	180 FT	245 FT	320 FT	540 FT	600 FT	660 FT	720 FT	780 FT		

WORK ZONE TRAFFIC CONTROL LEGEND	
SYMBOL	DESCRIPTION
	ARROW PANEL
	ARROW PANEL, CAUTION MODE
	ARROW PANEL SUPPORT OR TRAILER
	CHANGEABLE MESSAGE SIGN (PVMS)
	CHANNELIZING DEVICE
	CRASH CUSHION/TEMPORARY IMPACT ATTENUATOR
	DIRECTION OF TEMPORARY TRAFFIC DETOUR
	DIRECTION OF TRAFFIC
	FLAGGER
	FLAG TREE
	LUMINAIRE
	PAVEMENT MARKINGS THAT SHALL BE REMOVED FOR A LONG TERM PROJECT
	SIGN, TEMPORARY
	TEMPORARY BARRIER
	TEMPORARY BARRIER WITH WARNING LIGHTS
	TRAFFIC OR PEDESTRIAN SIGNAL
	TYPE III BARRICADE
	WARNING LIGHTS
	WORK SPACE
	WORK VEHICLE
	WORK VEHICLE W/ TRUCK MOUNTED ATTENUATOR
	TEMP/INTERIM PAVEMENT MARKING

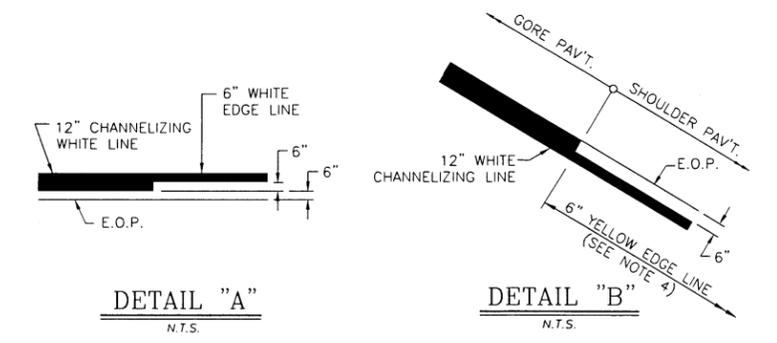
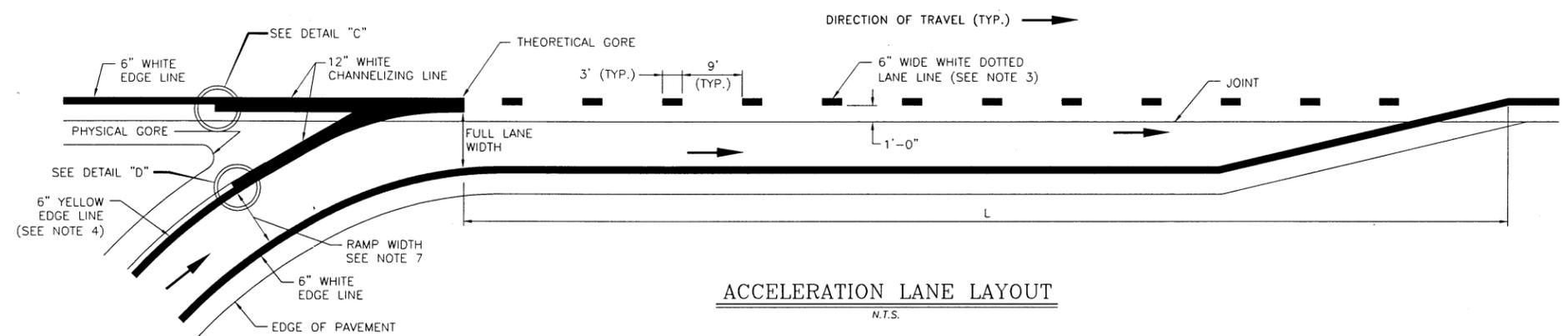
TABLE 6C-3 TAPER LENGTH CRITERIA FOR WORK ZONES	
TYPE OF TAPER	TAPER LENGTH (L)
MERGING TAPER	L
SHIFTING TAPER	L/2
SHOULDER TAPER	L/3
ONE-LANE, TWO-WAY TRAFFIC TAPER	100 FT MAXIMUM
DOWNSTREAM TAPER	100 FT PER LANE

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

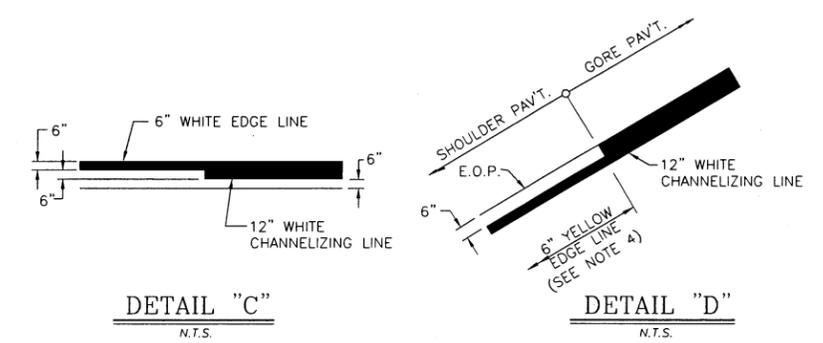
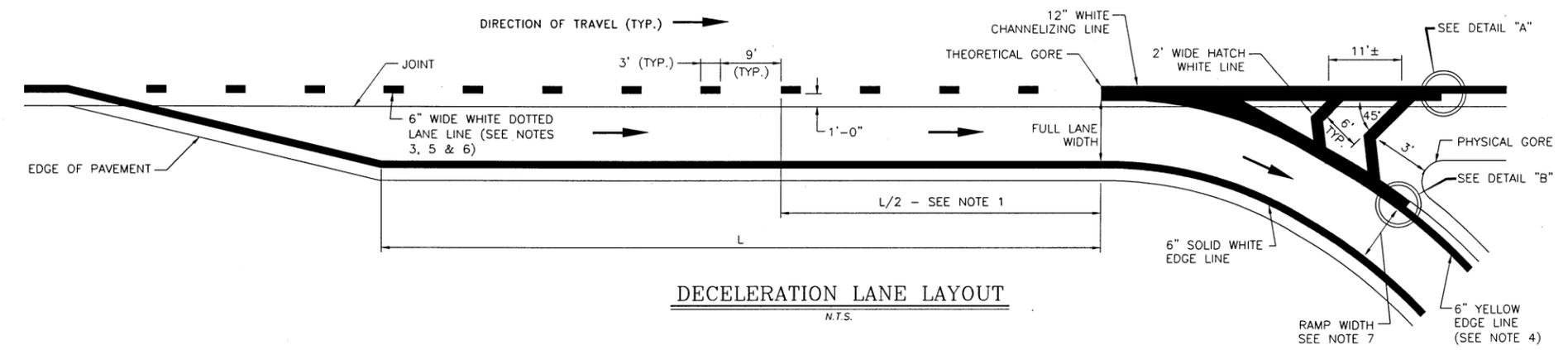


**Thruway Authority** | **Canal Corporation**

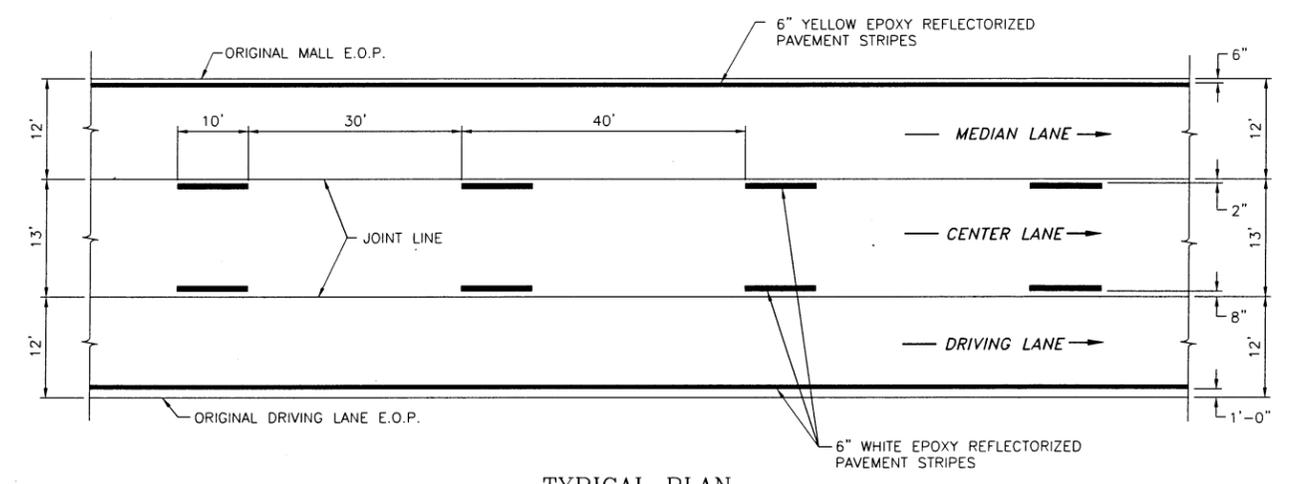
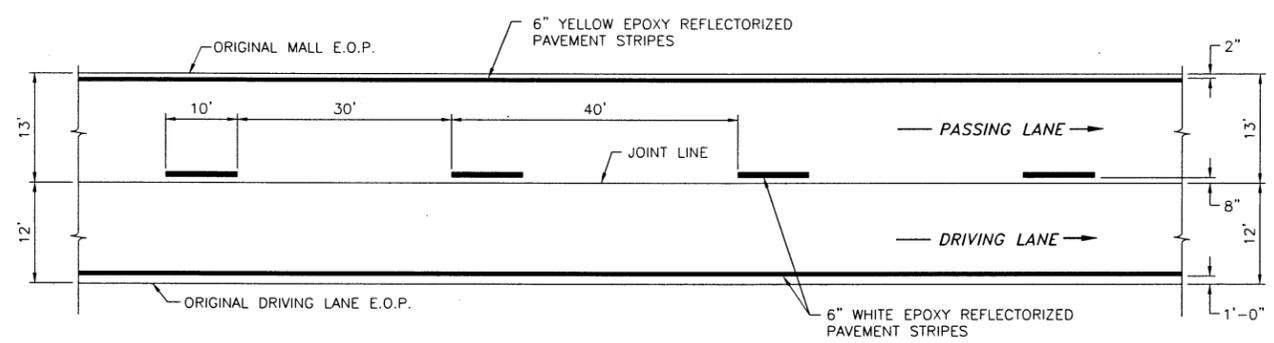
TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 03/16
TITLE OF DRAWING WORK ZONE TRAFFIC CONTROL TABLES AND LEGEND	DRAWING NUMBER: TL



**DECELERATION LANE DETAILS**  
N.T.S.



**ACCELERATION LANE DETAILS**  
N.T.S.



**TYPICAL PLAN REFLECTORIZED PAVEMENT MARKINGS - THREE LANE**  
N.T.S.

**NOTES:**

1. WHEN THE CONTRACT DOES NOT INCLUDE REPAVING THE GORE, THE EXISTING STRIPING PATTERN IN THE DECELERATION RAMP GORE SHALL BE REPAINTED UNLESS OTHERWISE NOTED.
2. WHEN THE CONTRACT PROVIDES FOR REPAVING OF THE DECELERATION LANE GORE, A "CHEVRON" PATTERN SHALL BE INSTALLED IN THE NEWLY PAVED GORE AS SHOWN IN THE "DECELERATION LANE LAYOUT".
3. IF A PARTIAL BARRIER AND/OR SKIP LINES EXIST AT AN ACCELERATION OR DECELERATION LANE, THEY SHALL BE REMOVED AND THE LANE SHALL BE RESTRIPIED AS SHOWN IN THE APPROPRIATE LANE LAYOUT.
4. YELLOW EDGE LINE ON RAMP SHALL EXTEND TO ACTUAL PHYSICAL GORE. THE PHYSICAL GORE IS A POINT WHERE A PHYSICAL BARRIER (I.E. SIGN POST, ATTENUATOR, OR OTHER FIXED OBJECT) OR LACK OF A PAVED SURFACE INHIBITS ROAD USER FROM CROSSING A RAMP TO A TRAVEL LANE OR VICE VERSA.
5. A 12" WIDE WHITE DOTTED LANE LINE SHALL BE USED AS A LANE DROP MARKING IN ADVANCE OF LANE DROPS AT EXIT RAMP TO DISTINGUISH A LANE DROP FROM A NORMAL EXIT RAMP.
6. WHITE DOTTED LANE LINES USED IN ADVANCE OF LANE DROPS SHOULD BEGIN AT LEAST 1/2 MILE IN ADVANCE OF THE THEORETICAL GORE.
7. WHEN STRIPES ARE REMOVED ON RAMP, THE CONTRACTOR SHALL NOTE THE EXISTING RAMP WIDTHS, STRIPE LOCATIONS AND REPLACE THE STRIPING IN THE SAME LOCATION UNLESS OTHERWISE NOTED IN THE CONTRACT DOCUMENTS.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.



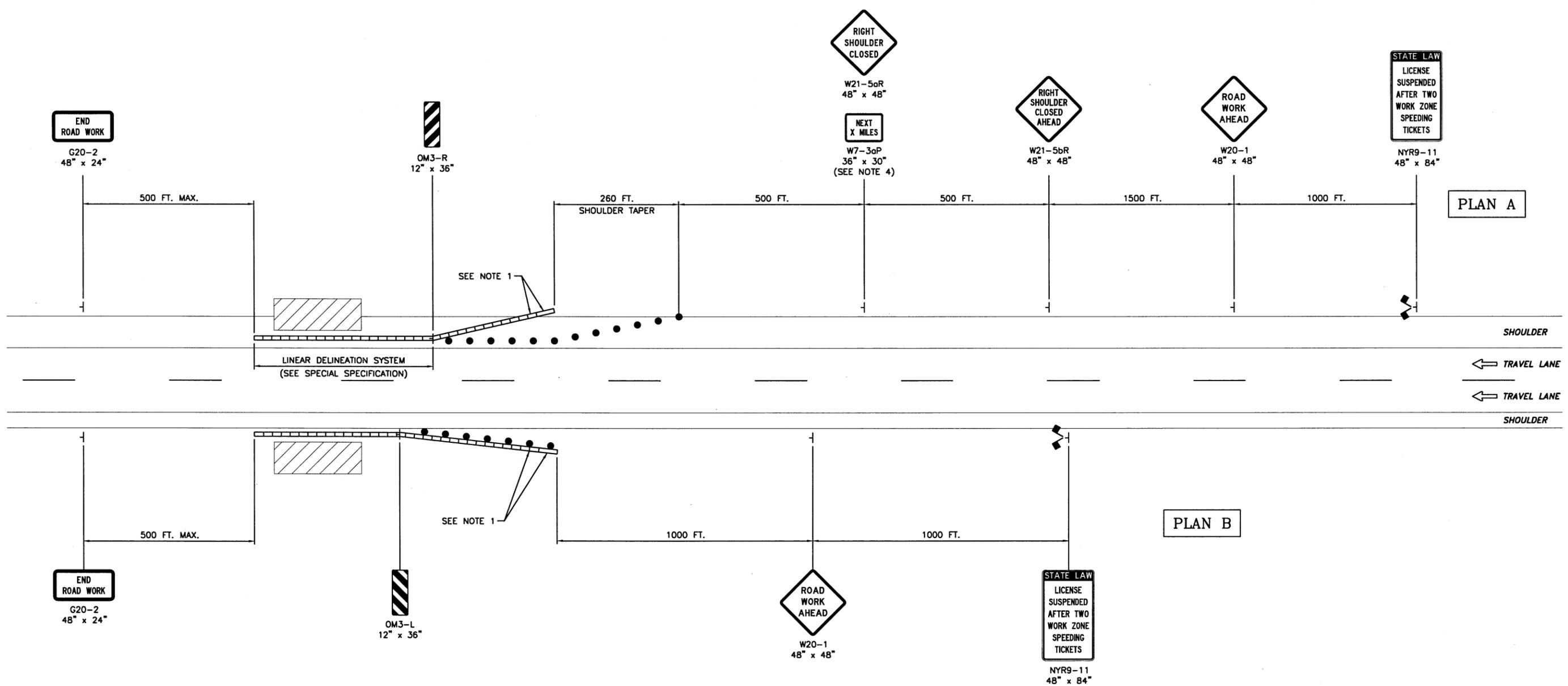
NEW YORK STATE THRUWAY AUTHORITY  
DEPARTMENT OF ENGINEERING  
200 SOUTHERN BLVD., ALBANY, N.Y. 12209

TITLE OF PROJECT  
**TITLE OF PROJECT LINE 1**  
**TITLE OF PROJECT LINE 2**  
LOCATION OF PROJECT  
**LOCATION OF PROJECT LINE 1**  
**LOCATION OF PROJECT LINE 2**  
TITLE OF DRAWING  
**PAVEMENT STRIPING DETAILS**  
**(ASPHALT PAVEMENT)**

CONTRACT NUMBER:  
**TA**  
DATE:  
**11/14**  
DRAWING NUMBER:  
**PMA-1**

PMA-1.dwg  
 CHECKED BY: TA  
 DRAFTED BY: CAD  
 CHECKED BY: J. SHINSKI  
 DESIGNED BY: R. COURNOYER  
 DESIGN SUPERVISOR: T. MAHAR

FILED  
 CHECKED BY: TA  
 DRAFTED BY: TA  
 CHECKED BY: J. SHINSKI  
 DESIGNED BY: R. COURNOYER  
 DESIGN SUPERVISOR: T. MAHAR



**WORK ZONE TRAFFIC CONTROL PLAN**  
N.T.S.

**NOTES:**

- FOR TEMPORARY CONCRETE BARRIER AND END TREATMENT DETAILS, REFER TO THE DESIGNED LAYOUT SHEET(S) IN THE CONTRACT PLANS.
- TWO DIFFERENT SIGNING AND DELINEATION PLANS ARE SHOWN:  
**PLAN A** SHALL BE USED WHERE TEMPORARY CONCRETE BARRIER IS INSTALLED ON THE SHOULDER. FOR LEFT SHOULDER APPLICATIONS, SUBSTITUTE "LEFT SHOULDER CLOSED AHEAD" SIGN (W21-5bL) AND "LEFT SHOULDER CLOSED" SIGN (W21-5aL) FOR RIGHT SHOULDER CLOSED SIGNS (W21-5bR AND W21-5aR).  
**PLAN B** SHALL BE USED WHERE TEMPORARY CONCRETE BARRIER IS INSTALLED OFF THE SHOULDER.
- SIGNING IS REQUIRED ONLY ON THE SIDE OF THE ROADWAY WHERE THE WORK SPACE IS LOCATED.
- THE "NEXT X MILES" SUPPLEMENTAL SIGN (W7-3aP) IS REQUIRED WHEN THE SHOULDER IS CLOSED FOR A DISTANCE GREATER THAN 2 MILES.

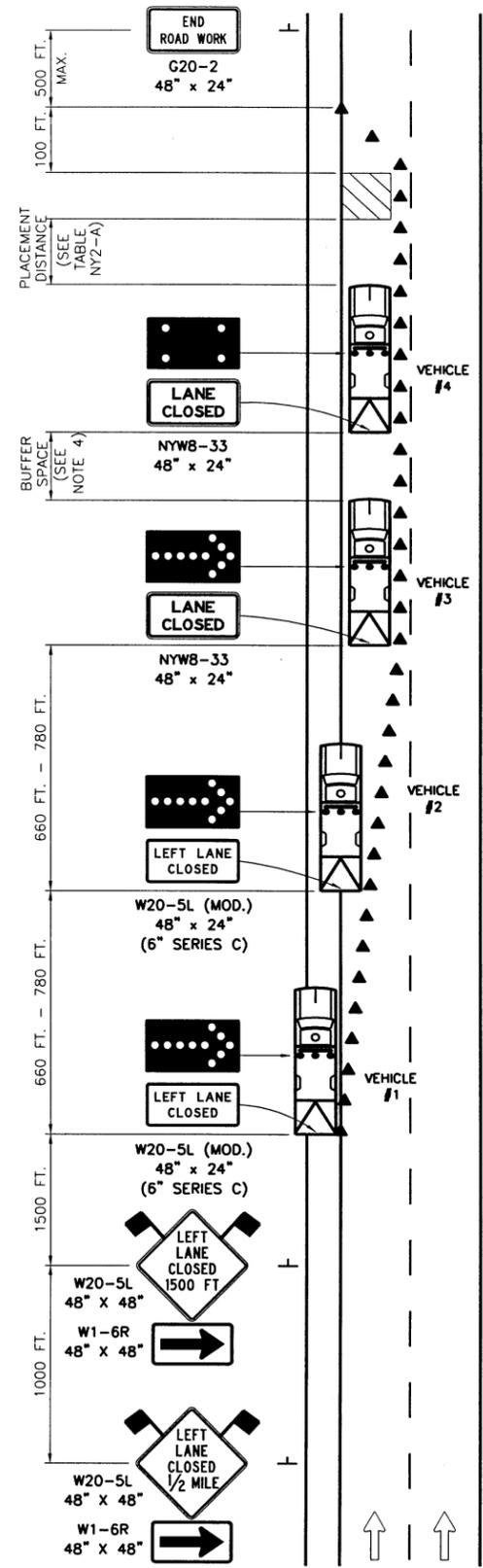
**CHANNELIZING DEVICE**  
**LEGEND**

- WHITE RETROREFLECTORIZED SHEETING, ASTM TYPE IX (CLASS E)
- ORANGE RETROREFLECTORIZED SHEETING, ASTM TYPE IX (CLASS E)
- NON-REFLECTORIZED ORANGE
- DRUMS @ 40' SPACING

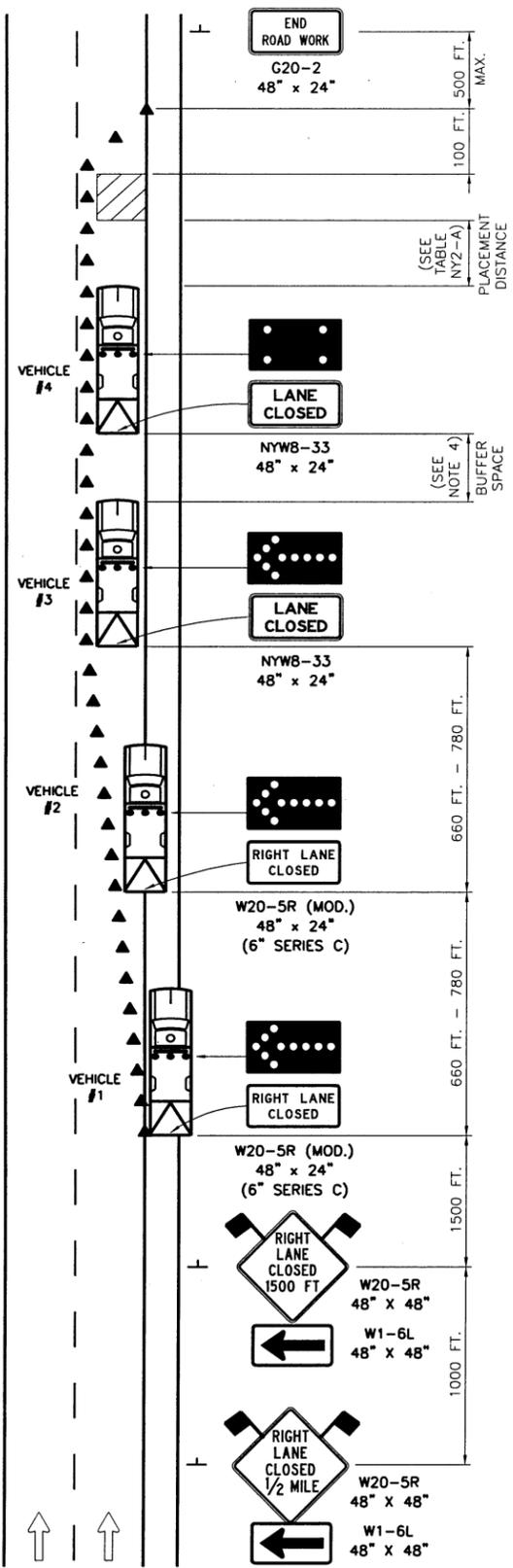
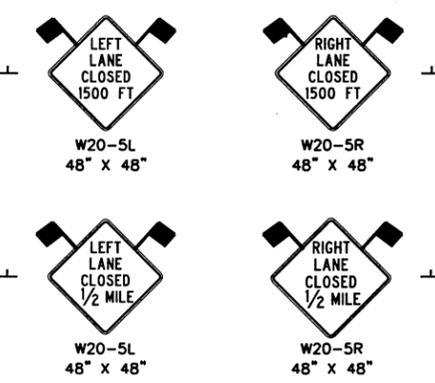
REVISIONS			
DATE	DESCRIPTION	BY	SYM.

<b>Thruway Authority</b>   <b>Canal Corporation</b>	TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
	LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 02/13
	TITLE OF DRAWING SIGNING AND DELINEATION FOR SHOULDER WORK SPACES WITH TEMPORARY CONCRETE BARRIER	DRAWING NUMBER: SCSO

DESIGN SUPERVISOR: T. MAHAR  
 DESIGNED BY: R. COURNOYER  
 CHECKED BY: J. SHINSKI  
 DRAFTED BY: TA  
 CHECKED BY: TA  
 FILE



**SHORT DURATION LEFT LANE CLOSURE**  
N.T.S.



**SHORT DURATION RIGHT LANE CLOSURE**  
N.T.S.

**GENERAL NOTES:**

1. USE OF THESE PLANS SHALL BE LIMITED TO A WORK SPACE LENGTH OF 1/2 MILE OR LESS AND A DAYTIME WORK DURATION OF UP TO 2 HOURS, OR NIGHTTIME WORK DURATION UP TO 1 HOUR.
2. TRAILER-MOUNTED ARROW PANELS MAY BE SUBSTITUTED FOR VEHICLES #1, #2, AND #3. TOW VEHICLES SHALL NOT REMAIN ATTACHED TO TRAILER-MOUNTED ARROW PANELS.
3. FOR VEHICLE #1, THE ATTENUATOR IS OPTIONAL FOR DAYTIME OPERATIONS. FOR NIGHTTIME OPERATIONS, ALL VEHICLES, INCLUDING VEHICLE #1 ON THE SHOULDER, SHALL BE EQUIPPED WITH AN ATTENUATOR.
4. THE MINIMUM BUFFER SPACE LENGTH SHALL BE THE PLACEMENT DISTANCE REQUIRED FOR BARRIER VEHICLES (SEE TABLE NY2-A). THE LENGTH OF THE BUFFER SPACE SHALL BE EXTENDED, AS DETERMINED BY THE ENGINEER, TO ENSURE ADEQUATE SIGHT DISTANCE FOR VEHICLES APPROACHING THE LANE CLOSURE TAPER.

**LEFT LANE CLOSURE NOTES:**

1. WHERE THE LEFT LANE IS TO BE CLOSED, VEHICLE #1 IS TO BE LOCATED COMPLETELY ON THE LEFT SHOULDER, VEHICLE #2 STRADDLES THE LEFT SHOULDER AND THE LEFT LANE, AND VEHICLE #3 IS IN THE LEFT LANE.
2. FOR A LEFT LANE CLOSURE IN AN AREA WHERE THE USABLE LEFT SHOULDER/MEDIAN WIDTH IS LESS THAN 6 FEET, LEFT SIDE SIGNS SHALL BE MOUNTED ON THE MEDIAN BARRIER. IF THE TOTAL MEDIAN WIDTH IS LESS THAN 6 FEET, LEFT SIDE SIGNS IN ADVANCE OF THE LANE CLOSURE TAPER AND THE "END ROAD WORK" (G20-2) SIGN SHALL NOT BE REQUIRED.
3. FOR A LEFT LANE CLOSURE IN AN AREA WHERE THE USABLE LEFT SHOULDER/MEDIAN WIDTH IS LESS THAN 8 FEET, VEHICLE #1 IS AS FAR LEFT ON THE LEFT SHOULDER AS CONDITIONS PERMIT, VEHICLE #2 STRADDLES THE LEFT SHOULDER AND THE LEFT LANE, AND VEHICLE #3 IS IN THE LEFT LANE.
4. IN AREAS WHERE THERE IS LITTLE TO NO LEFT SHOULDER/MEDIAN WIDTH, VEHICLE #1 AND VEHICLE #2 SHALL BE IN THE LEFT LANE. FOR THESE SITUATIONS, CONSULT THE ENGINEER FOR APPROPRIATE TEMPORARY TRAFFIC CONTROL UPGRADES.

**RIGHT LANE CLOSURE NOTES:**

1. WHERE THE RIGHT LANE IS TO BE CLOSED, VEHICLE #1 IS TO BE LOCATED COMPLETELY ON THE RIGHT SHOULDER, VEHICLE #2 STRADDLES THE RIGHT SHOULDER AND THE RIGHT LANE, AND VEHICLE #3 IS IN THE RIGHT LANE.
2. FOR A RIGHT LANE CLOSURE IN AN AREA WHERE THE USABLE LEFT SHOULDER/MEDIAN WIDTH IS LESS THAN 6 FEET, LEFT SIDE SIGNS SHALL BE MOUNTED ON THE MEDIAN BARRIER. IF THE TOTAL MEDIAN WIDTH IS LESS THAN 6 FEET, LEFT SIDE SIGNS SHALL NOT BE REQUIRED.

**LEGEND**

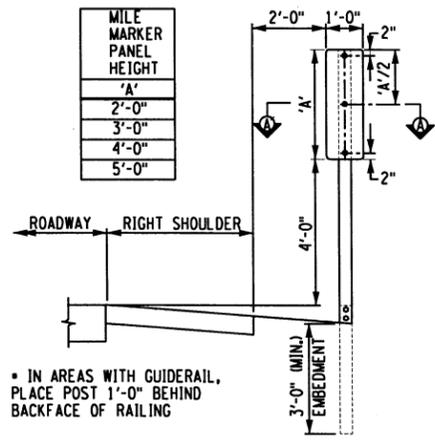
- ▲ TALL TRAFFIC CONES
- 40 FT. SPACING ON TAPER AND TANGENT

REVISIONS			
DATE	DESCRIPTION	BY	SYM

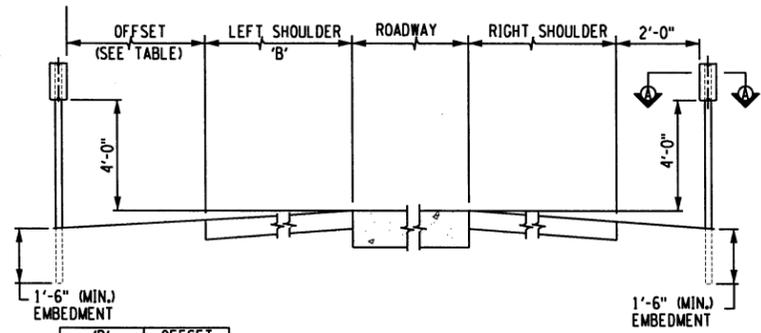


TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE: 02/13
TITLE OF DRAWING SHORT DURATION LANE CLOSURE	DRAWING NUMBER: SDLC

Fig 0  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: J. SHINSKI  
 DESIGNED BY: R. COURNOYER  
 DESIGN SUPERVISOR: T. MAHAR

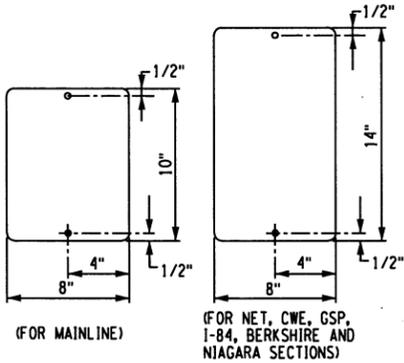


**MILE MARKER POST MOUNT INSTALLATION DETAIL**  
N.T.S.

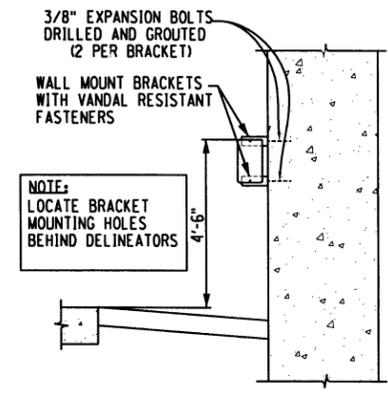
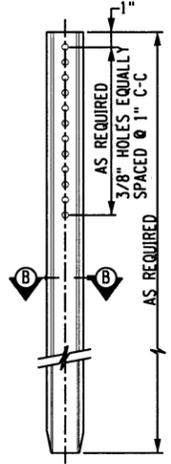
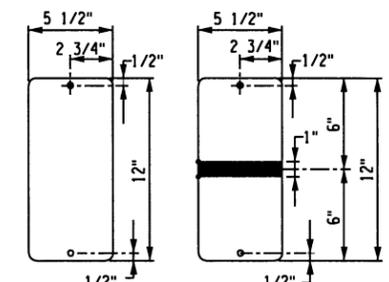
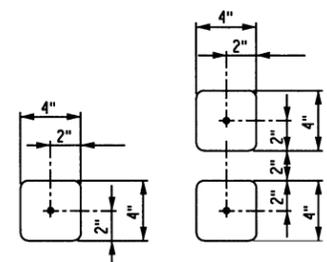


**TENTH MILE MARKER AND DELINEATOR POST MOUNT INSTALLATION DETAIL**  
N.T.S.

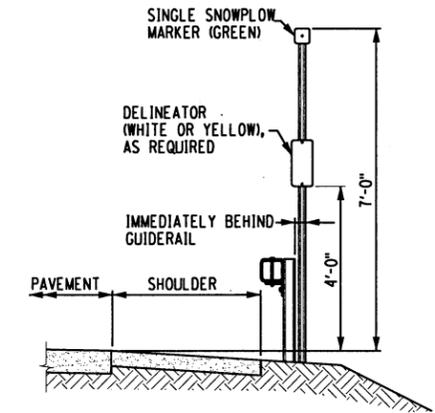
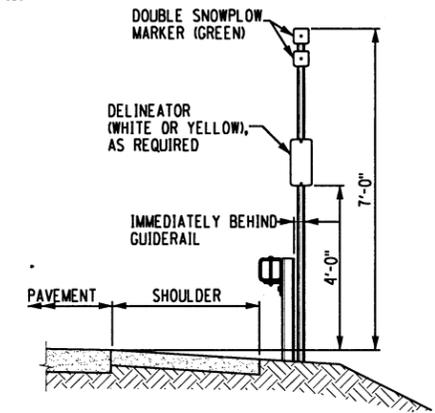
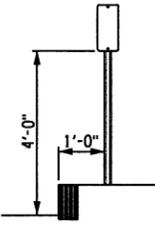
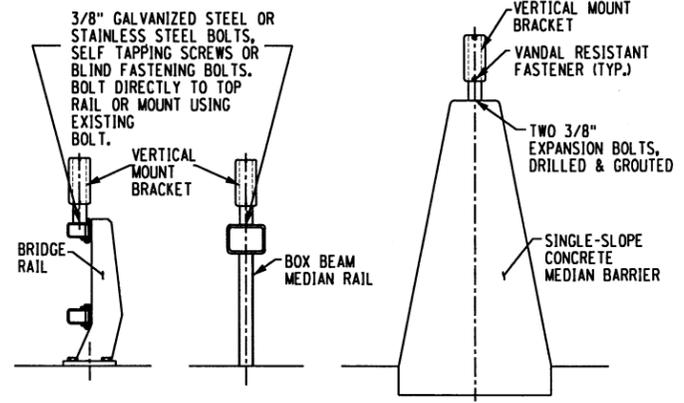
'B'	OFFSET
4'-0"	8'-0"
6'-0"	6'-0"
10'-0"	4'-0"
12'-0"	2'-0"



**TENTH MILE MARKER**  
N.T.S.



**TENTH MILE MARKER AND DELINEATOR WALL MOUNT INSTALLATION DETAIL**  
N.T.S.

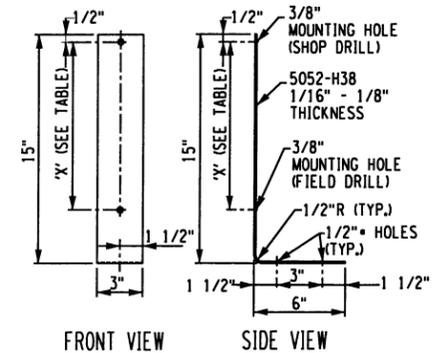
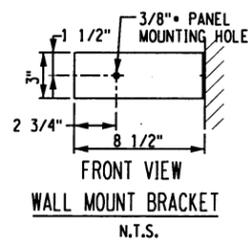
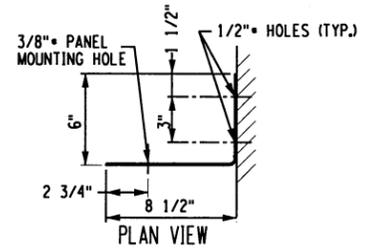


**VERTICAL MOUNT BRACKET NOTES:**

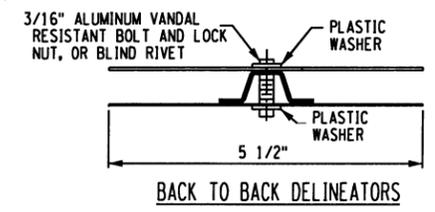
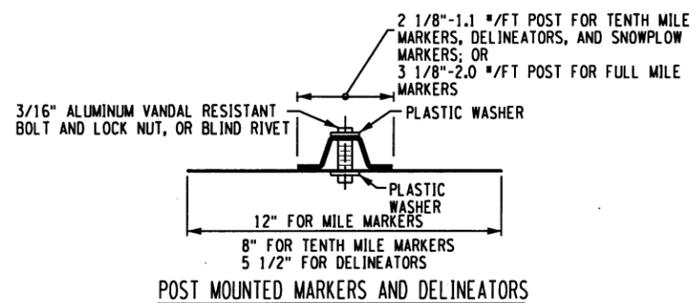
- WHEN GALVANIZED STEEL IS USED FOR BRACKETS, SPACERS THAT DO NOT INDUCE CORROSION MUST BE USED.
- BRACKETS MAY BE USED FOR MOUNTING DELINEATORS OR REFERENCE MARKERS ON BRIDGE RAIL, GUIDE RAIL, OR AS OTHERWISE NEEDED.

**MOUNTING HOLE SPACING TABLE**

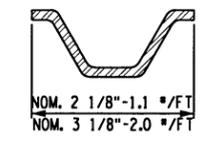
'X'	DESCRIPTION
11"	FOR 5 1/2" x 12" DELINEATOR
9"	FOR 8" x 10" TENTH MILE MARKER
13"	FOR 8" x 14" TENTH MILE MARKER



**VERTICAL MOUNT BRACKET**  
N.T.S.



**SECTION A-A**  
N.T.S.



- NOTES:**
- MATERIAL SHALL CONFORM TO EITHER A.S.T.M. A-499 OR A.S.T.M. A-36.
  - GALVANIZING SHALL CONFORM TO A.S.T.M. A-123.
  - REMOVE ALL BURRS AND SHARP EDGES.
  - TENTH MILE MARKERS AND DELINEATORS: FOR INSTALLATION WHERE THE DISTANCE FROM FINISHED GRADE TO TOP OF POST IS LESS THAN OR EQUAL TO 5'-0" USE 1.1 #/FT POST SIZE. WHERE THE DISTANCE IS GREATER THAN 5'-0" USE 2.0 #/FT POST SIZE UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.
  - MILE MARKERS (PANEL HEIGHTS OF 2'-0" TO 5'-0") SHALL BE MOUNTED ON 2.0 #/FT POSTS REGARDLESS OF POST HEIGHT UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS.

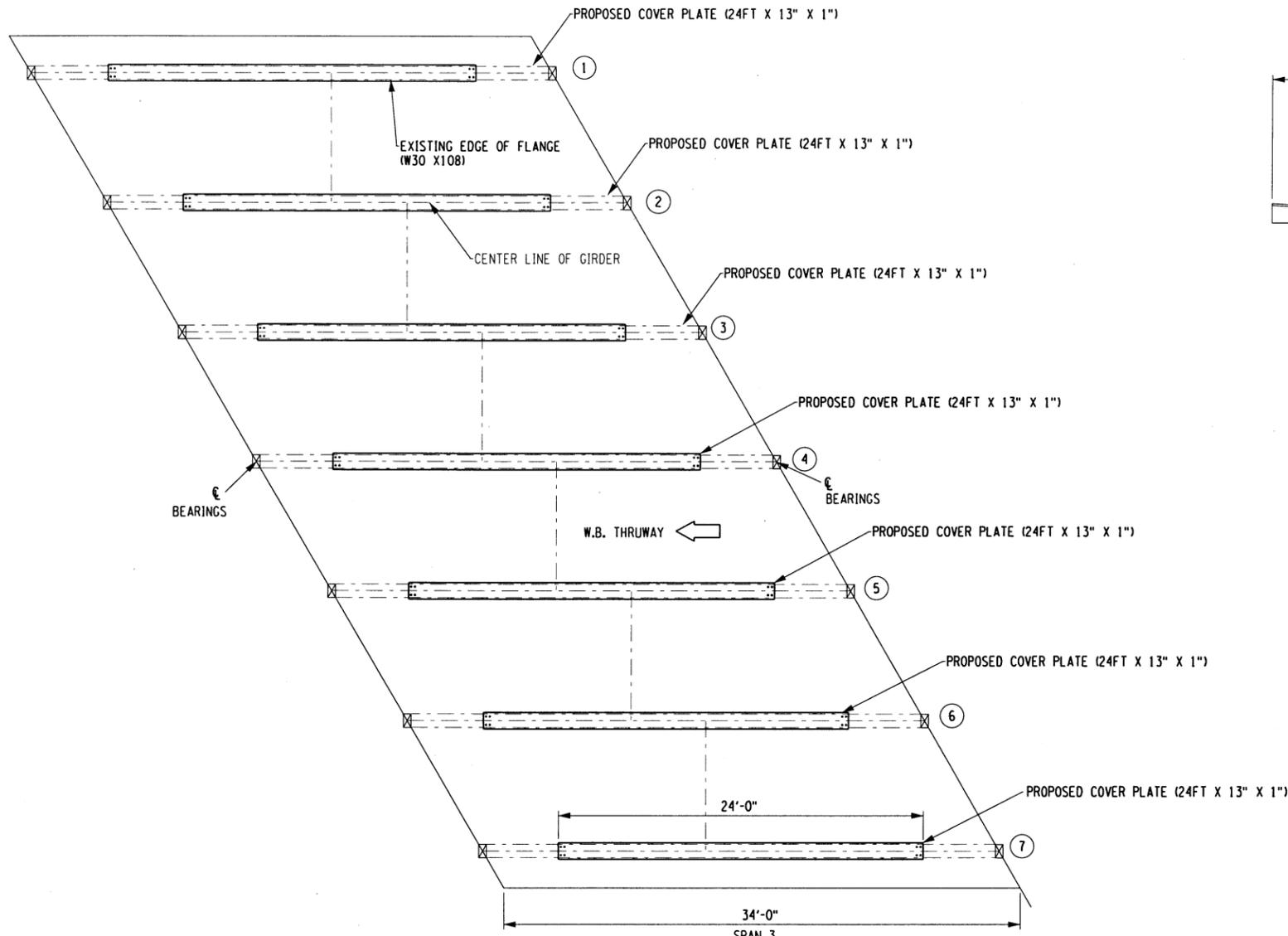
**REVISIONS**

DATE	DESCRIPTION	BY	SYM.
06/16	BORDER SHEET & NOTES	R. COURNOYER	△

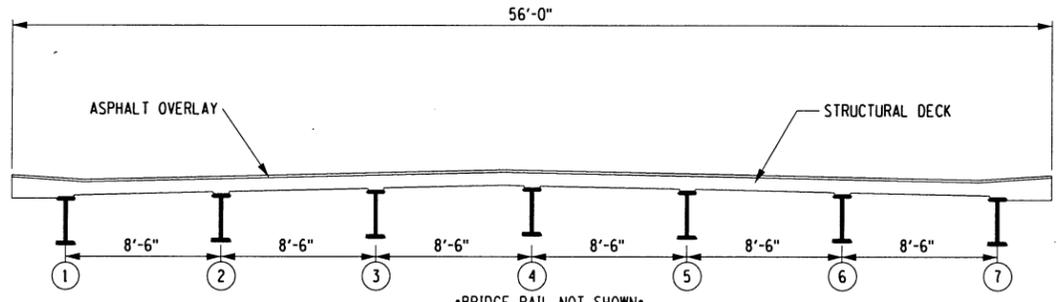


TITLE OF PROJECT	NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER:	TAB 16-33B
LOCATION OF PROJECT	TWY M.P. 356.69 & 356.70 PITTSFORD MENDON CENTER RD., MONROE COUNTY	DATE:	06/2016
TITLE OF DRAWING	MILE MRKR. TENTH MILE MRKR. DELINEATOR, AND SNOWPLOW MARKER INSTALLATION DETAILS	DRAWING NUMBER:	DMM-1

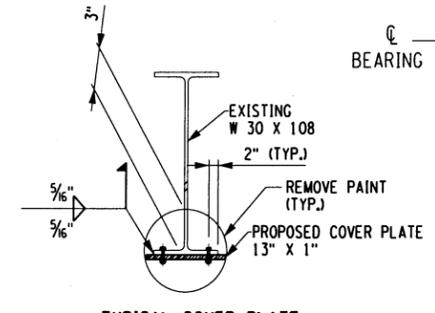
FILED  
 CHECKED BY: TA  
 DRAFTED BY: M. COX  
 CHECKED BY: A. SANOBARI  
 DESIGNED BY: M. COX  
 DESIGN SUPERVISOR: D. VOSBURGH



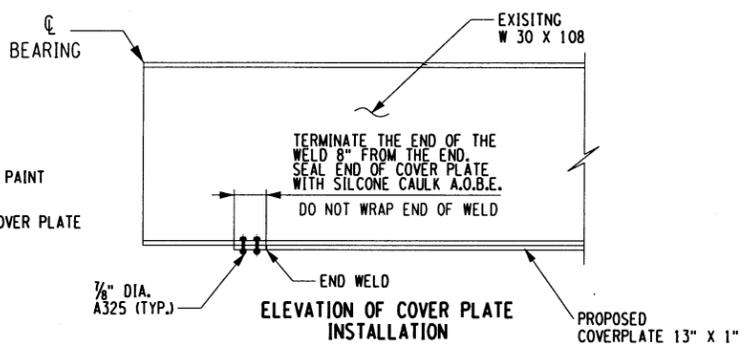
**M.P. 356.70 PROPOSED COVER PLATE PLAN**  
NTS



**PROPOSED COVER PLATE SECTION**  
NTS



**TYPICAL COVER PLATE INSTALLATION**  
NTS  
STRUCTURAL DECK NOT SHOWN FOR CLARITY



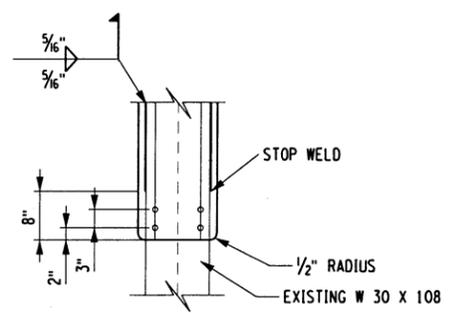
**ELEVATION OF COVER PLATE INSTALLATION**

**NOTES:**

- 1- THE CONTRACTOR SHALL REMOVE EXISTING PAINT PRIOR TO INSTALLING PROPOSED COVER PLATES COST(S) ASSOCIATED WITH REMOVING EXISTING PAINT, COLLECTING AND DISPOSING OF PAINT WASTE AND COATING SHALL BE INCLUDED IN THE CONTRACT PAINTING ITEMS.
- 2- LIVE LOAD OR ANY CONSTRUCTION LOAD SHALL BE SHIFTED OFF THE LANE IMMEDIATELY OVER THE TOP OF THE GIRDER(S) DURING WELDING OF THE COVERPLATES.
- 3- ALL FILLET WELD(S) SHALL BE MAGNETIC PARTICLE TESTED "PRIOR TO PAINTING" (COST INCLUDED IN ITEM 564.0501) ALL RUST SHALL BE REMOVED PRIOR TO PAINTING AT NO ADDITIONAL COST.
- 4- ALL STRUCTURAL STEEL SHALL BE ASTM A36 WITH MINIMUM YIELD STRENGTH OF 36 KSI, ALL BOLTS SHALL BE 7/8" DIAM. ASTM A325 (PLAIN), NUTS A563 GRADE DH (PLAIN) AND WASHER(S) F436 TYPE 1 (PLAIN) (ALL COST(S) INCLUDED IN ITEM 564.0501)
- 5- THE COST OF ALL THE WORK SHALL BE INCLUDED IN ITEM 564.0501 AND PAINTING ITEMS
- 6- ALL WELDING SHALL BE DONE IN ACCORDANCE WITH THE NEW YORK STATE STEEL CONSTRUCTION MANUAL
- 7- WELDING PROCEDURES SHALL BE SUBMITTED TO THE NYS THRUWAY AUTHORITY FOR APPROVAL
- 8- THE CONTRACTOR MAY INTRODUCE ONLY "ONE" SHOP SPLICE IN THE PROPOSED COVER PLATE
- 9- THE PROPOSED COVERPLATES SHALL BE INSTALLED IN THE FOLLOWING ORDER  
 A- CLEAN PAINT  
 B- CLAMP COVERPLATE(S) AND DRILL HOLES  
 C- INSTALL BOLTS  
 D- WELD COVERPLATES EITHER FROM ONE END TO THE OTHER OR FROM THE CENTER OUT TO THE COVERPLATE ENDS.
- 10- ALL BOLTED SURFACES SHALL BE PREPARED TO CLASS "B" SURFACE CONDITIONS (SSPC SP10)

**SUGGESTED SEQUENCE OF OPERATIONS:**

1. REMOVE EXISTING PAINT
2. CLAMP COVERPLATE TO BOTTOM FLANGE WITH BRIDGE CLAMP(S)
3. PREHEAT GIRDER(S) 250 DEGREES FAHRENHEIT
4. THE CONTRACTOR MAY ELECT TO TACK WELD THE PROPOSED COVERPLATE IN PLACE. ALL TACK WELDS SHALL BE A MINIMUM OF 1 1/2" LONG AND SHALL NOT BE PLACED WITHIN 2 FEET OF THE ENDS OF THE PROPOSED COVERPLATES. ALL TACK WELDS SHALL BE INCORPORATED INTO THE FINAL WELD.
5. THE CONTRACTOR SHALL COMPLETE WELDING OF EACH COVER PLATE BEFORE STARTING ANOTHER GIRDER



**PLAN VIEW OF COVER PLATE INSTALLATION**

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE. THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

ALTERED ON: \_\_\_\_\_

AFFIXED ON: 8/29/16

SIGNATURE: \_\_\_\_\_

STAMP: \_\_\_\_\_

REVISIONS			
DATE	DESCRIPTION	BY	SYM



TITLE OF PROJECT NYS THRUWAY BRIDGE REHABILITATION	CONTRACT NUMBER: TAB 16-33B
LOCATION OF PROJECT TWY MP. 356.69 & 356.70 PITTSFORD LOCATIMENDON CENTER RD., MONROE COUNTY	DATE: 08/2016
TITLE OF DRAWING MP 356.70 COVER PLATE INSTALLATION	DRAWING NUMBER: CP -1