

NEW YORK STATE THRUWAY AUTHORITY  
DEPARTMENT OF ENGINEERING

ALBANY, NY  
OCTOBER 5, 2016

AMENDMENT NO. 2  
TO

CONTRACT TAB 16-45B  
D214537  
REPAIR OF THE IMPACT DAMAGED BRIDGE AT MP 426.18  
IN THE BUFFALO DIVISION  
OF THE NEW YORK STATE THRUWAY  
IN ERIE COUNTY

IN THE LETTING OF  
OCTOBER 12, 2016

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**NOTE:** This amendment shall be attached to and become a part of the Proposal for Contract TAB 16-45B.

**NOTICE**

The following has been added to the supplemental information for this contract on the Authority's website:

- Record Plans
- Shop Drawings
- Information from Pre-Bid Meeting held on September 28, 2016

**PROPOSAL**

1. On Contract Proposal page 1, and page 187 and in all other locations in the Contract Proposal, **CHANGE** the COMPLETION DATE to December 6, 2016.
2. **DELETE** page 10-A1 and **SUBSTITUTE** the attached page 10-A2.
3. **ADD** page 10A-A2.
4. **DELETE** page 11-A1 and **SUBSTITUTE** the attached page 11-A2.
5. **DELETE** page 12 and **SUBSTITUTE** the attached page 12-A2.

**PLANS**

6. On the Title Sheet and subsequent plan sheets, **CHANGE** the total number of sheets from 21 to 22.
7. **DELETE** Drawing DT-1, Sheet 17, and **SUBSTITUTE** the attached Drawing DT-1, Sheet 17-A2.
8. **DELETE** Drawing DT-2, Sheet 18, and **SUBSTITUTE** the attached Drawing DT-2, Sheet 18-A2.
9. **ADD** the attached Drawing DT-3, Sheet 18A-A2.

The Bidder **MUST** complete Page **208** of the Proposal acknowledging receipt of this amendment. If the Bidder fails to complete the “Amendment Acknowledgement” sheet, his bid could be declared informal thereby delaying award of the contract.

**PLEASE BE GOVERNED ACCORDINGLY WHEN SUBMITTING BIDS.**

**Catherine T. Sheridan, P.E.**  
**Chief Engineer**

**SPECIAL NOTE**

Dates of available weekend closures:

November 4<sup>th</sup> @ 7PM until November 7<sup>th</sup> @ 5AM

November 11<sup>th</sup> @ 11PM until November 14<sup>th</sup> @ 5AM \*

November 18<sup>th</sup> @ 7PM until November 21<sup>st</sup> @ 5AM

December 2<sup>nd</sup> @ 7PM until December 5<sup>th</sup> @ 5AM

All above dates are only applicable for 2016.

Work done on the earlier weekend of these available dates is preferred due to the threat of potential inclement weather in the Buffalo area during this time of year.

\* Due to Veteran's Day Observance, the Interchange closure or any closure cannot be in place between the hours of 6:00 AM Thursday, November 10, 2016 and 11:00 PM Friday, November 11, 2016. This constitutes a 4 hour later start than the other available weekend closures, but the completion remains 5:00 AM Monday, November 14, 2016. All other conditions apply.

Exit 53 Repair - Overall Closure Schedule Availabilities:

**Ramp EB I-90 to I-190 NB (Top of tri-level)**

Friday – Close 1 lane at 7:00 PM

Close ramp at 10:00 PM

Closure remains in place until completion (safe for travel) – must be open by 5:00 AM Monday

**Ramp I-190 SB to I-90 EB (mid-level ramp)**

Friday - Close 1 lane at 7:00 PM

Close ramp at 10:00 PM

Closure remains in place until completion (safe for travel) – must be open by 5:00 AM Monday

**Mainline I-90 WB under tri-level**

**Friday -** Close 1 lane (right lane) at 8:00 PM  
Close 2 lanes (right and center lanes) at 9:00 PM  
Close all lanes (3) at 10:00 PM  
Reopen all I-90 WB lanes by 8:00 AM Saturday

**Saturday -** Close 1 lane (right lane) at 8:00 PM  
Close 2 lanes (right and center lanes) at 9:00 PM  
Close all lanes (3) at 10:00 PM  
Reopen all I-90 WB lanes by 10:00 am Sunday

The Contractor shall submit with their bid a statement of the intended closure windows they will use to complete the work so that the Authority can inform the public more specifically about the closures and the Authority can accomplish the proper outreach to all stakeholders.

In addition to the statement, although not binding, the Contractor shall submit the intended weekend of work they intend to engage the work.

**INCENTIVE/DISINCENTIVE CLAUSE****SPECIAL NOTE**

The Authority has granted use of a single weekend closure of the Interchange 53 Ramps serving traffic from 7:00 PM Friday evening until 5:00 AM Monday morning.

Failure to achieve opening of the full Interchange to traffic operations by 5:00 AM Monday morning will result in assessment of deductions - disincentive damages as provided in Table #1. Deductions outlined within Table #1 for each increment of time traffic is not returned to the ramp will be cumulative. These deductions – disincentives begin 5:01AM on Monday and continue until Completion of Work, as defined for this Special Note, is achieved. The maximum assessment for failure to return traffic operations due either to incomplete work at the impacted bridge or as a result of Contractor damage to other transportation infrastructure during this period of work is **\$150,000.**

Later than 5:00AM until 6:00 AM	\$1,250
Later than 6:00AM until 6:30 AM	\$1,500
Later than 6:30AM until 7:00AM	\$2,000
Later than 7:00AM until 7:30AM	\$2,250
Later than 7:30AM until 8:00AM	\$2,500
Later than 8:00AM until 8:30AM	\$3,000
Later than 8:30AM until 9:00AM	\$3,500
Later than 9:00AM until 3:00PM	\$1,500/hr.
Later than 3:00PM until 3:30PM	\$1,500
Later than 3:30PM until 4:00PM	\$2,000
Later than 4:00PM until 4:30AM	\$2,250
Later than 4:30PM until 5:00PM	\$2,500
Later than 5:00PM until 5:30PM	\$3,000
Later than 5:30PM until 6:00PM	\$3,500
Later than 6:00PM until 5:00AM	\$1,500/hr.

\* Closures of I-90 Mainline are limited to the restrictions as outlined in the special notes..

The importance of minimizing traffic impacts is so critical that the Thruway Authority has committed to pay an early completion incentive bonus for early return of traffic operations in less than the 58 weekend hours allotted for Completion of Work. The Thruway will pay this early completion incentive if all design plan requirements and specifications are met. This incentive shall be paid at a rate of \$3,000 per hour for every hour work is completed prior to 5:00 AM Monday. The maximum incentive payment is limited to \$90,000 (30 hrs. x \$3,000/hr.).

For this Special Note, Completion of Work shall be defined as: Finalizing all bridge superstructure work for the impacted bridge structure and site work necessary to open and return the full Interchange and Mainline to unimpeded traffic. This work also includes provision of stabilized grading of all disturbed areas caused by the Contractor operations to specified inclined slopes or flatter. Completion of the work also includes the removal of existing superstructure demolition waste/materials off site to a location of the Contractor's choice and responsibility. Completion of Work will not be considered as achieved until any

**INCENTIVE/DISINCENTIVE CLAUSE**

**SPECIAL NOTE**

damage to the Interchange 53 bridges or highway elements caused by the Contractor operations, that disrupt traffic at Interchange 53 and the highway mainline, are sufficiently corrected so that the affected feature(s) are returned to normal traffic and normal traffic flow.

As a minimum, the Contractor shall prepare a grading plan prior to beginning any weekend closure work that addresses at least the disturbed areas caused by crane placement. The Authority must be in agreement with the grading restoration plan for it to constitute meeting the project completion criteria. This plan should be approved prior to work under the weekend closure beginning.

In addition, closure windows relative to the I-90 Mainline Westbound traffic have been established by the Thruway. These allowable closure windows are found in the special notes. These closure windows consist of Friday night (10 hours max.) and Saturday night (12 hours max.). The Thruway wishes to minimize as much as possible these closures and their length and is offering incentives for early termination of these closure windows. An incentive of \$3,500 per hour will be offered for every hour the Contractor does not use the maximum allotted time for each closure window specified in the special note.

An equal value disincentive (\$3,500) applies for every hour the Contractor exceeds the maximum allowable hours of closure specified in the special note relative to the I-90 westbound closure.

The maximum incentive for each closure window period is \$35,000 or 10 hours x \$3,500.

The maximum disincentive for each closure period is \$42,000 or 12 hours x \$3,500.

CHECKED BY: J. SHINSKI  
 DRAFTED BY: J. DISHON  
 CHECKED BY: M. COFFI  
 DESIGNED BY: J. SHINSKI  
 DESIGN SUPERVISOR: M. COFFI

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|--|---|
| <p><b>DETOUR</b> M4-8 (30"x15") (Bk on Or)</p> <p><b>TO</b> M4-5 (30"x15") (Wh on Blue)</p> <p><b>EAST</b> M3-2 (30"x15") (Wh on Blue)</p> <p><b>INTERSTATE 90</b> M1-1 (36"x36")</p> <p><b>↑</b> M6-3 (21"x15") (Wh on Blue)</p> <p><b>(A)</b></p> <p><b>DETOUR</b> M4-8 (30"x15") (Bk on Or)</p> <p><b>TO</b> M4-5 (30"x15") (Wh on Blue)</p> <p><b>EAST</b> M3-2 (30"x15") (Wh on Blue)</p> <p><b>INTERSTATE 90</b> M1-1 (36"x36")</p> <p><b>↗</b> M5-2R (21"x15") (Wh on Blue)</p> <p><b>(C)</b></p> | <p><b>DETOUR</b> M4-8 (30"x15") (Bk on Or)</p> <p><b>TO</b> M4-5 (30"x15") (Wh on Blue)</p> <p><b>EAST</b> M3-2 (30"x15") (Wh on Blue)</p> <p><b>INTERSTATE 90</b> M1-1 (36"x36")</p> <p><b>→</b> M6-1R (21"x15") (Wh on Blue)</p> <p><b>(B)</b></p> <p><b>DETOUR</b> M4-8 (30"x15") (Bk on Or)</p> <p><b>TO</b> M4-5 (30"x15") (Wh on Blue)</p> <p><b>EAST</b> M3-2 (30"x15") (Wh on Blue)</p> <p><b>INTERSTATE 90</b> M1-1 (36"x36")</p> <p><b>↗</b> M6-2R (21"x15") (Wh on Blue)</p> <p><b>(D)</b></p> |
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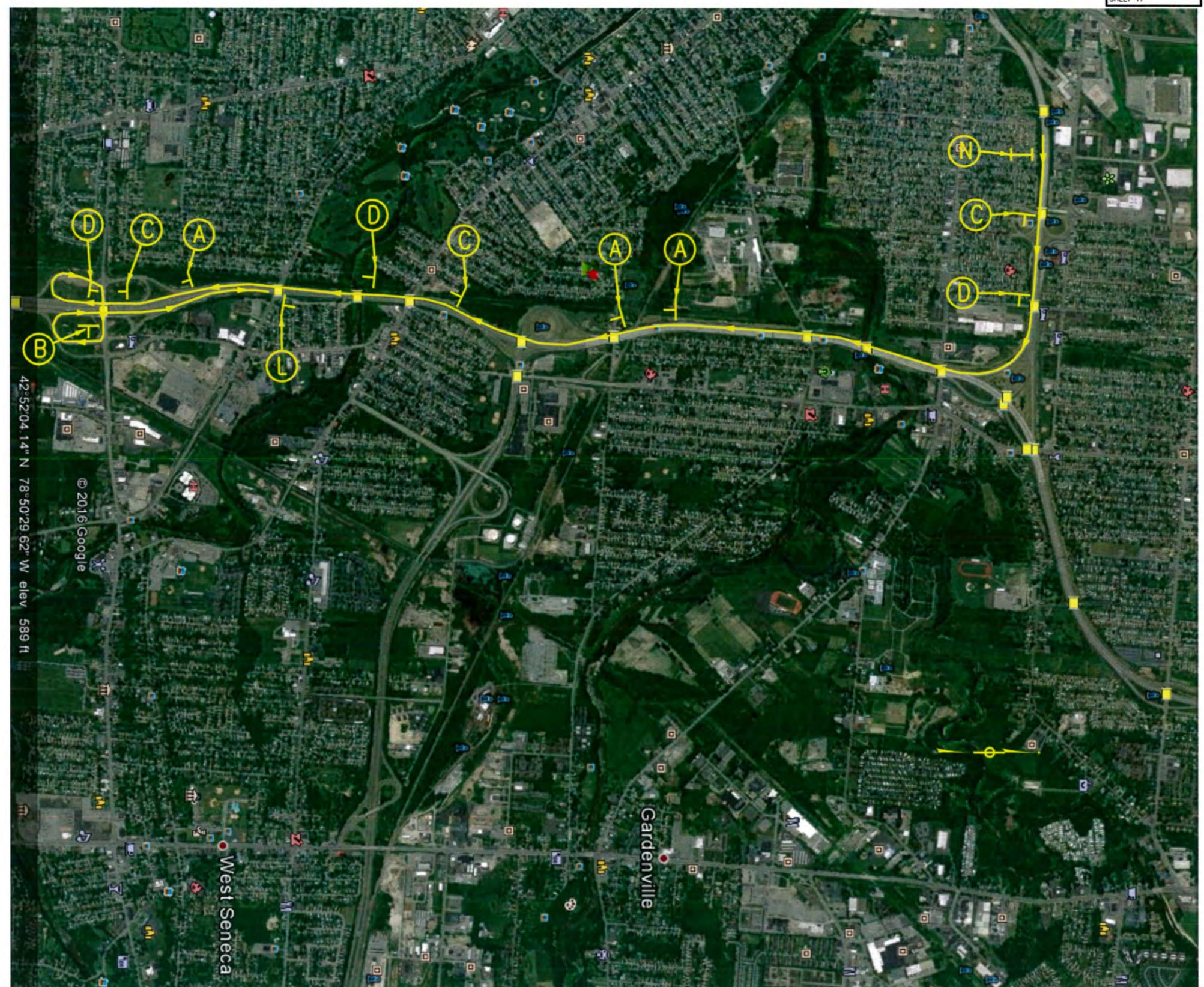
- (N)** PVMS ON I-190
- END DETOUR** M4-8a (30"x18") (Bk on Or)
- (L)**
- RAMP TO I-90 EB CLOSED**  
SUGGESTED DISPLAY • 1
- FOLLOW DETOUR**  
SUGGESTED DISPLAY • 2

**LEGEND**

— | — SIGN

— | — PORTABLE VMS

**NOTE:**  
SIGN LOCATIONS ARE APPROXIMATE ONLY. THE EXACT LOCATIONS SHALL BE A.O.B.E.



42°52'04.14" N 78°50'29.62" W elev 589 ft

ALTERED ON:	AFFIXED ON: 10/5/16
SIGNATURE:	SIGNATURE:
STAMP:	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

REVISIONS			
DATE	DESCRIPTION	BY	SYM.

	NEW YORK STATE THRUWAY AUTHORITY DEPARTMENT OF ENGINEERING 200 SOUTHERN BLVD., ALBANY, N.Y. 12209	TITLE OF PROJECT <b>REPAIRS TO THE IMPACT DAMAGED BRIDGE AT M.P. 426.18</b>	CONTRACT NUMBER: <b>TAB 16-45B</b>
	TITLE OF DRAWING <b>RAMP CLOSURE DETOUR SIGN PLAN I-90 EB</b>	LOCATION OF PROJECT <b>INTERCHANGE 53 RAMP BRIDGE OVER I-90</b>	DATE: <b>9/16</b>
			DRAWING NUMBER: <b>DT-1</b>



FILED  
 CHECKED BY: M. CIOFFI  
 DRAFTED BY: J. DISHON  
 CHECKED BY: J. SHINSKI  
 DESIGNED BY: C. SCHMIDT  
 DESIGN SUPERVISOR: M. CIOFFI

<b>DETOUR</b> TO <b>WEST</b> INTERSTATE <b>90</b>	M4-8 (30"x15") (Bk on Or) M4-5 (30"x15") (Wh on Blue) M3-4 (30"x15") (Wh on Blue) M1-1 (36"x36") M6-1L (21"x15") (Wh on Blue)	<b>DETOUR</b> TO <b>WEST</b> INTERSTATE <b>90</b>	M4-8 (30"x15") (Bk on Or) M4-5 (30"x15") (Wh on Blue) M3-4 (30"x15") (Wh on Blue) M1-1 (36"x36") M6-3 (21"x15") (Wh on Blue)	<b>DETOUR</b> TO <b>WEST</b> INTERSTATE <b>90</b>	M4-8 (30"x15") (Bk on Or) M4-5 (30"x15") (Wh on Blue) M3-4 (30"x15") (Wh on Blue) M1-1 (36"x36") M6-1R (21"x15") (Wh on Blue)
<b>DETOUR</b> TO <b>WEST</b> INTERSTATE <b>90</b>	M4-8 (30"x15") (Bk on Or) M4-5 (30"x15") (Wh on Blue) M3-4 (30"x15") (Wh on Blue) M1-1 (36"x36") M6-2L (21"x15") (Wh on Blue)	<b>DETOUR</b> TO <b>WEST</b> INTERSTATE <b>90</b>	M4-8 (30"x15") (Bk on Or) M4-5 (30"x15") (Wh on Blue) M3-4 (30"x15") (Wh on Blue) M1-1 (36"x36") M6-2R (21"x15") (Wh on Blue)	<b>DETOUR</b> TO <b>WEST</b> INTERSTATE <b>90</b>	M4-8 (30"x15") (Bk on Or) M4-5 (30"x15") (Wh on Blue) M3-4 (30"x15") (Wh on Blue) M1-1 (36"x36") M5-2R (21"x15") (Wh on Blue)

**S** PVMS ON I-90

**I-90 CLOSED AHEAD**      **FOLLOW DETOUR**

**SUGGESTED DISPLAY \* 1**      **SUGGESTED DISPLAY \* 2**

**END DETOUR**      M4-8a (30"x18") (Bk on Or)

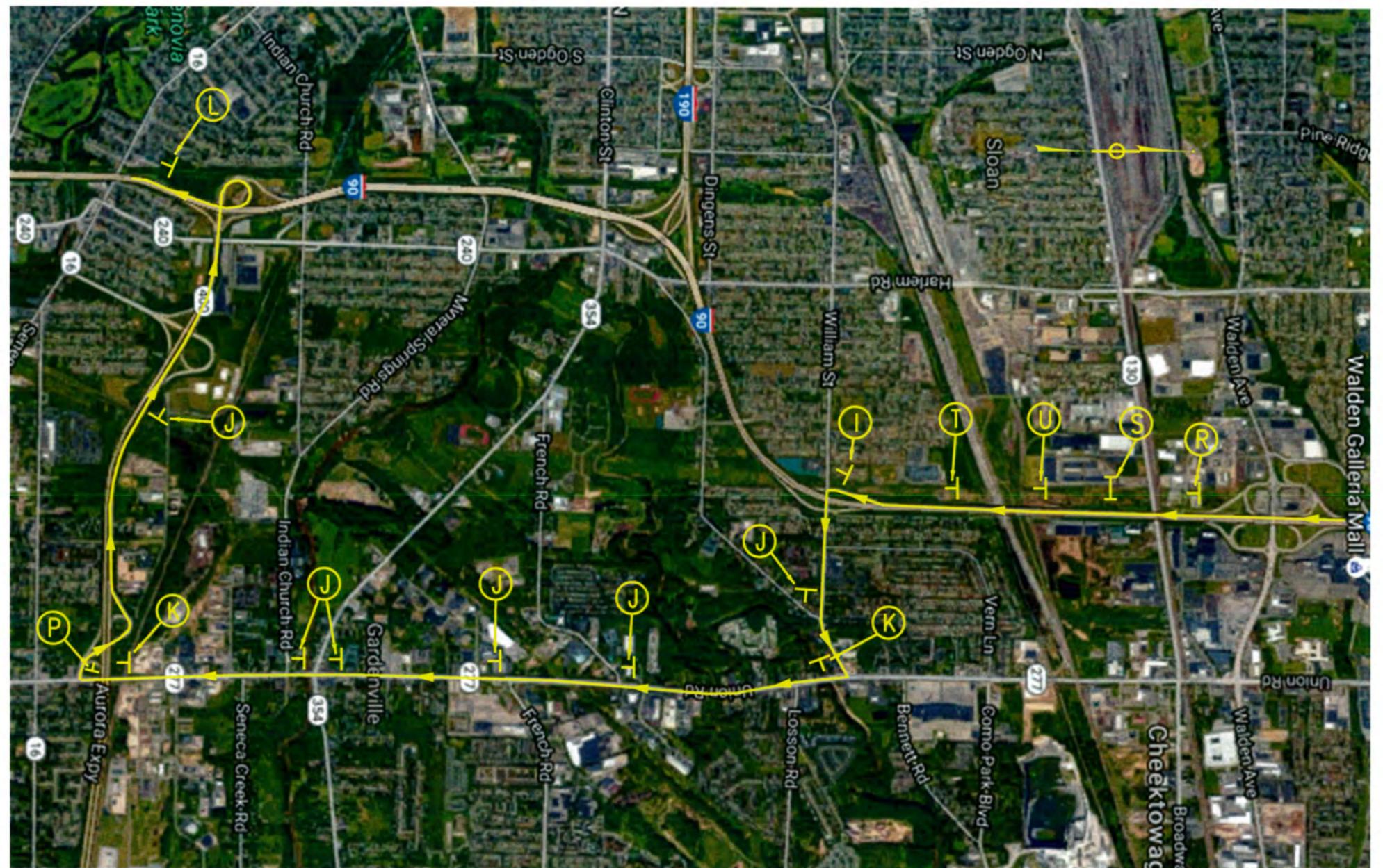
**L**

**LEGEND**

├ - SIGN

I - PORTABLE VMS

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ALTERED ON:	AFFIXED ON: 10/5/16
SIGNATURE: STAMP:	SIGNATURE: STAMP: 

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REVISIONS			
DATE	DESCRIPTION	BY	SYM.

**NEW YORK STATE OF OPPORTUNITY**

**Thruway Authority** | **Canal Corporation**

TITLE OF PROJECT REPAIRS TO THE IMPACT DAMAGED BRIDGE AT M.P. 426.18	CONTRACT NUMBER: TAB 16-45B
LOCATION OF PROJECT INTERCHANGE 53 RAMP BRIDGE OVER I-90	DATE: 9/16
TITLE OF DRAWING WESTBOUND I-90 CLOSURE	DRAWING NUMBER: DT-3