



**SYRACUSE DIVISION
BUNDLED BRIDGES**

TAS 17-37B, Contract No. D800001

REQUEST FOR PROPOSALS

**INSTRUCTIONS TO PROPOSERS
GENERAL INSTRUCTIONS**

Final, October 5, 2017

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1.9 MWBE UTILIZATION

This solicitation is subject to New York State provisions concerning the utilization of Minority and Women Business Enterprises (MWBE). All New York State MWBE requirements are applicable to this solicitation.

The overall MWBE participation goal for the Contract is described in ITP, Appendix A.

The Proposer must be responsive in submitting required documentation and forms and ensure that all information is timely, complete, accurate and truthful. The Proposal must provide participation information to the fullest extent possible; but may establish participation milestones and develop reasonably-close participation estimates with supporting justification as appropriate where participation information cannot be fully-finalized at time of award.

The Proposer must demonstrate a proactive good faith effort to recruit MWBEs and achieve commitments for work items prior to submission of the Proposal. While making commitments to firms for all MWBE opportunities are ideal prior to the proposal submission, the Authority recognizes it may not be practical. Where Form LDB Table 1 is not achieving the MWBE Goal, prior to submission of the proposal, commitments of specific work items, Form LDB Table 2, must be completed.

Failure to provide commitments to meet the established goal for the contract and failure to meet the good faith efforts will be grounds for rejection of the Proposal as non-responsive.

Only those MWBE firms that are certified under the New York State Empire Development Corporation are eligible to be used for goal attainment. Firms certified in states other than New York, or going through the certification process in New York are not eligible to be used for goal attainment unless they obtain certification under the New York State Empire Development Corporation prior to starting work on this Project. MWBE certification is not an endorsement of the quality or performance of the business but simply an acknowledgment of the firm's status as an MWBE.

The list of MWBE firms certified in New York State can be accessed via the following link to the New York State Unified Certification Program Business Directory website:

Empire State Development Corporation
Office of Minority and Women's Business Development
625 Broadway
Albany, NY 12207

<http://ny.newnycontracts.com/>

While only such MWBE-certified firms will count toward attaining the MWBE goal, the utilization of certified small businesses and MWBE/DBE firms is also encouraged.

If at the time of proposal submission the Proposer has committed towards the MWBE goal through the combined use of commitments to MWBE firms (Form LDB Table 1) and commitments to work items to be performed by MWBE during the time of the contract (Form LDB Table 2), the Proposer shall submit the Solicitation Log (Form AAP-10), together with other documentation that provides evidence of good faith efforts. If at the time of proposal the

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The Authority will not consider any correspondence delivered in any way except as specified above, except when the Authority may engage in One-on-One meetings with Proposers, as the Authority deems necessary.

Each Proposer is responsible for reviewing the RFP prior to the dates specified for submission of inquiries in the ITP Appendix A and for requesting clarification or interpretation of any discrepancy, deficiency, ambiguity, error, or omission contained therein, or of any provision that the Proposer otherwise fails to understand. The responses will not be considered part of the Contract but may be relevant in resolving any ambiguities in the Contract. Inquiries resulting in any modifications to the RFP will be documented in Addenda.

2.4 ONE-ON-ONE MEETINGS

Refer to ITP Appendix A for details on any One-on-One meetings the Authority may convene during the Project's Proposal period.

2.5 FREEDOM OF INFORMATION LEGISLATION

2.5.1 Freedom of Information Law (FOIL)

The Authority will maintain a non-public process for the duration of this DB procurement. Pursuant to Section 87(2)(c) of the Freedom of Information Law (FOIL), all records related to this procurement, including, but not limited to, SOQs, evaluation and Short-List procedures, proposals, evaluation and selection procedures, and, if applicable, any records created during the evaluation and selection process, will remain deniable records until the Contract has been fully executed and delivered.

All records pertaining to this procurement will become public information after execution of the Contract, unless such records are determined to be deniable under FOIL. Unless otherwise provided by law, records marked as "Exempt from Disclosure under FOIL" in a SOQ or Proposal submitted by an unsuccessful Proposer, which meet the requirements of FOIL, are not subject to inspection at any time by third parties under FOIL, and the records so marked by the successful Proposer in its SOQ and Proposal will remain confidential. Labeling all pages as "confidential" or "proprietary" or "Exempt from Disclosure under FOIL" is unacceptable – such proposals will not be accepted.

If a Proposer submits information in its Proposal that it believes to be deniable records under FOIL and that it wishes to protect from disclosure, the Proposer must do the following:

- A) Clearly mark all financial information, trade secrets, or other information "Exempt from Disclosure under FOIL" in its Proposal at the time the Proposal is submitted and include a cover sheet identifying each section and page which has been so marked;
- B) with respect to each such section and page, include a statement with its Proposal justifying the Proposer's determination that the identified information constitutes deniable records under FOIL; and
- C) provide an additional version of the Proposal submission in which confidential information has been redacted such that the redacted Proposal could without any further modification be inspected at any time by third parties under FOIL. The content of the redacted Proposal shall be identical to that of the original Proposal in all respects other than the redaction of deniable records.

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Proposers shall designate in writing those portions of the proposal that contain trade secrets or other proprietary information that are to remain confidential; that the material designated as confidential shall be readily separable from the entity's proposal.

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Unless otherwise provided by law, confidential business information provided to the Authority is not subject to inspection at any time by third persons under Article Six of the New York State Public Officers Law.

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2.5.2 Freedom of Information Act (FOIA)

FOIA applies to agencies and authorities of the Executive Branch of the US Government, including FHWA. FOIA establishes a presumption that records in the possession of such agencies and authorities are available to the public, subject to certain exemptions. FHWA has adopted a policy of clear presumption of disclosure on all FOIA requests. Therefore, all records in the possession of FHWA pertaining to this procurement may be disclosed under FOIA, except to the extent specifically exempted from disclosure under the act, which exemptions include trade secrets and commercial or financial information obtained from a person that is privileged or confidential. To the extent such information of the Design-Builder is included in the executed Contract or otherwise provided to FHWA or other federal agency or Authority pursuant to this procurement, and the Design-Builder wishes to protect it from disclosure, the Design-Builder shall clearly mark such records "Exempt from Disclosure under FOIA."

2.6 . PROPOSAL STIPEND

The Authority has decided that the payment of Stipends, under certain conditions, is proper as a part of this Design-Build Project.

Only Proposers that submit Proposals that (1) achieve a "pass" rating on all Pass/Fail Evaluation Factors; (2) meet or exceed the minimum qualifying quality based evaluation threshold of 60 points as required in the RFP; (3) are Competitive and Reasonable; and (4) are not selected by the Authority ("Stipend-Eligible Proposer") shall be eligible to be paid a Stipend within approximately 120 days after the execution of the Contract or the rejection of all Proposals. Proposers that do not meet the above criteria shall have no claim to a Stipend or compensation in any form based upon any legal or equitable theory. The Proposer awarded the Contract will not be eligible to receive a Stipend.

Stipend-Eligible Proposers must execute the Stipend Agreement and provide the Authority with the licenses required by Article 3 of the Stipend Agreement no later than 20 calendar days after the Proposal Due Date. Extensions may be granted at the sole discretion of the Authority. Any Stipend-Eligible Proposer that fails to execute the Stipend Agreement and provide the Authority with the licenses required by Article 3 of the Stipend Agreement no later than 20 calendar days after the Proposal Due Date, will forfeit all rights to receipt of a Stipend.

No Proposer will be eligible for payment of any Stipend Amount if the Proposer has filed an unsuccessful protest of the procurement process, award, or cancellation of the procurement. In addition, as a condition of accepting payment of any Stipend Amount, the Proposer shall agree to not file any protest of the procurement process, award, or cancellation of the procurement after accepting payment of the Stipend.

In consideration for paying the Stipend and executing the Stipend Agreement (see Appendix E, Form SA), the Authority may use any ideas or information contained in the Proposal, including Alternative Technical Concepts (ATCs) if requested, in connection with the Project or in

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5.0 PRE-PROPOSAL MEETINGS

5.1 INFORMATIONAL MEETINGS

- A) The Authority may hold joint informational meetings with all Proposers at any time prior to the Proposal Due Date; and
- B) The Authority may hold informational meetings with individual Proposers at any time prior to the specified Proposal Due Date. If individual informational meetings are offered to one or more Proposers, they will be offered to all Proposers.

5.2 ATTENDEES

If any informational meeting is held, the Proposer will be expected to attend with appropriate members of its proposed Key Personnel, and if required by the Authority, senior representatives of the proposed Contractor, the proposed Designer, the proposed Construction Inspection Firm and the proposed Materials and Testing Firm or Laboratory.

5.3 QUESTIONS AND RESPONSES

If any individual informational meeting is held, a question asked by any Proposer where a response is expected, will be recorded and the question and response will be provided in writing to all Proposers via the Project web site.

6.0 CONTRACT AWARD AND EXECUTION

6.1 CONTRACT AWARD

Unless all Proposals are rejected or this procurement is cancelled, the Contract shall be Awarded to the responsive Proposer offering a fully compliant Proposal that provides the "best value" to the State taking into consideration the quality and price factors discussed in this ITP.

Within five (5) business days after the selected Proposer is notified it has been selected as the Best Value Proposal, the selected Proposer shall:

- A) Notify the Authority in writing of the name and address of its agent for service of legal process for this Project. The Proposer shall not change that authorized agent without prior written notice to the Authority; and
- B) Provide the Authority, in writing, the Proposer's Federal Internal Revenue Service Employer Identification Number.

Following selection of the Best Value Proposal, the Authority will deliver, for execution, sets of the Contract Documents to the selected Proposer. Within five (5) business days of receipt of said execution documents, the selected Proposer shall deliver to the Authority the following:

- A) The executed DB Agreement together with evidence (if not previously provided) as to the authority, power, and capacity of said individuals to bind the proposer to the Contract. If the selected Proposer is a joint venture or partnership, the DB Agreement must be executed by all joint venture members, or all partners.
- B) Required Payment and Performance Bonds (Form PAB and Form PEB);

Insurance requirements and forms available at:
<http://www.thruway.ny.gov/business/consultants/forms/index.html#insurance>

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The following insurance documents are required:

- ACORD 25 (05/2010) → Available through insurance agency
- Supplemental Insurance Form (TA-W51349-9) → Available on NYSTA website
- Workers Compensation Forms (pick one) → Available through insurance agency
 - Form C-105.2 (private) or Form U-26.3 (State Insurance Fund)
 - Form SI-12 (Self Insurance) or Form GSI-105.2 (Group Self-Insurance)
 - CE-200
- Disability Benefits Insurance (pick one) → Available through insurance agency
 - Form DB-120.1
 - Form DB-155 (Self-Insurance)
 - CE-200 (for exemptions)

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- C) Any other documents or requirements reasonably requested by the Authority in connection with the finalization of the Contract.

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Failure to comply with the above requirements is a default and may result in forfeiture of the Proposal Bond.

6.2 EXECUTION OF CONTRACT

In case of failure or refusal on the part of the successful Proposer to deliver the duly executed Contract to the Authority within the five (5) business day period herein mentioned, the amount of the Proposal Bond may be forfeited and paid to the Authority .

The selected Proposer shall have the right to withdraw the Proposal without penalty if the Authority fails to return a fully executed Contract to the Proposer on or before the later of: (i) 45 days following receipt of the signed Contract from the successful Proposer, Payment and Performance Bonds and other required documents; or (ii) 120 days following the Proposal Due Date.

If the selected Proposer fails to execute and deliver the DB Agreement within the time periods identified above, award of the Contract may be made to the Proposer whose Proposal was the next best value to the Authority, taking into consideration the quality and price factors identified below, or the Work may be re-advertised and completed under a different contract or otherwise, as the Authority may decide.

The Contract will not be effective until the DB Agreement has been fully executed and delivered by all of the parties thereto.

6.3 OBLIGATION TO AWARD

The Authority shall be under no obligation to Award the Contract to:

- A) the Proposer submitting the lowest priced Proposal; or
- B) any Proposer, which has been included on the Short-List, or
- C) to award the Contract at all.

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**SYRACUSE DIVISION
BUNDLED BRIDGES**

TAS 17-37B, Contract D800001

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**APPENDIX A
PROJECT INFORMATION**

Final, October 5, 2017

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- Survey/Topographic Digital Terrain Model
- Hydraulic Cross Sections
- Hydraulic Studies
- CADD and InRoads Files
- Bridge Inspection Reports
- Bridge Load Ratings
- As-Built or Record Plans of Roadways and Structures within the project limits.
- Traffic Count and Other Traffic Information
- Geotechnical Data Report
- Subsurface Soil Borings and Bore Hole Location Plan
- Hazardous Material Screening Reports
- Wetland Delineation Reports
- Utility owner and permit information

Additional documents may be posted to the project website when they become available. Proposers are advised to check the Project web site regularly.

Reference Documents are located at the following Web site address:

<http://www.thruway.ny.gov/business/design-build/index.html>

A5.0 PROCUREMENT SCHEDULE

A5.1 ANTICIPATED PROCUREMENT SCHEDULE

The Authority anticipates the following procurement schedule for the Contract:

Activity	Date
Final RFP to Shortlisted Firms	October 5, 2017
Date Proposers may start submitting ATCs for review	October 6, 2017
Proposal period one-on-one meetings with all Proposers.	October 13, 2017 – November 3, 2017
Final date for Proposers to submit ATC's for review	November 08, 2017
Final date for requests for changes to Proposer's organization and personnel	November 09, 2017
Final date for Authority's responses to ATCs submitted for review	November 13, 2017
Final date for receipt of Proposer questions	November 17, 2017
Final date for Proposers to respond to conditional approval of ATC's	November 27, 2017
Issue Date for Final Addendum and/or answers to Proposer questions	November 27, 2017
Proposal Due Date	December 08, 2017
Post Proposal meetings (if required)	TBD

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Selection of Best Value	January 12, 2018
Limited Negotiations (if required)	TBD
Contract Award	March 02, 2018
Notice to Proceed	March 15, 2018

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This is a tentative schedule. All dates set forth in the preceding table and in this RFP are subject to change in the Authority's sole discretion. To the extent that dates are changed, the Authority shall notify the Proposers by Addendum.

A5.2 PROPOSAL DUE DATE

The completed Proposal shall be delivered to the Authority's Designated Representative at the address specified in Section A8.0, no later than 12:00 P.M. (midday) (Eastern Time), on the date specified in Section A5.1 (the "Proposal Due Date").

A6.0 CONFLICT OF INTEREST

It is prohibited to hire any person or organization that has a "conflict of interest". Because of their prior work, the following firms have been identified as having conflicts of interest that prevent their consideration for the pending Project. Due to a conflict of interest based on services currently being provided that are related to this Project, Proposers may not include the services of the following firm(s):

- Stantec
- Fisher Associates
- Popli Design Group
- Parsons Brinkerhoff
- WSP

Proposers utilizing firm(s) identified above will be disqualified from participating in this Project.

A7.0 MWBE PARTICIPATION GOAL

The overall MWBE participation goal for the Contract is established at 13% of the total Contract price.

A8.0 AUTHORITY'S DESIGNATED REPRESENTATIVE

The Authority's Designated Representative for this Procurement is:

James Chicoine and Michael Doyle
Attention: Syracuse Division Bundled Bridges Design-Build Project
Office of Capital and Contracts Management
New York State Thruway Authority
200 Southern Blvd., 2nd Floor
Albany, New York 12209, USA

[email: SyracuseDB@thruway.ny.gov](mailto:SyracuseDB@thruway.ny.gov)

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A9.3 STATEMENTS AT MEETINGS

Nothing stated at any meeting will modify the ITP or any other part of the RFP unless it is incorporated in an Addendum issued pursuant to ITP Section 2.3.1 or, in the case of an ATC, approved in writing in accordance with ITP Section A11.1.

A10.0 PROPOSAL STIPEND

Subject to the requirements and limitations set forth in the Stipend Agreement, the Authority shall pay to the Stipend-Eligible Proposer, and the Stipend-Eligible Proposer agrees to accept as full compensation for its Work Product, an amount (the "Stipend Amount") equal to 50% of the Proposer's total Qualified Costs, as substantiated in accordance with Article 4 (D) & (E) of the Stipend Agreement, not to exceed the amount listed in Article 4 (H) of the Stipend Agreement. Overhead costs are eligible. For qualified costs see Appendix G, Abbreviations and Definitions. Relative to overhead costs, the engineering firms established and current overhead rates with the Authority and NYSDOT will be used. However should the engineering firms overhead rates not be current or not accepted by the Authority and/or NYSDOT the overhead rate will default to 125%. The contractor's costs can include a 110% overhead rate.

A11.0 ALTERNATIVE TECHNICAL CONCEPTS (ATCS)

The Authority has chosen to use the confidential ATC process set forth in this ITP Appendix A, Section A11.0 to allow innovation and flexibility to be incorporated into the Proposals and considered in making the selection decision, to avoid delays and potential conflicts in the design associated with deferring of technical concept reviews to the post-award period and, ultimately, to obtain the best value for the public.

The ATC process allows a Proposer to submit for pre-approval, on a confidential basis, proposed alternatives to the requirements of Contract Documents, Part 3 – Project Requirements, and design solutions included in the Contract Documents. The Authority will not approve any ATC that entails a deviation from the requirements of the as-issued Contract Documents unless the Authority determines, in its sole discretion, that the proposed end product based on the deviation is equal to or better than the end product absent the deviation.

Any ATC that has been approved may be included in the Proposal. If an ATC is conditionally approved, the Proposer must respond, accept, and meet the conditions set forth by the Authority by the date indicated in the ITP Appendix A, if the Proposer intends to use the ATC in their Proposal.

A11.1 SUBMITTAL AND REVIEW OF ATCS

A Proposer may submit ATCs for review to the Authority until the date identified in ITP Appendix A. All ATCs shall be submitted in writing, using Form ATC included in ITP Appendix E – Forms, to the Authority's Designated Representative at the e-mail address identified in ITP Appendix A, with a cover letter clearly identifying the submittal as a request for review of an ATC under this ITP. If the Proposer does not clearly designate its submittal as an ATC, the submission will not be treated as an ATC by the Authority.

The Authority will review each ATC submitted and will use best efforts to provide a response within two weeks. Proposers submitting multiple ATCs shall indicate an order of priority to assist the Authority in determining which ATCs should be reviewed first.

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REQUEST FOR PROPOSALS

INSTRUCTIONS TO PROPOSERS

**APPENDIX B
ADMINISTRATIVE SUBMITTAL
REQUIREMENTS**

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5, 2017**

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B2.2.3 Forms and Certificates

Proposers shall submit the Forms in the format as they are provided in the ITP, and shall not alter them. This includes:

- not embedding links to external web pages or photographs,
- not increasing the page length of the Form, unless explicitly allowed on the Form,
- including all Footers and Headers shown on the Forms,
- not inserting company logo's.

Any Form found to be altered may, at the discretion of the Authority's Office of Capital and Contracts Management, be removed from the Proposal before evaluation.

In addition to the Form of Proposal (Form FP) and Appendix to Form of Proposal, (Form FP(A)) described in Section B2.1, provide the following:

- a) Form AAP-10 MWBE Solicitation Log;
- b) Form AR, Acknowledgement of Receipt of RFP, Addenda and Responses to Questions;
- c) Form C, Proposer's Representative;
- d) Form CR, Commitment to Assign Identified Resources to Project, providing a written commitment, signed by the designated Project Principal, that the resources identified in the Proposal, including Key Personnel and identified design staff, will be available and assigned to the Project if the Proposer is awarded the Contract, to the extent such assignment remains within the control of the Proposer;
- e) Form EEO, Equal Employment Opportunity Certification;
- f) Form KP, Key Personnel Information
- g) Form L-3, Authorization to Provide Professional Services in New York State;
- h) Form LC, Lobbying Certificate;
- i) Form LLL, Disclosure of Lobbying Activities
- j) Form LDB, List of Proposed MWBEs;
- k) Form LSI, Letter of Subcontract Intent;
- l) Form NC, Non-Collusion Affidavit, certifying that the Proposal is not the result of, and has not been influenced by, collusion
- m) Form RFC, Request for Change (Changes in Participants or Key Personnel (if approved by the Authority));
- n) Form SDU, Schedule of Proposed MWBE Utilization; and
- o) Form U, Conflict Questionnaire. Organizational Conflicts of Interest Disclosure Statement (see ITP Section B2.3.4).

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B2.2.4 Conflict of Interest Disclosure

Provide a conflict of interest disclosure statement on Form U (ITP Appendix E) if and as required by ITP Appendix F, identifying and describing any potential Organizational Conflicts of Interest and any relevant facts concerning past, present or currently planned interests that may

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Table B
Format of Volume 1

Proposal Section No.	Required Information	ITP Reference
Section 1 (Proposer's Offer)	<ul style="list-style-type: none"> Form FP, Form of Proposal Form FP(A), Appendix to Form of Proposal 	B2.1
Section 2 (Proposer Information)	<ul style="list-style-type: none"> Summary of Organization (2 single sided pages) Changes in Organization (Form RFC)(If applicable) Licensing Information 	B2.2
Section 3 (Forms and Certificates)	<ul style="list-style-type: none"> Form AAP-10 MWBE Solicitation Log Form AR, Acknowledgement of Receipt of RFP, Addenda and Responses to Questions Form C, Proposer's Representative Form CR, Commitment to Assign Identified Resources to Project Form EEO, Equal Employment Opportunity Certification Form KP, Key Personnel Information Form L-3, Authorization to Provide Professional Services in New York State Form LC, Lobbying Certificate Form LLL, Disclosure of Lobbying Activities Form LDB, List of Proposed MWBEs Form LSI, Letter of Subcontract Intent Form NC, Non-Collusion Affidavit Form SDU, Schedule of Proposed MWBE Utilization Form U, Conflict Questionnaire Surety Commitment Letters 	B2.2
Appendix A	<ul style="list-style-type: none"> Evidence of Authorization Joint and Several Liability Statement (If Applicable) Organizational Documents 	B2.3
Appendix B	<ul style="list-style-type: none"> Updated Financial Information 	B2.3.4

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Form G, Gantt Chart

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**SYRACUSE DIVISION
BUNDLED BRIDGES**

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REQUEST FOR PROPOSALS

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**APPENDIX C
TECHNICAL PROPOSAL
SUBMITTAL REQUIREMENTS**

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C1.0 GENERAL INSTRUCTIONS

This ITP Appendix C provides the general instructions and establishes the content and formatting requirements for the Technical Proposal, Volumes 2, 2A and 2B. Additional criteria is outlined in ITP Appendix A.

Each Proposer should submit the Technical Proposal required pursuant to this ITP Appendix C, organized, separated and labeled in accordance with the format in Table C.

The submittals should be limited to the page limitations (if any) specified in this ITP Appendix C. Each sheet shall be 8.5" by 11" and printed double sided, unless otherwise stated below. Text shall be in a standard font, a minimum of ten points in height, single-spaced. All design drawings submitted with Proposals shall be printed single-sided on 11" by 17" sheets, and all as-printed text font sizes on plans shall be at least 8 points or per NYSDOT HDM Standards.

C2.0 DESIGN-BUILDER'S ORGANIZATION AND PROCESS

C2.1 KEY PERSONNEL

The Proposer shall include Form R – Summary Individual's Experience for each of the Key Personnel identified in the ITP Appendix A, outlining his/her experience and qualifications.

The content of each Form R that should be filled in includes:

- A) Proposed role on Project;
- B) Relevant licenses, registrations and certifications;
- C) Total years of professional experience and years of experience performing the work the individual would perform on this Project; Form R for Quality Manager should include an attached sheet that provides a description of experience in quality systems based on ISO 9001 if applicable;
- D) Relevant project experience including project names, locations and total construction costs; the individual's start and end dates on each project; the individual's role on each project; the duties performed on each project. ~~Contact Information should be owners or clients for whom the individual has performed project work for in the past five (5) years and should not be current employers of the individual. The proposed key personnel individual shall have performed the work duties being evaluated, in the past year.~~
- E)
- F) Employment time with participant;
- G) Percent time (percentage of working time) allocated/committed to this Project for each 12 month period of the Project from its NTP;
- H) If more than one key position is to be filled by the same individual, so indicate.

The Proposer should include Form KP in Volume 1 to communicate any approved changes in the Proposer's proposed roster of Key Personnel, relative to the Proposer's SOQ submission. For each change in Key Personnel since the SOQ, the Proposer should include in the Proposal with Form KP a copy of the written approval received from the Authority for such change (see ITP Section 1.15), details of such Key Personnel's role and a completely filled in Form R of the substitute personnel. If no changes in Key Personnel have been requested since the SOQ, Proposers should use Form KP to state that there is no change relative to the SOQ.

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- F) Provide plans showing the approximate limits of existing element removals that will be required to complete the Project.
- G) Table of minimum vertical clearances to be provided at each span that lists the dimension, location on bridge, and location on crossing road. Drawings may be used in combination with or in place of the table.
- H) The elevation views of the sheets submitted by the proposer should include the Aesthetic enhancements. More specifically, the abutment, wingwalls, piers and concrete barrier sections. Refer to Part 3 Section 10 for more description of the Aesthetic requirements.

C3.3 DESIGN BUILD CONSTRUCTION APPROACH (MEANS AND METHODS)

C3.3.1 Overall Construction Sequence of the Work

Provide a narrative describing the overall construction sequence of the Work in the Contract, including all staging areas, as well as the final permanent footprint of the constructed improvements. The narrative **shall** discuss the logistics and challenges of constructing the project elements while meeting the Work Zone Traffic Control requirements, and shall discuss why the sequence was chosen, how the sequence benefits the Authority, and why it is the best solution for constructing the project elements. The Narrative **shall** also discuss the requirements as stated under ITP, Appendix A, Section A12.1.2.3.

C3.3.2 Work Zone Traffic Control (WZTC)

Provide a narrative describing the proposed WZTC including a description of the Proposer's plan to maintain the safety and use of traffic corridors and minimize disruption on existing roads as a result of the construction work. At a minimum, the Work Zone Traffic Control narrative should:

- A) Describe the major phases of the Work;
- B) Include complete typical sections by phase, including information regarding maintenance of access and egress. It should provide phase notes and details regarding sequence of work activities (e.g., specialized equipment needs and false-work);
- C) Identify each affected road and access way within the vicinity of the Project site, and describe the potential impacts, mitigation measures, limitations of use, and the number and duration of time that each road and access way may be impacted in performing the Work, including information regarding detours;
- D) Describe the Proposer's approach to accommodate local events, emergency service providers and commercial vehicles; and
- E) Describe how the WZTC will be coordinated with adjacent projects to provide safe passage of traffic.

C3.3.3 Protection of Existing Facilities

Provide a narrative describing the proposed methods of protecting existing facilities, including a description of the specific means the Proposer intends to use to minimize impacts to existing utilities, private residences, local roads and properties adjacent to or within the Project Limits. This description should identify how the Proposer intends to mitigate impacts due to vibrations conditions and other effects of the Proposer's construction operations.

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(pdf). The compact disk shall be labeled "Syracuse Division Bundled Bridges, Project INITIAL BASELINE PROGRESS SCHEDULE, then the Proposer's name.

C4.2 SCHEDULE OF CONTRACT DURATIONS

The Proposer should submit Form SCD.

C4.3 GANTT CHART

The Proposer should submit the Gantt Chart, supplied on Form G, which is a simplification of the Initial Baseline Progress Schedule showing all major construction activities. The Gantt Chart shall be presented in hard copy printed on an 11" by 17" sheets with all as-printed font sizes at least 8 point. For the purposes of this section, the following are examples of major construction activities (if applicable):

- Start of Construction
- Bridge Demolition of each span and each stage;
- Substructure construction;
- Superstructure construction;
- Approach slab construction;
- Highway paving work;
- Traffic Signal Work; and
- Utility relocations
- Construction Completion
- Demolition from each site and overall,

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C5.0 FORMAT OF VOLUME 2

Organize Volume 2 in the format shown in Table C, with the cover of the volume labeled as follows:

<p>Syracuse Division Bundled Bridges DESIGN-BUILDER'S PROPOSAL VOLUME 2 TECHNICAL PROPOSAL</p> <p>PROPOSER: _____</p>
--

Provide the Technical Proposal submittals in Volume 2 in the order set forth in Table C. Separate the individual submittals in Volume 2 with tabs labeled as outlined in Table C (e.g., "Design-Build Quality Control Plan"), and use a copy of the Table C as a checklist for the basis of the table of contents for Volume 2 (which shall be the first page of Volume 2).

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Table C
Format of Volume 2

Proposal Component	Reference
Volume 2, Section A – Design-Build Organization and Process	
Volume 2, Section A1 – Key Personnel	
Key Personnel Form R	C2.1
Volume 2, Section A2 – Overall Design-Build Team Organization and Approach to Quality	
Design-Build Organization Chart (Narrative, Max 5 pages plus 11" x 17" org. chart)	C2.2.1
Design-Build Team Communication Protocol (Narrative, max. 5 pages plus 11" x 17" communication graphic)	C2.2.2
Design-Build Quality Control Plan (max. 15 pages plus org. chart)	C2.2.3
Volume 2, Section B – Design Build Approach to the Project (Technical Solutions)	
Volume 2, Section B1– Project Understanding	
Project Understanding (Narrative, max. 6 Pages)	C3.1
Volume 2, Section B2 – Design-Build Approach to Design	
Design Narrative (Narrative, max. 10 pages)	C3.2.1
Copies of Authority's approval letters for each ATC that is incorporated into the Proposer's Proposal along with each submitted ATC that was approved and used.	C3.2.1
Volume 2, Section B3 – Design Build Construction Approach (Means and Methods)	
Overall Project Construction Sequence (max. 6 pages)	C3.3.1
Work Zone Traffic Control (max. 3 pages)	C3.3.2
Protection of Existing Facilities (max. 1 page)	C3.3.3
Utility Work (max. 1 page)	C3.3.4
Drainage Modifications (max. 1 page)	C3.3.5
Volume 2A, Attachment A – Design Drawings (separate 11" x 17" binder)	
Project Limits	C3.2.2
General Configurations	C3.2.2
Construction Phasing	C3.2.2
Demolition Limits	C3.2.2
Work Zone Traffic Control	C3.3.2
Volume 2B, Attachment B – Project Schedule	
Initial Baseline Progress Schedule (max. 3 pages)	C4.1
Initial Baseline Progress Schedule Narrative (max. 3 pages)	C4.1
Form SCD – Schedule of Contract Durations	C4.2
Form G – Gantt Chart (max. 4 pages)	C4.3

Note: Volume 2A, Attachment A – Design Drawings, shall be submitted in a separate 11" x 17" binder.

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SYRACUSE DIVISION BUNDLED BRIDGES

TAS 17-37B, Contract No. D800001

REQUEST FOR PROPOSALS INSTRUCTIONS TO PROPOSERS

APPENDIX E

FORMS

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Final, October 5, 2017

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**FORM AR - ACKNOWLEDGMENT OF RECEIPT OF RFP,
ADDENDA AND RESPONSES TO QUESTIONS**

(to be attached to Volume 1 of Proposal)

NAME OF PROPOSER

We hereby acknowledge receipt of the Syracuse Division Bundled Bridges, Design-Build Project RFP, dated Final, October 5, 2017 and subsequent responses to questions and Addenda issued by the Authority, as listed below.

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Add additional lines in tables below, if needed.

Addendum number:	Date issued by Authority:

Responses to questions number:	Date issued by Authority:

SIGNED	
DATE	
NAME (printed or typed)	
TITLE	

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**FORM R
SUMMARY OF INDIVIDUAL'S EXPERIENCE**

Form R shall be completed by the Proposer for the Key Personnel indicated in ITP Appendix A, as well as any key personnel that have been changed since submission of the SOQ. Add lines/pages as necessary. Under "Contact Information", provide the contact name, phone number, and e-mail address for the Project Owner. NYSTA reserves the right to contact any Project Owner to verify the information provided. "Project Owner" is not a Prime Consultant or Prime Contractor for which the Firm served as a subconsultant or subcontractor. "Project Name" should include the relevant contract number if available.

Name of Proposer:					
Name of Firm:					
Individual's Name:					
Is Applicant Licensed as a Professional Engineer in the State of New York?				Yes:	No:
NYS Professional Engineering License Number					
Education:					
Years of Experience					
Years with this Firm					
Title for this Project:					
12 Month period (NTP to month 12) (beg mo - end mo)	Percentage	12 Month period (13th to 24th month) (beg mo - end mo)	Percentage	12 Month period (25th to completion date) (beg mo - end mo)	Percentage
Total number of year's experience meeting requirements stated in Appendix A for Title above:					
Please complete the information below to confirm the total year's experience stated above					
Project Name:					
Project Owner:					
Contact Information:					
Project Cost Value					
Title on Project:					
% Qualifying time spent on Project					
Individual's Start Date on Project:		Individual's End Date on Project:		Individual's Total Time on Project *:	
Project Description:					
Individual's Duties and Responsibilities:					
Project Name:					
Project Owner:					

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FORM SA
STIPEND AGREEMENT

COMPTROLLER'S CONTRACT NO.:

PIN:

PROJECT: Stipend Agreement for the Preparation & Submission of the Design Build Proposal for Syracuse Division Bundled Bridges

This Agreement made this ___ day of _____, 20___, by and between NEW YORK STATE THRUWAY AUTHORITY (hereinafter "NYSTA"), whose principal office is located at 200 Southern Blvd., Albany, New York 12209, and _____, duly organized and existing under the laws of the State of New York, having its principal office at _____ (hereinafter referred to as "Proposer").

WITNESSETH:

WHEREAS, NYSTA is currently procuring a design-build contract (the "Contract") for the _____ Project (the "Project");

WHEREAS, the Proposer submitted a proposal ("the Proposal") in response to the Request for Proposals issued by NYSTA on _____, as amended by any addenda (as amended, the "RFP");

WHEREAS, the Proposal met the criteria set forth in Article 4(A) of this Stipend Agreement; and

WHEREAS, the Authority has not awarded the Contract to the Proposer.

NOW, THEREFORE, the parties agree as follows:

ARTICLE 1. DOCUMENTS FORMING THIS AGREEMENT. The Agreement consists of the following in the following descending order of precedence:

1. Appendix A – New York State Required Contract Provisions
2. Agreement Form – this document titled "Stipend Agreement"
3. Attachment 1 to Stipend Agreement

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ARTICLE 2. WORK PRODUCT AND INTELLECTUAL PROPERTY RIGHTS. The term "Work Product" shall mean ideas or information, and the expression of ideas or information, included in the Proposal or otherwise submitted or communicated in any manner by or on behalf of the Proposer to NYSTA during the Project's procurement process at any time prior to the awarding of the Contract. Work Product does not include any sensitive and confidential financial information regarding the Proposer that was included in the Proposal.

ARTICLE 3. NYSTA'S RIGHTS TO WORK PRODUCT. Under the terms of this Agreement, NYSTA may use the Work Product for the purposes of the Project or any future

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FORM SCD
SCHEDULE OF CONTRACT DURATIONS

Table SCD - 1

OVERALL PROJECT COMPLETION			
ACTIVITY	DURATION (Calendar Days past NTP) <i>(See Note 6)</i>	PROJECTED COMPLETION DATE (MM/DD/YYYY)	LIQUIDATED DAMAGES AMOUNT (PER DAY) BONUS DAYS (See Note 2 & 4)
PROJECT COMPLETION (See Notes 1, 2)			\$20,000.00

Table SCD - 2

SPECIFIC DEFINED COMPLETION MILESTONE				
MILESTONE (See Notes 3, 4)	PROJECTED CLOSING DATE (MM/DD/YYYY)	PROJECTED OPENING DATE (MM/DD/YYYY)	DURATION (Calendar Days)	LIQUIDATED DAMAGES AMOUNT (PER DAY) BONUS DAYS (See Note 3 & 4)
ALL TRAFFIC PERMANENTLY TRANSFERRED ONTO THE NEW (BRIDGE STRUCTURE / ROADWAY / OTHER) (See Note 5)				
BIN 5512980 MP 240.48				\$2000
BIN 5512790 MP 262.01				\$2000

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New York State Thruway Authority

Notes:

- 1.) The Project Completion Date, to be included in the DB Agreement, Article 2.3, shall be defined by the number of calendar days past NTP as proposed by the successful Proposer and agreed to by the Authority. Project Completion for the purposes of this Form SCD-1 is defined as all construction activities completed, no additional impacts to traffic, and complete demobilization from the work site(s). Remaining paperwork (i.e. As-Builts, close-out documentation, payments) may occur after the Project Completion date for the purposes of this Form SCD.
- 2.) Liquidated Damages will be assessed, in the amount indicated, for failure to achieve Project Completion by the Project Completion Date in accordance with Articles 2.3 and 16 of the DB Agreement.
- 3.) Liquidated Damages will be assessed, in the amount indicated for failure to achieve the Defined Completion Milestone Date in accordance with Article 2.2 and 16 of the DB Agreement.
- 4.) An Early Completion Bonus will be paid for: each calendar day work is completed in advance of the Overall Project Completion date submitted by the Design-Builder; each calendar day work is completed in advance of the Defined Completion Milestone Date(s) submitted by the Design-Builder. Such bonus days shall be based on the durations / number of days listed in Tables SCD-1, SCD-2, subject to the daily bonus amounts and maximum bonus amounts listed in SP-6.
- 5.) "All traffic permanently restored onto the new bridge structure / roadway / other" means no additional lane or shoulder closures in the specified direction of the new structure, including auxiliary lanes.
- 6.) The duration shall be inclusive of the Notice to Proceed date and the projected completion date.

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The Proposer commits to meet the Contract Durations specified above.

PROPOSER	
SIGNED	
DATE	
NAME (printed or typed)	
TITLE	

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**SYRACUSE DIVISION
BUNDLED BRIDGES**

TAS 17-37B, Contract No. D800001

**REQUEST FOR PROPOSALS
INSTRUCTIONS TO PROPOSERS**

**APPENDIX G
ABBREVIATIONS AND DEFINITIONS**

Final, October 5, 2017

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the end product absent the deviation and approval of any deviations from the Project Environmental Approvals.

“Authority” means the New York State Thruway Authority.

“Award” means the decision of the Authority to accept a responsive Proposal from a responsible Proposer that provides the Best Value to the Authority for the work identified in the RFP, subject to the execution and approval of a satisfactory Contract, provision of Labor and Material and Performance Bonds to secure the payment and performance thereof, provision of such insurance as is required under the Contract, and the satisfaction of such other conditions as may be specified or otherwise required by law.

“Baseline Progress Schedule” means the time-scaled, Critical Path network schedule, updated from time to time in accordance with the Contract and depicting the subordinate activities and their durations, sequences, and interrelationships that represent the Design-Builder’s Work plans; the Design-Builder’s Work Breakdown Structure (WBS) for designing, constructing, and completing the Project.

“Clarifications” means a written or oral exchange of information that takes place between a Proposer and the Authority after the receipt of all SOQs during the evaluation process. The purpose of Clarifications is to address minor ambiguities, omissions, errors or mistakes and clerical revisions in an SOQ.

“Competitive and Reasonable” means a Proposer’s proposed Lump Sum Price for the Contract that does not exceed 150% of the Lump Sum Price for the Contract included in the selected Best Value Proposal.

“Construction Inspection (CI)” means to inspect all construction operations and to enforce all safety measures (for employees and the traveling public) performed by the Design-Builder to ensure conformance with the contract documents. This includes performing daily inspection and testing activities in accordance with all the requirements set forth in Authority policies, manuals, engineering bulletins, and engineering instructions; preparation of applicable MURK forms; preparation of monthly estimates; monitoring compliance to safety procedures, including fall protection and work zone traffic control (WZTC) requirements; monitoring compliance to environmental requirements. Construction Inspection also includes Contract Administration functions including, but not limited to keeping required records, monitoring the DB Contractor’s progress, monitoring certified payroll compliance and processing of payments, monitoring adherence to Equal Opportunity and Labor requirements contained in the contract, taking measurements as required for payment, and maintaining a contemporaneous project diary documenting conformance with the contract documents. The Scope of Work to be performed as part of the Construction Inspection task may be changed after the RFQ Phase.

“Construction Inspection Professional Engineering Firm” means an independent Engineering firm, licensed in New York State to perform Engineering Services and having experience in Construction Inspection as defined herein. This firm shall be included as part of the Proposer’s team during the RFP Phase and shall be hired by the Design-Builder to perform Construction Inspection for the Project. The Construction Inspection Professional Engineering Firm shall report to the Design-Builder’s Quality Manager and supervise the Materials Testing Firm or Laboratory performing sampling and testing of materials.

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“Design Manager” means the Design-Builder’s designated person who shall have primary responsibility for coordination and oversight of all the Project Designs including design plans, calculations, and specifications. He/She shall be a registered Professional Engineer in the State of New York.

“Design Quality Assurance Engineer” means the Authority’s representative with primary responsibility for monitoring and/or auditing the Design-Builder’s design and engineering activities for compliance with the Contract requirements and the Design-Builder’s Quality Control Plan.

“Design Quality Control Engineer” means the person appointed by the Design-Builder who reports directly to the Design-Builder’s Quality Manager and is responsible for the QC of all Work conducted by the Designer. The Design QC Engineer shall be a New York-licensed professional engineer with similar experience as the Design Manager. The Design QC Engineer shall ensure that checkers are assigned for each design discipline and for each Design Unit and that they are properly scheduled.

“Designer” means a Principal Participant, Required Design-Build team member, or in-house designer that has primary responsibility for design services for the Project.

“Disadvantaged Business Enterprise (DBE)” means a for-profit, small business concern as defined pursuant to Section 3 of the Federal Small Business Act (Public Law 85-536, as amended) and Small Business Administration regulations implementing it (13 CFR Part 121) that is at least 51 percent owned by one or more individuals who are both socially and economically disadvantaged or, in the case of a corporation, in which 51 percent of the stock is owned by one or more such individuals and whose management and daily business operations are controlled by one or more of the socially and economically disadvantaged individuals who own it, which meets the definitions set forth in 49 Code of Federal Regulations (CFR) 26.

“Engineer-in-Charge (EIC)” means the Authority’s Project Manager or designated representative when used in the NYSDOT/Authority standard specifications when used in the Archaeological Work Plan (AWP), the Construction Protection Plan (CPP), and the Stormwater Pollution Prevention Plan (SWPPP), Engineer-in-Charge (EIC) means the Design-Builder’s Resident Engineer.

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“Executive Director” means the Executive Director of the New York State Thruway Authority.

“Equity Participant” means any Person holding (directly or indirectly) a 15% or greater interest in the Proposer.

“Fabricator” means an individual, partnership, firm, Limited Liability Company (LLC), corporation, or joint venture with which the Design-Builder subcontracts to assemble, construct, or otherwise substantially alter material or supplies into assemblies, components, or finished items for inclusion into the work prior to resale.

“Fast Track Design” means the process of performing the design of a project in increments of the final design for the purpose of allowing the project construction to begin before the final design of a project is completed. Fast Track Design will allow the project design and construction activities to overlap and occur simultaneously, thereby shortening the total duration of those activities.

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“Good Faith Efforts (GFE)” means the efforts to achieve a MWBE goal or other MWBE requirement which, by their scope, intensity, and appropriateness to the objective, can reasonably be expected to fulfill the program requirement. Good Faith Efforts shall meet the requirements of 49 Code of Federal Regulations (CFR) 26.53.

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“Independent Assurance (IA)” means activities that are undertaken in accordance with 23 CFR 637.203(a)(2), providing an unbiased and independent evaluation of all the sampling and testing procedures, equipment calibration, and qualifications of personnel (Design-Builder’s or Authority’s) used in the Acceptance Program, including the Design-Builder’s QC. The Independent Assurance (IA) agent for the Project will be designated by the Authority.

“Instructions to Proposers (ITP)” means those documents containing directions for the preparation and submittal of information by the Proposers in response to the RFP.

“Lead Civil Engineer” means the Design-Builder’s designated person who shall have primary responsibility for the design of all Highway and Traffic elements of the Project. Such individual shall be a licensed New York State Professional Engineer.

“Lead Geotechnical Engineer” means the Design-Builder’s designated person who shall have primary responsibility for the design of all Geotechnical elements of the Project. Such individual shall be a licensed New York State Professional Engineer.

“Lead Hydraulic Engineer” means the Design-Builder’s designated person who shall be responsible for meeting the RFP hydraulic requirements and making hydraulic determinations related to the project. Such individual shall be a licensed New York State Professional Engineer and shall be the Responsible Engineer regarding hydraulic design.

“Lead Structural Engineer” means the Design-Builder’s designated person who shall have primary responsibility for the design of all Structural elements of the Project. Such individual shall be a licensed New York State Professional Engineer.

“Lead Principal Participant” means the Principal Participant that is designated by the Proposer as having the lead responsibility for managing the Proposer’s organization.

“Manufacturer” means a Manufacturer as an entity that operates or maintains a factory or establishment that produces on its premises the material, equipment, or supplies obtained by the Design-Builder for incorporation into the Project.

“Material” means any approved material acceptable to the Authority and conforming to the requirements of the Specifications.

“Materials Testing Firm or Laboratory” means an independent testing firm or Laboratory having experience in performing Quality Control activities as defined herein. This firm shall be included as part of the Proposer’s team during the RFQ and RFP Phase and shall be hired by the Design-Builder to perform Quality Control sampling and Testing activities for the Project. The Materials Testing Firm or Laboratory shall report to the Construction Inspection Professional Engineering firm.

“Minority-owned Business Enterprise (MBE)” means a business enterprise, including a sole proprietorship, partnership, or corporation that is a small business with at least 51% owned by

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one or more minority group members and meets the definition set forth in Article 15-A of the New York State Executive Law.

“Non-responsive Price Proposal” means a Proposer’s Price Proposal which is incomplete or which contains mathematical errors.

“Notice to Proceed” means written notice to the Design-Builder to proceed with some or all of the Work as specified in the Contract including, when applicable, the beginning date of the Contract Time. See also, Work Order.

“Oversight” means actions by the Authority to satisfy itself that the Design-Builder is designing, constructing and managing the Work in accordance with the Contract Documents. It includes actions identified in the Contract Documents by the terms QA, Independent Assurance, Verification Sampling and Testing; compliant/compliance, accept/acceptance, inspect/inspection, audit, ensure, certify, confirm, review, verify or terms of similar import. Authority comments as a result of Oversight are conveyed to the Design-Builder through Consultation and Written Comment and shall be documented in a Non-Conformance Report if appropriate. Neither the activity of Oversight nor the lack of Consultation and Written Comment on the part of the Authority shall be construed to relieve the Design-Builder and its organization from the responsibility and costs for meeting all Contract and regulatory requirements.

“Performance Bond” means the bond, in the form set forth in the RFP or as otherwise approved by the Authority, executed by the Design-Builder and its Surety or Sureties, guaranteeing performance of all Work in compliance with the requirements of the Contract Documents, including all Orders on Contract, Amendments, and Supplemental Agreements pertaining thereto.

“Person” means any individual, firm, corporation, company, sole proprietorship, limited liability company (LLC), joint venture, voluntary association, partnership, trust, unincorporated organization, or other legal entity.

“Plans” means the official Design Plans and applicable Standard Sheets, which show the location, character, dimensions, and details of the Work to be performed. Also, the Design-Builder’s Design Plans showing profiles, typical cross sections, and other details; Work Plans; or exact reproductions which show the location, character, dimensions, and general or specific details of the Work to be done.

“Price Proposal” means the portion of the Proposal described in Appendix D.

“Principal Participant” means any of the following entities:

- A) The Design-Builder (or Proposer);
- B) If the Proposer is a partnership, joint venture, or limited liability company, any general partner or any member of the partnership, joint venture or LLC; and/or
- C) Any Equity Participant.

“Professional Engineer” means a Professional Engineer licensed or otherwise authorized to practice engineering under Article 145 and registered or otherwise authorized under Article 130 or the New York State Education Law.

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“Project” means the improvements to be designed and constructed by the Design-Builder and all other Work products to be provided by the Design-Builder in accordance with the Contract Documents.

“Project Manager” means the Design-Builder’s designated representative responsible for all aspects of the Work, including construction oversight, design oversight, project finances, project scheduling, etc. Disputes regarding design or construction that cannot be resolved with the designer or in the field will be brought to the attention of the Design-Builder’s Project Manager for resolution.

“Project Superintendent” means the Design-Builder’s on-site designated representative who oversees the construction of the Design-Build Contract, including directing and coordinating the activities of the Design-Builder’s workforce and all subcontractors, ensuring that the work progresses according to schedule, and ensuring that material and equipment are delivered to the site on time, etc.

“Proposal” means a proposal submitted by a Proposer in response to the RFP, including any revisions thereto. If the RFP requests submittal of best and final offers, the term ‘Proposal’ means the best and final offer submitted by the Proposer, including any revisions thereto.

“Proposal Bond” means the security furnished with a Proposal to guarantee that the Proposer will enter into the Contract if the Proposer’s Proposal is accepted and satisfies all other conditions of Award.

“Proposal Due Date” means the date specified in the ITP on which the Proposal is due to the Authority’s Designated Representative.

“Proposal Information” means the documents so designated in the ITP and submitted to the Authority by the Proposer/Design-Builder in accordance with the ITP that will be included in the Contract Documents. The Proposal Information is part of the Quality Proposal.

“Proposer” means a Person on the Shortlist.

“Proposer’s Representative” means an individual authorized to bind a Proposer who is designated in writing by the Proposer (in the Proposer’s SOQ or other written notice to the Authority’s Designated Representative) as the Proposer’s sole point of contact for the purposes of communications with the Authority during the procurement of this Project.

“Qualified Costs” means the direct costs and overhead costs that are allowable and reasonable, and incurred by the Proposer, the Proposer’s team, or third-parties acting at the direction of the Proposer in the production of the Work Product. Examples of qualified costs (subject to limitations of any other contract stipulations such as limits on hourly rates or not to exceed Government travel rates) can include the following:

- Compensation of employees time charges related to preparation of the Proposal;
- Cost of materials acquired, consumed, or expended related to preparation of the Proposal;
- Cost of equipment utilized related to preparation of the Proposal; and
- Travel expenses incurred related to preparation of the Proposal.

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“Test” means methods adopted by the Authority and the Design-Builder to ascertain the quality, character, and acceptability of Materials and processes utilized in performing the Contract.

“Traffic Impact Day” means any day, or part of a day, on which the number and/or width of travel or turning lanes on a given roadway or structure are reduced from the existing number and/or width, or any day on which traffic is detoured (either off-site or on-site with the introduction of additional traffic signals, stop signs, yield signs, and/or turning movement restrictions) from a given roadway or structure. Shoulder or Parking Lane closure(s) only do not constitute a Traffic Impact Day.

“Traffic Impact Duration” means the number of consecutive Calendar Days between the date of the first Traffic Impact Day for a given roadway or structure and the date of the last Traffic Impact Day for that roadway or structure.

“Unbalanced Price Proposal” means a Price Proposal may be unbalanced either Materially or Mathematically. A Materially Unbalanced Price Proposal is a Price Proposal that generates a reasonable doubt that awarding the Contract to the Proposer submitting the price Proposal will result in the lowest ultimate cost to the Authority. A Mathematically Unbalanced Price Proposal is a Price Proposal containing lump sum or Unit Price items that do not reasonably reflect the actual costs plus a reasonable proportionate share of the Proposer’s anticipated profit, overhead costs, and other indirect costs.

“Unit Price” means the price per unit of measure specified for items of Work in accordance with any unit priced Orders on Contract.

“Utility” means a Person, corporation, municipality, or public authority engaged in the distribution of electricity, gases, petroleum products, water, steam, the collection of wastewater, the operation of traffic control systems, or the provision of telecommunication services.

“Utility Owner” means the owner or operator of any Utility (including Persons and Governmental Persons).

“Verification Sampling and Testing” means Sampling and testing performed by the Authority, or by a firm retained by the Authority, to validate the Design-Builder’s QC sampling and test data that was used in the acceptance decision. Verification sampling and testing is conducted whenever the Design-Builder’s Quality Control data (Construction Inspection and Sampling and Testing of Construction Materials) is used in the acceptance decision. Verification is performed by the Authority, or its agent, to validate the Design-Builder’s data.

“Warranties” means the written commitments of the Design-Builder as set forth in the Contract regarding quality and performance over a specified period of time after Final Acceptance of the Project.

“Weakness” means a flaw in the SOQ and/or RFP that increases the risk of unsuccessful Contract performance. A significant Weakness in the SOQ or RFP is a flaw that appreciably increases the risk of unsuccessful Contract performance.

“Women-owned Business Enterprise (WBE)” means a business enterprise, including a sole proprietorship, partnership, or corporation that has the following attributes:

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SYRACUSE DIVISION BUNDLED BRIDGES

TAS 17-37B, Contract D800001

DB CONTRACT DOCUMENTS

PART 3 PROJECT REQUIREMENTS

Final, October 5, 2017

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9.3.11 Operational Classification

The mainline bridges (bridges carrying Thruway traffic) over local roads or water are classified as critical. Interchange 35, BIN 5510090 is classified as essential. BINS 5512790 and 5512980 are classified as other.

9.4 CONSTRUCTION REQUIREMENTS

9.4.1 Dewatering and Groundwater Control

The Design-Builder shall be responsible for evaluating the potential need for dewatering and groundwater control, and for implementing such measures as appropriate, and shall evaluate the effects on existing facilities resulting from any dewatering and draw down.

9.4.2 Deep Foundations

The Design-Builder shall design and provide integrity and/or capacity testing of all deep foundations, in accordance with NYSDOT standards. If applicable, integrity testing requirements on drilled shafts and test shafts shall comprise of, as a minimum, crosshole sonic logging on all drilled shafts.

As part of the As-Built Plans, the Design-Builder shall provide installation records for all deep foundations installed, in accordance with NYSDOT standards.

The Design-Builder shall report the results of all foundation installation inspections and rock socket observations.

9.4.3 Soil and Rock Excavations and Embankments For Roadway Foundation

Excavations and embankments for roadway foundations shall be constructed so that post construction settlement is expected to remain within two inches of the profile grade line at any point along the entire alignment. Also, prior to the Project's final acceptance, differential settlement along travel lane and shoulder surfaces shall not exceed two inches over a 100-foot length along the alignment (longitudinal direction), or over one half inch along a ten foot length in the transverse direction or within ten feet of any approach slab or edge of structure.

9.4.4 Condition Surveys

9.4.4.1 Pre-Construction Condition Survey

The Design-Builder shall conduct a pre-construction inspection and survey of the existing condition of all structures and properties within 100 feet of vibration or settlement causing construction activities for the purposes of generating photographic and video documentation of existing damage, leaks and cracks, in accordance with the requirements of Special Specification 634.99010017. The pre-construction condition survey shall form the basis against which all new cracks, existing progressive cracks, or damage will be measured.

In its preparation for the pre-construction survey, the Design-Builder shall ensure that the pre-construction condition survey encompasses at a minimum all properties within areas that are identified by the Design-Builder to be potentially prone to: (i) ground vibration levels, expressed

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the same Aesthetic enhancement. On BIN 5510130 only the concrete barrier requires Aesthetic treatments.

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Any superstructure steel that utilizes a protective coating of paint shall be painted with a coating coordinated with Authority, but should be as similar to the superstructure steel for bearings, etc. An example would be for a Weathering steel superstructure, the paint coating should meet Weathered Brown Guide Rail Paint as specified in Standard Specification 708.

10.4 DEMOLITION REQUIREMENTS

10.4.1 Scope

The Design-Builder shall demolish and remove the existing bridge superstructures, piers, abutments, foundations, retaining walls, and pavement within the Project Limits in a safe and environmentally acceptable manner.

The demolition of the existing Bridge shall include all existing superstructure elements and all substructure elements as per Standards and BD Sheets except as indicated below and/or in accordance with environmental permitting. Where new foundations are placed at the locations of existing foundations the existing foundations shall be removed to the extent needed to construct the new foundations.

- For BIN 5512790 - Substructure removal of Pier 3 (the Northern pier) shall be removed to the top of footings or 2 feet below original ground surface whichever is less.
- For BIN 5512980 – Substructure removal of Pier 3 (the Northern pier) shall be removed to the top of footings or 2 feet below original ground surface, whichever is less.

For BIN 5510090 – Substructure removal of Pier 3 (the Northern pier) shall be removed to the top of the footings or 2 feet below original ground surface whichever is less. The Design-Builder shall test for the presence of Hazardous Materials in all structures to be disturbed to ensure the handling, removal and disposal is done in accordance with all applicable laws and standards.

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The abatement of all Hazardous Materials shall be completed to the greatest extent possible prior to any demolition taking place unless a legal variation from related laws, rules and regulations can be obtained. If the Hazardous Material has been identified through the Hazardous or Asbestos Screening document and/or the record plans, the Design-Builder is responsible for all costs. Should Hazardous Material or Asbestos be found and information related to its presence not previously available to the Design-Builder, all costs associated with abatement, containment, removal, and disposal shall be covered under the Fixed Force Account item.

The Design-Builder shall perform all Work with care so that any materials that are to remain in place, or that are to remain the property of the Authority shall not be damaged. If the Design-Builder damages any materials that are to remain in place or which are to become or to remain the property of the Authority, the damaged materials shall be repaired or replaced in a manner satisfactory to the Authority at no cost to the Authority.

The Design-Builder shall ensure that no aspects of the Works have a detrimental effect on public safety or the environment.

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The Design-Builder shall assume responsibility for safety and maintenance of all existing structures within the Project Limits, identified for removal in accordance with DB §105-12.

Utility connections shall be discontinued and capped in accordance with the requirements of the utilities companies or the Authority prior to demolition works.

10.4.2 Standards

The Design-Builder shall perform the demolition activities in accordance with the Contract Requirements and the applicable Standards, Codes and Manuals listed in Section 1.6 unless otherwise stipulated in this Project Requirement, or otherwise applicable to the Project.

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10.5 CONSTRUCTION REQUIREMENTS

The Design-Builder shall develop erection procedures for the bridges that include complete detailed erection sequence drawings; erection stresses in permanent and temporary members; bent and false work reactions determined for each construction stage.

10.5.1 Construction Vehicles on Bridge

The Design-Builder is prohibited from running equipment that does not operate on rubber tires (milling machines, rollers, etc.) across new bridge decks unless proper precautions (mats, etc.) are provided to prevent damage to the deck. The methods used to move equipment across bridge decks shall be subject to approval by the Construction Inspection Professional Engineering Firm with comments/acceptance from the CQAE.

10.6 LOAD RATING REQUIREMENTS

Prior to any bridge(s) in this Project being opened to traffic, including temporary bridges, the Design-Builder shall provide to the Authority, the necessary load rating requirements, including Virtis load rating files, as per NYSDOT standards and manuals for review and acceptance by the Design Quality Assurance Engineer. The Design-Builder shall take the necessary steps to ensure that proper allocated time is afforded the Design-Quality Assurance Engineer, to fully execute a proper review and the Design-Builder shall make provisions in their CPM schedule addressing such submittal, review, and acceptance. The load rating for all permanent bridges shall be a resulting factor of 1.1. Inventory with an operating resulting factor higher. A resulting factor of 1.0 is not acceptable.

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Before the new bridge(s) can be opened to traffic, the load rating shall be presented to the Authority's Quality Assurance Engineer for acceptance. The Quality Manager shall supply a certification statement that the load rating followed the accepted Quality Control Plan and the results are in compliance with the requirements of the contract documents.

10.7 DELIVERABLES

Deliverables shall be as stated elsewhere in the RFP documents.

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13.3.1.1 Power Supply Requirements

For reference, the lighting installation shall comply with the following:

- A) Meet all requirements of NFPA 70 – National Electrical Code (NEC);
- B) All outdoor electrical enclosures shall be type 316 stainless steel, rated NEMA 4X or a higher degree of protection; and
- C) Meet all requirements of applicable IEEE and ANSI power engineering standards.

13.3.1.2 Removal of Existing Equipment

All disconnected luminaires, light poles, and associated equipment shall be removed and disposed of by the Design-Builder. All wiring, switches, panels, cabinets, enclosures, and other electrical equipment shall be removed and disposed of by the Design-Builder.

13.3.2 Permanent Lighting System

13.3.2.1 General

The Design-Builder shall be responsible for ensuring that the permanent lighting system meets the following requirements:

- A) Provides illumination such that the road surface illumination meets or exceeds the uniformity and the illuminance and/or luminance criteria during darkness;
- B) Utilizes energy efficient and long-life, low maintenance lighting technologies that are found on the Authority's approved lists;
- C) Can be fully and seamlessly integrated into the existing permanent lighting elements adjacent to the Project limits;
- D) Utilizes a photo-control switch system that automatically activates lighting before dusk and deactivates the system past dawn;
- E) Contains surge suppression devices for protection against damage by lightning strikes and complying with NFPA-780 and UL 96;
- F) Provides fixtures that are water tight and intended for a marine/industrial environment; and
- G) Utilizes lighting components that are readily available and not proprietary equipment.

13.3.2.2 Construction Requirements

The Design-Builder shall use materials listed on the NYSDOT approved list of materials and consistent with the details shown on the Authority's Standard Sheets.

The Design-Builder shall provide permanent lighting materials that satisfy the Project Requirements and applicable codes. In addition, the Design-Builder shall:

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SECTION 14 INTELLIGENT TRANSPORTATION SYSTEMS (ITS)

14.1 SCOPE

The Design-Builder shall perform all work necessary to design, furnish, build, and install temporary and/or permanent Elements of the Transit system.

14.2 STANDARDS

The Design-Builder shall perform ITS activities in accordance with the Contract Requirements, the applicable Standards, Design Codes and Manuals cited in Section 1.6, unless otherwise applicable to the Project, and the following additional Standards:

14.2.1 Standards

- **NEC: National Electric Code**

14.3 REQUIREMENTS

The ITS System work in the Project shall consist of the following:

A) Demolition of Existing TRANSMIT Elements

The removal of TRANSMIT elements, where required, shall be removed by qualified technicians, including cabinets and all electronics, and shall be disposed of properly. The Design-Builder shall remove TRANSMIT elements located at Milepost 278.93 (BIN 5510090).

The Authority will disconnect the existing coaxial cable in the TRANSMIT cabinet. The Authority requires a two (2) day advance notification for disconnecting the coaxial cable.

The coaxial cable between the pullbox and the cabinet shall be removed and disposed of by the Design-builder. The Design-builder shall remove and dispose of all TRANSMIT antenna, conduit, and cable on the bridge and buried in the ground up to the pullbox.

Existing equipment to remain, damaged as a result of improper removal or handling of any of the components shall be replaced with new in kind equipment at the Design-Builder's expense.

B) Installation of TRANSMIT Elements

Install four (4) TRANSMIT antennas under the new bridge. The antennas are to be centered over the travel lanes. There shall be two antennas over each direction of traffic, one in each lane. The antenna shall meet the requirements of Item 683.3011-25 TRANSMIT Antenna.

The Design-builder shall run coaxial cable from each antenna to the TRANSMIT cabinet, with no splices in between.

The Design-builder shall bring the four individual conduits from the antenna into an existing junction box in the southwest quadrant from the bridge. The four cables shall be run in one

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3-inch conduit from the junction box in the ground to the pull box. The cables shall be run through the pull box into the existing conduit to the TRANSMIT cabinet.

The Authority will reconnect the coaxial cable at the TRANSMIT cabinet. The Authority requires a two (2) day advance notification for reconnecting the coaxial cable.

The Authority contact for ITS elements in Syracuse Division is Jim Ryan, ITS Maintenance Supervisor at (315) 438-2368.

14.4 SYSTEM TEST PROCEDURES

The Authority will be responsible for testing the installed equipment to verify that it has been installed correctly, is performing as specified, and supports overall system operations. After the ITS elements are installed, the Design-Builder shall contact Jim Ryan at (315) 438-2368 to coordinate testing of system. Design-Builder shall provide a two (2) day advance notification for testing of the system. The durations of the test is one (1) day.

14.4.1 System Acceptance Testing

The Authority will require a two (2) day advance notice for testing the ITS System for acceptance. Contact Jim Ryan at (315) 438-2368 for notification of testing. The authority will perform the test.

If any equipment should fail, those subsystems affected by the failed equipment shall be subject to additional testing. The Authority will determine which equipment has been affected by the failure and subject to additional testing.

14.5 DOCUMENTATION REQUIREMENTS

The Design-Builder shall incorporate all ITS elements into the As-Built drawings for the contract.

SECTION 15

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SECTION 15 WORK ZONE TRAFFIC CONTROL AND ACCESS

15.1 SCOPE

The Design-Builder shall be responsible for the planning and provision of Work Zone Traffic Control (WZTC), required to perform the Project Work until Project Completion. This Project Requirement applies to any roads, ramps, cross roads, local streets, maintenance roads, driveways, and active paths within and/or affected by the Project.

The Design-Builder shall provide WZTC for the safe and efficient movement of people, goods, and services through the Project area(s) while maintaining access and minimizing negative impacts to residents, commuters, businesses, and Authority maintenance operations.

Note that, as used in this section, "Work Zone Traffic Control plan" or "WZTC plan" is the equivalent of "Maintenance and Protection of Traffic plan" or "MPT plan" as described in Chapter 16 of the Highway Design Manual (HDM).

15.2 STANDARDS

The Design-Builder shall perform the work zone traffic control activities in accordance with the Contract Requirements and the applicable Standards, Design Codes and Manuals listed in Section 1.6, unless otherwise stipulated in this Project Requirements, or otherwise applicable to the Project.

15.3 REQUIREMENTS

15.3.1 General Requirements

Offsite detours that meet Authority standards may be proposed unless otherwise noted below. Except as noted below, offsite detours shall utilize the shortest route on State-owned (NYSDOT) highways that have no bridges with load postings, R-posted restrictions, or vertical clearance restrictions, or that are or will be under construction during the detour period. The detours shall be fully signed. The total length of detour, as measured from one approach end to the bridge along the detour route to the other approach end of the bridge, shall not exceed 10 miles.

- Offsite detours are permitted for BIN 5512980 and 5512790 only. The offsite detour to be implemented for each bridge closure is found in Part 7 Engineering Data.
- All mainline bridges, BIN 1020079, 5516071, 5516072, 5009929, and 5510130 shall maintain two lanes of traffic in each direction at all times, except as noted in Section 15.3.5.
- For BINS 5512980 and 5512790 if closures are not employed, one lane of traffic in each direction shall be maintained at all times, except as noted in Section 15.3.5.
- For BIN 1020079 access to the off ramps shall be maintained at all times with deceleration ramps employed.
- For BIN 5510090 one lane of traffic in each direction shall be maintained at all times, except as noted in Section 15.3.5.

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- Wednesday Holidays – Beginning 6:00 AM the Tuesday before the holiday and ending 6:00 AM the next business day (Thursday).
- Thursday Holidays – Beginning 6:00 AM the Wednesday before the holiday and ending 6:00 AM the following Monday.

15.3.4 Access to Commercial Properties and Driveways

The Design-Builder shall provide uninterrupted access to all commercial properties and driveways within the Project Limits at all times.

15.3.5 Closure Restrictions

Additional lane closures and their time periods can be found on the Thruway Authority's Standard Sheets for the Syracuse Area.

For BIN 5512790, No bridge closures, lane closures, or restrictions shall exist in the month of June of any year of this contract due to events that generate additional traffic.

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For BIN 1020079, on Mohawk Street 2 travel lanes southbound and 1 travel lane northbound shall be maintained as a minimum during construction. These reductions can last no longer than 15 calendar days total, for work to remove and replace eastbound mainline structures of BIN 1020079 and 15 calendar days total, for work to remove and replace westbound mainline structures of BIN 1020079. The left turn signal must be maintained.

For BIN 5009929, on Oriskany Blvd. 1 lane in each direction shall be maintained during construction. These reductions can last no longer than 15 calendar days total, for work to remove and replace eastbound mainline structures of BIN 5009929 and 15 calendar days total, for work to remove and replace westbound mainline structures of BIN 5009929.

For BIN 5516071 and 5516072, on Millers Grove Road, alternating one way traffic shall be maintained during construction using temporary traffic signals.

15.3.6 Minimum Lane Widths during Construction

In general, the Design-Builder shall maintain a minimum travel lane width of 11 feet during construction. Shoulder widths during construction shall be 1 foot minimum.

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For BIN 1020079, the travel lanes on Mohawk Street shall be a minimum of 10 feet with shoulder width minimum of 1 foot.

For BIN 5009929, the travel lanes on Oriskany Blvd. shall be a minimum of 10 feet with shoulder width minimum of 1 foot.

For BIN 5516071 and 5516072, the travel lane on Miller's Grove Road shall be 10 foot minimum.

15.3.7 Portable Variable Message Signs

The Design-Builder shall provide, as a minimum, eight (8) Portable Variable Message Signs, but more should the Design-Builder's design dictate, for the duration of this Contract. The

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Portable Variable Message Signs shall be deployed as necessary for the various WZTC schemes developed in coordination and concurrence/acceptance by the Authority's Project Manager. The portable variable message signs provided shall meet the requirements of Item No. 619.111112 (Portable Variable Message Boards with Cellular Communication).

The development of messages for the Variable Message Sign(s) shall be the responsibility of the Authority's CQAE and operations staff at the Authority's Division Office.

The Design-Builder shall contact the Authority's CQAE at least two weeks prior to placement of any Variable Message Sign regarding their location and receive concurrence of the location.

15.3.8 Temporary and Interim Pavement Markings

The Design-Builder shall provide temporary and interim pavement markings during all construction phases conforming to the requirements of the Standard Specifications.

15.3.9 Coordination with Division Traffic Management Center

The Design-Builder is advised that the Authority's Division Office will provide support for the Project's WZTC activities. Therefore, coordination among the Authority's Construction Quality Assurance Engineer, Design-Builder, and Authority's Division Office, will be required for all WZTC activities, particularly with respect to the use of Variable Message Signs (VMS) in the Project area.

The Design-Builder shall notify the Authority's Project Manager of all lane and/or shoulder closures prior to implementation. The Authority's Project Manager will, in turn, notify the Authority's Division Office.

The Design-Builder is responsible for preparing the Road Work Form and submitting it to the Authority's Project Manager. The Road Work Form must be submitted to the Authority's Project Manager a minimum of seven (7) calendar days in advance of scheduled closures. The Authority's Project Manager will respond within four (4) calendar days after receipt of the Road Work Form.

The Road Work Form shall be supplied to the Best Value selected Design-Builder for use on this project.

15.3.10 Emergency Response and Transportation Management Plans

The Design-Builder shall notify the Authority's CQAE immediately following any impacts to motorists due to construction activities and/or unforeseen circumstances. The CQAE will be responsible for disseminating the information to the appropriate personnel/entities for appropriate response to mitigate impacts to motorists.

The Design-Builder shall prepare an Emergency Response Plan to be implemented in the event the roadway is shut down for unforeseen or unplanned circumstances. The Plan shall be implemented when the anticipated duration of closure exceeds twenty (20) minutes. The Plan shall be submitted to the Authority's Project Manager for review and comment a minimum of two weeks prior to the beginning of Work. Work on this Project shall not begin until the Design-Builder receives written notification from the Authority's Project Manager that the Emergency

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Response Plan has been reviewed by the Authority and all Authority comments have been resolved.

The Emergency Response Plan shall include a notification and communication plan that describes how the Design-Builder will promptly inform the appropriate personnel/entities of an unforeseen or unplanned circumstance. No later than 30 calendar days following NTP, the Authority's Project Manager will provide the Design-Builder with a list of personnel and entities that need to be contacted in this section of the Emergency Response Plan.

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The Design-Builder shall also provide the Authority's Project Manager a Transportation Management Plan (TMP) per FHWA's Final Rule on Work Zone Safety and Mobility, 23 CFR 630 Subpart J. The intent of the TMP is to minimize impacts to the travelling public and to provide continuity of reasonably safe and efficient road user flow and highway worker safety. The Emergency Response Plan shall be a component of the TMP and shall be located in the contingency section of the TMP.

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15.3.11 Lifting Operations

The Design-Builder shall be aware that under no circumstances shall lifting operations for bridge superstructure elements, overhead sign structures, or any other items, be carried out over active traffic lanes. All such operations shall at a minimum require short-duration roadway closures in accordance with the provisions of this Section 15.

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BIN 5512790 Design Speed 45 mph Lane Width(s) 10ft Shoulder width(s) Right Shoulder 4ft Left Shoulder -		Thruway Mainline Under 70mph 12ft 12ft 8ft	
BIN 5512980 Design Speed 60 mph Lane Width(s) 12ft Shoulder width(s) Right Shoulder 6ft Left Shoulder		Thruway Mainline Under 70mph 12ft 12ft 8ft	
BIN 5516072 & 5516071 Design Speed 70 mph Lane Width(s) 12ft Shoulder width(s) Right Shoulder 12ft Left Shoulder 8ft		<u>Millers Grove Road</u> 60mph 11ft 4ft 4ft	
BIN 5510090 Design Speed Lane Width(s) Shoulder width(s)	<u>Tangent Section</u> 30mph 12ft Right Shoulder 6ft Left Shoulder 4ft	<u>Ramp</u> 30mph 16ft 6ft 4ft	<u>Thruway Mainline Under</u> 70mph 12ft 12ft 8ft
BIN 5510130 Design Speed 70 mph Lane Width(s) 12ft Shoulder width(s) Right Shoulder 12ft Left Shoulder 8ft			

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18.3.3 Barrier Systems and Impact Attenuators

The Design-Builder shall remove and dispose of all existing barrier systems within the Project limits, and replace with new barrier systems to current Authority Standards.

The limits of work for new roadside and new median barrier shall be the lesser of the following:

- 1) The point where barrier is no longer warranted; or
- 2) A point where the proposed barrier can be transitioned to an existing barrier system which conforms to current standards.

All existing barrier systems that are removed shall become property of the Design-Builder.



SYRACUSE DIVISION BUNDLED BRIDGES

TAS 17-37B, Contract D800001

DB CONTRACT DOCUMENTS PART 4 UTILITY REQUIREMENTS

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~~Draft, August 31, 2017~~

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4.3.3 Design Builder's Coordination Requirements

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The Design-Builder shall be responsible for coordination with utility owners. It is important that Utility Owners be kept informed of the Design-Builder's activities and schedule. In addition to satisfying any requirements set forth in applicable Governmental Rules and Standards, including but not limited to Part 753, the One-Call notification requirements referenced in DB § 107-15, and in any DB Utility Work Agreements that may have been executed, the Design-Builder shall undertake the following activities, which have been identified by the Authority as important to utility owners:

- A) Keep utility owners well informed of construction schedules and notify the utility owners at least twenty-four hours in advance of any work in the vicinity of the utility owners' facilities, that will not impact service;
- B) Keep utility owners well informed of changes that affect their facilities;
- C) In addition to any required notice, give the utility owners a minimum of 48 hours' notice of potential impacts to service, unless longer notification times are specified elsewhere in this Part 4 or any DB Utility Agreements that may have been executed;
- D) Ensure utility owners are involved in making the decisions that affect their own facilities and services;
- E) Cooperate with the utility owners to solve relocation/installation issues to the extent that such relocations/installations are consistent with the Design-Builder's Scope of Work as otherwise set forth in the Contract Documents and without causing the Authority to incur any unnecessary expense to the Project, or causing the utility owners to incur unnecessary expense;
- F) Act diligently in continuing the positive relationship that the Authority has developed with the utility owners; and
- G) Coordinate with those utility owners who perform their own work by scheduling adequate time to accomplish their work.

4.3.4 Design Reviews

The Design-Builder shall invite affected utility owners to participate in all pertinent Design-Builder's and Authority's Design Reviews (see DB § 111).

Some utility owners may design and/or construct any required utility relocations and revisions for their utilities. The Design-Builder shall be required to incorporate these utility designs into its own design prior to the Design Review.

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At a minimum, the Design-Builder shall submit the items listed in Table 4.9-1 to the Authority.

Table 4.9-1 – Deliverables
(If Applicable)

Deliverable	Number of Copies		Submittal Schedule
	Hardcopy	Electronic	
Utility Tracking Report	3	1 (PDF)	Weekly until Physical Completion.
Utility Design Sheet	3	1 (PDF)	Two days prior to initial meeting with utility owner
DB Utility Work Agreements	2	1 (PDF)	Seven days after construction of the utility identified

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APPENDIX A UTILITY REQUIREMENTS

The Authority has reviewed the potential Project limits and has made a preliminary assessment of which utility facilities located within the potential Project limits may be impacted by the Project

The Authority has conducted advanced utility coordination with the utility companies listed below.

A-1 UTILITY COMPANIES

Tables A-1 through A-7 lists the utility companies with facilities located on, under or above the Project roadways and/or structures:

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Table A-1 Utility Contacts – BIN 1020079

Utility Owner	Contact	Contact #	E Mail
TELECOM	LEE KELLY	315-459-5057	Lee.Kelly@Verizon.com
ELECTRIC	THOMAS GOZELSKI	716-831-7756	Thomas.Gozelski@us.ngrid.com
	FRANK SCIORTINO	315-428-5075	
	DONALD AMBROSE	315-440-2115	Donald.Ambrose@nationalgrid.com
NATURAL GAS	JOE SANTARO	315-428-5436	Joseph.Santaro@nationalgrid.com
CABLE	PATRICK BUCKLEY	315-634-6053	pat.buckley@twcable.com
LOCAL WATER	SCOTT BLAIS	315-866-0150	office@village.herkimer.ny.us
SANITARY SEWER	SCOTT BLAIS	315-866-0150	office@village.herkimer.ny.us
FIBER OPTIC	GREG SHELDON	518-436-3143	Gregory.Sheldon@thruway.ny.gov

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Table A-2 Utility Contacts – BIN 5009929

Utility Owner	Contact	Contact #	E Mail
TELECOM	LEE KELLY	315-459-5057	Lee.Kelly@Verizon.com
ELECTRIC	THOMAS GOZELSKI	716-831-7756	Thomas.Gozelski@us.ngrid.com
	FRANK SCIORTINO	315-428-5075	
	DONALD AMBROSE	315-440-2115	Donald.Ambrose@nationalgrid.com
NATURAL GAS	JOE SANTARO	315-428-5436	Joseph.Santaro@nationalgrid.com
CABLE	PATRICK BUCKLEY	315-634-6053	pat.buckley@twcable.com
LOCAL WATER	JOHN WATERS	315-724-9812	
SANITARY SEWER	JOHN WATERS	315-724-9812	
PETROLEUM PIPELINE	SCOTT HOHN	315-374-1737 315-223-1523	sdhohn@buckeye.com
FIBER OPTIC	GREG SHELDON	518-436-3143	Gregory.Sheldon@thruway.ny.gov

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Table A-3 Utility Contacts – BIN 5512980

Utility Owner	Contact	Contact #	E Mail
Verizon-Phone	Sandy Kogan	908-203-4827	
National Grid-Elec	Don Ambrose	315-440-2115	donald.ambrose@nationalgrid.com
National Grid-Gas	Joe Santoro	315-428-5436	joseph.santoro@nationalgrid.com
Harron-Aldelphia-Cable	Steve Moran	315-797-8111	
LOCAL WATER	NA		
NYSTA/G4S-Fiber Optic	Greg Sheldon	518-436-3143	gregory.sheldon@thruway.ny.gov

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Table A-4 Utility Contacts – BIN 5516071 & 5516072

Utility Owner	Contact	Contact #	E Mail
TELECOM	LEE KELLY R. BONANZA	315-459-5057 315-738-7348	Lee.Kelly@Verizon.com
HARRON- ADELPHIA	STEVE MORAN	315-797-8111	
FIBER OPTIC	GREG SHELDON	518-436-3143	Gregory.Sheldon@thruway.ny.gov

Table A-5 Utility Contacts – BIN 5510090

Utility Owner	Contact	Contact #	E Mail
TELECOM	SANDY KOGAN LEE KELLY	908-203-4827 315-459-5057	Lee.Kelly@Verizon.com
ELECTRIC	THOMAS GOZELSKI	716-831-7756	Thomas.Gozelski@us.ngrid.com
GAS	SYRACUSE SUBURBAN GAS		
FIBER OPTIC	GREG SHELDON	518-436-3143	Gregory.Sheldon@thruway.ny.gov
FIBER OPTIC	AUTRY MEEKER	501-748-5234	Autry.Meeker@Windstream.com

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Table A-6 Utility Contacts – BIN 5512790

Utility Owner	Contact	Contact #	E Mail
Village of Canastota (elec., sewer, water)	Larry Carpenter	315-697-7559	larry.carpenter@canastota.com
National Grid (Electric)	Donald Ambrose	315-440-2115	donald.ambrose@nationalgrid.com
Buckeye Partners (Petroleum)	Steve Hohn	315-223-1523	shohn@buckeye.com
Fiber Optic	Greg Sheldon	518-436-3143	gregory.sheldon@thruway.ny.gov

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A-2.1.4 Water and Sewer

A-2.1.4.1 Village of Herkimer

The existing waterline is located on Mohawk St. approximately 30 ft. west of the Mohawk St. centerline.

A-2.1.4.2 Village of Herkimer Sanitary Sewer

The existing sanitary sewer line is located approximately 100 ft. south of the NYS Thruway centerline.

A-2.1.5 Other Utilities – Fiber Optic

A-2.1.6 NYS Thruway Fiber Optic Line/G4S

The existing fiber optic line is located approximately 110 ft. south of the NYS Thruway centerline. In the southwestern quadrant as the fiber passes the utility pole, the line is 3.17' deep. In the southeastern quadrant, the line is 40' south of the southern fascia, and is 3.33' deep.

A-2.1.7 Utility Service Connections

None known.

A-2.2 BIN 5009929

A-2.2.1 Telecommunications

A-2.2.1.1 Verizon Communications

Existing aerial facilities cross Oriskany Blvd. and are located approximately 100 ft. north of the NYS Thruway centerline then run adjacent to the east side of Oriskany Blvd. approximately 35 ft. east of the Oriskany Blvd. centerline. As the utility runs along Oriskany Blvd., the line located at a point 25' north of Pier 1 and goes underground. The line runs parallel to Oriskany Blvd. and is underneath the sidewalk. The utility becomes aerial again at a pole located 31' south of Pier 1. It is anticipated that the project will not have an impact on this utility.

A-2.2.1.2 Spectrum Cable

Existing aerial facilities cross Oriskany Blvd. approximately 100 ft. north of the NYS Thruway centerline then run adjacent to the east side of Oriskany Blvd. approximately 35 ft. east of the Oriskany Blvd. centerline. As the utility runs along Oriskany Blvd., the line located at a point 25' north of Pier 1 and goes underground. The line runs parallel to Oriskany Blvd. and is underneath the sidewalk. The utility becomes aerial again at a pole located 31' south of Pier 1. It is anticipated that the project will not have an impact on this utility.

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A-2.2.2 Electric

A-2.2.2.1 National Grid

Existing aerial facilities cross Oriskany Blvd. approximately 100 ft. north of the NYS Thruway centerline then run adjacent to the east side of Oriskany Blvd. approximately 35 ft. east of the Oriskany Blvd. centerline. Aerial facilities are also located approximately 125 ft. south of the NYS Thruway centerline along Watkins Street.

A-2.2.3 Natural Gas and Petroleum

A-2.2.3.1 National Grid

An existing 12" steel gas line is located approximately 40 ft. west of Oriskany Blvd. in the immediate vicinity of the structure. North of the bridge, the gas line is located approximately 45 ft. east of Oriskany Blvd.

A-2.2.3.2 Buckeye Partners, L.P. Petroleum Pipeline

The existing petroleum pipeline runs parallel to the NYS Thruway approximately 140 ft. south of the NYS Thruway centerline.

A-2.2.4 Water and Sewer

A-2.2.4.1 Oneida County – Water Quality & Pollution Control

An existing waterline is located approximately 35 ft. east of Oriskany Blvd.

A-2.2.4.2 Oneida county – Water Quality & Pollution Control Sanitary Sewer

The existing sanitary sewer line is located solely north of the NYS Thruway mainline approximately 35 ft. east of Oriskany Blvd.

A-2.2.5 Other Utilities – Fiber Optic

A-2.2.5.1 NYS Thruway Fiber Optic Line

The existing fiber optic line runs parallel to the NYS Thruway approximately 90 ft. north of the NYS Thruway centerline.

A-2.2.6 Utility Service Connections

None known.

A-2.3 BIN 5512980

A-2.3.1 Telecommunications

A-2.3.1.1 Verizon Communications

Aerial, 52' +/- west of bridge centerline.

Deleted: Final October 5, 2017

New York State Thruway Authority

A-2.3.1.2 Harron/Aldelphia Cable

Aerial, 52' +/- west of bridge centerline.

A-2.3.2 Electric

A-2.3.2.1 National Grid

Aerial, 52' +/- west of bridge centerline.

A-2.3.3 Natural Gas and Petroleum

A-2.3.3.1 National Grid

Underground, 195' +/- south of Thruway centerline.

A-2.3.4 Water and Sewer

A-2.3.4.1 Local Water Company Name – N/A

None known.

A-2.3.5 Other Utilities – Fiber Optic

A-2.3.5.1 NYS Thruway Fiber Optic Line/G4S

The fiber optic utility runs east and west underneath span 4 between Pier 3 and the end abutment (north abutment). The line is located approximately 9' off the abutment as at the western fascia. It is anticipated to be 7' deep at this location. The line is approximately 12' from the abutment at the eastern fascia. It is anticipated to be 12' deep at this location. The fiber optic line is proposed to be relocated, see Section A.3.3.4.3

Deleted: Underground, 130' +/- north of Thruway centerline.

A-2.3.6 Utility Service Connections - none

None known.

A-2.4 BIN 5516071 & 5516072

A-2.4.1 Telecommunications

A-2.4.1.1 Verizon Communications

Existing underground telephone is located within 4" plastic conduit approximate 180 ft. west of the existing structures.

A-2.4.1.2 Harron-Adelphia Cable

An existing aerial cable crosses the NYS Thruway mainline approximately 180 ft. west of the existing structures.

Deleted: Final October 5, 2017

New York State Thruway Authority

A-2.4.2 Electric

A-2.4.2.1 Company Name – N/A

A-2.4.3 Natural Gas and Petroleum

A-2.4.3.1 Company Name – N/A

A-2.4.4 Water and Sewer

A-2.4.4.1 Local Water Company Name – N/A

None known.

A-2.4.5 Other Utilities – Fiber Optic

A-2.4.5.1 NYS Thruway Fiber Optic Line/G4S

The existing Fiber Optic line is located approximately 64 ft. north from the westbound shoulder edge of the NYS Thruway Authority mainline. The line is approximately 24' from the end of the northwest wingwall. The line is 4' deep at this location. From the end of the northeast wingwall, the line is 34' from the end, and 3 1/2' deep.

A-2.4.6 Utility Service Connections

None known.

A-2.5 BIN 5510090

A-2.5.1 Telecommunications

A-2.5.1.1 Verizon Communications

An existing Verizon owned conduit is located under the Thruway mainline.

A-2.5.2 Electric

A-2.5.2.1 National Grid

Existing underground electric is located in the vicinity of the existing structure.

A-2.5.3 Natural Gas and Petroleum

A-2.5.3.1 Syracuse Suburban Gas

An existing gas line is located approximately 150 ft. north of the NYS Thruway mainline west of the existing structure.

A-2.5.4 Water and Sewer

A-2.5.4.1 Local Water Company Name – N/A

None known.

Deleted: Final October 5, 2017

New York State Thruway Authority

A-2.5.5 Other Utilities - Fiber Optic

A-2.5.5.1 NYS Thruway Fiber Optic Line/G4S

The fiber optic utility runs east and west underneath span 4 between Pier 3 and the end abutment (north abutment). The line is located approximately 12' off the abutment at the western fascia. It is anticipated to be 9 1/2' deep at this location. The line is approximately 8' from the abutment at the eastern fascia. The line is anticipated to be 5 1/2' deep at this location. The fiber optic line is proposed to be relocated, see Section A-3.5.3.5

Deleted: The existing Fiber Optic line is located approximately 42 ft. north from the westbound shoulder edge of the NYS Thruway Authority mainline.

A-2.5.5.2 Windstream Communications DBA Intellifiber Networks Inc.

The existing Fiber Optic line is located in Verizon conduit under the Thruway mainline.

Deleted: <#>NYS Transmit/CCTV¶
<#>The existing transmittal is attached to the westbound fascia girder. There is an existing CCTV junction box located southwest of the bridge approximately 83 ft. west of the NYS Thruway ramp centerline.¶

A-2.5.6 Utility Service Connections

None known.

A-2.6 BIN 5512790

A-2.6.1 Telecommunications

N/A

A-2.6.1.1 Company Name – N/A

A-2.6.1.2 Cable Company Name – N/A

A-2.6.2 Electric

A-2.6.2.1 National Grid

Aerial lines are approximately 70' east of centerline of N. Main St.

Deleted: approx.

A-2.6.2.2 Village of Canastota

Underground electric lines are approximately 80' west of centerline of N. Main St. (May be abandoned).

Deleted: approx.

A-2.6.3 Natural Gas and Petroleum

A-2.6.3.1 Buckeye Partners Corp.

Underground pipe (high pressure) are approximately 30' north of bridge.

Deleted: approx.

A-2.6.4 Water and Sewer

A-2.6.4.1 Village of Canastota

Underground lines 12" sewer & 2" water are located approximately 100' west of centerline of N. Main St.

Deleted: S

Deleted: approx.

Deleted: Final October 5, 2017

New York State Thruway Authority

A-2.6.5 Other Utilities – Fiber Optic

A-2.6.5.1 NYS Thruway Fiber Optic Line/G4S

approximately. The fiber optic utility runs east and west underneath span 4 between Pier 3 and the end abutment (north abutment). The line is located approximately 5' off the abutment at the western fascia. It is anticipated to be 15'-36' deep at this location. The line is approximately 11 1/2' from the abutment at the eastern fascia. The line is anticipated to be 15'-30' deep at this location. The fiber optic line is proposed to be relocated, see Section A-3.6.5.1.

Deleted: Underground line is located approx.

Deleted: 40' to 46' north of and parallel to west bound right travel lane.

Deleted: ¶

A-2.6.6 Utility Service Connections

None known.

A-2.7 BIN 5510130

A-2.7.1 Telecommunications

A-2.7.1.1 Companies Name – N/A

None known.

A-2.7.1.2 Cable Companies Name – N/A

None known.

A-2.7.2 Electric

Companies Name – N/A

A-2.7.3 Natural Gas and Petroleum

A-2.7.3.1 Buckeye Partners Corp.

One (high-pressure petroleum) underground cross pipe is located approximately 120' west of the box culvert. Another (high pressure) underground pipe is located in the NYSTA ROW and approximately 15' north of and approximately parallel to the eastbound ROW fence.

Deleted: approx.

Deleted: approx.

Deleted: approx.

A-2.7.4 Water and Sewer

A-2.7.4.1 Companies Name – N/A

None known.

A-2.7.5 Other Utilities – Fiber Optic

A-2.7.5.1 NYS Thruway Fiber Optic Line/G4S

approximately. The fiber optic utility runs east and west and 5' north of the northwest wingwall. The line is anticipated to be 12' deep at this location. The line crosses under Bear Trap Creek, and is located approximately 16' north of the northeast wingwall. It is anticipated that the line is approximately 2 1/2' deep at this location.

Deleted: Underground line is located approx.

Deleted: 25' to 48' north of and parallel to west bound right travel lane and very near west wingwall on westbound right.

Deleted: ¶

Deleted: Final October 5, 2017

New York State Thruway Authority

A-3 UTILITY RELOCATION BY OTHERS

The Design-Builder shall be aware that all time frames for utility relocation work presented in this section are approximate and are predicated on the assumption of a single relocation to the new, permanent utility locations. Should the Design-Builder's design, means and methods require interim utility relocations, the Design-Builder shall be responsible for coordinating with the affected utilities to determine the time frames required for any and all interim relocations.

A-3.1 BIN 1020079

A-3.1.1 Telecommunications

A-3.1.1.1 Verizon Communications

None anticipated.

A-3.1.1.2 Spectrum Cable

None anticipated.

A-3.1.2 Electric

A-3.1.2.1 National Grid

~~None anticipated.~~

A-3.1.3 Natural Gas and Petroleum

A-3.1.3.1 National Grid

~~None anticipated.~~

A-3.1.4 Water and Sewer

A-3.1.4.1 Village of Herkimer

None anticipated.

A-3.1.4.2 Village of Herkimer Sanitary Sewer

None anticipated.

A-3.1.4.3 Other Utilities – Fiber Optic

A-3.1.4.4 NYS Thruway Fiber Optic Line/G4S

None anticipated.

Deleted: Existing underground electric is located adjacent to the existing east abutment approximately 115 ft. east of the centerline of Mohawk St. and approximately 75 ft. north of the NYS Thruway centerline. During bridge demolition, the electric line will be in conflict with the removal of the existing abutment. Relocation of the electric line is anticipated to take approximately...

Deleted: ¶

Deleted: The existing gas line is located approximately 60 ft. west of the Mohawk St. centerline and adjacent to the existing west abutment. A connection to the main service line is also located near the east abutment approximately 60 ft. east of the Mohawk St. centerline. During bridge replacement, the gas line will be in conflict with the removal of the existing abutments. Relocation of the gas line is anticipated to take approximately...

Deleted: ¶

Deleted: Final October 5, 2017

New York State Thruway Authority

A-3.2 BIN 5009929

A-3.2.1 Telecommunications

A-3.2.1.1 Verizon Communications

None anticipated.

A-3.2.1.2 Spectrum Cable

None anticipated.

A-3.2.2 Electric

A-3.2.2.1 National Grid

Existing overhead electric lines are located approximately 80 ft. north of the NYS Thruway centerline before crossing over the NYS Thruway approximately 40 ft. east of the Oriskany Blvd. centerline. Relocation of the electric line is anticipated to take approximately 15 weeks from notification. National Grid requires 14 weeks lead time for preliminary engineering and scheduling. Physical relocation time on site is approximately one week. Contact Don Ambrose: National Grid; Senior Program Manager at (315) 440-2115.

Deleted: Existing overhead telephone cables are located approximately 90 ft. north of the NYS Thruway centerline before crossing over the NYS Thruway approximately 40 ft. east of the Oriskany Blvd. centerline. Relocation of the telephone line is anticipated to take approximately...

Deleted: Existing aerial facilities that run adjacent to the east side of Oriskany Blvd. approximately 35 ft. east of the Oriskany Blvd. centerline will require relocation. Relocation of the cable line is anticipated to take approximately...

Deleted: ...

A-3.2.3 Natural Gas and Petroleum

A-3.2.3.1 National Grid

None anticipated.

A-3.2.3.2 Buckeye Partners, L.P. Petroleum Pipeline

None anticipated.

Deleted: The existing 12" steel gas line is located adjacent to the existing piers approximately 40 ft. west of the Oriskany Blvd. centerline. Relocation of the gas line is anticipated to take approximately.....

A-3.2.4 Water and Sewer

A-3.2.4.1 Oneida County – Water Quality & Pollution Control

None anticipated.

A-3.2.4.2 Oneida county – Water Quality & Pollution Control Sanitary Sewer

None anticipated.

Deleted: The existing waterline is located adjacent to the existing piers approximately 40 ft. east of the Oriskany Blvd. centerline. Relocation of the waterline is anticipated to take approximately...

A-3.2.4.3 Other Utilities – Fiber Optic

A-3.2.4.4 NYS Thruway Fiber Optic Line/G4S

None anticipated.

Deleted: Final October 5, 2017

New York State Thruway Authority

A-3.3 BIN 5512980

A-3.3.1 Telecommunications

A-3.3.1.1 Verizon Communications

None anticipated.

A-3.3.1.2 Harron-Adelphia Cable

None anticipated.

A-3.3.2 Electric

A-3.3.2.1 National Grid

None anticipated.

A-3.3.3 Natural Gas and Petroleum

A-3.3.3.1 National Grid

None anticipated.

A-3.3.4 Water and Sewer

A-3.3.4.1 Companies Name – N/A

A-3.3.4.2 Other Utilities – Fiber Optic

A-3.3.4.3 NYS Thruway Fiber Optic Line/G4S

The Fiber optic line will be relocated 2' to the north of the existing Pier 3 footing, and running parallel to the footing. The approximate depth of the line will be 4'. The fiber optic line will be relocated before the Notice to Proceed.

Deleted: None anticipated

Deleted: ¶

A-3.4 BIN 5516071 & 5516072

A-3.4.1 Telecommunications

A-3.4.1.1 Verizon Communications

None anticipated.

A-3.4.1.2 Harron-Adelphia Cable

None anticipated.

A-3.4.2 Electric

A-3.4.2.1 Companies Name – N/A

None anticipated.

Deleted: Final October 5, 2017

New York State Thruway Authority

A-3.4.2.2 Natural Gas and Petroleum

A-3.4.2.3 Companies Name – N/A

None anticipated.

A-3.4.2.4 Water and Sewer

A-3.4.2.5 Companies Name – N/A

None anticipated

A-3.4.2.6 Other Utilities – Fiber Optic

A-3.4.2.7 NYS Thruway Fiber Optic Line/G4S

None anticipated.

A-3.5 BIN 5510090

A-3.5.1 Telecommunications

A-3.5.1.1 Verizon Communications

None anticipated.

A-3.5.2 Electric

A-3.5.2.1 National Grid

None anticipated.

A-3.5.3 Natural Gas and Petroleum

A-3.5.3.1 Syracuse Suburban Gas

None anticipated.

A-3.5.3.2 Water and Sewer

A-3.5.3.3 Companies Name – N/A

None anticipated.

Deleted: Final October 5, 2017

New York State Thruway Authority

A-3.5.3.4 Other Utilities - Fiber Optic

A-3.5.3.5 NYS Thruway Fiber Optic Line/G4S

A-3.5.3.6 The Fiber optic line will be relocated 2' to the north of the existing Pier 3 footing, and running parallel to the footing. The approximate depth of the line will be 4'. The fiber optic line will be relocated before the Notice to Proceed.

Deleted: None anticipated.

A-3.5.3.7 Windstream Communications DBA Intellifiber Networks Inc.

None anticipated until existing location has been fully documented.

Deleted: <#>¶
<#>NYS Transmit/CCTV¶
<#>Existing transmittal and associated connections will require relocation from existing structure to proposed structure. Existing CCTV junction box may require grade adjustments depending on the proposed limits of work.¶
<#>¶

A-3.6 BIN 5512790

A-3.6.1 Telecommunications

A-3.6.1.1 Companies Name – N/A

None anticipated.

A-3.6.2 Electric

A-3.6.2.1 National Grid and Village of Canastota

None anticipated.

A-3.6.3 Natural Gas and Petroleum

A-3.6.3.1 Buckeye Partners Corporations

None anticipated

Deleted: Possible relocation required.

A-3.6.4 Water and Sewer

A-3.6.4.1 Village of Canastota

None anticipated.

A-3.6.5 Other Utilities – Fiber Optic

A-3.6.5.1 NYS Thruway Fiber Optic Line/G4S

The Fiber optic line will be relocated 2' to the north of the existing Pier 3 footing, and running parallel to the footing. The approximate depth of the line will be 4'. The fiber optic line will be relocated before the Notice to Proceed. Possible relocation required.

Deleted: None anticipated.

Deleted: ¶

A-3.7 BIN 5510130

A-3.7.1 Telecommunications

A-3.7.1.1 Companies Name - NA

None anticipated.

Deleted: Final October 5, 2017

New York State Thruway Authority

A-3.7.2 Electric

A-3.7.2.1 Companies Name – N/A

None known.

A-3.7.3 Petroleum and Natural Gas

A-3.7.3.1 Buckeye Partners Corporation

None anticipated.

Deleted: Possible relocation required of line approximately 15ft north and closely parallel to the east bound ROW fence.

A-3.7.4 Water and Sewer

A-3.7.4.1 Companies Name – N/A

None known.

Deleted: ¶

A-3.7.5 Other Utilities – Fiber Optic

A-3.7.5.1 NYS Thruway Fiber Optic/G4S

None anticipated.

Deleted: Possible relocation required.

A-4 UTILITY RELOCATIONS BY THE DESIGN-BUILDER

The Design-Builder shall be responsible for coordinating the relocation of all utility services which are impacted by the Project, including the maintenance and protection of those utilities not listed below, participation in all meetings, preparing minutes of meetings, performing plan reviews, ground preparation, performing survey and markout required for utility relocations as well as excavating test pits as necessary to facilitate resolution of design utility conflict tables to final conflict resolution tables. The following sections describe the anticipated Work to be performed and coordination required with each utility owner.

A-4.1 BIN 1020079

A-4.1.1 Telecommunications

A-4.1.1.1 Verizon Communications

None anticipated.

A-4.1.1.2 Spectrum Cable

None anticipated.

A-4.1.2 Electric

A-4.1.2.1 National Grid

None anticipated.

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New York State Thruway Authority

A-4.1.3 Natural Gas and Petroleum

A-4.1.3.1 National Grid

None anticipated.

A-4.1.4 Water and Sewer

A-4.1.4.1 Village of Herkimer Sanitary Sewer

None anticipated.

A-4.1.4.2 Village of Herkimer

None anticipated.

A-4.1.5 Other Utilities – Fiber Optic

A-4.1.5.1 NYS Thruway Fiber Optic Line/G4S

None anticipated.

A-4.2 BIN 5009929

A-4.2.1 Telecommunications

A-4.2.1.1 Verizon Communications

None anticipated.

A-4.2.1.2 Spectrum Cable

None anticipated.

A-4.2.2 Electric

A-4.2.2.1 National Grid

None anticipated.

A-4.2.2.2 Buckeye Partners, L.P. Petroleum Pipeline

None anticipated.

A-4.2.3 Natural Gas and Petroleum

A-4.2.3.1 National Grid

None anticipated.

A-4.2.4 Water and Sewer

A-4.2.4.1 Oneida County – Water Quality & Pollution Control

None anticipated.

Deleted: <#>¶

Deleted: Final October 5, 2017

New York State Thruway Authority

A-4.2.4.2 Oneida County – Water Quality and Pollution Control Sanitary Sewer

None anticipated.

A-4.2.5 Other Utilities – Fiber Optic

A-4.2.5.1 NYS Thruway Fiber Optic Line/G4S

None anticipated.

A-4.3 BIN 5512980

A-4.3.1 Telecommunications – N/A

A-4.3.1.1 Verizon Communications

None anticipated

A-4.3.1.2 Harron-Adelphia Cable

None anticipated.

A-4.3.2 Electric – N/A

A-4.3.2.1 National Grid

None anticipated.

A-4.3.3 Natural Gas and Petroleum

A-4.3.3.1 National Grid

None anticipated.

A-4.3.4 Water and Sewer

A-4.3.4.1 Companies Name – N/A

A-4.3.5 Other Utilities – Fiber Optic

A-4.3.5.1 NYS Thruway Fiber Optic Line/G4S

None anticipated.

A-4.4 BIN 551671 & 5516072

A-4.4.1 Telecommunications

A-4.4.1.1 Verizon Communications

None anticipated.

A-4.4.1.2 Harron-Adelphia Cable

None anticipated.

Deleted: <#>¶

Deleted:

Deleted: Final October 5, 2017

New York State Thruway Authority

A-4.4.2 Electric

A-4.4.2.1 Companies Name – N/A

None anticipated.

A-4.4.3 Natural Gas and Petroleum

A-4.4.3.1 Company Name – N/A

None anticipated.

A-4.4.4 Water and Sewer

A-4.4.4.1 Companies Name – N/A

None anticipated.

A-4.4.5 NYS Thruway Fiber Optic Line/G4S

None anticipated.

A-4.5 BIN 5510090

A-4.5.1 Telecommunications

A-4.5.1.1 Verizon Communications

None anticipated.

A-4.5.2 Electric

A-4.5.2.1 National Grid

None anticipated.

A-4.5.3 Natural Gas and Petroleum

A-4.5.3.1 Syracuse Suburban Gas

None anticipated.

A-4.5.4 Water and Sewer

A-4.5.4.1 Companies Name – N/A

None anticipated.

A-4.5.5 Other Utilities – Fiber Optic

A-4.5.5.1 NYS Thruway Fiber Optic Line/G4S

None anticipated.

Deleted: Final October 5, 2017

New York State Thruway Authority

A-4.5.5.2

A-4.5.5.3 Windstream Communications DBA Intellifiber Networks Inc.

None anticipated.

A-4.6 BIN 1022790

A-4.6.1 Telecommunications – N/A

A-4.6.1.1 Telecommunication Company Name - N/A

None anticipated.

A-4.6.1.2 Cable Companies Name – N/A

None anticipated.

A-4.6.2 Electric

A-4.6.2.1 National Grid and Village of Canastota

None anticipated.

A-4.6.3 Natural Gas and Petroleum

A-4.6.3.1 Buckeye Partners Corporation

None anticipated.

A-4.6.4 Water and Sewer

A-4.6.4.1 Village of Canastota

None anticipated.

A-4.6.5 Other Utilities – Fiber Optic

A-4.6.5.1 NYS Thruway Fiber Optic Line/G4S

None anticipated.

A-4.7 BIN 5510130

A-4.7.1 Telecommunications

Companies Name - N/A

None anticipated.

A-4.7.1.1 Cable Companies Name – N/A

None anticipated.

Deleted: <#>NYS Transmit/CCTV
<#>Existing transmittal and associated connections will require relocation from existing structure to proposed structure. Existing CCTV junction box may require grade adjustments depending on the proposed limits of work.

Deleted: Final October 5, 2017

New York State Thruway Authority

A-4.7.2 Electric

A-4.7.2.1 Companies Name – N/A

None anticipated.

A-4.7.3 Natural Gas and Petroleum

A-4.7.3.1 Buckeye Partners Corporation

None anticipated.

A-4.7.4 Water and Sewer - N/A

A-4.7.4.1 Companies Name – N/A

None anticipated.

A-4.7.5 Other Utilities – Fiber Optic

A-4.7.5.1 NYS Thruway Fiber Optic Line/G4S

None anticipated.

A-5 DESIGN BUILD UTILITY DOCUMENTS

The Design-Builder shall provide documentation regarding the coordination and locations of the impacted utilities to the Authority's Project Manager, and the Authority's Project Manager shall coordinate with headquarters Utility Engineer. The required documents are: utility conflict/resolution table with proposed locations, utility plans, and Special Note of Utility Coordination.

The documentation shall be used to secure the Final DB Utility Work Agreements (DB-HC140) with each impacted utility company and any required Municipal Agreements.

Deleted: ¶
¶

Deleted: Final October 5, 2017



SYRACUSE DIVISION BUNDLED BRIDGES

TAS 17-37B, Contract D800001

DB CONTRACT DOCUMENTS

PART 6

RFP PLANS

Final, October 5, 2017

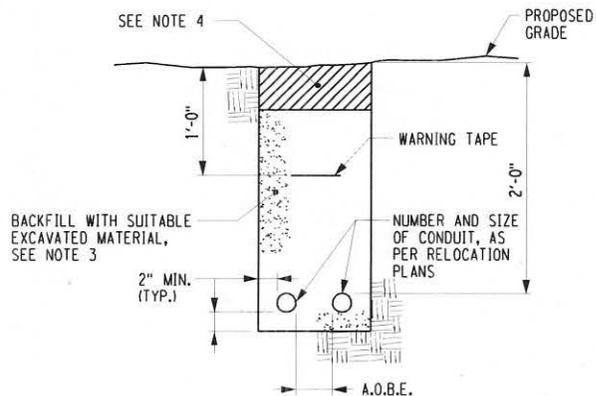
~~**Draft, August 31, 2017**~~

DIRECTIVE PLANS

~~There are no Indicative Plans for this project.~~

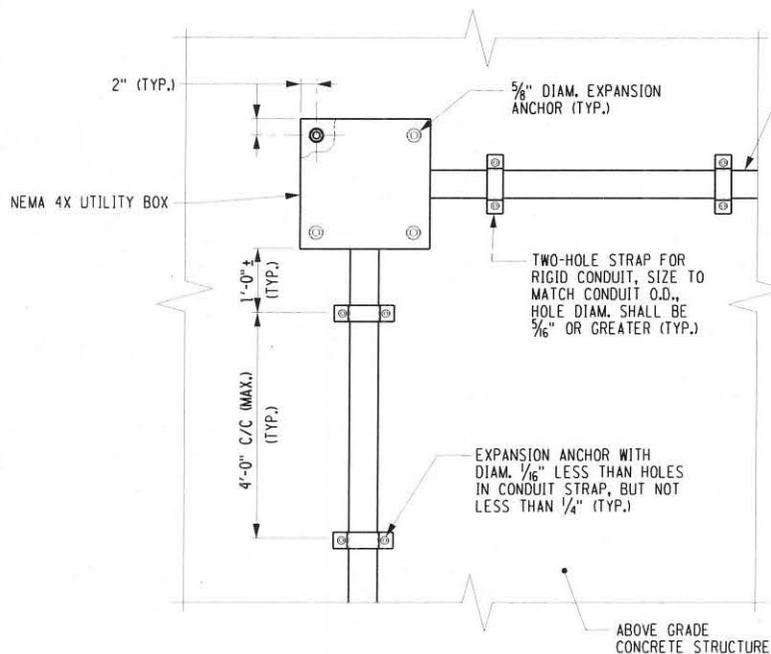
CONDUIT EXCAVATION NOTES:

1. ALL UNDERGROUND CONDUIT SHALL BE MARKED IN THE TRENCH USING MARKING TAPE AS SPECIFIED BY NYSTA. PAYMENT TO BE TO BE INCLUDED UNDER ITEM 206.03.
2. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THE MARKING TAPE SHALL BE PLACED APPROXIMATELY 12" BELOW FINISHED GRADE DURING BACKFILLING OPERATIONS.
3. SUITABLE BACKFILL MATERIAL SHALL BE GRANULAR SOIL FREE OF ROCK GREATER THAN 1 1/2" IN SIZE. PLACE IN 6" (MAX.) LIFTS AND TAMP AS DIRECTED BY THE ENGINEER.
4. RESTORE TOP SURFACE WITH 4" OF TOPSOIL IN ACCORDANCE WITH ITEM 206.03. TURF ESTABLISHMENT SHALL BE IN ACCORDANCE WITH ITEM 610.1601.



TYPICAL CCTV AND TRANSMIT CONDUIT EXCAVATION AND BACKFILL DETAIL

NTS



CONDUIT FOR WIRING AS INDICATED ON UTILITY RELOCATION PLAN (TYP.)

MOUNTING NOTES:

1. ALL NEW HARDWARE REQUIRED FOR THE MOUNTING OF CONDUIT AND BOXES SHALL BE IN ACCORDANCE WITH, AND PAID FOR UNDER, ITEM 206.03120025. HARDWARE MANUFACTURER SHALL BE APPROVED BY THE ENGINEER.
2. ALL FASTENER HARDWARE SHALL BE STAINLESS STEEL. LOCKWASHERS OR JAM NUTS ARE REQUIRED FOR ALL BOLTED CONNECTIONS.
3. ALL CLAMPS, BRACKETS, U-BOLTS, BACKPLATES, AND SIMILAR MOUNTING HARDWARE SHALL BE GALVANIZED.
4. DIAMETER AND LENGTH OF DRILLED HOLES FOR EXPANSION ANCHORS SHALL BE IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS. FOR JUNCTION BOX MOUNTING, DRILLED AND GROUTED ANCHORS MAY BE SUBSTITUTED FOR EXPANSION ANCHORS, WITH ALL WORK IN ACCORDANCE WITH STD. SPEC. SECT. 586 AND PAID FOR UNDER ITEM 206.03120025.

TYPICAL JUNCTION BOX AND CONDUIT MOUNTING DETAILS

ITEM 206.03120025

NTS



TAS 17-37B / D800001

TITLE:

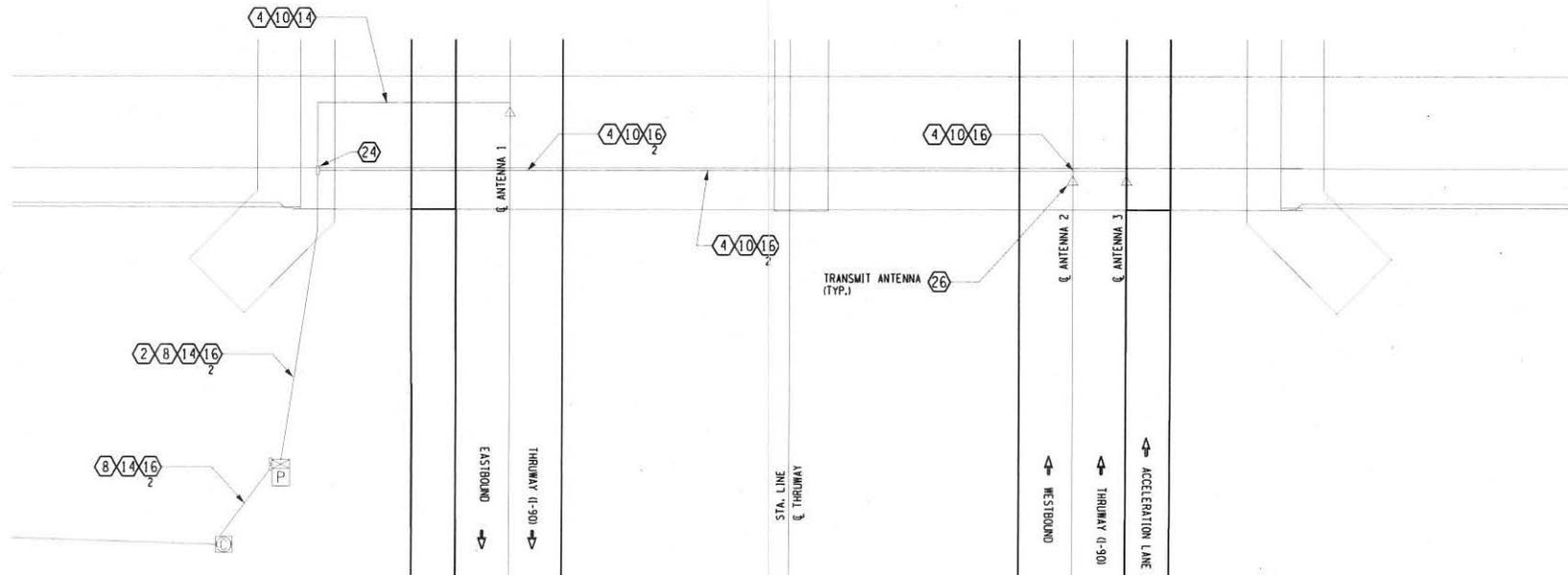
MISCELLANEOUS UTILITY DETAILS

DRWN BY: JA
CHKD BY: JA

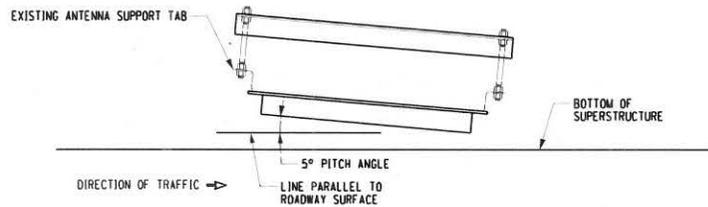
DATE: 9/27/17

SCALE: N.T.S.

DWG. NO. UT-2



ITS DIRECTIVE PLANS



NOTES:

1. FOR VERTICAL RUNS FROM GROUND TO CABINET, RIGID PLASTIC CONDUIT SHALL TRANSITION TO STEEL.
2. ANY NEW HARDWARE THAT MAY BE REQUIRED FOR THE PROPOSED TRANSMIT SYSTEM RELOCATION, INCLUDING, BUT NOT LIMITED TO, CLAMPS, BOLTS, BRACKETS, AND FASTENERS SHALL BE IN ACCORDANCE WITH ITEM 206.03120025.
3. ALL EXISTING EQUIPMENT WITHIN THE CABINET, INCLUDING THE TRANSMIT TAG READER, SHALL REMAIN AND BE PROTECTED BY THE CONTRACTOR THROUGHOUT THE DURATION OF THE CONTRACT.
4. ALL CHANGES IN CONDUIT DIRECTION SHALL BE DONE UTILIZING SWEEPS, TEES, LB TYPE FITTINGS, ETC. ARE ALLOWED ONLY WHERE ANTENNA WIRE EXITS THE MAIN CONDUIT RUN ON THE BRIDGE SUPERSTRUCTURE.
5. ALL CONDUIT, SUPPORTS AND ANTENNAE OVER THE THRUWAY MAINLINE SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 16.5' TO THE GROUND BELOW. IN ADDITION, NO FEATURE OF THE SYSTEM SHALL BE LOCATED BELOW THE BOTTOM FLANGE OF THE FASCIA GIRDER. FINAL VERTICAL POSITION SHALL BE APPROVED BY THE ENGINEER.
6. THE INTENDED POSITIONS FOR THE RELOCATED ANTENNAE ARE THE CENTERLINES OF THE THRUWAY TRAVEL LANES.

REF.	ITEM	DESCRIPTION
(2)	206.03	CONDUIT EXCAVATION, BACKFILL & SURFACE RESTORATION
(4)	206.03120025	CONDUIT INSTALLATION ON ABOVE GRADE STRUCTURES
(6)	680.520505	CONDUIT, RIGID PLASTIC, CLASS 1, 1 1/2" (NOTE 2)
(8)	680.520508	CONDUIT, RIGID PLASTIC, CLASS 1, 3" (NOTE 2)
(10)	680.520108	CONDUIT, METAL STEEL, ZINC COATED, 3"
(14)	680.77510025	TRANSMIT COAXIAL CABLE - TYPE A
(16)	680.77520025	TRANSMIT COAXIAL CABLE - TYPE B
(24)	683.50000025	NEWA 4X STAINLESS STEEL ENCLOSURE (16"x16"x8")
(26)	683.80700025	TRANSMIT ANTENNA

 NEW YORK STATE OF OPPORTUNITY.		Thruway Authority	
TAS 17-37B / D800001			
TITLE: ITS DIRECTIVE PLANS			
DRWN BY: JA CHKD BY: JA	DATE: 9/27/17	SCALE: N.T.S.	DWG. NO. UT-1



SYRACUSE DIVISION BUNDLED BRIDGES

TAS 17-37B, Contract D800001

DB CONTRACT DOCUMENTS PART 8

SPECIAL SPECIFICATIONS

Final October 5, 2017

~~Draft, August 31, 2017~~

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This *Part 8 – Special Specifications* provides access to, and details the Project-specific requirements for the use of, the following documents:

1. NYSDOT/Authority Standard Specifications and Construction Materials
2. NYSDOT/Authority Engineering Information Issuances
3. NYSDOT/Authority Special Specifications.

NYSDOT Standard Specifications and Construction Materials

The Design-Builder shall use the NYSDOT Standard Specifications Construction Materials in coordination with *Part 5 – Special Provisions*.

The NYSDOT Standard Specifications Construction Materials can be accessed at the following internet link:

<https://www.dot.ny.gov/main/business-center/engineering/specifications/busi-e-standards-usc>.

NYSDOT/Authority Engineering Information Issuances

The Design-Builder shall use the relevant NYSDOT/Authority engineering information issuances, which include:

1. Engineering Instructions (EI);
2. Engineering Bulletins (EB);
3. Engineering Directives (ED).

The above listed engineering information issuances can be accessed at the following internet link:

<https://www.dot.ny.gov/main/business-center/consultants/forms-publications-and-instructions/engineering-information-issuance-system>

NYSDOT Special Specifications

The Design-Builder may submit to use NYSDOT/Authority Special Specifications which are listed in the Electronic Pay Item Catalog (e-PIC) (requires Authority acceptance) and which have received General Approval, and **shall** use any NYSDOT/Authority Special Specifications which are referenced in this Part 8 or elsewhere in the Contract Documents. Delete and ignore sections in the NYSDOT/Authority Special Specifications titled *Method of Measurement* and *Basis of Payment* from the NYSDOT/Authority Special Specifications.

NYSDOT Special Specifications can be accessed at the following internet link:

<https://www.dot.ny.gov/main/business-center/engineering/specifications/special-specifications-us>.

The NYSTA Special Specifications may be accessed at the following internet link:

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New York State Thruway Authority

Via a written request provided at the One on One meetings with names of firm's personnel needing access.

The following Special Specifications are attached herein:

ITEM 206.03120025 – CONDUIT INSTALLATION ON ABOVE GRADE STRUCTURES

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ITEM 557.2101XX09 – INTERNAL CURING HIGH PERFORMANCE CONCRETE WITH CORROSION INHIBITOR – TYPE XX FRICTION

ITEM 557.21020016 – FIELD CAST JOINTS BETWEEN PRECAST CONCRETE UNITS

ITEM 557.51090018 – INTERNAL CURING CONCRETE FOR SUPERSTRUCTURE SLABS WITH INTEGRAL WEARING SURFACE – BOTTOM FORMWORK REQUIRED – TYPE 09 FRICTION

ITEM 557.54090018 – INTERNAL CURING CONCRETE FOR STRUCTURAL APPROACH SLAB WITH INTEGRAL WEARING SURFACE – TYPE 09 FRICTION

ITEM 611.19010024 – POST-PLANTING CARE WITH REPLACEMENT

ITEM 634.99020017 – VIBRATION MONITORING (NONBLASTING)

ITEM 680.7751--25 – TRANSMIT COAXIAL CABLE – TYPE A

ITEM 680.7752--25 – TRANSMIT COAXIAL CABLE – TYPE B

ITEM 683.3010--25 – TRANSMIT TAG READER

ITEM 683.3011--25 – TRANSMIT ANTENNA

ITEM 683.5000--25 – NEMA 4x STAINLESS STEEL ENCLOSURE

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ITEM 800.01000015 – DESIGN BUILD – DESIGN SERVICES

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ITEM 800.02000015 – DESIGN BUILD – CONSTRUCTION INSPECTION SERVICES

ITEM 800.03000015 – DESIGN BUILD – QUALITY CONTROL SERVICES

ITEM 800.04000015 – DESIGN BUILD – FORCE ACCOUNT WORK

ITEM 800.05000015 – DESIGN BUILD – SITE MOBILIZATION

ITEM 800.06000015 – DESIGN BUILD – CONSTRUCTION WORK

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In the event of a discrepancy between the version of any Special Specification attached herein and the version available from the NYSDOT/Authority web site listed above, the version included in these Contract Documents shall apply.